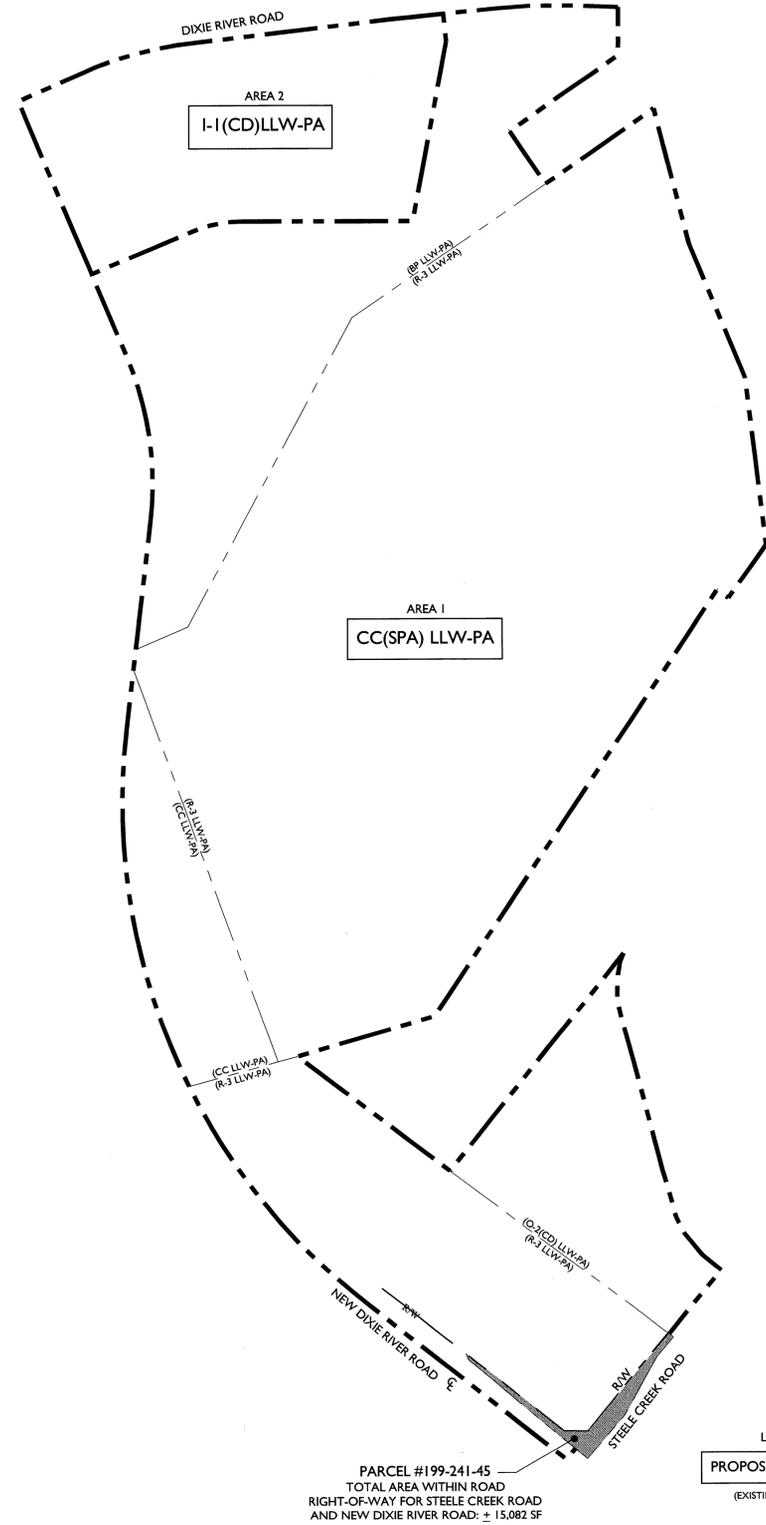


SCHMATIC SITE PLAN



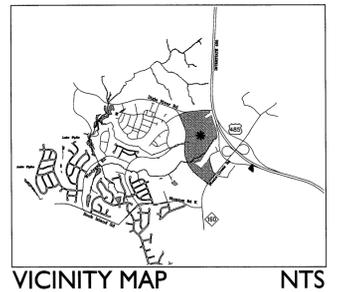
ZONING BOUNDARY MAP

AREA 1: CC(SPA) LLW-PA

Beginning at a POINT in the centerline of New Dixie River Road; THENCE through the property of Steele Creek Limited Partnership as recorded in Deed Book 9064, page 72 and Deed Book 23039, page 631 of the Mecklenburg County Register of Deeds the following five (5) courses and distances: 1) North 67 degrees 01 minutes 48 seconds East for a distance of 258.65 feet to a point; 2) With a curve to the right having an arc length of 120.27 feet, a radius of 300.00 feet, and being subtended by a chord bearing of North 78 degrees 30 minutes 54 seconds East for a distance of 119.47 feet; 3) THENCE North 90 degrees 00 minutes 00 seconds East for a distance of 404.12 feet to a point; 4) THENCE North 10 degrees 47 minutes 03 seconds East for a distance of 427.55 feet to a point; 5) THENCE North 05 degrees 55 minutes 11 seconds West for a distance of 64.14 feet to a point marking the centerline of Dixie River Road; THENCE with the centerline of Dixie River Road the following six (6) courses and distances: 1) North 84 degrees 04 minutes 49 seconds East for a distance of 12.50 feet to a point; 2) THENCE North 84 degrees 27 minutes 38 seconds East for a distance of 116.54 feet to a point; 3) THENCE North 85 degrees 35 minutes 32 seconds East for a distance of 81.31 feet to a point; 4) THENCE North 90 degrees 00 minutes 00 seconds East for a distance of 63.55 feet to a point; 5) THENCE South 88 degrees 08 minutes 54 seconds East for a distance of 77.36 feet to a point; 6) THENCE South 87 degrees 21 minutes 41 seconds East for a distance of 42.94 feet to a point; THENCE with the property of Steele Creek Limited Partnership the following eleven (11) courses and distances: 1) South 00 degrees 12 minutes 45 seconds West for a distance of 112.11 feet to a point; 2) THENCE South 55 degrees 33 minutes 02 seconds West for a distance of 311.27 feet to a point; 3) THENCE South 34 degrees 29 minutes 41 seconds East for a distance of 149.98 feet to a point; 4) THENCE North 55 degrees 37 minutes 02 seconds East for a distance of 309.66 feet to a point; 5) THENCE South 14 degrees 31 minutes 32 seconds East for a distance of 329.57 feet to a point; 6) THENCE South 22 degrees 30 minutes 35 seconds East for a distance of 343.20 feet to a point; 7) THENCE South 06 degrees 52 minutes 46 seconds East for a distance of 394.92 feet to a point; 8) THENCE South 35 degrees 34 minutes 08 seconds West for a distance of 153.79 feet to a point; 9) THENCE North 58 degrees 04 minutes 04 seconds West for a distance of 32.97 feet to a point; 10) THENCE South 33 degrees 50 minutes 24 seconds West for a distance of 120.07 feet to a point; 11) THENCE South 73 degrees 39 minutes 38 seconds West for a distance of 337.45 feet to a point marking the property of David F. Moody & Donna C. Moody as recorded in Deed Book 7461, page 609 and Steele Creek Limited Partnership as recorded in Deed Book 9064, page 72; THENCE with the property of David F. Moody & Donna C. Moody the following three (3) courses and distances: 1) THENCE South 53 degrees 06 minutes 08 seconds East for a distance of 448.85 feet to a point; 2) THENCE North 38 degrees 52 minutes 28 seconds East for a distance of 392.83 feet to a point; 3) THENCE North 38 degrees 52 minutes 28 seconds East for a distance of 263.07 feet to a point marking the centerline of Trojan Drive; THENCE with the centerline of Trojan Drive the following fifteen (15) courses and distances: 1) South 21 degrees 45 minutes 08 seconds West for a distance of 21.45 feet to a point; 2) THENCE South 09 degrees 51 minutes 57 seconds West for a distance of 31.13 feet to a point; 3) THENCE South 02 degrees 28 minutes 58 seconds West for a distance of 30.78 feet to a point; 4) THENCE South 00 degrees 00 minutes 00 seconds East for a distance of 30.75 feet to a point; 5) THENCE South 13 degrees 13 degrees 36 minutes 08 seconds East for a distance of 45.36 feet to a point; 6) THENCE South 15 degrees 27 minutes 14 seconds East for a distance of 105.39 feet to a point; 7) THENCE South 14 degrees 47 minutes 42 seconds East for a distance of 146.52 feet to a point; 8) THENCE South 14 degrees 39 minutes 20 seconds East for a distance of 121.54 feet to a point; 9) THENCE South 16 degrees 11 minutes 07 seconds East for a distance of 76.53 feet to a point; 10) THENCE South 14 degrees 37 minutes 15 seconds East for a distance of 31.69 feet to a point; 11) THENCE South 26 degrees 36 minutes 58 seconds East for a distance of 41.85 feet to a point; 12) THENCE South 39 degrees 22 minutes 10 seconds East for a distance of 29.43 feet to a point; 13) THENCE South 39 degrees 18 minutes 25 seconds East for a distance of 38.02 feet to a point; 14) THENCE South 49 degrees 57 minutes 23 seconds East for a distance of 43.65 feet to a point; 15) THENCE South 52 degrees 22 minutes 52 seconds East for a distance of 14.69 feet to a point marking the right of way of Steele Creek Road; THENCE with the right of way of Steele Creek Road South 39 degrees 44 minutes 16 seconds West for a distance of 574.61 feet to a point marking the centerline of New Dixie River Road; THENCE with the right of way of New Dixie River Road the following five (5) courses and distances: 1) North 52 degrees 07 minutes 42 seconds West for a distance of 545.43 feet to a point; 2) THENCE with a curve to the right having an arc length of 1586.40 feet, a radius of 1500.00 feet, and being subtended by a chord bearing of North 23 degrees 27 minutes 45 seconds West for a distance of 1513.50 feet; 3) THENCE North 06 degrees 50 minutes 08 seconds East for a distance of 489.33 feet to a point; 4) THENCE with a curve to the left having an arc length of 396.40 feet, a radius of 762.00 feet, and being subtended by a chord bearing of North 08 degrees 04 minutes 02 seconds West for a distance of 391.94 feet; 5) THENCE North 22 degrees 58 minutes 12 seconds West for a distance of 210.55 feet to the POINT OF BEGINNING. Said area containing 73.22 Acres, more or less.

AREA 2: I-1(CD)LLW-PA

Beginning at a point marking the centerline of New Dixie River Road; THENCE with the centerline of New Dixie River Road North 22 degrees 52 minutes 07 seconds West for a distance of 441.53 feet to a point marking the centerline of Dixie River Road; THENCE with the centerline of Dixie River Road the following six (6) courses and distances: 1) THENCE North 66 degrees 06 minutes 46 seconds East for a distance of 199.42 feet to a point; 2) THENCE with a curve to the right having an arc length of 198.86 feet, a radius of 1019.61 feet, and being subtended by a chord bearing of North 75 degrees 13 minutes 51 seconds East for a distance of 198.54 feet; 3) THENCE North 83 degrees 24 minutes 16 seconds East for a distance of 344.84 feet to a point; 4) THENCE North 83 degrees 22 minutes 52 seconds East for a distance of 163.47 feet to a point; 5) THENCE North 84 degrees 56 minutes 34 seconds East for a distance of 66.17 feet to a point; 6) THENCE North 84 degrees 09 minutes 46 seconds East for a distance of 59.44 feet to a point; THENCE through the right of way of Dixie River Road South 05 degrees 55 minutes 11 seconds East for a distance of 64.14 feet to a point; THENCE through the property of Steele Creek Limited Partnership, as recorded in Deed Book 9064, page 72 of the Mecklenburg County Register of Deeds the following four (4) courses and distances: 1) South 10 degrees 47 minutes 03 seconds West for a distance of 427.55 feet to a point; 2) THENCE North 90 degrees 00 minutes 00 seconds West for a distance of 404.12 feet to a point; 3) THENCE with a curve to the left an arc length of 120.27 feet, a radius of 300.00 feet, and being subtended by a chord bearing of South 78 degrees 30 minutes 54 seconds West for a distance of 119.47 feet; 4) THENCE South 67 degrees 01 minutes 48 seconds West for a distance of 258.65 feet to the POINT OF BEGINNING. Said area containing an area of 9.38 Acres, more or less.



FOR PUBLIC HEARING  
 PETITION #: 2010-011  
 CITY OF CHARLOTTE  
 PETITIONER: STEELE CREEK (1997)  
 LIMITED PARTNERSHIP  
 REVISIONS:  
 January 19, 2010 - Resubmittal per Planning Staff Comments  
 February 19, 2010 - Resubmittal per Planning Staff Comments

## SITE DATA

- ACREAGE 82.60 (GROSS)
- EXISTING ZONING: R-3 LLW-PA, BP LLW-PA, CC LLW-PA AND O-2(CD) LLWPA BY REZONING PETITION NO. 2006-078
- PROPOSED ZONING: CC LLW-PA, CC LLW-PA (SPA) AND I-1(CD) LLWPA AND 5 YEAR VESTED RIGHT PER SECTION 1.110 OF THE ORDINANCE
- PROPOSED USES: A COMBINATION OF GENERAL AND MEDICAL OFFICE USES AS WELL AS RETAIL, RESTAURANT AND PERSONAL SERVICE USES, HOTEL USES AND INDUSTRIAL FLEX SPACE AS MORE SPECIFICALLY DESCRIBED BELOW.

### A. GENERAL PROVISIONS.

- THESE DEVELOPMENT STANDARDS FORM A PART OF THE TECHNICAL DATA SHEET ASSOCIATED WITH THE REZONING PETITION FILED BY STEELE CREEK 1997, LP (THE "PETITIONER" OR "DEVELOPER") TO ACCOMMODATE DEVELOPMENT OF A CORPORATE OFFICE PARK OF LIGHT MANUFACTURING, SUPPORT RETAIL, PERSONAL SERVICE, HOTEL AND RESTAURANT USES IN A PEDESTRIAN-FRIENDLY DEVELOPMENT PATTERN TO BE KNOWN AS STEELE CREEK CORPORATE PARK ON AN APPROXIMATELY 82.60 ACRE SITE GENERALLY LOCATED BETWEEN STEELE CREEK ROAD, DIXIE RIVER ROAD, NEW DIXIE RIVER ROAD AND I-485 (THE "SITE"). DEVELOPMENT OF THE SITE WILL BE GOVERNED BY THE TECHNICAL DATA SHEET. THESE DEVELOPMENT STANDARDS AND THE APPLICABLE PROVISIONS OF THE CITY OF CHARLOTTE ZONING ORDINANCE IN EXISTENCE AT THE TIME OF APPROVAL OF THIS PETITION (THE "ORDINANCE"), UNLESS THE TECHNICAL DATA SHEET OR THESE DEVELOPMENT STANDARDS ESTABLISH MORE STRINGENT STANDARDS, THE REGULATIONS ESTABLISHED UNDER THE ORDINANCE FOR THE COMMERCIAL CENTER (CC) AND THE LIGHT INDUSTRIAL DISTRICT (I-1) ZONING CLASSIFICATIONS, AS APPLICABLE, SHALL GOVERN DEVELOPMENT TAKING PLACE ON THE SITE.

CONSEQUENTLY, EXCEPT AS OTHERWISE EXPRESSLY SPECIFIED ON THE TECHNICAL DATA SHEET AND IN THESE DEVELOPMENT STANDARDS, THE ULTIMATE LAYOUT OF THE DEVELOPMENT PROPOSED, THE EXACT ALIGNMENTS OF STREETS AND POINTS OF ACCESS, THE NUMBERS, THE SIZE, LOCATIONS AND PLACEMENTS OF BUILDINGS AND PARKING AREAS, AND THE DEPICTIONS OF SUCH ELEMENTS ON THE REZONING PLAN ARE PRELIMINARY GRAPHIC REPRESENTATIONS OF THE TYPES AND QUALITY OF DEVELOPMENT PROPOSED. THEY MAY, THEREFORE, BE ALTERED OR MODIFIED DURING DESIGN DEVELOPMENT AND CONSTRUCTION DOCUMENT PHASES SUBJECT TO THE ACCOMPANYING DEVELOPMENT STANDARDS AND SECTION 6.206 (2) OF THE ORDINANCE. PARKING LAYOUTS FOR SURFACE AND STRUCTURED PARKING MAY BE MODIFIED TO ACCOMMODATE FINAL BUILDING LOCATIONS AND PARKING SPACES MAY BE LOCATED INSIDE OR OUTSIDE DEVELOPMENT AREA BOUNDARIES TO THE EXTENT PERMITTED BY THE ORDINANCE. SIDEWALKS GENERALLY DEPICTED ON THE REZONING PLAN ARE INTENDED TO REFLECT THE GENERAL PEDESTRIAN CIRCULATION FOR DEVELOPMENT ON THE SITE BUT THE SPECIFIC LOCATIONS OF SUCH SIDEWALKS MAY BE SUBJECT TO MINOR VARIATIONS THAT DO NOT MATERIALLY CHANGE THE DESIGN INTENT GENERALLY DEPICTED ON THE REZONING PLAN.

### B. PERMITTED USES AND BUILDING AREA RESTRICTIONS.

- THIS REZONING IS INTENDED TO ACCOMMODATE A MIX OF USES, INCLUDING OFFICE, RETAIL, RESTAURANT, PERSONAL SERVICES, HOTEL AND INDUSTRIAL FLEX SPACE USES AS WELL AS ASSOCIATED SURFACE AND STRUCTURED PARKING FACILITIES, AND ACCESSORY USES AND STRUCTURES ALLOWED IN THE CC AND I-1 ZONING DISTRICT, AS APPLICABLE. THESE USES WILL BE PLACED ON THE SITE IN SUCH A MANNER AS TO BE CONNECTED BY PUBLIC STREETS OR PRIVATE DRIVES, OPEN SPACE, AND/OR PEDESTRIAN FEATURES.
- FOR EASE OF REFERENCE, THE REZONING PLAN SETS FORTH FOUR (4) PARCELS AS GENERALLY DEPICTED ON THE TECHNICAL DATA SHEET AS PARCELS A, B, C AND D (EACH A "PARCEL" OR A "COMPONENT", AND COLLECTIVELY THE "PARCELS" OR "COMPONENTS").
- IT IS UNDERSTOOD THAT THE COMMON BOUNDARIES BETWEEN EACH OF THE PARCELS (AND THE RIGHTS AND RESTRICTIONS PERTAINING TO ALL SUCH PARCELS), MAY BE ADJUSTED UPON THE MUTUAL AGREEMENT OF THE OWNERS OF SUCH PARCELS, SUBJECT TO THE APPROVAL OF THE PLANNING DIRECTOR. THE ZONING BOUNDARIES INDICATED MAY NOT SHIFT AND WILL BE MAINTAINED, UNLESS AMENDED AS PERMITTED BY CHAPTER 50X (6) OF THE ORDINANCE.
- THE FOLLOWING USES WILL BE PERMITTED ON PARCELA SUBJECT TO THE FOLLOWING LIMITATIONS:
  - PARCELA WILL BE ZONED CC.
  - RETAIL AND/OR RESTAURANTS (WITH AND WITHOUT A DRIVE THROUGH WINDOW) AND A GAS STATION CONVENIENCE STORE WILL BE ALLOWED ON PARCELA.
  - UP TO 20,000 SQUARE FEET OF GROSS FLOOR AREA AS DEFINED BELOW WILL BE ALLOWED ON PARCELA.
  - ONE (1) OR TWO (2) PRINCIPAL BUILDINGS MAY BE CONSTRUCTED ON PARCELA.
  - TWO (2) USES WITH DRIVE-THROUGH WINDOWS WILL BE ALLOWED ON PARCELA, A BUT ONLY ONE RESTAURANT WITH A DRIVE-THROUGH WINDOW WILL BE ALLOWED ON PARCELA.
  - A GAS STATION CONVENIENCE STORE MAY BE SUBSTITUTED FOR ONE OF THE USES WITH A DRIVE THROUGH WINDOW.
  - ACCESSORY USES AND STRUCTURES ASSOCIATED WITH THE ALLOWED PRINCIPLE USES IN THE CC ZONING DISTRICT WILL BE ALLOWED ON PARCELA.

- THE FOLLOWING USES WILL BE PERMITTED ON PARCELA SUBJECT TO THE FOLLOWING LIMITATIONS:
  - PARCELA WILL BE ZONED CC.
  - RETAIL AND/OR RESTAURANTS (WITH AND WITHOUT A DRIVE THROUGH WINDOW) AND A GAS STATION CONVENIENCE STORE WILL BE ALLOWED ON PARCELA.
  - UP TO 20,000 SQUARE FEET OF GROSS FLOOR AREA AS DEFINED BELOW WILL BE ALLOWED ON PARCELA.
  - ONE (1) OR TWO (2) PRINCIPAL BUILDINGS MAY BE CONSTRUCTED ON PARCELA.
  - TWO (2) USES WITH DRIVE-THROUGH WINDOWS WILL BE ALLOWED ON PARCELA, A BUT ONLY ONE RESTAURANT WITH A DRIVE-THROUGH WINDOW WILL BE ALLOWED ON PARCELA.
  - A GAS STATION CONVENIENCE STORE MAY BE SUBSTITUTED FOR ONE OF THE USES WITH A DRIVE THROUGH WINDOW.
  - ACCESSORY USES AND STRUCTURES ASSOCIATED WITH THE ALLOWED PRINCIPLE USES IN THE CC ZONING DISTRICT WILL BE ALLOWED ON PARCELA.

FOR PURPOSES OF THE DEVELOPMENT LIMITATIONS SET FORTH, THE TERM "GROSS FLOOR AREA" OR "GFA" SHALL MEAN AND REFER TO THE SUM OF THE GROSS HORIZONTAL AREAS OF EACH FLOOR OF A PRINCIPAL BUILDING, AND ANY ACCESSORY BUILDINGS OR STRUCTURES ON THE SITE MEASURED FROM THE OUTSIDE OF THE EXTERIOR WALLS OR FROM THE CENTER LINE OF PARTY WALLS PROVIDED, HOWEVER, THAT THE TERMINAL WALLS OF THE BUILDING ARE NOT STRUCTURED PARKING FACILITIES, OR RELATED ACCESS AREAS, AREAS USED FOR BUILDING AND EQUIPMENT ACCESS (SUCH AS STAIRS, ELEVATOR SHAFTS AND MAINTENANCE CRAWL SPACES) PROVIDED, AREAS DEVOTED TO OUTDOOR DINING ARE NOT INTENDED TO BE INCLUDED IN THE CALCULATION OF GROSS FLOOR AREA, BUT ANY SUCH RELATED AREAS SHALL BE CONSIDERED FOR PARKING PURPOSES. NOTWITHSTANDING THE FOREGOING, THE DEFINITION OF GFA IN THE ORDINANCE WILL BE USED TO CALCULATE THE MAXIMUM FLOOR AREA RATIOS ESTABLISHED BY THE ORDINANCE.

- THE FOLLOWING USES WILL BE PERMITTED ON AND ALLOCATED TO PARCELA B, SUBJECT TO THE FOLLOWING LIMITATIONS:

- PARCELA B WILL BE ZONED CC.
- GENERAL AND MEDICAL OFFICE, HOTEL, CIVIC, BANK USES AND PERSONAL SERVICE USES WILL BE ALLOWED ON PARCELA B.
- UP TO 35,000 SQUARE FEET OF GROSS FLOOR AREA AS DEFINED ABOVE WILL BE ALLOWED ON PARCELA B.
- NOTWITHSTANDING THE NUMBERS OF BUILDINGS SHOWN ON SCHEMATIC SITE PLAN, UP TO FIVE (5) PRINCIPAL BUILDINGS MAY BE CONSTRUCTED ON PARCELA B.
- ONE BANK WITH ACCESSORY DRIVE THROUGH WINDOWS WILL BE ALLOWED ON PARCELA B.
- A HOTEL WITH UP TO 120 ROOMS WILL BE ALLOWED ON PARCELA B. IF A HOTEL IS CONSTRUCTED ON PARCELA B, THE AMOUNT OF ALLOWED SQUARE FOOTAGE WILL BE REDUCED BY 25,000 SQUARE FEET OF GROSS FLOOR AREA.
- ACCESSORY USES AND STRUCTURES ASSOCIATED WITH THE ALLOWED PRINCIPLE USES IN THE CC ZONING DISTRICT, DRIVE THROUGH WINDOWS ASSOCIATED WITH A BANK WILL BE ALLOWED ON PARCELA B. OTHER USES WITH ACCESSORY DRIVE THROUGH WINDOWS WILL NOT BE ALLOWED ON PARCELA B.

- THE FOLLOWING USES WILL BE PERMITTED ON AND ALLOCATED TO PARCELA C, SUBJECT TO THE FOLLOWING LIMITATIONS:

- PARCELA C WILL BE ZONED CC.
- GENERAL AND MEDICAL OFFICE, RETAIL, RESTAURANT, PERSONAL SERVICES, CIVIC AND INDOOR RECREATIONAL USES WILL BE ALLOWED ON PARCELA C.
- UP TO 620,000 SQUARE FEET OF GROSS FLOOR AREA AS DEFINED ABOVE WILL BE ALLOWED ON PARCELA C.
- NOTWITHSTANDING THE NUMBERS OF BUILDINGS SHOWN ON SCHEMATIC SITE PLAN, UP TO 15 PRINCIPAL BUILDINGS MAY BE CONSTRUCTED ON PARCELA C.
- OF THE TOTAL ALLOWED SQUARE FOOTAGE 10,000 SQUARE FEET OF GROSS FLOOR AREA MAY BE UTILIZED FOR A FREE STANDING BUILDING THAT MAY CONTAIN ONE OR MORE OF THE FOLLOWING USES: RESTAURANTS, PERSONAL SERVICES AND RETAIL USES ON PARCELA C.
- ALL OTHER NON-OFFICE USES INCLUDING WITHOUT LIMITATION ANY ADDITIONAL RETAIL, PERSONAL SERVICE AND RESTAURANT USES MUST BE INTEGRATED INTO MULTI-STORY OFFICE BUILDINGS THAT CONTAIN AT LEAST 50,000 SQUARE FEET OF GENERAL OR MEDICAL OFFICE USES AND MAY ONLY BE LOCATED ON THE GROUND FLOOR, THE INTENT BEING TO ALLOW ACCESSORY RETAIL, RESTAURANT AND PERSONAL SERVICE USES PRIMARILY FOR THE TENANTS OF THE OFFICE BUILDINGS.
- ACCESSORY USES AND STRUCTURES ASSOCIATED WITH THE ALLOWED PRINCIPLE USES IN THE CC ZONING DISTRICT WILL BE ALLOWED ON PARCELA C. USES WITH DRIVE THROUGH WINDOWS WILL NOT BE ALLOWED.

- THE FOLLOWING USES WILL BE PERMITTED ON AND ALLOCATED TO PARCELA D, SUBJECT TO THE FOLLOWING LIMITATIONS:

- PARCELA D WILL BE ZONED I-1(CD).
- LIGHT MANUFACTURING WITH ASSOCIATED WAREHOUSE AND DISTRIBUTION SPACE, RETAIL, RESTAURANTS, PERSONAL SERVICES AND GENERAL AND MEDICAL OFFICE USES WILL BE ALLOWED ON PARCELA D.
- UP TO 400,000 SQUARE FEET OF GROSS FLOOR AREA AS DEFINED ABOVE WILL BE ALLOWED ON PARCELA D.
- NOTWITHSTANDING THE NUMBERS OF BUILDINGS SHOWN ON SCHEMATIC SITE PLAN UP TO SIX (6) PRINCIPAL BUILDINGS MAY BE CONSTRUCTED ON PARCELA D.
- ACCESSORY USES AND STRUCTURES ASSOCIATED WITH THE ALLOWED PRINCIPLE USES IN THE I-1 ZONING DISTRICT WILL BE ALLOWED ON PARCELA D. USES WITH DRIVE THROUGH USES WILL NOT BE ALLOWED ON PARCELA D.

### C. ACCESS POINTS.

- THE TOTAL NUMBER OF INGRESS/EGRESS POINTS TO THE SITE FROM THE EXISTING AND PROPOSED THROUGH ROADS THAT ABUT THE SITE SHALL BE THE SAME AS THE NUMBER SHOWN ON THE TECHNICAL DATA SHEET. THE EXACT LOCATIONS MAY VARY SOMEWHAT FROM THOSE DEPICTED BASED UPON FINAL DESIGN AND LOCALTIONAL REQUIREMENTS AS REGULATED BY CDOT AND NCDOT.

- THE TOTAL NUMBER OF INGRESS/EGRESS POINTS INDICATED ALONG THE INTERNAL PUBLIC STREETS AND PRIVATE DRIVES ON THE TECHNICAL DATA SHEET ARE FOR ILLUSTRATIVE PURPOSES AND DO NOT LIMIT THE NUMBER OF ACCESS POINTS THAT MAY BE REQUIRED ALONG THE INTERNAL PUBLIC AND PRIVATE DRIVES. CONSEQUENTLY THE NUMBER AND PLACEMENT OF ACCESS POINTS ALONG THE SITES INTERNAL STREETS MAY BE MODIFIED BY THE PETITIONER AS LONG AS THE DESIGN AND LOCALTIONAL REQUIREMENTS MEET THE REQUIREMENTS OF CDOT AND NCDOT.

- STREETS OR DRIVES WITHIN THE SITE WILL BE EITHER PUBLIC OR PRIVATE AS INDICATED ON THE TECHNICAL DATA SHEET. REGARDLESS OF WHETHER THE INTERNAL STREETS OR DRIVES ARE PUBLIC OR PRIVATE, THEY SHALL REMAIN OPEN AND ACCESSIBLE TO THE PUBLIC. THIS DOES NOT PRECLUDE THE PETITIONER FROM TEMPORARILY CLOSING A PRIVATE DRIVE OR A PUBLIC STREET FOR, BUT NOT LIMITED TO, SPECIAL EVENTS, FARMERS MARKETS, FESTIVALS, CONCERTS, BLOCK PARTIES OR OTHER SIMILAR EVENTS. ALL STREETS NOT REFERENCED AS BEING PUBLIC IN THE REZONING PLAN SHALL BE CONSIDERED PRIVATE DRIVES FOR THE PURPOSES OF THE PETITION.

### D. ROADWAY IMPROVEMENTS AND PHASING.

THE PETITIONER WILL MAKE THE FOLLOWING ROADWAY IMPROVEMENTS:

- PHASE ONE IMPROVEMENTS. THE FOLLOWING ROADWAY IMPROVEMENTS (THE "PHASE ONE IMPROVEMENTS") MUST BE COMPLETED PRIOR TO ISSUANCE OF FINAL CERTIFICATES OF OCCUPANCY FOR ANY OF THE USES ALLOWED ON PARCELA A OR PARCELA B (THE "PHASE ONE DEVELOPMENT"). PROVIDED THAT THE IMPROVEMENTS ASSOCIATED WITH ACCESSSES A, B, AND C AS DESCRIBED IN I-1 G BELOW WILL BE COMPLETED AS EACH OF THE ACCESSSES IS CONSTRUCTED:

- NORTHBOUND STEELE CREEK ROAD. RESTRIPE THE NORTHBOUND APPROACH TO THE INTERSECTION OF NEW DIXIE RIVER ROAD AND STEELE CREEK ROAD TO PROVIDE AN ADDITIONAL DUAL LEFT TURN LANE WITH 450 FEET OF STORAGE AND APPROPRIATE TAPERS.
- WESTBOUND THROUGH NEW DIXIE RIVER ROAD. CONSTRUCT AN ADDITIONAL WESTBOUND THROUGH LANE ON NEW DIXIE RIVER ROAD AT STEELE CREEK ROAD TO RECEIVE THE DUAL LEFTS AND DROP THE ADDITIONAL THRU LANE WITH TAPERS APPROPRIATELY AFTER THE INTERSECTION WITH ACCESS "C".
- EASTBOUND LEFT NEW DIXIE RIVER ROAD. CONSTRUCT AN ADDITIONAL EASTBOUND LEFT TURN LANE ON NEW DIXIE RIVER ROAD AT STEELE CREEK ROAD WITH 400 FEET OF STORAGE AND APPROPRIATE TAPERS.
- MEDIAN STEELE CREEK ROAD & TROJAN DRIVE. CONTINUE THE MEDIAN TO THE NORTH ALONG STEELE CREEK ROAD AND EXTEND PAST TROJAN DRIVE 100 FEET TO RESTRICT LEFT TURN MOVEMENTS.
- ACCESS A. CONSTRUCT (i) THE NORTHBOUND APPROACH AT ACCESS A WITH ONE INGRESS AND ONE EGRESS LANE AND (ii) A WESTBOUND LEFT TURN ON TROJAN DRIVE WITH 150 FEET OF STORAGE WITH APPROPRIATE TAPERS.
- ACCESS B. CONSTRUCT (i) SOUTHBOUND APPROACH AT ACCESS B WITH ONE INGRESS AND ONE EGRESS LANE.
- ACCESS C. CONSTRUCT (i) THE WESTBOUND APPROACH AT ACCESS C WITH ONE INGRESS LANE AND TWO (2) EGRESS LANES (SUCH EGRESS LANES BEING AN EXCLUSIVE LEFT TURN LANE WITH 400 FEET OF STORAGE AND COMBINATION THROUGH-RIGHT TURN LANE); (ii) AN EASTBOUND LEFT TURN LANE ON NEW DIXIE RIVER ROAD WITH 150 FEET OF STORAGE WITH APPROPRIATE TAPERS; AND (iii) INSTALL A TRAFFIC SIGNAL AT ACCESS C WHEN TRAFFIC SIGNAL CONTROL IS JUSTIFIED BY CDOT, BASED ON APPLICABLE ENGINEERING AND SAFETY STANDARDS (THE COST OF ITEM (ii) TO BE EITHER SHARED BETWEEN THE PETITIONER AND OTHERS, OR SOLELY THE RESPONSIBILITY OF THE PETITIONER IF OTHERS ARE NOT READY TO DEVELOP). THE PETITIONER MAY AT ITS DISCRETION ELECT TO CONSTRUCT THE SIGNAL WITH STEEL POSTS AND MAST ARMS AT NO COST TO CDOT.

- PHASE TWO IMPROVEMENTS. AS DEVELOPMENT ON PARCELS C AND D (THE "PHASE TWO DEVELOPMENT") OCCURS, THE PETITIONER WILL COMPLETE THE FOLLOWING IMPROVEMENTS IN ACCORDANCE WITH THE FOLLOWING PROVISIONS:

- NEW DIXIE RIVER ROAD EXTENSION. NEW DIXIE RIVER ROAD WILL BE EXTENDED FROM (i) THE TERMINUS OF THE CITY OF CHARLOTTE CP PROJECT LOCATED ON THE WESTERLY SIDE OF THE TRIBUTARY THAT GOES UNDER NEW DIXIE RIVER ROAD, AS GENERALLY DEPICTED ON THE REZONING PLAN, TO THE INTERSECTION OF DIXIE RIVER ROAD AND BEREWICK COMMONS PARKWAY (ACCESS E) PRIOR TO ISSUANCE OF FINAL CERTIFICATES OF OCCUPANCY FOR MORE THAN 120,000 SQUARE FEET OF ALLOWED USES ON PARCELA C, AND (ii) ITS INTERSECTION WITH BEREWICK COMMONS PARKWAY TO SANDHAVEN WAY PRIOR TO ISSUANCE OF A FINAL CERTIFICATE OF OCCUPANCY FOR MORE THAN 420,000 SQUARE FEET OF ALLOWED USES ON PARCELS C AND D. IT IS UNDERSTOOD THAT THE COST OF THE IMPROVEMENTS SET FORTH IN (i) ABOVE SHALL BE BORNE JOINTLY BETWEEN THE PETITIONER AND THE DEVELOPERS OF THE BEREWICK DEVELOPMENT (WEST OF NEW DIXIE RIVER ROAD). IN ADDITION, A TRAFFIC SIGNAL AT NEW DIXIE RIVER ROAD AND SANDHAVEN WAY WILL BE INSTALLED WHEN TRAFFIC SIGNAL CONTROL IS JUSTIFIED BY CDOT, BASED ON APPROPRIATE ENGINEERING AND SAFETY STANDARDS (THE COST OF ITEM (ii) TO BE EITHER SHARED BETWEEN THE PETITIONER AND OTHERS, OR SOLELY THE RESPONSIBILITY OF THE PETITIONER IF OTHERS ARE NOT READY TO DEVELOP).

- WHEN THE PETITIONER DESIGNS AND CONSTRUCTS THE EXTENSION OF BEREWICK COMMONS PARKWAY THROUGH PARCEL C, INCLUDING STUBBING THE STREET TO TAX PARCEL # 199-241-22 (CURRENTLY MUHAMMAD INVESTMENT GROUP), THE STUB STREET TO TAX PARCEL # 199-241-22 SHALL BE DESIGNED AND CONSTRUCTED AS 5 CUSTOMARY THROUGH THE CITY OF CHARLOTTE'S SUBDIVISION PROCESS. IN ADDITION, THE PETITIONER WILL COOPERATE AT NO MATERIAL COST TO PETITIONER, WITH THE CITY ON THE DESIGN OF THE PROPOSED STUB STREET. THE STUB STREET SHALL BE DESIGNED IN PLAN AND PROFILE USING CITY TOPOGRAPHY, FROM BEREWICK COMMONS PARKWAY (WITHIN PARCEL C) TO TROJAN DRIVE TO DETERMINE IF THIS STREET CONNECTION IS FEASIBLE.

- STEELE CREEK ROAD AND BROWN CREEK ROAD. A NORTHBOUND RIGHT TURN LANE ON STEELE CREEK ROAD WITH 475 FEET OF STORAGE AND APPROPRIATE TAPERS WILL BE CONSTRUCTED PRIOR TO THE ISSUANCE OF FINAL CERTIFICATES OF OCCUPANCY FOR MORE THAN 350,000 SQUARE FEET OF DEVELOPMENT ON PARCELA C AND/OR PARCEL D.
- STEELE CREEK ROAD AND SHOPTON ROAD WEST. A NORTHBOUND THROUGH LANE ON STEELE CREEK ROAD WITH 400 FEET OF STORAGE AND APPROPRIATE TAPERS WILL BE CONSTRUCTED PRIOR TO THE ISSUANCE OF FINAL CERTIFICATES OF OCCUPANCY FOR MORE THAN 470,000 SQUARE FEET OF OFFICE USES OR A COMBINATION OF OFFICE AND RETAIL USES THAT EXCEEDS 400,000 SQUARE FEET OF OFFICE USES AND 50,000 SQUARE FEET OF RETAIL USES.

THE FOLLOWING TWO IMPROVEMENTS (ITEMS D, AND E BELOW) MUST BE CONSTRUCTED PRIOR TO THE ISSUANCE OF FINAL CERTIFICATES OF OCCUPANCY FOR MORE THAN 525,000 SQUARE FEET OF OFFICE USES OR A COMBINATION OF OFFICE USES AND RETAIL USES THAT EXCEEDS 450,000 SQUARE FEET OF OFFICE USES AND 50,000 SQUARE FEET OF RETAIL USES.

- STEELE CREEK ROAD AND I-485 SOUTHBOUND RAMP. A NORTHBOUND RIGHT TURN LANE WILL BE CONSTRUCTED ON STEELE CREEK ROAD WITH 200 FEET OF STORAGE AND APPROPRIATE TAPERS; AND
- STEELE CREEK ROAD AND NEW DIXIE RIVER ROAD. THE SOUTHBOUND RIGHT TURN LANE ON STEELE CREEK ROAD SHALL BE EXTENDED TO NEW DIXIE RIVER ROAD BY 125 FEET TO PROVIDE 275 FEET OF STORAGE WITH APPROPRIATE TAPERS.

THE FOLLOWING IMPROVEMENTS TO ACCESS LOCATIONS WITHIN PARCELS C AND D WILL BE CONSTRUCTED BY THE PETITIONER CONCURRENTLY WITH CONSTRUCTION OF THE APPLICABLE ACCESS.

- ACCESS D. CONSTRUCT WESTBOUND APPROACH AT ACCESS D WITH ONE INGRESS AND ONE EGRESS LANE.
- ACCESS E. CONSTRUCT/INSTALL (i) WESTBOUND APPROACH AT ACCESS E WITH AN EXCLUSIVE LEFT TURN LANE WITH 150 FEET OF STORAGE AND APPROPRIATE TAPERS AND A COMBINATION THROUGH-RIGHT TURN LANE; (ii) A SOUTHBOUND LEFT TURN LANE ON NEW DIXIE RIVER ROAD WITH 150 FEET OF STORAGE WITH APPROPRIATE TAPERS; AND (iii) A TRAFFIC SIGNAL AT ACCESS E WHEN THE TRAFFIC SIGNAL IS JUSTIFIED BY CDOT, BASED UPON APPLICABLE ENGINEERING AND SAFETY STANDARDS (THE COST OF ITEM (ii)) TO BE EITHER SHARED BETWEEN THE PETITIONER AND OTHERS, OR SOLELY THE RESPONSIBILITY OF THE PETITIONER IF OTHERS ARE NOT READY TO DEVELOP).

- ACCESS F. CONSTRUCT (i) THE WESTBOUND APPROACH AT ACCESS F WITH ONE INGRESS LANE AND TWO EGRESS LANES AS EXCLUSIVE LEFT AND RIGHT TURN LANES; AND (ii) A SOUTHBOUND LEFT TURN LANE ON NEW DIXIE RIVER ROAD WITH 150 FEET OF STORAGE WITH APPROPRIATE TAPERS.
- ACCESS G. CONSTRUCT WESTBOUND APPROACH AT ACCESS G WITH ONE INGRESS AND ONE EGRESS LANE.
- ACCESS H. CONSTRUCT (i) NORTHBOUND APPROACH AT ACCESS H WITH ONE INGRESS LANE AND ONE EGRESS LANE AND (ii) A RAISED CONCRETE MEDIAN ON DIXIE RIVER ROAD AT ACCESS H TO RESTRICT LEFT TURN MOVEMENTS FROM THE INTERSECTION WITH NEW DIXIE RIVER ROAD A DISTANCE OF 100 FEET PAST ACCESS H.
- ACCESS I. CONSTRUCT (i) THE NORTHBOUND APPROACH AT ACCESS I WITH ONE INGRESS LANE AND TWO EGRESS LANES AS EXCLUSIVE LEFT AND RIGHT TURN LANES; AND (ii) A WESTBOUND LEFT TURN LANE ON DIXIE RIVER ROAD WITH THE MAXIMUM AMOUNT OF STORAGE AND PAVEMENT THAT CAN BE ACCOMMODATED BEFORE IMPACTING THE EXISTING BRIDGE OVER I-485.

- RIGHT-OF-WAY ACQUISITION. IT IS POSSIBLE THAT THE PETITIONER WILL HAVE TO ACQUIRE OFF SITE RIGHT-OF-WAY TO COMPLETE SOME OF THESE ROADWAY IMPROVEMENTS. IF AFTER THREE (3) REASONABLE EFFORTS HAVE BEEN MADE BY THE PETITIONER TO ACQUIRE SUCH RIGHT-OF-WAY ON MARKET RATE TERMS, SUCH EFFORTS TO BE EXPENDED OVER A PERIOD OF 90 DAYS, THE PETITIONER WILL BE SUCCESSFUL, THE CITY OF CHARLOTTE WILL ASSIST IN THE ACQUISITION OF RIGHT-OF-WAY IN ACCORDANCE WITH ITS STANDARD POLICIES. IN EACH EVENT, THE PETITIONER WILL REIMBURSE THE CITY FOR THE ALL THE COSTS ASSOCIATED WITH THIS RIGHT-OF-WAY ACQUISITION.

### E. ARCHITECTURAL STANDARDS.

- AT LEAST 80% OF EACH SUCH BUILDING FAÇADE EXCLUSIVE OF WINDOWS, DOORS AND ROOFS WILL BE CONSTRUCTED OF BRICK, STONE, SIMULATED STONE, PRE-CAST STONE OR PRE-CAST CONCRETE. STUCCO OR EIFS ACCENTS WILL BE ALSO ALLOWED. NO VINYL SIDING SHALL BE USED ON THE SITE EXCEPT THAT VINYL MAY BE USED ON SOFFITS, WINDOWS, AND RAILINGS OF PATIOS AND STAIRS.
- IF LIGHT MANUFACTURING USES (INCLUDING WITHOUT LIMITATION FLEX SPACE) IS CONSTRUCTED WITHIN PARCEL D, LOADING DOCKS AND LOADING DOCKWAYS MUST BE ORIENTED TO THE INTERIOR OF THE SITE AWAY FROM NEW DIXIE RIVER ROAD OR EXISTING DIXIE RIVER ROAD.
- ALL DUMPSTER AND RECYCLING AREAS WILL BE ENCLOSED BY A SOLID WALL WITH ONE SIDE BEING A DECORATIVE WOODEN GATE. THE SOLID WALL WILL UTILIZE BUILDING MATERIALS SIMILAR TO OR IDENTICAL TO THE BUILDING MATERIALS USED ON THE ADJOINING PRINCIPLE USE BUILDINGS. IF ONE OR MORE SIDES OF A DUMPSTER AREA ARE A SIDE OR REAR WALL OF A BUILDING, THEN THE SIDE OR REAR WALL MAY BE SUBSTITUTED FOR A SIDE.

### F. LANDSCAPING, BUFFERING, SCREENING REQUIREMENTS.

- BUFFERS AS REQUIRED BY THE ORDINANCE WILL BE PROVIDED AS INDICATED ON THE TECHNICAL DATA SHEET. THE PETITIONER RESERVES THE RIGHT TO REDUCE ANY OF THESE REQUIRED BUFFERS AS ALLOWED BY THE ORDINANCE. IN ADDITION, IF THE ADJOINING LAND USE OR ZONING SHOULD CHANGE SO THAT A BUFFER IS NO LONGER REQUIRED BY THE ORDINANCE, THE PETITIONER WILL NO LONGER BE REQUIRED TO PROVIDE SUCH BUFFER.
- THE PETITIONER WILL PROVIDE ONE-HALF (½) OF A CLASS A BUFFER ALONG DIXIE RIVER ROAD WHEN AN INDUSTRIAL USE IS CONSTRUCTED OR ESTABLISHED WITHIN PARCEL D, ACROSS DIXIE RIVER ROAD FROM RESIDENTIALLY ZONED OR USES PROPERTY. IF AN INDUSTRIAL USE ARE ESTABLISHED WITHIN PARCEL D OR THE ZONING AND LAND USE ACROSS DIXIE RIVER ROAD CHANGES, SUCH A BUFFER WILL NOT BE REQUIRED.
- IN LOCATIONS WITHIN THE 35 FOOT SETBACK OF PARCEL D WHERE A CLASS A BUFFER IS NOT REQUIRED, THE PETITIONER WILL PROVIDE ENHANCED LANDSCAPE MATERIALS THAT WILL CREATE AN ATTRACTIVE SETBACK THAT SCREENS THE PARKING AREA LOCATED BETWEEN THE BUILDINGS AND NEW AND EXISTING DIXIE RIVER ROADS. THIS ENHANCED LANDSCAPING WILL MEET CLASS C BUFFER STANDARDS.
- ALL ROOF-MOUNTED MECHANICAL EQUIPMENT WILL BE SCREENED FROM VIEW FROM ADJOINING PUBLIC RIGHTS-OF-WAY AND ABUTTING PROPERTIES AS VIEWED FROM GRADE.
- THE PETITIONER WILL ESTABLISH A PROPERTY OWNERS ASSOCIATION FOR DEVELOPMENT ON THE SITE. THE INTEREST OF THIS PROPERTY OWNERS ASSOCIATION WILL BE TO COORDINATE THE LANDSCAPE AND HARDSCAPE TREATMENTS ALONG STEELE CREEK ROAD AND NEW DIXIE RIVER ROAD WITH THE OTHER PROPERTY OWNERS AT THIS INTERSECTION. AS THE PROPERTIES ON THE OTHER QUADRANTS OF THE INTERSECTION DEVELOP THE PETITIONER WILL INVITE THOSE PROPERTY OWNERS TO BECOME PART OF THE ASSOCIATION TO ASSURE THE TREATMENT OF THE FOUR QUADRANTS IS COORDINATED.

### G. SETBACKS/REQUIRED YARDS, AND STREETSAPES.

- A 35 FOOT MINIMUM SETBACK WILL BE ESTABLISHED ALONG STEELE CREEK ROAD, NEW DIXIE RIVER ROAD, TROJAN DRIVE AND EXISTING DIXIE RIVER ROAD. ALONG I-485 A 50 FOOT SETBACK WILL BE PROVIDED. THESE SETBACKS WILL BE COMPLETED FROM FUTURE OR EXISTING RIGHT-OF-WAY FOR EACH OF THESE ROADS WHICH EVER IS GREATEST. FUTURE RIGHT-OF-WAY WILL BE BASED ON THE CLASSIFICATION AND RIGHT-OF-WAY ESTABLISHED FOR EACH ROAD BY THE THOROUGHFARE PLAN.
- IF ONE-HALF (½) OF A CLASS A BUFFER IS REQUIRED ALONG DIXIE RIVER ROAD FOR PARCEL D, THE SETBACK WILL BE INCREASED TO 485 FEET. THE CLASS A BUFFER IS REQUIRED WHEN A RESIDENTIAL USE OR RESIDENTIAL ZONING IS PRESENT ACROSS DIXIE RIVER ROAD IF INDUSTRIAL USES ARE CONSTRUCTED ON PARCEL D.

- THE PETITIONER WILL PROVIDE A MINIMUM SETBACK OF 6 FEET AS MEASURED FROM THE BACK OF CURB ALONG THE INTERNAL PUBLIC STREETS AS GENERALLY DEPICTED ON THE REZONING PLAN. ALONG PUBLIC STREETS THIS SETBACK REPRESENTS A REDUCTION OF THE REQUIRED CC SETBACK AS ALLIED TO THE DEVELOPMENT OF THE BEREWICK DEVELOPMENT (WEST OF NEW DIXIE RIVER ROAD). IN ADDITION, A TRAFFIC SIGNAL AT NEW DIXIE RIVER ROAD AND SANDHAVEN WAY WILL BE INSTALLED WHEN TRAFFIC SIGNAL CONTROL IS JUSTIFIED BY CDOT, BASED ON APPROPRIATE ENGINEERING AND SAFETY STANDARDS (THE COST OF ITEM (ii) TO BE EITHER SHARED BETWEEN THE PETITIONER AND OTHERS, OR SOLELY THE RESPONSIBILITY OF THE PETITIONER IF OTHERS ARE NOT READY TO DEVELOP).

- ALONG THE INTERNAL PUBLIC STREETS THE REQUIRED CC SETBACK OF 35 FEET MAY ALSO BE REDUCED FOR ACCESSORY PARKING AREAS THAT ARE ADJACENT (ALONG SIDE) TO A PRINCIPLE BUILDING. THE ACCESSORY PARKING MUST REMAIN BEHIND THE ESTABLISHED BUILDING SETBACK.

- A "BUILDING EDGE" HAS BEEN PROVIDED WITHIN PORTIONS OF THE PROPOSED BUILDING/PARKING ENVELOPES GENERALLY DEPICTED ON THE TECHNICAL DATA SHEET. WHEN A BUILDING IS CONSTRUCTED WITHIN A BUILDING/PARKING ENVELOPE THAT CONTAINS SUCH A "BUILDING EDGE", THE BUILDING MUST BE CONSTRUCTED ALONG THE "BUILDING EDGE" DEPICTED. IF A BUILDING/PARKING ENVELOPE CONTAINS MORE THAN ONE "BUILDING EDGE" LOCATION, THE BUILDING CONSTRUCTED WITHIN SUCH ENVELOPE MUST BE PLACED ALONG ONE OF THE "BUILDING EDGE" LOCATIONS DEPICTED BUT NOT ALL OF SUCH LOCATIONS. PARKING AND VEHICULAR MANUEVERING MAY NOT BE LOCATED BETWEEN THE PROPOSED BUILDING AND THE "BUILDING EDGE" THAT APPLIES TO SUCH BUILDING/PARKING ENVELOPE. THE PROVISIONS OF THIS SECTION MAY BE ALTERED TO PERMIT GREATER FLEXIBILITY IN DEVELOPMENT WITH THE PRIOR WRITTEN CONSENT OF THE PLANNING DIRECTOR.

- A UNIFIED STREETSCAPE TREATMENT WILL BE IMPLEMENTED THROUGHOUT THE SITE ALONG EXISTING AND PROPOSED PUBLIC STREETS AS GENERALLY DEPICTED ON THE REZONING PLAN. THE STREETSCAPE TREATMENTS WILL INCLUDE AT A MINIMUM: (i) AN EIGHT (8) FOOT PLANNING STRIP, (ii) A SIX (6) FOOT SIDEWALK, (iii) PEDESTRIAN SCALE DECORATIVE LIGHTING AND (iv) STREET TREES. THESE PROPOSED STREETSCAPE TREATMENTS ARE MORE SPECIFICALLY SHOWN ON SHEET RZ-2.0, SUBJECT, HOWEVER, TO ANY CONFLICTS WITH EXISTING UTILITY EASEMENTS.

- ALONG THE PRIVATE DRIVES, THE PETITIONER WILL PROVIDE A 16 FOOT BUILDING SETBACK AS MEASURED FROM THE BACK OF THE CURB IN LOCATIONS WHERE A SIX (6) FOOT SIDEWALK IS PROVIDED; IF A SIDEWALK IS NOT PROVIDED, A 14 FOOT BUILDING SETBACK WILL BE PROVIDED.

- ALONG THE PRIVATE DRIVES WHERE A PARKING FACILITY ABUTS THE STREET, THE PETITIONER WILL PROVIDE A NINE (9) FOOT SIDEWALK AN EIGHT (8) FOOT PLANTING STRIP AND A FOOT SCREEN PLANTING STRIP (BUT SUCH PLANTING STRIP WILL ONLY BE FIVE (5) FEET IF A WHEEL STOP IS PROVIDED). ALONG PRIVATE DRIVES WHERE A SIDEWALK IS NOT PROVIDED, THE PETITIONER WILL PROVIDE A NINE (9) FOOT PLANTING STRIP IN LOCATIONS WHERE A PARKING FACILITY ABUTS THE DRIVE. THIS PLANTING STRIP WILL ACCOMMODATE A STREET TREE AND SCREEN PLANTINGS (BUT SUCH PLANTING STRIP WILL ONLY BE EIGHT (8) FEET IF A WHEEL STOP IS PROVIDED).

- A SIX (6) FOOT SIDEWALK WILL BE PROVIDED ON BOTH SIDES OF THE PRIVATE DRIVE THAT INTERSECTS WITH NEW DIXIE RIVER ROAD AT ACCESS C. THE SIDEWALK WILL BE PROVIDED ON BOTH SIDES OF THIS PRIVATE DRIVE AT ITS INTERSECTION WITH NEW DIXIE RIVER ROAD TO THE FIRST INTERSECTION WITHIN PARCEL B.

- ALONG I-485, THE STREETScape WILL CONSIST OF A WELL LANDSCAPED SETBACK WITH ADDITIONAL STREET TREES AND PRESERVED NATURAL AREAS. AT A MINIMUM WITHIN THE 50 LANDSCAPED SETBACK ALONG I-485 THE PETITIONER WILL PROVIDE TREES AND SHRUBS EQUAL TO THE NUMBER REQUIRED FOR A CLASS B BUFFER. EXISTING TREES PRESERVED WITHIN THE LANDSCAPE SETBACK MAY BE USED TO MEET THIS STANDARD. THE ARRANGEMENT AND PLACEMENT OF THE TREES AND SHRUBS WITHIN THE LANDSCAPE SETBACK WILL BE AT THE PETITIONER'S DISCRETION.

- THE PETITIONER RESERVES THE RIGHT TO SUBSTITUTE A BIO-SWALE/RAIN GARDEN FOR THE TYPICAL EIGHT (8) FOOT PLANTING STRIP ALONG BOTH SIDES OF ANY OF THE PROPOSED PRIVATE DRIVES. THE PETITIONER WILL CONSULT WITH CITY ENGINEERS ON STORM WATER SERVICES ON THE DESIGN OF THE PROPOSED BIO-SWALE/RAIN GARDEN DURING THE LAND DEVELOPMENT REVIEW PROCESS.

- SIDEWALK AND PLANTING STRIPS ALONG INTERNAL STREETS AND DRIVES MAY BE INSTALLED IN PHASES IN ASSOCIATION WITH NEARBY DEVELOPMENT AND AS THE PROJECT DEVELOPS.

- A SIDEWALK NETWORK WILL BE PROVIDED WITHIN THE SITE SO THAT EACH BUILDING AND USE SHALL BE CONNECTED VIA A FIVE (5) FOOT SIDEWALK TO THE SIDEWALK ALONG THE ABUTTING INTERNAL STREETS AND DRIVES AS WELL AS THE SIDEWALK ALONG THE ABUTTING EXTERNAL PUBLIC STREETS.

### H. OPEN SPACE AND NATURAL AREA (PCCO).

- AN IMPROVED OPEN SPACE CONTAINING AT LEAST 40 ACRES WILL BE PROVIDED ON THE PORTION OF PARCEL C GENERALLY DEPICTED ON THE TECHNICAL DATA SHEET. THIS OPEN SPACE AREA WILL BE IMPROVED WITH SEATING AREAS, LANDSCAPE AREAS AND/OR WALKING PATHS. A WATER QUALITY STORM WATER DETENTION FEATURE MAY BE PART OF THE OPEN SPACE IMPROVEMENTS. IF LOCATED IN THE OPEN SPACE, THE WATER QUALITY STORM WATER FEATURE WILL BE DESIGNED AS A WET POND.

- THE PETITIONER SHALL MAKE GOOD FAITH DILIGENT EFFORTS TO SAVE THE EXISTING TREES LOCATED WITHIN THE NATURAL AREAS IDENTIFIED ON THE TECHNICAL DATA SHEET. TO THE EXTENT REQUIRED BY THE ORDINANCE, ADDITIONAL NATURAL AREAS WILL BE IDENTIFIED DURING THE DESIGN DEVELOPMENT PHASES OF THE PROJECT.

### I. PARKING.

- PARKING MAY BE PROVIDED BY WAY OF SURFACE OR STRUCTURED PARKING FACILITIES WITHIN THE BUILDING/PARKING ENVELOPES GENERALLY DEPICTED ON THE TECHNICAL DATA SHEET. PARALLEL AND ANGLED PARKING MAY BE PROVIDED ALONG INTERNAL PUBLIC AND PRIVATE DRIVES AS INDICATED ON ACCOMPANYING CROSS-SECTIONS.

- PARKING FOR USES LOCATED IN PARCEL C MAY BE LOCATED WITHIN PARCEL D, CONVERSELY PARKING FOR USES LOCATED WITHIN PARCEL D, MAY BE LOCATED WITHIN PARCEL C.

### J. LIGHTING.

- AS THE PROJECT DEVELOPS, PEDESTRIAN SCALE LIGHTING WILL BE INSTALLED ALONG BOTH INTERIOR STREETS AND STREETS THAT BORDER THE SITE.

- ALL FREESTANDING LIGHTING AND ALL EXTERIOR LIGHTING ON BUILDINGS WILL BE FULLY SHIELDED AND FULL CUT-OFF TYPE FIXTURES DOWNWARDLY DIRECTED. NO WALL "BANK" TYPE LIGHTING SHALL BE USED, BUT ATTACHED DECORATIVE LIGHTING FIXTURES SUCH AS SCONCES MAY BE USED. THE MAXIMUM HEIGHT OF ANY FREESTANDING LIGHTING FIXTURES, INCLUDING ITS BASE, SHALL NOT EXCEED 25 FEET.

### K. SIGNS.

- FOR THE PURPOSE OF SIGNAGE REQUIREMENTS UNDER THE ORDINANCE, PARCELS A AND B WILL BE TREATED TOGETHER AS A MULTI-TENANT PROPERTY (IE MIXED USE SHOPPING CENTER). THIS WILL ALLOW THE PETITIONER TO CONSTRUCT ONE (1) SHOPPING CENTER IDENTIFICATION OR BUSINESS SIGN UP TO 50 SQUARE FEET IN SIZE AND EIGHT (8) FEET HIGH ALONG EITHER NEW DIXIE RIVER ROAD OR STEELE CREEK ROAD TO IDENTIFY TENANTS ON THE INTERIOR OF THE SITE AND ONE (1) NON-TENANT COMMUNITY IDENTIFICATION SIGN WITH NO MORE THAN 20 SQUARE FEET OF SIGN AREA AND A MAXIMUM HEIGHT OF 20 FEET AT THE INTERSECTION OF NEW DIXIE RIVER ROAD AND STEELE CREEK ROAD. THE DESIGN OF THIS COMMUNITY SIGN WILL BE COORDINATED WITH THE SIGN FOR BEREWICK TOWN CENTER SO AS TO CREATE A UNIFIED APPEARANCE. IN ADDITION, EACH FREE STANDING USE WILL BE ALLOWED ONE GROUND MOUNTED IDENTIFICATION SIGNS UP TO 50 SQUARE FEET IN SIZE AND FOUR (4) FEET HIGH.

- FOR SIGNAGE PURPOSES, PARCELS C AND D WILL BE TREATED TOGETHER AS AN "OFFICE PARK." SIGNAGE WILL BE ALLOWED AS PRESCRIBED FOR OFFICE PARK DEVELOPMENT IN SECTION 13.109 (2)(I) AND 13.109 (2)(J) OF THE ORDINANCE. THIS WILL ALLOW THE PETITIONER TO CONSTRUCT IDENTIFICATION AND DIRECTORY SIGNS WITH A MINIMUM OF ONE (1) DETACHED IDENTIFICATION SIGN AND ONE DETACHED DIRECTORY SIGN BE ALLOWED PER STREET FRONT; IDENTIFICATION SIGNS WILL BE A MAXIMUM OF 50 SQUARE FEET IN SIZE AND DIRECTORY SIGNS WILL BE A MAXIMUM OF 25 SQUARE FEET IN SIZE AND HEIGHT OF SEVEN (7) FEET. THE PETITIONER RESERVES THE RIGHT TO REQUEST AND UTILIZE THE PLANNED DEVELOPMENT FLEXIBILITY OPTION OF THE SIGN REGULATIONS. IF A PLANNED DEVELOPMENT FLEXIBILITY OPTION IS APPROVED, THE NUMBER, SIZE AND HEIGHT OF THE SIGNS MAY BE INCREASED BEYOND WHAT IS INDICATED ABOVE. "ON PREMISE" SIGNS WILL BE ALLOWED WITHIN EACH OF THE PARCELS. THESE IDENTIFICATION SIGNS MAY BE UP TO ONE SQUARE FOOT FOR EVERY TWO (2) LINEAR FEET OF FRONTAGE UP TO 50 SQUARE FEET IN SIZE AND UP TO SEVEN (7) FEET IN HEIGHT.

- WALL SIGNS WILL BE ALLOWED AS PRESCRIBED BY THE ORDINANCE FOR THE CC AND I-1(CD) ZONING DISTRICTS, AS APPLICABLE.

### L. STORM WATER MANAGEMENT AND WATER QUALITY.

- DEVELOPMENT ON THE SITE SHALL COMPLY WITH THE CITY OF CHARLOTTE POST CONSTRUCTION CONTROLS ORDINANCE (PCCO).
- THE SITE WILL COMPLY WITH THE REQUIREMENTS OF THE HIGH DENSITY OPTION OF THE LOWER LAKE WYLLIE WATERSHED OVERLAY DISTRICT.

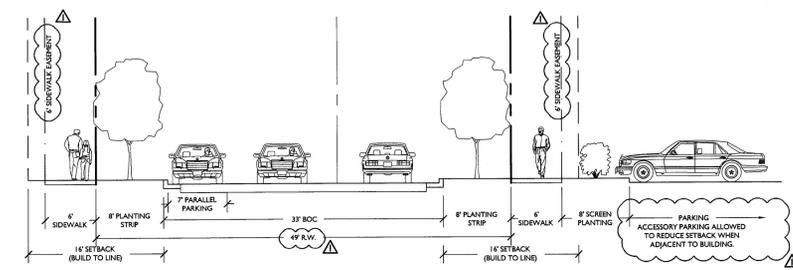
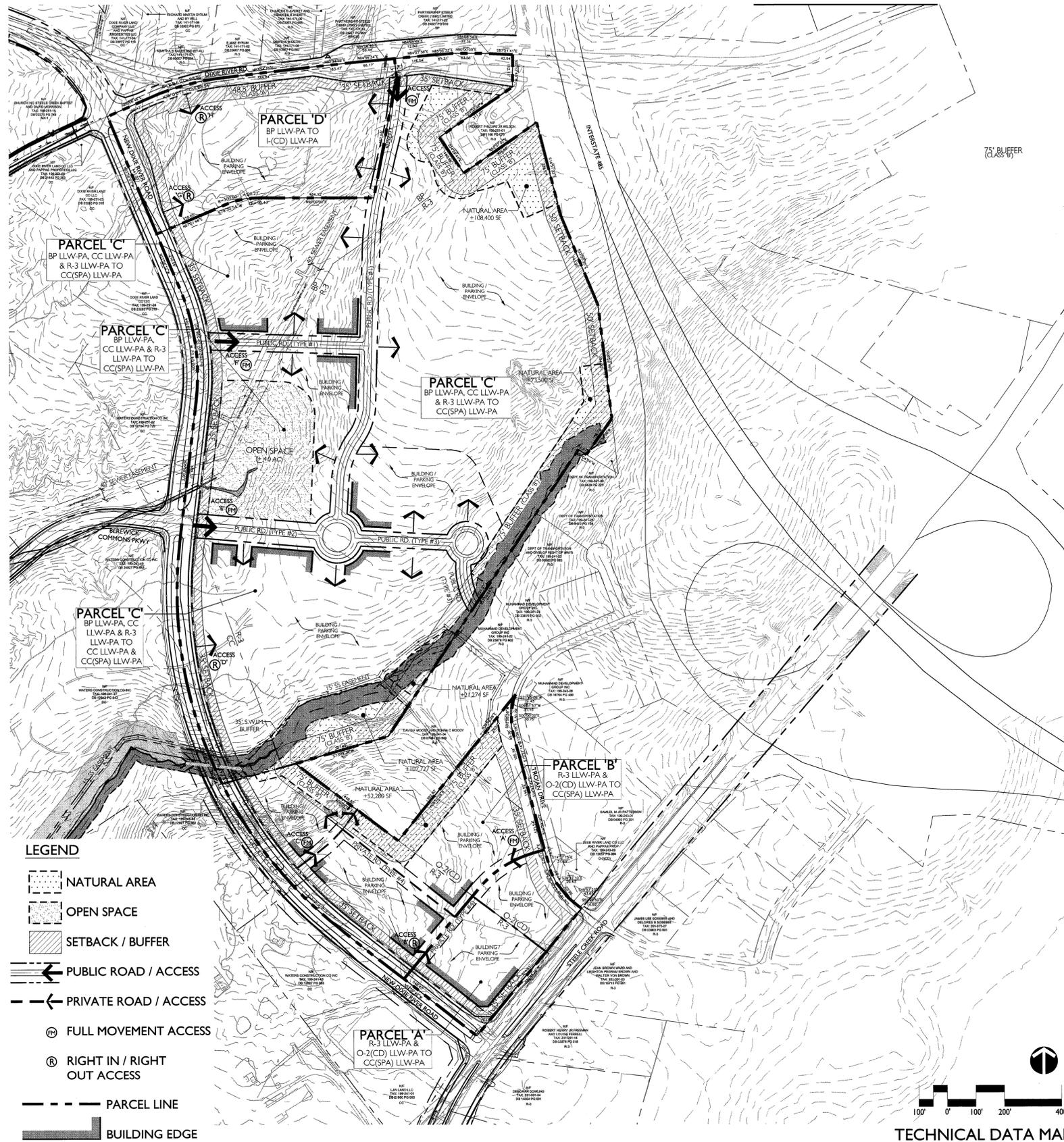
- WATER QUALITY AND STORM WATER DETENTION PONDS WILL BE LOCATED THROUGHOUT THE PROJECT. THE LOCATION AND SIZE OF THESE STRUCTURES WILL BE DETERMINED DURING THE DESIGN DEVELOPMENT PHASE AS DETAILED ENGINEERING DRAWINGS ARE DEVELOPED. PETITIONER RESERVES THE RIGHT TO ALLOW WATER QUALITY AND STORM WATER DETENTION PONDS LOCATED ON A PARCEL TO SERVE BUILDINGS AND USES LOCATED ON OTHER PARCELS THROUGHOUT THE SITE.

- BIO-RETENTION FACILITIES MAY BE LOCATED WITH THE SETBACKS AND BUFFERS ESTABLISHED ON THE TECHNICAL DATA SHEET FOR THE PURPOSE OF SATISFYING WATER QUALITY PERFORMANCE STANDARDS. BIO-RETENTION LANDSCAPING SHALL BE APPLIED TO SATISFY BUFFER PLANTING REQUIREMENTS ALONG THE PORTION OF THE BUFFER OCCUPIED BY THE BIO-RETENTION FACILITIES. CUSTOMARY STORM WATER DETENTION AREAS THAT ARE NOT RELATED TO WATER QUALITY PERFORMANCE STANDARDS WILL NOT BE ALLOWED WITHIN THE SETBACKS OR BUFFERS ESTABLISHED ON THE TECHNICAL DATA SHEET.

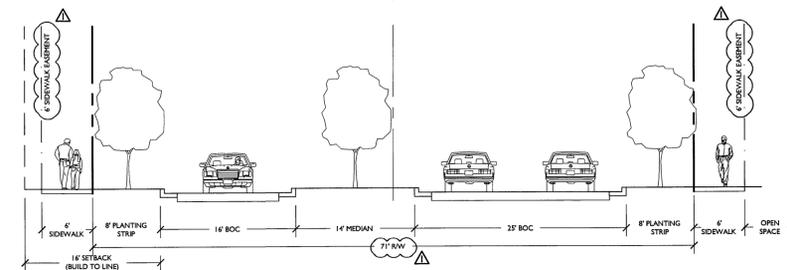
- A WATER QUALITY, STORM WATER DETENTION WET POND MAY BE LOCATED WITHIN THE OPEN SPACE AREA LOCATED ON PARCEL C. IF THIS WATER QUALITY, STORM WATER DETENTION AREA IS LOCATED WITHIN THE OPEN SPACE AREA IT WILL BE DESIGNED AS AN AMBINIT AND FEATURE OF THE OPEN SPACE.

### M. SOLID WASTE MANAGEMENT PLAN.

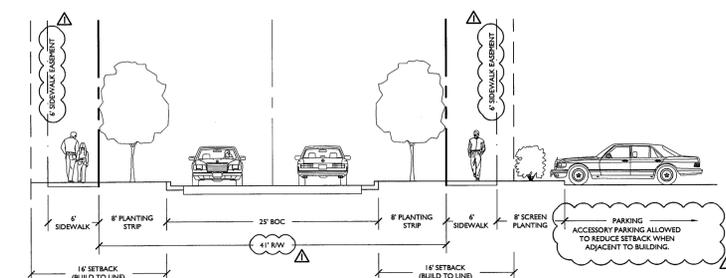
- THE PETITIONER SHALL SUBMIT TO THE MECKLENBURG COUNTY SOLID WASTE DEPARTMENT A SOLID WASTE MANAGEMENT PLAN PRIOR TO INITIATING DEMOLITION AND/OR CONSTRUCTION ACTIVITIES. THE SOLID WASTE MANAGEMENT PLAN WILL INCLUDE, AT A MINIMUM, THE PROCEDURES THAT WILL BE USED TO RECYCLE ALL CLEAN WOOD, METAL, AND CONCRETE GENERATED DURING DEMOLITION AND CONSTRUCTION ACTIVITIES. ADDITIONALLY, THE SOLID WASTE MANAGEMENT PLAN WILL SPECIFY THAT ALL LAND CLEARING AND INERT DEBRIS SHALL BE TAKEN TO A PERMITTED FACILITY. THE PLAN SHALL ALSO INCLUDE A REQUIREMENT THAT A MONTHLY REPORTING OF ALL TONNAGE DISPOSED AND RECYCLED WILL BE MADE TO THE



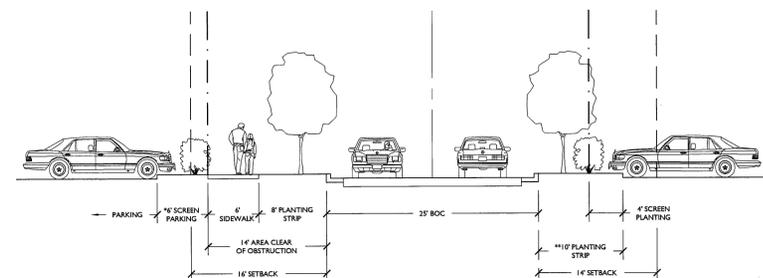
SECTION AA: PUBLIC ROAD - TYPE #1  
COMMERCIAL URBAN STREET SECTION / 16' BUILDING SETBACK FROM BACK OF CURB



SECTION BB: PUBLIC ROAD - TYPE #2  
COMMERCIAL URBAN STREET SECTION / 16' SETBACK FROM BACK OF CURB



SECTION CC: PUBLIC ROAD - TYPE #3  
COMMERCIAL URBAN STREET SECTION / 16' SETBACK FROM BACK OF CURB



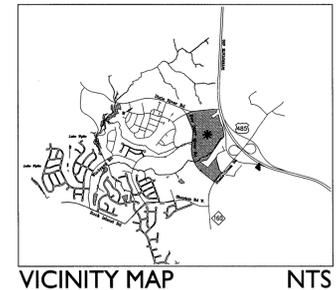
SECTION DD: PRIVATE DRIVE - TYPE #4  
COMMERCIAL URBAN STREET SECTION

**PARCEL 'A'**  
TOTAL AREA (GROSS): 4.36 AC  
EXISTING ZONING: R-3 LLW-PA & O-2(CD) LLV-PA  
PROPOSED ZONING: CC(SPA) LLV-PA

**PARCEL 'B'**  
TOTAL AREA (GROSS): 10.51 AC  
EXISTING ZONING: R-3 LLW-PA & O-2 LLV-PA  
PROPOSED ZONING: CC(SPA) LLV-PA

**PARCEL 'C'**  
TOTAL AREA (GROSS): 58.35 AC  
EXISTING ZONING: BP LLW-PA, CC LLW-PA & R-3 LLW-PA  
PROPOSED ZONING: CC LLW-PA & CC(SPA) LLV-PA

**PARCEL 'D'**  
TOTAL AREA (GROSS): 9.38 AC  
EXISTING ZONING: BP LLW-PA  
PROPOSED ZONING: I-1(CD) LLV-PA



FOR PUBLIC HEARING  
PETITION #: 2010-011  
CITY OF CHARLOTTE  
PETITIONER: STEELE CREEK (1997)  
LIMITED PARTNERSHIP

REVISIONS:  
January 19, 2010 - Resubmittal per Planning Staff Comments  
February 19, 2010 - Resubmittal per Planning Staff Comments

TECHNICAL DATA MAP

TYPICAL STREET SECTIONS