

Rezoning Transportation Analysis

Petition Number: 2019-115

General Location Identifier: 22350150, 22350160, 22316102

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Revision Log:

Date	Description
09-29-2019	First Review

General Review Information

The site is 454.24 acres generally bound by Ballantyne Commons Parkway (major thoroughfare, city maintained), N Community House Road (major thoroughfare, city maintained), I-485 (freeway, state maintained), and McAlpine Creek. The site is in a center outside Route 4. The petition includes three phases as part of the overall development. Phase I and Phase II are bisected by US 521. The current traffic impact study is only studying Phase I and Phase II. A future traffic impact study will be performed for Phase III.

Active Projects Near the Site:

- I-485 NCDOT Project
 - Widening including managed lanes and rapid transit.
- US-521/Johnston Rd
 - Feasibility study to determine needed improvements

CDOT's review of this rezoning petition is intended to ensure consistency with the Transportation Action Plan (TAP) which seeks to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petitioner is requesting to retain the remaining entitlements from Rezoning Petition 2011-044. These entitlements and approved transportation improvements will be transferred to parcels on the rezoning packet. All the transportation improvements for Rezoning Petition 2011-044 have been completed.

NCDOT and CDOT are requiring a Traffic Impact Study (TIS) for the review of the Development Phase I and II. TIS scope comments were sent with our letter dated 8/28/2019. In conjunction with the TIS scope comment letter, which is still pending a response from the petitioner, please address the following rezoning petition comments.

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Petition Number: 2019-115

General Location Identifier: 22350150, 22350160, 22316102

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Office Golf Course Hotel Quality Restaurant Day Care Center Medical Office	3,971,730 sf 18 holes 592 rooms 44,460 sf 8,310 sf 140,980 sf	53,570	Tax Record
Entitlement with Current Zoning	Office Apartments Hotel	6,525,000 sf 600 dwellings 600 rooms	70,590*	RZ 2011-044
Proposed Uses for current TIS (PH I & PH II)	Townhomes Apartments (mid) Apartments (high) Office Grocery Fast Food w/DT Retail Amphitheater	300 dwellings 1400 dwellings 800 dwellings 400,000 sf 125,000 sf 5,000 sf 170,000 sf	41,618**	Traffic Impact Study: 08-08-19
<p>* Entitlement with Current Entitlements Approved Rezoning 2011-044 performed a Traffic Impact Study, and all the roadway improvements for this petition have been constructed.</p> <p>** Phase I & II Development Rezoning Petition 2019-115 is performing a new traffic impact study to provide transportation mitigations for the new proposed land uses.</p> <p>Phase III Development will perform a traffic impact study to provide transportation improvements to mitigate for those proposed entitlements.</p>				

Outstanding Issues

Strikeout = Not an outstanding issue

1. **Curbline** The proposed zoning district has a setback measured from back of the existing or proposed future curbline.
 - a. **Johnston Road/521:** Future curb and gutter will be determined by the future NCDOT US 521 project. US 521 is an NCDOT state maintained and controlled access facility.
 - b. **Ballantyne Commons Parkway:** The future location of curb and gutter is in its existing location.
 - c. **Ballantyne Corporate Place:** The future location of curb and gutter is in its existing location.

Rezoning Transportation Analysis

Petition Number: 2019-115

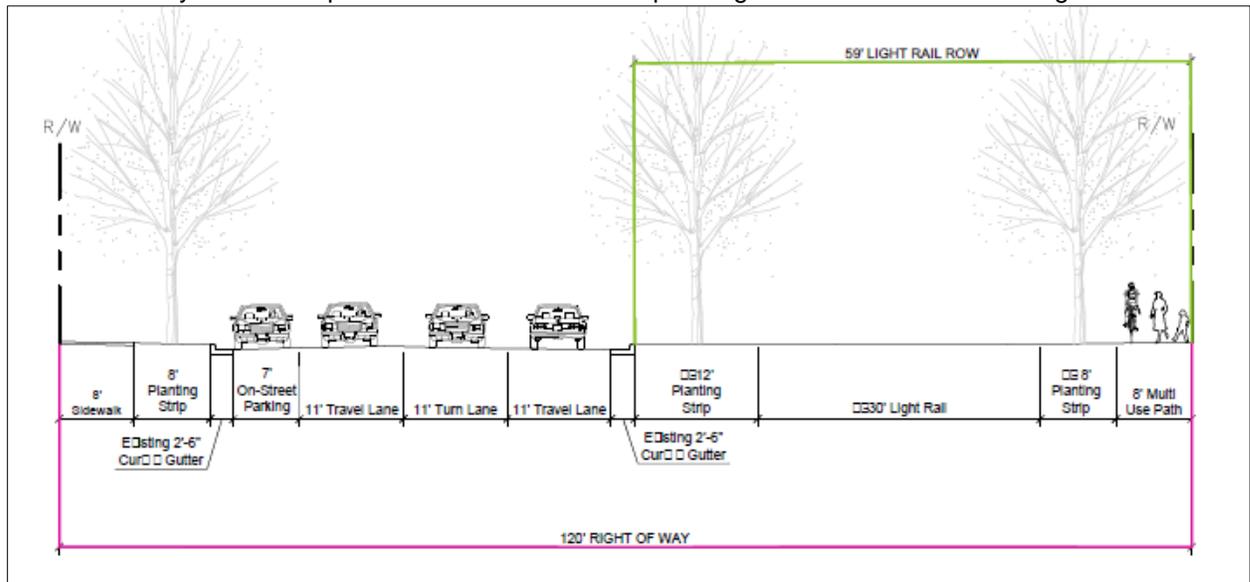
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The site plan should show the curb and gutter labeled and dimensioned from the centerline for each road

2. **Traffic Study** A Traffic Impact Study is necessary for the complete review of this petition.

Staff will receive the petition and begin work on it, but the public hearing will not be scheduled until the TIS is received. This will allow for the minimum time necessary for CDOT to review and approve the study and reach agreement with the petitioner on the required transportation commitments, and have them included on the last site plan submitted no fewer than 4 weeks prior to the public hearing. Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT and NCDOT. The traffic study needs to be submitted to the City by the end of November 2019.

- a. The petitioner should revise the site plan, conditional notes, and maps to match the maps provided as part of the Traffic Impact Study exhibits.
3. The petitioner should revise the site plan and conditional notes to show a table that clearly shows the existing 2011-044 entitlements, proposed entitlements, and combined entitlements.
 - a. The site plan and conditional notes should clearly show 2011-044 entitlement carryover quantities, as well graphically indicate where the development may occur within the site.
 4. The petitioner should revise conditional note "d. Phase III Development & Major Transportation Event" under V. Transportation/Roadway Improvements & Implementation stating a traffic impact study will be provided for Phase III development prior to the permitting of any portion of Phase III development.
 5. The petitioner should revise the site plan and conditional notes to show the proposed cross-section for Ballantyne Corporate Place incorporating the future light rail.



6. The petitioner should revise the plan and provide Sheet RZ-10 – Transportation Improvements Map/Drawings, as referenced in multiple sections of Development Notes.
7. The petitioner should revise the conditional notes by adding language to optional request #9 on sheet RZ-7.0B stating the locations for the bike facilities will be easily and safely accessible to streets, greenways, and doors of buildings.
 - a. The petitioner should revise the site plan and conditional notes to include pedestrian and bicycle signals, as this will be a walkable development.

Rezoning Transportation Analysis

Petition Number: 2019-115

General Location Identifier: 22350150, 22350160, 22316102

- b. The petitioner should revise the site plan and conditional notes to commit to pedestrian lighting within the development to encourage walkability during all times of the day.

The petitioner should revise the site plan and conditional notes showing the internal road network cross-sections meeting USDG Standards Per Chapter 20 Subdivision ordinance and the City's Charlotte BIKES policy. The petitioner should include the USDG street typical each cross-section is meeting.

8. Sheet RZ-8.0B shows 2 "Section D" cross-sections and conflict with the street network on Sheet RZ-8.0A. The petitioner should revise the site plan showing the correct matching cross-sections between Sheets RZ-8.0A and RZ-8.0B.
9. The petitioner should revise the site plan and conditional notes to commit to reallocating space on Ballantyne Corporate Place bridge over Johnston Road/521 to be 2 travel lanes, buffered bike lanes, and 8.5' sidewalks.
10. Ballantyne Corporate Place is an identified Overland Connector on the greenway master plan. The petitioner should revise the site plan and conditional notes to commit to construct a 12' Multi-Use Path for the entire length of Ballantyne Corporate Place.
11. The petitioner should revise the site plan and conditional note(s) to show the proposed greenway connection on Sheet RZ-8.0A does not conflict with the I-485 Inner express lane flyover NCDOT will build.
12. The petitioner should revise the site plan and conditional notes to commit to construct a 12-foot multi-use path along both sides of Johnston Road/521 and across the site frontage. The multi-use path can be outside of the right-of-way in a permanent public easement.
13. The petitioner should revise the site plans and conditional notes to provide a pedestrian and bicycle connection when a street may not be feasibility under the subdivision ordinance due to physical constraints to still meet the intent of a good grid network. These types of connections will provide a more robust grid network (like downtown Charlotte) to help reduce the number of vehicular trips within the development.
14. The petitioner should revise the site plan and conditional notes to provide a phasing plan for all transportation improvements (vehicular, pedestrian, bicycle) for all phases to better understand the Transportation network will be constructed for the entire development.
15. The petitioner should revise the site plan and conditional notes to commit to construct a 12-foot multi-use path on Ballantyne Commons Parkway across the site frontage.
16. The petitioner should revise the site plan and conditional notes to commit to a Transportation Demand Management (TDM) Plan. The plan should be as detailed as possible to capture the different modes to reduce single vehicle occupancy vehicles within the development, as well as origin and destination trips to the development. The plan should include a monitoring component verifying the TDM strategies effectiveness and to help determine whether modifications is needed.
17. The petitioners should revise the site plan and conditional notes to identify and commit to future bus stop locations, bus shelters, bus pullouts for future bus rapid transit, as part of the future CATS plans.
18. The petitioner should revise the site plan and conditional notes to commit to a Traffic Management Plan for the proposed amphitheater events during the week, and weekends. The plan should be a comprehensive plan to take into consideration weekday and weekend peak times to reduce overall delays on internal and external public streets.

Rezoning Transportation Analysis

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19. The petitioner should revise the site plan and conditional notes to identify and commit to dedicate land for the future light corridor within the development.
20. The petitioner should revise the site plan and conditional notes to commit to provide a robust and comprehensive Wayfinding Plan for the entire campus. The plan should include vehicular wayfinding for outer loop roads, internal streets, as well as pedestrian wayfinding once visitors have parked their vehicles.
21. The petitioner should revise the site plan to add a note specifying dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible.
22. The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.
- ~~23. Resolved Sample format for comment that is resolved~~

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.