

Rezoning Transportation Analysis

Petition Number: 2019-130

General Location Identifier: 14920352

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Revision Log:

Date	Description
10-16-19	First Review

General Review Information

The site is at the unsignalized intersection of Park Road (major thoroughfare, city maintained) and Drexel Place (local, city maintained). The site is in a center inside Route 4 and is within the limits of the Park Woodlawn Area Plan.

Active Projects Near the Site:

- No active projects

CDOT's review of this rezoning petition is intended to ensure consistency with the Transportation Action Plan (TAP) which seeks to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on a major thoroughfare. The proposed development zoning is an urban zoning with an ordinance requirement of an 8-foot sidewalk and 8-foot planting strip on all public street frontages. The ordinance requirements are part of the City's Charlotte WALKS and Vision Zero policy. The petitioner should revise the site plan to meet the ordinance requirements and the outstanding items listed below.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Medical Office Single Family	2,300 sf 1 dwelling	90	Tax Record
Entitlement with Current Zoning	Retail	38,000 sf	3,110	RZ 2008-033
Proposed Zoning	Retail	3,500 sf	620	Site Plan: 08-13-19

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Outstanding Issues

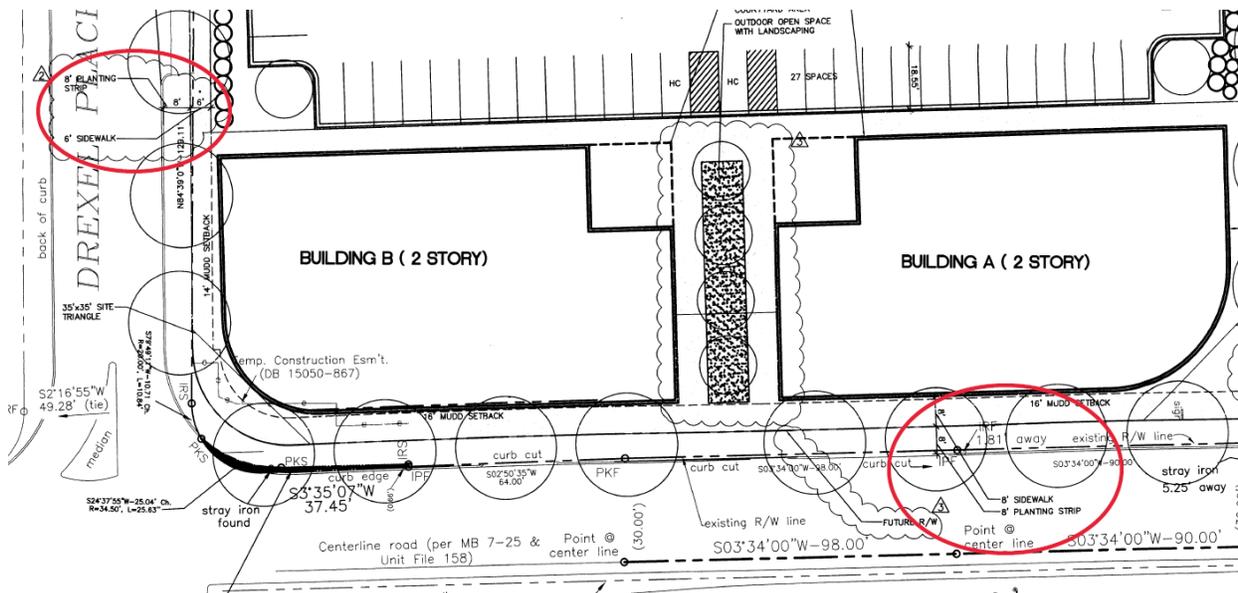
Strikeout = Not an outstanding issue

1. **Curbline** The proposed zoning district has a setback measured from back of the existing or proposed future curbline.
 - a. **Park Road:** The existing back of curb is in the correct future location.
 - b. **Drexel Place:** The existing back of curb is in the correct future location.

The site plan should show the curb and gutter labeled and dimensioned from the centerline for each road.

2. **Traffic Study** A Traffic Impact Study is not necessary for the complete review of this petition. If during the permitting process the site generates more than 2500 daily trips, then a traffic study will be required.
3. The site is part of approved Rezoning 2008-033 which commits to streetscape improvements that meet MUDD Ordinance, Chapter 19 Ordinance, Park Woodlawn Area Plan, and the adopted Charlotte Walks policy. The petitioner should revise the site plan and conditional note(s) showing the same commitments:
 - a. Park Road: 8-foot sidewalk and an 8-foot planting strip between the back of curb and the sidewalk.
 - b. Drexel Place: 6-foot sidewalk and an 8-foot planting strip between the back of curb and the sidewalk.

Below is approved Rezoning 2008-033 with the streetscape for reference:



4. Park Road is a major thoroughfare inside Route 4 with a future right-of-way of 80 feet as identified in the Council adopted Comprehensive Transportation Plan and CRTPO. The petitioner should revise the site plan and conditional notes committing to the dedication of right-of-way, fee simple, of 40 feet from centerline of the road to meet minimum requirements
5. The petitioner should revise the site plan and update Conditional Note Transportation 4.B. to include that the parking shall meet ordinance requirements.

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6. The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued.
7. ~~Resolved Sample format for comment that is resolved~~

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.