



Land Planning
+
Landscape Architecture
+
Civil Engineering
+
Urban Design

**CITY OF
CHARLOTTE**
600 East 4th Street
Charlotte
North Carolina, 28202

**CMPD
EASTWAY
STATION**
3505 Central Avenue
Charlotte
North Carolina, 28205

**REZONING
PLAN**

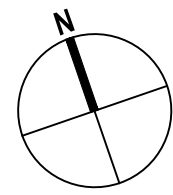
PETITION # 2012-065

Project No.
4131

Issued
03/26/12

Revised

1	05.15.12	ADDRESS CITY REVIEW COMMENTS
2	09.28.12	ADDRESS NEIGHBORHOOD COMMENTS



SCALE: 1"=30'

RZ1.0

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REZONING PETITION # 2012-065
REVISED 09.28.12



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MARCH 4, 2011. PROVIDED BY THE SCHNEIDER CORP., 8001
ARROWRIDGE DRIVE, CHARLOTTE, NC 28273, (704) 697-5900

REZONING SUMMARY

PETITIONER & OWNER: CITY OF CHARLOTTE
600 E. FOURTH STREET
CHARLOTTE, NC 28202

REZONING SITE AREA: 3.244 ACRES

TAX PARCEL #: 095-132-06, 095-132-07,
095-132-08, 095-132-09
095-132-10, 095-132-11

EXISTING ZONING: R-4, R-22MF

PROPOSED ZONING: NS

EXISTING USES: SINGLE FAMILY

PROPOSED USES: POLICE AND NE
SERVICES WITH
STRUCTURES

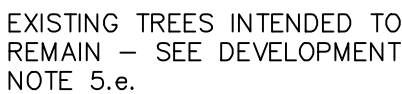
MAX. BUILDING HEIGHT: AS ALLOWED BY THE ORDINANCE

MAX. BUILDING AREA:	2	PRIMARY	15,500	SQ.FT.
		ACCESSORY	2,000	SQ.FT.
		CANOPY	5,000	SQ.FT.

PARKING:	ONE (1) SPACE PER 300 SQ.FT. AS REQUIRED FOR GOVERNMENT BUILDINGS
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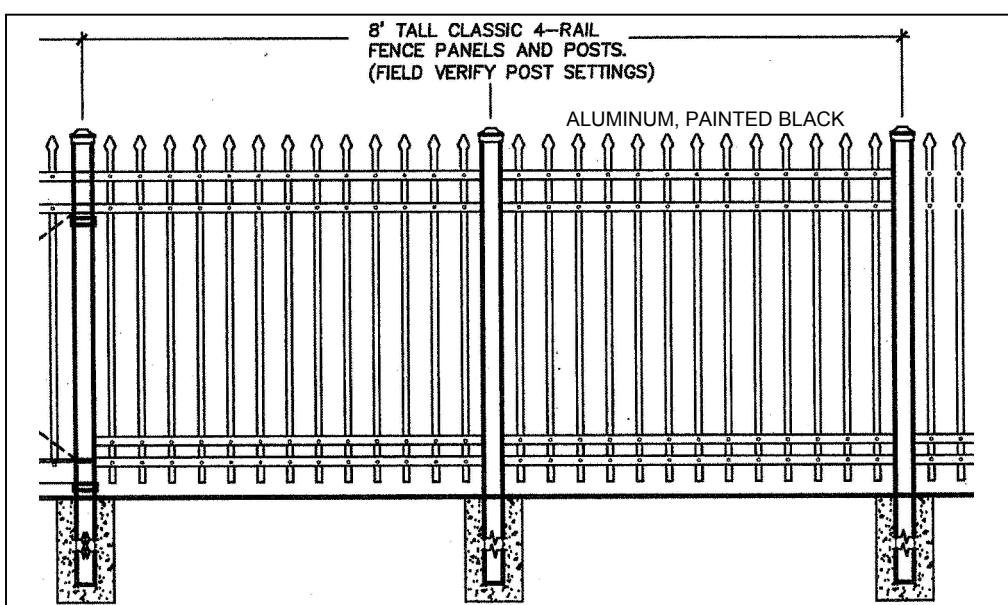
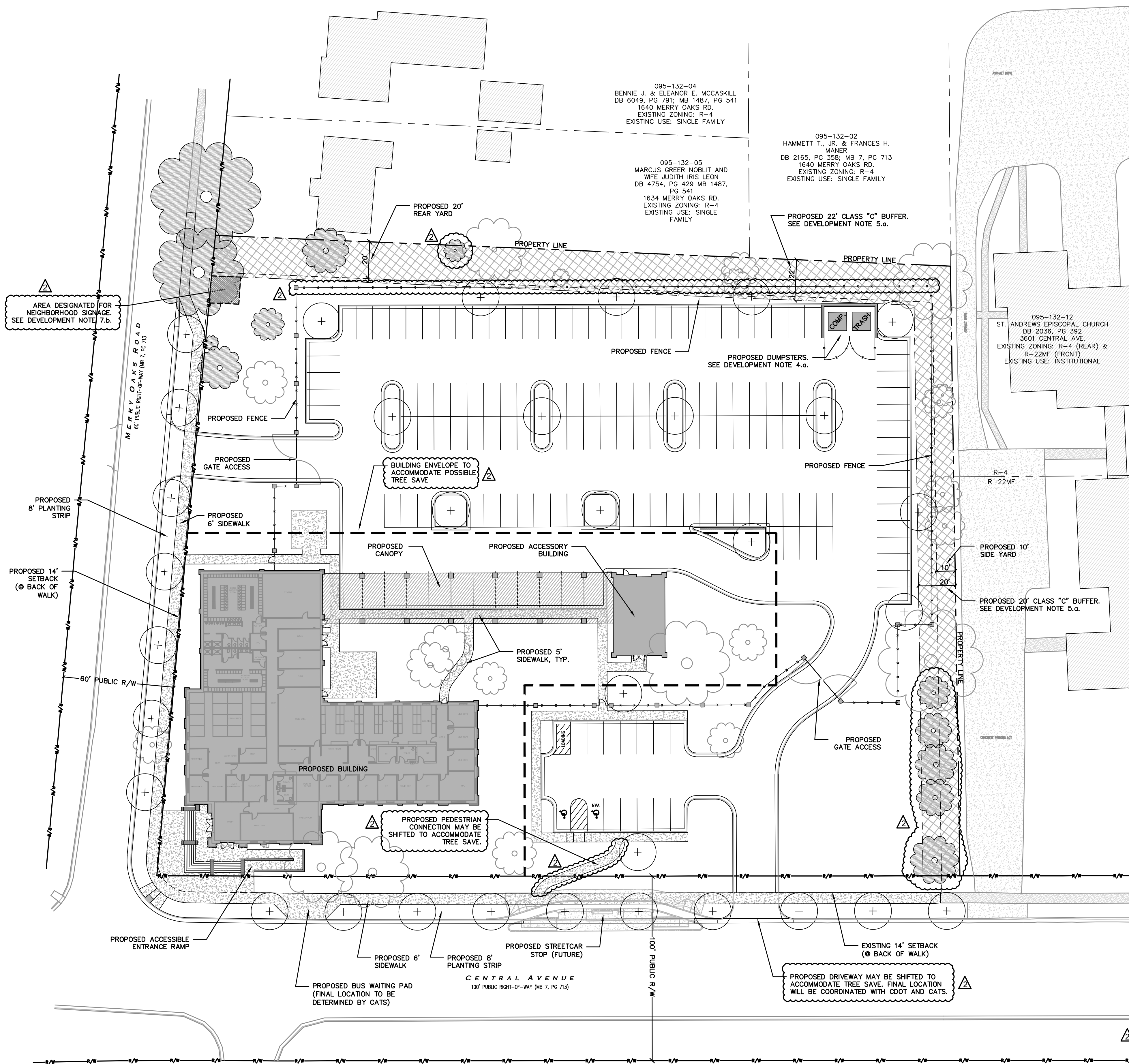
OPEN SPACE: A MINIMUM OF 15% OF THE SITE WILL BE ESTABLISHED AS TREE SAVE AREAS AS DEFINED BY THE TREE ORDINANCE

TREE LEGEND



RECEIVED

By mcataldo at 2:05 pm, Sep 28, 2012



FENCE DETAIL

N.T.S. (1

DEVELOPMENT NOTES

1. General Provisions:

- These Development Standards are part of the Rezoning Petition filed by The City of Charlotte to accommodate development of a Charlotte Police Department facility on an approximately 3.2 acre site located at the intersection of Central Avenue and Merry Oaks Road (the "Site").
- b. The development of the Site will be governed by these Development Standards (collectively referred to as the "Rezoning Plan") as well as the applicable provisions of the City of Charlotte Zoning Ordinance (the "Ordinance"). Unless the Rezoning Plan establishes more stringent standards, the regulations established under the Ordinance for the NS zoning district classification shall govern development taking place on the Site.
- c. The development depicted on the Rezoning Plan is schematic in nature, and except as otherwise specified in these Development Standards, is intended to describe the possible arrangements of uses and site elements. The building will generally be located at the corner of Central Avenue and Merry Oaks Road and parking/maintenance will not be allowed between the building and the street. The schematic depictions of the uses, parking areas, structures and buildings, and other site elements set forth on the Rezoning Plan should be reviewed in conjunction with the provisions of these Development Standards. Consequently, except as otherwise expressly specified on this Rezoning Plan, the ultimate layout of the development proposed, the exact alignments of streets and points of access, the numbers, the size, configuration and placements of buildings and parking areas, and the depictions other site elements on the Rezoning Plan are preliminary graphic representations of the types and quality of development proposed. They may, therefore, be altered or modified during design development and construction document phases. Changes to the Rezoning Plan will be reviewed and approved as allowed by Section 6.207(2).
- d. Parking layouts for surface parking may be modified to accommodate final building locations and parking spaces may be located within the development area boundaries to the extent permitted by the Ordinance. Sidewalks generally depicted on the Rezoning Plan are intended to reflect the general pedestrian circulation for development on the Site but the specific locations of such sidewalks may be subject to variations that do not materially change the design intent generally depicted on the Rezoning Plan.
- 2. Permitted Uses & Development Area Limitation :**
- a. The Site may be developed with police and neighborhood services offices with accessory uses allowed in the NS zoning district.
- 3. Access:**
- a. Access to the Site will be from Central Avenue and Merry Oaks Road.
- 4. Architectural Standards:**
- a. Dumpster and/or recycling areas are provided and will be enclosed by a solid wall or fence with one side being a decorative gate. The wall or fence used to enclose the dumpster will be architecturally compatible with the building materials and colors used on the building. If one or more sides of a dumpster area adjoin a side or rear wall of a building, then the side or rear wall may be substituted for a side.
- 5. Streetscape, Buffers and Landscaping:**
- a. Buffers will be provided as required by the Ordinance. A 22-foot class "C" buffer will be provided along the northern property boundary and a 20-foot class "C" buffer will be provided along the eastern property boundary. If the existing vegetation within this buffer does not meet the standards for a class "C" buffer, additional vegetation to meet the standards of a class "C" buffer will be provided.
- b. Screening requirements of the Ordinance will be met.
- c. Above ground backflow preventers will be screened from public view and will be located outside of the required setbacks.
- d. Portions of the street frontage along Central Avenue may be utilized for utility structures for associated transit related systems. All above ground utility structures shall be screened.
- e. Due to the zoning and site constraints (building driveway locations and the parking and site circulation requirements by the Petitioner), the Petitioner has indicated several existing trees that are intended to be saved even though the root zone under the trees will be impacted beyond that which is recommended by the City Urban Forester and City Arborist. To enhance the chances for survival, the petitioner will take care in locating the tree protection fence to minimize the grading impacts of the construction and may look at minor adjustments to the placement of the site features impacting the trees. No grading, storage of materials, fill placement or compaction will be permitted within the tree protection fence area.
- 6. Environmental Features:**
- a. The Petitioner shall comply with the Charlotte City Council approved and adopted Post Construction Controls Ordinance.
- b. The Site will comply with the Tree Ordinance.
- 7. Signage:**
- a. Signage as allowed by the Ordinance will be provided.
- b. Neighborhood signage structure will be allowed in the designated area as shown on the plan and will be at City of Charlotte expense. Neighborhood signage will be at Neighborhood expense and shall be approved by the City of Charlotte.
- 8. Lighting:**
- a. All new lighting shall be full cut-off type lighting fixtures excluding lower, decorative lighting that may be installed along the driveways, sidewalks, and parking areas.
- b. Detached lighting on the Site will be limited to 25 feet in height.
- c. No "wall pak" lighting will be allowed, however architectural lighting on building facades, such as sconces, will be permitted.
- 9. Amendments to the Rezoning Plan :**
- a. Future amendments to the Rezoning Plan (which includes these Development Standards) may be applied for by the then Owner or Owners of the applicable Development Area portion of the Site affected by such amendment in accordance with the provisions of Chapter 6 of the Ordinance.
- 10. Binding Effect of the Rezoning Application :**
- a. If this Rezoning Petition is approved, all conditions applicable to the development of the Site imposed under the Rezoning Plan will, unless amended in the manner provided under the Ordinance, be binding upon and inure to the benefit of the Petitioner and subsequent owners of the Site and their respective heirs, devisees, personal representatives, successors in interest or assigns.



View from corner of Central Ave. and Merry Oaks Rd.



View West along Central Ave.

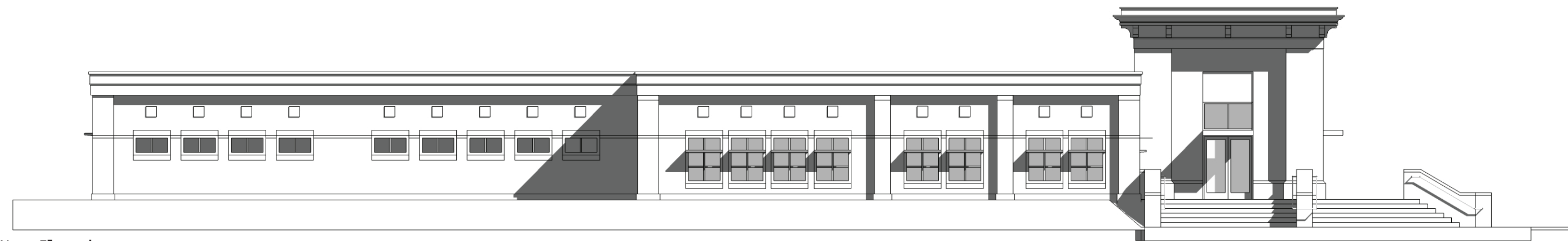


View along Central Avenue

PLEASE NOTE: THESE ILLUSTRATIVE BUILDING ELEVATIONS REFLECT AN ARCHITECTURAL STYLE AND A QUALITY OF THE BUILDING THAT MAY BE CONSTRUCTED ON THE SITE (THE ACTUAL BUILDING CONSTRUCTED ON THE SITE MAY VARY FROM THESE ILLUSTRATIONS).



View along Merry Oaks Rd.



West Elevation



South Elevation