

Curbline

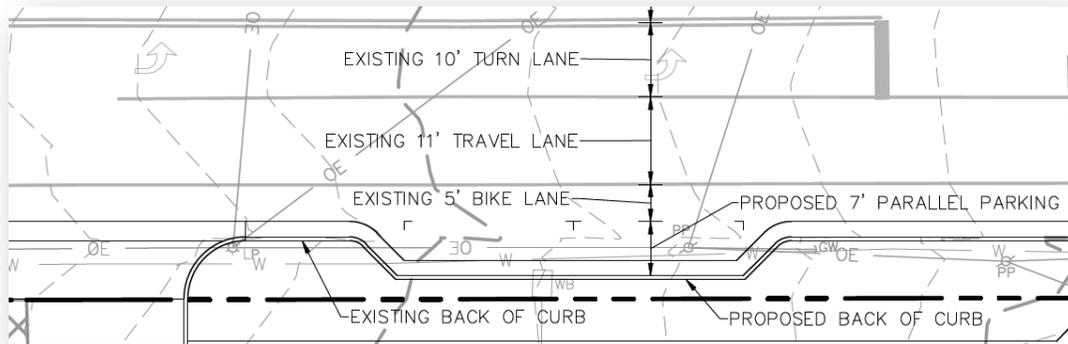
The proposed zoning district has a setback measured from an existing or proposed future curbline. The location of the future curbline is TBD (to be determined prior to public hearing).

Resolved Issues

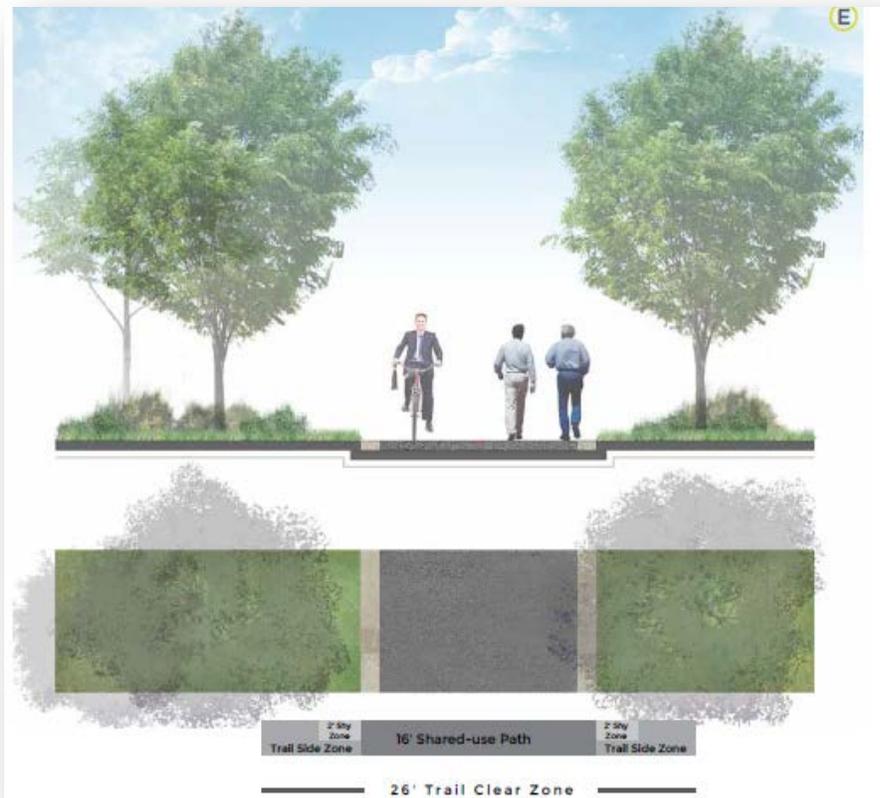
None

Outstanding Issues

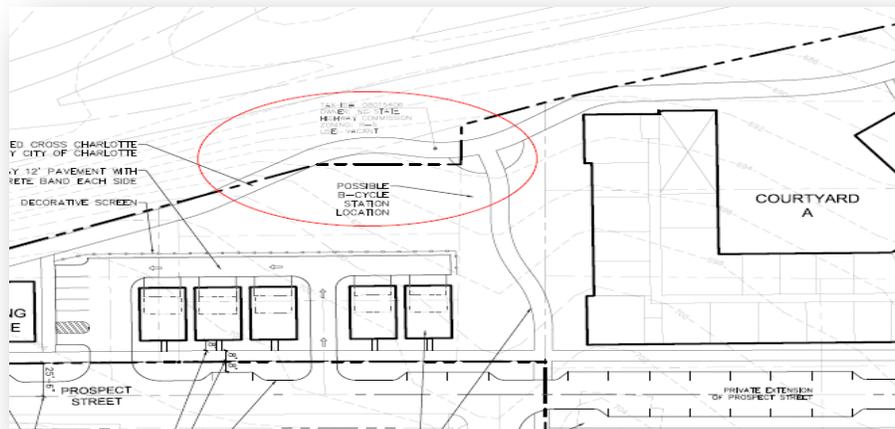
1. CDOT requests that the petitioner construct the new back of curb on E. 10th St. in a location that allows for an eleven foot travel lane, new 5' bike lane, and on-street parallel parking. This curbline should match the curbline being proposed for petition 2016-045 (See below). Please revise the site plan to clearly show this new back of curb location accurately.



2. CDOT requests that the petitioner construct the new back of curb on Siegle Ave. in a location that allows for a new 6' bike lane in addition to the on-street parking that is currently being proposed on the site plan. Please revise the site plan to clearly show this new back of curb location accurately.
3. CDOT recommends that the petitioner construct the Cross Charlotte Trail and dedicate a 26' permanent easement for maintenance. The cross section for this segment of the trail should be a 16' paved shared use path with an additional 5' of clear zone space on both sides (total of 26' wide). Please see the proposed cross section below:



4. CDOT recommends that the petitioner arrange a meeting to discuss certain trail alignment options that could allow for the most optimal design possible, while addressing some of CDOT's concerns with building edges and other constraints.
5. CDOT does not support the abandonment of Jackson Ave. However, CDOT does support the proposed 10' wide trail connection as shown and we request that this remains in place. It is CDOT's understanding that leaving the paper right of way in place here will not create larger setbacks than what are already proposed in this area (please confirm this with The Planning Department). This is consistent with comments made on petition 2016-045.
6. CDOT is currently seeking a Tiger Grant for a bicycle/pedestrian bridge that would cross US-74 parallel to the existing Central Avenue Bridge. The exact tie in location is yet to be determined but it is anticipated to be in the general area as shown below:



For this reason CDOT recommends that a note be added to the site plan stating that “in the case that the City’s Tiger Grant is awarded for the Cross Charlotte Trail Bridge, the developer agrees to provide the needed construction easements and any permanent easements in this area to allow for the construction of this bridge”.

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) will require a driveway permit(s) to be submitted to CDOT for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a

proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.