



			trip generation.
5/20/2016	No Change	-	-

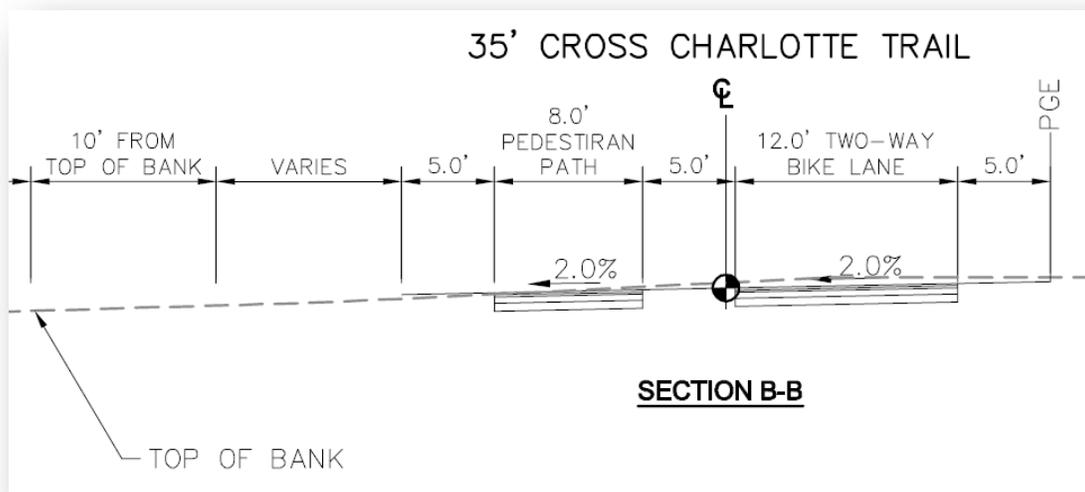
### Curbline

The proposed zoning district has a setback measured from an existing or proposed future curbline. The location of the future curbline is still an outstanding issue for CDOT.

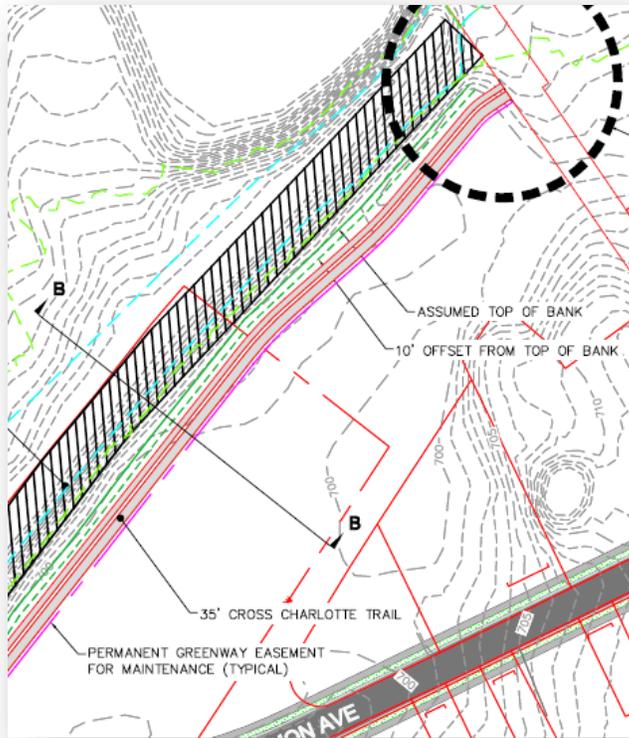
### Resolved Issues

1. CDOT requests that the petitioner clarify their intent for the use of existing unused right of way (see below). The site plan proposes this right of way for site access. If that is the case what are the improvement plans for this right of way? In addition, how will it function with Philemon Ave., Craighead Rd., and the Cross Charlotte Trail?
2. CDOT requests that the petitioner revise a portion of their proposed alignment for Philemon Ave. to match the proposed centerline alignment that has been studied by Engineering and Property Management. Contact Sharon Buchanan, Project Manager in Engineering & Property Management, to obtain the most up to date CAD file containing the proper alignment. The studied alignment keeps the roadway embankments outside of the FEMA Floodway.
3. CDOT recommends that the petitioner construct the Cross Charlotte Trail per the cross section and along the alignment shown below, including a permanent 35' easement be dedicated for future maintenance of this trail.

### Cross Section

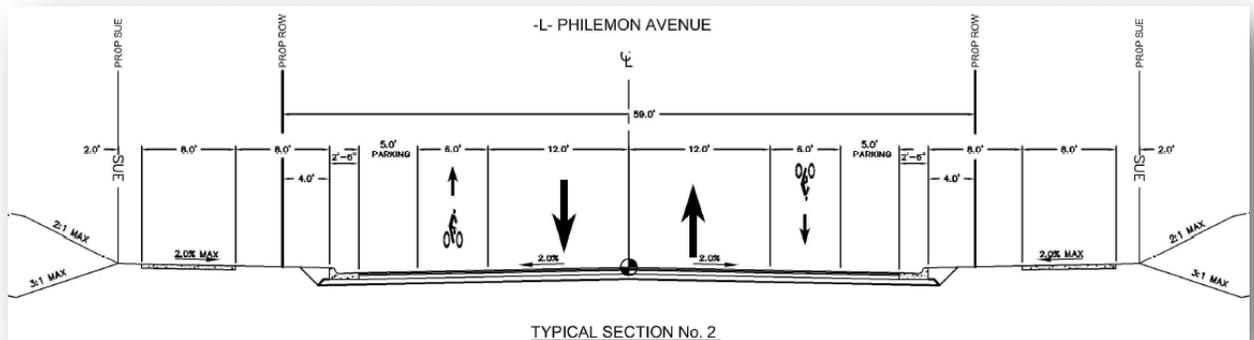


### Alignment



### Outstanding Issues

1. CDOT request that the petitioner revise the proposed typical section shown on sheet RZ-2 to include on-street parallel parking and bike lane. This matches the proposed cross section for the future CIP project that could extend Philemon Ave below.



2. The petitioner should revise the site plan to add a Transportation Note stating that the final design alignment and full cross section of Philemon Avenue, as determined through coordination with the City's Engineering & Property Management Project Manager, will be accommodated on this site. A half-section is not adequate.

3. CDOT requests that the petitioner install the back of curb along Craighead Rd. in a location that adds a 6' bike lane, on-street parallel parking, 8' planting strip, 8' sidewalk while maintaining existing centerline and travel lane width.
4. The petitioner should revise the site plan Greenways notes as follows:
  - a. Correct cross section label from "Little Sugar Creek Greenway" to "Cross Charlotte Trail"
  - b. Note H
    - i. Remove "Greenway" from the second sentence.
    - ii. Add to fourth sentence: "Petitioner shall grant to the City a minimum of 35' wide easement..."
    - iii. Replace the fourth sentence with "The developer shall not be responsible for the maintenance of the XCLT."
  - c. Note I – Remove "Greenway" from the third sentence.
  - d. Replace Note J with – Petitioner agrees to obtain permits for and construct an 8 foot wide pedestrian pathway and a separated 12 foot wide two-way bicycle facility within the minimum 35' XCLT easement. The final alignment and cross section of the XCLT will be determined during permitting in coordination with the CDOT's Project Manager. Specific details for partnering between the City and the Developer for this facility will be determined during permitting.
  - e. Note K – Petitioner may dedicate right of way as noted for XCLT but City really only needs the public greenway/trail easement. Petitioner may change not to this effect if desired.
  - f. Remove Note M
5. The petitioner should revise the site plan to add a note stating: "All transportation and greenway commitments must be completed and approved before the site's first building certificate of occupancy is issued."

### **Advisory Information**

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) will require a driveway permit(s) to be submitted to for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.

4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.