

SITE LEGEND:

-  APPROXIMATE LOCATION OF 100 YEAR FLOOD PLAN.
-  APPROXIMATE LOCATION OF 100' S.W.I.M. BUFFER.
-  SETBACK LOCATIONS (FRONT 22' B.O.C., REAR 10', & SIDEYARD 0'-5').

SITE DATA:

SITE TABULATION:
 EXISTING BUILDING 'A': +/- 17,000 SQ.FT.
 EXISTING BUILDING 'B': +/- 14,000 SQ.FT.
 EXISTING SITE PARKING: 85 STALLS

TOTAL EXISTING SITE PARKING: **85 STALLS**
 HANDICAP: 3 STALLS
 STANDARD (8'-6" X 18'-6"): 82 STALLS

TOTAL EXISTING BIKE PARKING: **0 UNITS**
 LONG TERM: 0 UNITS
 SHORT TERM: 0 UNITS

IMPERVIOUS SURFACES:
 BUILDING A: 17,000 SQ.FT.
 BUILDING B: 14,000 SQ.FT.
 PARKING AND WALKS: 45,957.0 SQ.FT.
 TOTAL: 76,957.0 SQ.FT. 94%

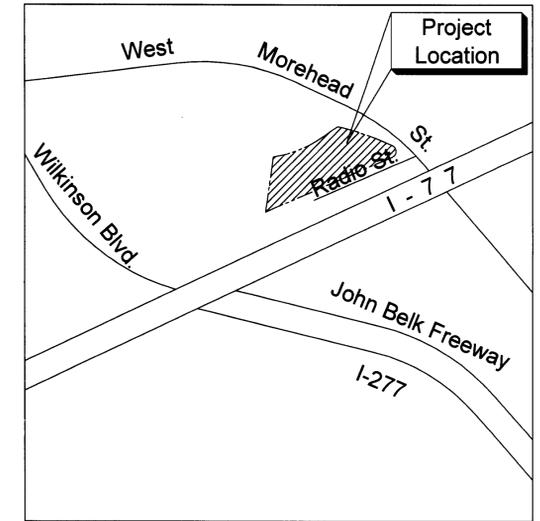
PERVIOUS SURFACES:
 LANDSCAPE: 4,903.0 SQ.FT.
 TOTAL: 4,903.0 SQ.FT. 6%

TOTAL SITE AREA: 81,860 / 1.88 SQ.FT. / ACRES

NOTES:

REFER TO SHEET RZ-3.0 FOR DEVELOPMENT NOTES.

VICINITY MAP



SITE DATA CONT.:

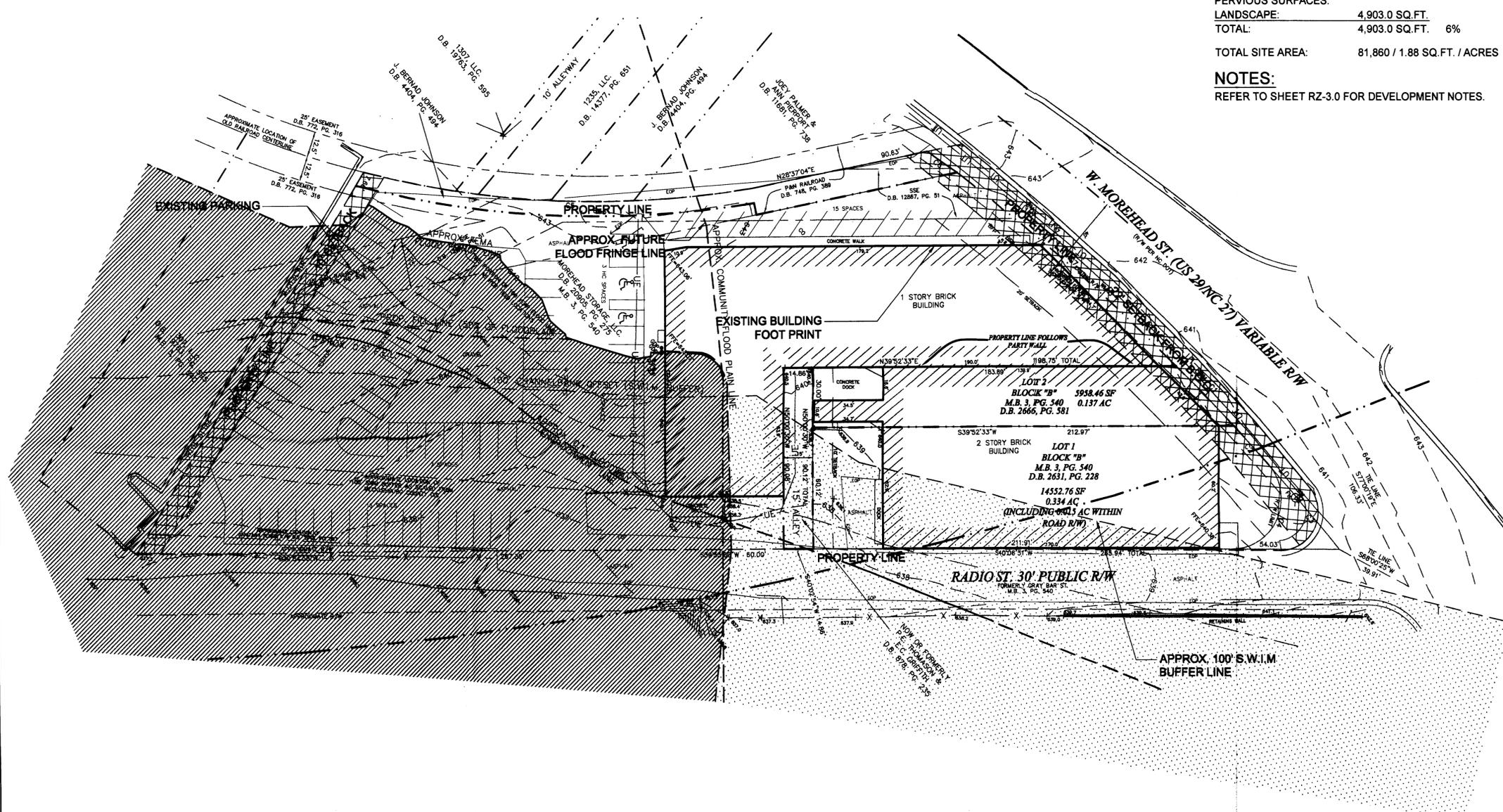
EXISTING ZONING: B-1 (BUSINESS DISTRICT) PED
 TOTAL SITE AREA: ±1.88 ACRES
 PROPOSED ZONING: B-D (CD) PED - OPTIONAL

PARCELS INCLUDED IN REZONING:

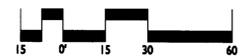
- A. B-1 (BUSINESS DISTRICT) PED - BUILDING 'B'
 JOHN CAMPBELL, A. SCOTT CAMPBELL,
 & CHARLES A. DIGGERS
 D.B. 2631 PG. 228
 PARCEL ID# 07325401
- B. B-1 (BUSINESS DISTRICT) PED - BUILDING 'B'
 JOHN CAMPBELL, A. SCOTT CAMPBELL,
 & CHARLES A. DIGGERS
 D.B. 2666 PG. 540
 PARCEL ID# 07325402
- C. B-1 (BUSINESS DISTRICT) - BUILDING - 'A'
 MOREHEAD STORAGE, LLC.
 D.B. 20905 PG. 275
 PARCEL ID# 07325403

ADJOINERS PARCELS:

1. MUDD-0 (MIXED USE BUSINESS DISTRICT)
 JOEY PALMER & ANNE PIERPORT
 D.B. 11681 PG. 738
 PARCEL ID# 07325404
2. B-1 (BUSINESS DISTRICT)
 J. BERNAD JOHNSON
 D.B. 4404 PG. 494
 PARCEL ID# 07325405
3. B-1 (BUSINESS DISTRICT)
 1235, LLC.
 D.B. 14377 PG. 851
 PARCEL ID# 07325406
4. MUDD (MIXED USE DEVELOPMENT DISTRICT)
 1307, LLC
 D.B. 19763 PG. 595
 PARCEL ID# 07325407
5. MUDD (MIXED USE DEVELOPMENT DISTRICT)
 1307, LLC
 D.B. 19763 PG. 595
 PARCEL ID# 07325411



**APPROVED BY
 CITY COUNCIL**
 FEB 18 2008



DISCLAIMER: ALL EXISTING LAND INFRASTRUCTURE AND ENVIRONMENTAL CONDITIONS INFORMATION WAS TAKEN FROM THE CITY OF CHARLOTTE'S GIS/PLANIMETRIC DATA.

CHARLOTTE/MECKLENBURG
 REQUIREMENTS: PETITION #:
 "FOR PUBLIC HEARING" 2007-113

REVISIONS:
 CHARM/MECK PLANNING 09/26/07
 CHARM/MECK PLANNING 11/12/07
 CHARM/MECK PLANNING 12/14/07
 CHARM/MECK PLANNING 02/15/08

DATE: 06/04/07
 DESIGNED BY: LDB
 DRAWN BY: LDB
 CHECKED BY: MRC
 SCALE: AS NOTED
 PROJECT #: 1006186
 SHEET #:

PROPOSED PARKING FOR ADJACENT PARCELS. (SEE DEVELOPMENT STANDARDS NOTE 6.4)

COVERED LONG-TERM AND SHORT-TERM BIKE STORAGE

ALL PROPOSED PLANTING AREAS WITHIN PARKING AREA TO MITIGATE STORM WATER RUN OFF WHEN APPLICABLE.

EXISTING 10' REAR YARD SET BACK. 10' WIDE LANDSCAPE SCREEN REQUIRED FOR PARKING.

EXISTING ASPHALT PARKING LOT TO BE RE-STRIPED

PROPOSED CABLE RAIL RETENTION SYSTEM FOR STORM WATER AND FLOOD REQUIREMENTS

PROPOSED LOCATION POND/SAND FILTER FOR WATER QUALITY.

PROPOSED 8' WIDE CONCRETE WALK TO BE CONSTRUCTED AND DEDICATED TO MCPR FOR IRWIN CREEK GREENWAY CONNECTION. STUB TO PROPERTY LINE FOR FUTURE CONNECTION.

EXISTING 0' SIDE YARD SET BACK. 5' WIDE LANDSCAPE SCREEN REQUIRED FOR PARKING.

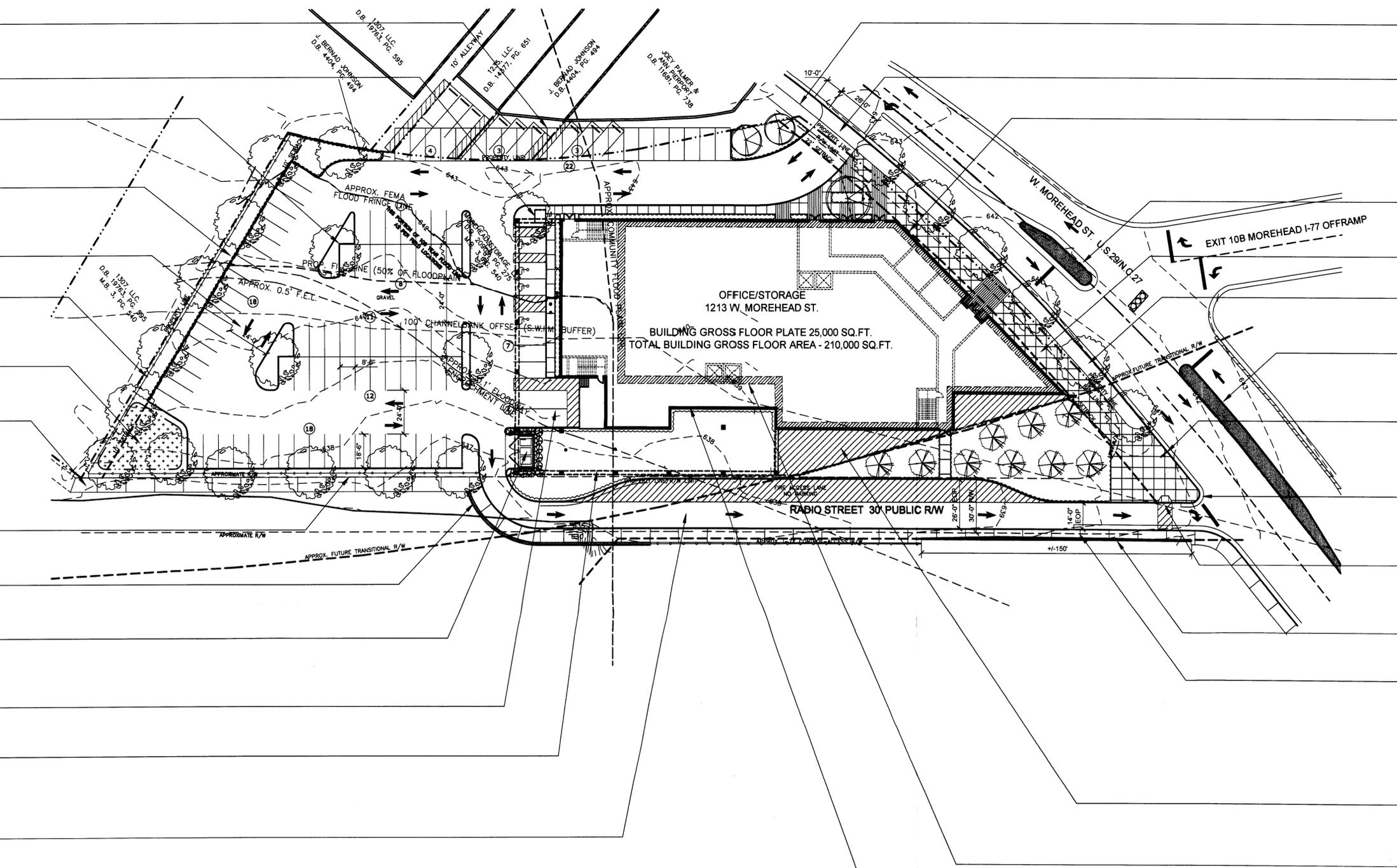
PROPOSED 6' H. RETAINING WALL TO HOLD EXISTING STREET GRADE WITH 42" HIGH GUARD RAIL.

PROPOSED TRASH ENCLOSURE LOCATION W/ 6' HIGH MASONRY WALLS AND STEEL DOORS AND LOCKABLE HARDWARE.

TRUCK LOADING AND UNLOADING MANEUVERING AREA.

INDICATES BUILDING OVERHANG OF FLOORS ABOVE ON THE SOUTH & EAST SIDES. COLUMN LOCATION TO BE DETERMINED.

RADIO STREET (PUBLIC RW) TO BE ONE -WAY EGRESS ONLY AS INDICATED.



PROPOSED 8'-0" WIDE SIDEWALK TO ALIGN WITH ADJACENT WALK PER P.E.D. REQUIREMENTS.

PROPOSED NEW DRIVE ACCESS FOR INGRESS & EGRESS.

22'-0" FRONT SET BACK TO BE MEASURED FROM EXISTING BACK OF CURB OR PROPOSED BACK OF CURB WHICHEVER IS GREATER, PER P.E.D. REQUIREMENTS.

EXISTING STRIPED MEDIAN PER CITY OF CHARLOTTE MOREHEAD STREET IMPROVEMENT PLAN.

ACCENT PAVING ALONG BUILDING FRONT.

PROPOSED NEW 8'-0" WIDE PARKWAY PLANTING PER P.E.D. REQUIREMENTS.

PROPOSED NEW 8' WIDE, 6" HIGH CONCRETE MEDIAN WITH MOUNTABLE CURB. TO BE REVIEWED AND APPROVED BY CDOT.

PROVIDE CITY APPROVED SIGNAGE TO INDICATE "NO RIGHT HAND TURN ALLOWED" FROM MOREHEAD ONTO RADIO AS REQUIRED BY CDOT.

PROPOSED NEW CURB AND GUTTER EXTENSION WITH 5' CURB RADIUS. TO BE REVIEWED AND APPROVED BY CDOT.

PROPOSED NEW CURB CUT AND ACCESSIBLE RAMP AS REQUIRED. STRIPE PAVEMENT FOR PEDESTRIAN CROSSING.

INTERSTATE 77 OVERPASS WALL AND LIMITS OF CONTROL-ACCESS

PROPOSED IRWIN CREEK OVERLAND CONNECTION WITH 5' WIDE CONCRETE WALK FOR 150' TO BE CONSTRUCTED AND DEDICATED TO MCPR FOR IRWIN CREEK GREENWAY CONNECTION. CONNECT WITH 8' WIDE WALK BEYOND WALL.

INDICATES PROPOSED BUILDING LINE (FLEXIBLE SQ.FT.) ADJACENT TO TRANSITIONAL RIGHT-OF-WAY.

DASHED LINE INDICATES LOCATION OF OPTIONAL SUB-LEVEL FLOOR (BASEMENT). PROVIDE WATER PROOFING AS REQUIRED.

PROPOSED BUILDING FINISH FLOOR ELEVATION TO BE ABOVE FLOOD PLAN ELEVATION. PROVIDE WATERPROOFING AS REQUIRED.

SITE DATA:

EXISTING ZONING: B-1 (BUSINESS DISTRICT) PED
 TOTAL SITE AREA: ±1.88 ACRES
 PROPOSED ZONING: B-D (CD) PED -OPTIONAL

SITE TABULATION:

PROPOSED BUILDING GROSS	210,000 SQ.FT	
FIRST FLOOR GROSS:	25,000 SQ.FT.	OFFICE/STORAGE
SECOND FLOOR GROSS:	33,000 SQ.FT.	STORAGE
THIRD FLOOR GROSS:	33,000 SQ.FT.	STORAGE
FOURTH FLOOR GROSS:	33,000 SQ.FT.	STORAGE
FIFTH FLOOR GROSS:	33,000 SQ.FT.	STORAGE
SIXTH FLOOR GROSS:	33,000 SQ.FT.	OFFICE/STORAGE
OPTIONAL BASEMENT GROSS:	20,000 SQ.FT.	STORAGE
TOTAL PROPOSED SITE PARKING:	81 STALLS	
HANDICAP:	4 STALLS	
STANDARD (8'-6" X 18'-6"):	77 STALLS	
TOTAL MAXIMUM PARKING STALLS REQUIRED:	82 STALLS	
TOTAL MINIMUM PARKING STALLS REQUIRED:	67 STALLS	
TRUCK LOADING (10' X 30'):	2 STALLS	

SITE TABULATION CONT:

TOTAL PROPOSED BIKE PARKING:	9 UNITS
LONG TERM:	7 UNITS
SHORT TERM:	2 UNITS
IMPERVIOUS SURFACES:	
BUILDING:	25,000 SQ.FT.
PARKING AND WALKS:	42,645 SQ.FT.
TOTAL:	67,645 SQ.FT. 83%
PERVIOUS SURFACES:	
LANDSCAPE:	14,035 SQ.FT.
TOTAL:	14,035 SQ.FT. 17%
TOTAL SITE AREA:	81,860 SQ.FT / 1.88 . ACRES
PROPOSED IRWIN CREEK ACCESS:	
TO PROVIDE AND CONSTRUCT AN 8' WIDE CONCRETE SIDEWALK WHERE APPLICABLE FOR FUTURE OPEN SPACE CONNECTION.	
TOTAL CONCRETE WALK:	4,500 SQ.FT.
TOTAL DEDICATION:	2,800 SQ.FT.

- PROPOSED USES:**
- OFFICE/STORAGE.
 - SIZE:
 - BUILDING GROSS FLOOR PLATE - 25,000 SQ.FT.
 - OVERALL BUILDING GROSS - 210,000 SQ.FT.
 - 5-10% GROSS OFFICE - 10,000-20,000 SQ.FT.
 - FIRST FLOOR (MIN. 5,000 / MAX. 10,000)
 - SIXTH FLOOR (MIN. 5,000 / MAX. 10,000)
 - 90-95% GROSS STORAGE - 190,000-200,000 SQ.FT.
 - HEIGHT:

PROPOSED BUILDING HEIGHT SHALL BE A MAXIMUM HEIGHT OF 75', NOT TO EXCEED 6 STORIES AS MEASURED FROM THE MAIN ENTRY OF THE GROUND LEVEL OF THE BUILDING TO THE TOP OF THE ROOF STRUCTURE, EXCLUDING ARCHITECTURAL TOWERS, FEATURES AND/OR ELECTRICAL/MECHANICAL PENTHOUSES OR SCREENS.
 - PARKING:

(17 MIN./34 MAX./PEID) 8.5' X 18.5' PARKING STALLS REQUIRED AT 1 STALL/600 SQ.FT OFFICE (PED) AND (317 MIN./333 MAX.) REQUIRED AT 1 STALL/600 SQ.FT. STORAGE (PED) 2 - 10' X 30' LOADING/UNLOADING SPACES REQUIRED AT 1 STALL/ 100,000 SQ.FT. (PER P.E.D REQUIREMENTS)

PROPOSED USES CONT:

- PARKING CONT:

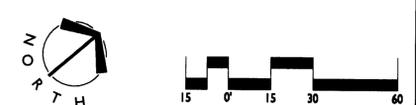
OPTIONAL REQUEST FOR STORAGE USE. - AT 0.25 STALLS/1000 SQ.FT. (48 MIN./50 MAX. STALLS REQUIRED)

NOTES:

REFER TO SHEET RZ-3.0 FOR DEVELOPMENT NOTES.

CONNECTION AND CROSS-ACCESS EASEMENT WITH ADJACENT PARCEL (ID# 70325407) CANNOT BE PROVIDED DUE TO EXISTING SITE CONSTRAINTS BY BOTH PARCELS.

SIGNAGE TO BE PROVIDED ON SITE WITHIN THE DESIGNATED PARKING AREA TO INDICATE THAT THE PARKING LOT IS LOCATED WITHIN THE COMMUNITY FLOOD PLAIN AND IS SUBJECT TO FLOODING. SIGNS SHALL BE POSTED FOR PUBLIC SAFETY.



CHARLOTTE/MECKLENBURG

REQUIREMENTS: "FOR PUBLIC HEARING"

PETITION #: 2007-113

DEVELOPMENT STANDARDS

February 08, 2008

GENERAL PROVISIONS

- 1.1 Unless more stringent standards are established by the Technical Data Sheet or these Development Standards, and subject to the optional provisions set out below, all development standards established under the City of Charlotte Zoning Ordinance (the "Ordinance") for the B-D zoning district classification and the Pedestrian Overlay District classification ("PED") shall be followed in connection with development taking place on the Site. As provided in the Ordinance, the standards and requirements of the PED, subject to the optional provisions set out below, shall take precedence over the standards and requirements of the B-D zoning district.
- 1.2 The development depicted on the Schematic Site Plan is intended to reflect the arrangement of proposed uses on the Site, but the final configuration, placement and size of the individual site elements may be altered or modified within the limits prescribed by the Ordinance and the standards established by the Technical Data Sheet and these Development Standards during design development and construction phases. Parking layouts may be modified to accommodate the final building location.

PERMITTED USES

- 2.1 The Site may be devoted only to the following uses:
 - 2.1.1 a self storage facility, such facility's rental and management office and any accessory uses that are clearly incidental and related thereto; and
 - 2.1.2 office uses and any accessory uses that are clearly incidental and related thereto.
- 2.2 All storage shall be located within the building located on the Site, and outside storage of any type, including the outside storage of moving vans, trailers, vehicles and boats, shall not be permitted.
- 2.3 The storage of hazardous materials is prohibited.

MAXIMUM GROSS FLOOR AREA

- 3.1 The total maximum gross floor area of the building to be constructed on the Site shall be 210,000 square feet. A minimum of 10,000 square feet and a maximum of 20,000 square feet of the gross floor area of the building to be constructed on the Site shall be devoted to office uses, with the remainder of the building being devoted to self storage uses.
- 3.2 A minimum of 5,000 square feet of the gross floor area of the first floor of the building to be constructed on the Site shall be devoted to office uses, and a minimum of 5,000 square feet of the gross floor area of the sixth floor of the building shall be devoted to office uses.
- 3.3 The basement of the building that is depicted or noted on the Schematic Site Plan may be constructed at the option of the Petitioner, and the Petitioner shall not be required to construct the basement.

SETBACKS, SIDE YARDS AND REAR YARDS/TRANSITIONAL RIGHT OF WAY

- 4.1 As depicted on the Technical Data Sheet and the Schematic Site Plan, development of the Site shall comply with the setback, side yard and rear yard requirements established under the Ordinance for the PED.
- 4.2 As provided under the Ordinance, the minimum setback shall be measured from the back of the existing or future curb line of West Morehead Street, whichever is greater.
- 4.3 The building and parking and loading areas to be located on the Site shall not be located within the 175 foot transitional right of way for Interstate 77 as depicted on the Schematic Site Plan. A building and parking setback from the 175 foot transitional right of way shall not be required in accordance with the PED.
- 4.4 No buildings, parking spaces or maneuvering areas may be located within the setback.

SCREENING/TREE ORDINANCE/LANDSCAPING

- 5.1 Screening shall conform with the standards and treatments specified in Section 10.803(8) of the Ordinance.
- 5.2 All roof mounted mechanical equipment placed on the building to be constructed on the Site will be screened from view at grade from adjoining public rights of way and abutting properties.
- 5.3 Pursuant to Section 10.803(8)(b) of the Ordinance, dumpsters or trash handling areas must always be screened from adjacent property and from public view with a minimum 6-foot high solid and finished masonry wall with a solid and closeable gate. A solid wooden fence may be substituted if the dumpsters or trash handling areas are not visible from a public street or transitway. Dumpsters are not allowed in any required setback or yard space.
- 5.4 Development of the Site shall comply with the City of Charlotte Tree Ordinance.
- 5.5 Development of the Site shall meet the landscaping requirements of the Ordinance.

PARKING

- 6.1 Off-street vehicular parking will be provided at the rate of 1 parking space per 600 square feet of floor area devoted to office uses pursuant to Section 10.803(6)(a) of the Ordinance.
- 6.2 Pursuant to Sections 10.808 through 10.811 of the Ordinance [Optional Provisions], off-street vehicular parking will be provided at the rate of .25 parking spaces per 1,000 square feet of floor area devoted to self storage uses.
- 6.3 Bicycle parking will meet the minimum requirements set out in the Ordinance.

DEVELOPMENT STANDARDS CONT.

February 08, 2008

PARKING CONT.

- 6.4 As more particularly depicted on the Schematic Site Plan, up to 22 parking spaces are proposed to be located along the western boundary line of the Site and on those adjoining parcels of land designated as Tax Parcel Nos. 073-254-06, 073-254-05 and 073-254-04 (the "Adjoining Parcels"). These 22 parking spaces, or as many as can actually be located within the relevant area, shall be constructed, if at all, by the owners of the Adjoining Parcels and maintained by the owners of the Adjoining Parcels. The owners of the Adjoining Parcels will be responsible for obtaining all permits and approvals for the construction of these parking spaces, including obtaining any variances that may be required from the City of Charlotte Floodplain Regulations or any other ordinances. The Petitioner will grant easements to the owners of the Adjoining Parcels for the initial construction of the parking spaces and the perpetual maintenance and use thereof, and perpetual access easements to the owners of the Adjoining Parcels over the driveway located along the Site's western boundary line for access to the parking spaces for the construction and use thereof.
- 6.5 On December 19, 2007, the City of Charlotte Zoning Board of Adjustment granted a variance to permit the relevant portions of the parking areas to be located within the Community Floodplain or Floodway as depicted on the Schematic Site Plan.
- 6.6 Subject to obtaining all requisite approvals and permits, the Petitioner may construct a structured parking deck on that portion of the Site devoted to parking areas.

LIGHTING

- 7.1 The maximum height of the light source (light bulb) detached from a building shall be 20 feet.
- 7.2 All free standing lighting fixtures installed on the Site shall be capped and fully shielded and the illumination downwardly directed so that direct illumination does not extend past any property line of the Site. Consideration will be given to the impact of lighting both within and without the perimeter of the Site. Items for consideration will include intensity, cut-off angles, color, energy efficiency and shielding of sources of light, the intent being to eliminate glare towards public streets and adjacent properties.
- 7.3 No wall pack style light fixtures will be permitted on the Site, and any wall mounted light fixtures installed on the Site shall be capped and fully shielded so that illumination does not extend past any property line of the Site.
- 7.4 Light fixtures shall not be installed within the tree islands located in the parking areas on the Site.

SIGNS

- 8.1 All signs placed on the Site will be erected in accordance with the requirements of Section 10.804(1)(e) and Chapter 13 of the Ordinance.
- 8.2 Pursuant to Section 10.804(1)(e)(1) of the Ordinance, signs located on any building wall of a structure shall have a maximum sign surface of all signs on one wall not to exceed 5% of the building wall area to which the signs are attached, up to a maximum of 100 square feet. Wall signs may be increased by 10% per sign in lieu of a ground mounted or monument sign.

ACCESS POINTS

- 9.1 The number of vehicular access points to the Site shall be limited to the number depicted on the Technical Data Sheet.
- 9.2 The placement and configuration of each access point are subject to any minor modifications required to accommodate final site and architectural construction plans and designs and to any adjustments required for approval by the City of Charlotte Department of Transportation and/or the North Carolina Department of Transportation.
- 9.3 As depicted on the Schematic Site Plan, the Radio Street connection to West Morehead Street may be utilized only as an exit onto West Morehead Street, and vehicular movements shall be limited to right out movements from Radio Street onto West Morehead Street.
- 9.4 Subject to the approval of NCDOT and/or CDOT as applicable, the Petitioner shall construct geometric improvements at the intersection of Radio Street and West Morehead Street to restrict vehicular movements to right out movements from Radio Street onto West Morehead Street.

FIRE PROTECTION

- 10.1 Adequate fire protection in the form of fire hydrants will be provided to the Mecklenburg County Fire Marshal's specifications. Plans for the building to be constructed on the Site will be submitted to the Fire Marshal's office for approval before the construction of the building commences.

SIDEWALKS/GREENWAY CONNECTION

- 11.1 Sidewalks and plaza areas will be installed as depicted on the Schematic Site Plan.
- 11.2 As depicted on the Schematic Site Plan, an 8 foot planting strip and an 8 foot sidewalk shall be installed along the Site's frontage on West Morehead Street.
- 11.3 As depicted on the Schematic Site Plan, the Petitioner shall construct and dedicate to the Mecklenburg County Park and Recreation Department a concrete sidewalk along the eastern edge of Radio Street and the eastern edge of the Site for an Irwin Creek Greenway connection. Due to site constraints, the width of this concrete sidewalk shall vary from 5 feet to 8 feet as depicted on Schematic Site Plan.

DEVELOPMENT STANDARDS CONT.

February 08, 2008

ARCHITECTURAL CONTROLS

- 12.1 The maximum height of the building to be constructed on the Site shall be 75 feet, not to exceed 6 stories.
- 12.2 The building to be constructed on the Site must be located within the building envelope lines depicted on the Technical Data Sheet.
- 12.3 Attached are architectural renderings of the elevations of the front, rear, right side and left side facades of the building to be constructed on the Site, and they are intended to portray the basic character and quality of the building proposed to be constructed on the Site. Accordingly, the building to be constructed on the Site shall be designed and constructed so that each facade is substantially similar in appearance to the corresponding attached architectural rendering. The primary exterior building materials to be utilized shall be masonry with pre-cast accents on the first 5 stories of the building and stucco on the sixth story. The windows on the sixth floor shall be a storefront system with steel or aluminum cornice, and the exterior doors fronting on West Morehead Street shall be aluminum or steel storefront doors.
- 12.4 Direct access to the individual self storage units located in the building to be constructed on the Site will not be provided from the exterior of the building, as access to the individual storage units will be provided by internal hallways. Covered unloading and loading areas into the building will be provided.
- 12.5 Office uses shall be located on perimeter portions of the first floor and the sixth floor of the building to be constructed on the Site.
- 12.6 The design of the building to be constructed on the Site shall comply with the urban design standards of the PED set out in Section 10.804 of the Ordinance.

STORM WATER

- 13.1 The Petitioner shall construct water quality best management practices ("BMPs") to achieve 85% total suspended solid removal for the entire post-development runoff volume for the runoff generated from the first 1-inch of rainfall. BMPs must be designed and constructed in accordance with the N.C. Department of Environment and Natural Resources Best Management Practices Manual, April 1999, Section 4.0 (Design Standards shall be met according to the City of Charlotte Best Management Practices Manual, when available). Use of Low Impact Development techniques is optional.

OPTIONAL PROVISIONS

Pursuant to Sections 10.808 through 10.811 of the Ordinance, the Petitioner requests the approval of the following optional provisions:

- 14.1 Off-street vehicular parking will be provided at the rate of .25 parking spaces per 1,000 square feet of floor area devoted to self storage uses.

VARIANCE CONDITIONS

The following three conditions were imposed on the variance granted by the City of Charlotte Zoning Board of Adjustment on December 19, 2007 to permit the relevant portions of the parking area to be located within the Community Floodway or Floodway:

- 15.1 The Petitioner shall provide to the Zoning Administrator and Floodplain Manager a certificate of the design of a cabling system that is reasonably sufficient to ensure against the danger that materials allowed to be placed in the floodway as a result of the variance might be swept away onto other lands to the injury of others during a Community Base Flood. The cable rail system is generally depicted on the Schematic Site Plan.
- 15.2 The Petitioner shall install clear signage at each entrance into the Site that the parking lot may be subject to flooding.
- 15.3 The use of the building on the Site is to be that set forth in the application and specifically not residential in nature.

AMENDMENTS TO REZONING PLAN

- 16.1 Future amendments to the Technical Data Sheet and these Development Standards may be applied for by the then Owner or Owners of the Site in accordance with the provisions of Chapter 6 of the Ordinance.

BINDING EFFECT OF THE REZONING APPLICATION

- 17.1 If this Rezoning Petition is approved, all conditions applicable to the development of the Site imposed under these Development Standards and the Technical Data Sheet will, unless amended in the manner provided under the Ordinance, be binding upon and inure to the benefit of the Petitioner and subsequent owners of the Site and their respective heirs, devisees, personal representatives, successors in interest or assigns.

CHARLOTTE/MECKLENBURG	
REQUIREMENTS:	PETITION #:
" FOR PUBLIC HEARING "	2007-113

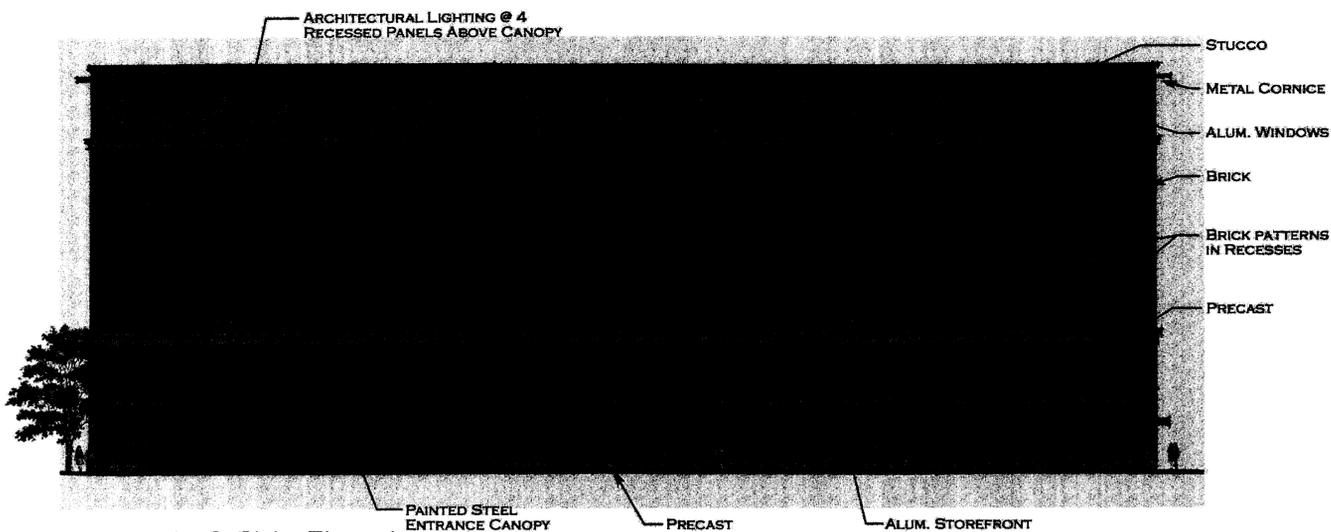
1213 MOREHEAD
REZONING PETITION-2007-113
CHARLOTTE, NORTH CAROLINA
DEVELOPMENT STANDARDS

LandDesign®
223 N. Graham Street, Charlotte, NC 28202
V: 704.333.0325 F: 704.333.3746
www.LandDesign.com

REVISIONS:
CHARMECK PLANNING - 09/26/07
DRAWINGS: LDB
CHARMECK PLANNING - 11/12/07
O.C. BY: MRC
CHARMECK PLANNING - 12/14/07
CHARMECK PLANNING - 02/15/08

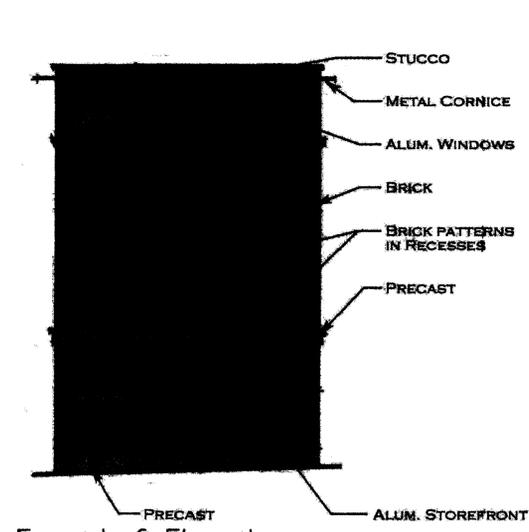
DATE: 06/04/07
DESIGNED BY: LDB
DRAWN BY: MRC
O.C. BY: MRC
SCALE: NONE
PROJECT #: 1006186
SHEET #:

RZ-3.0



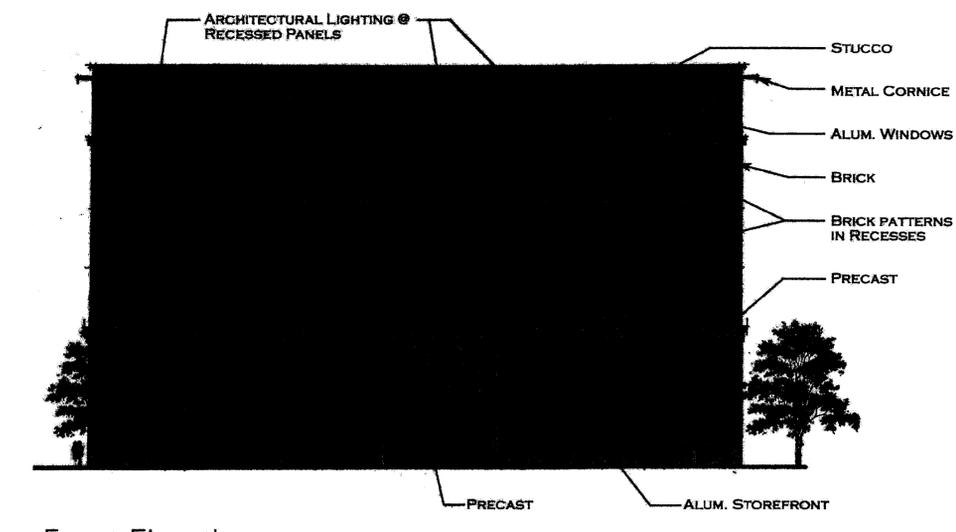
Left Side Elevation

Scale: 1/16" = 1'-0" (Drive entrance off W. Morehead Street)



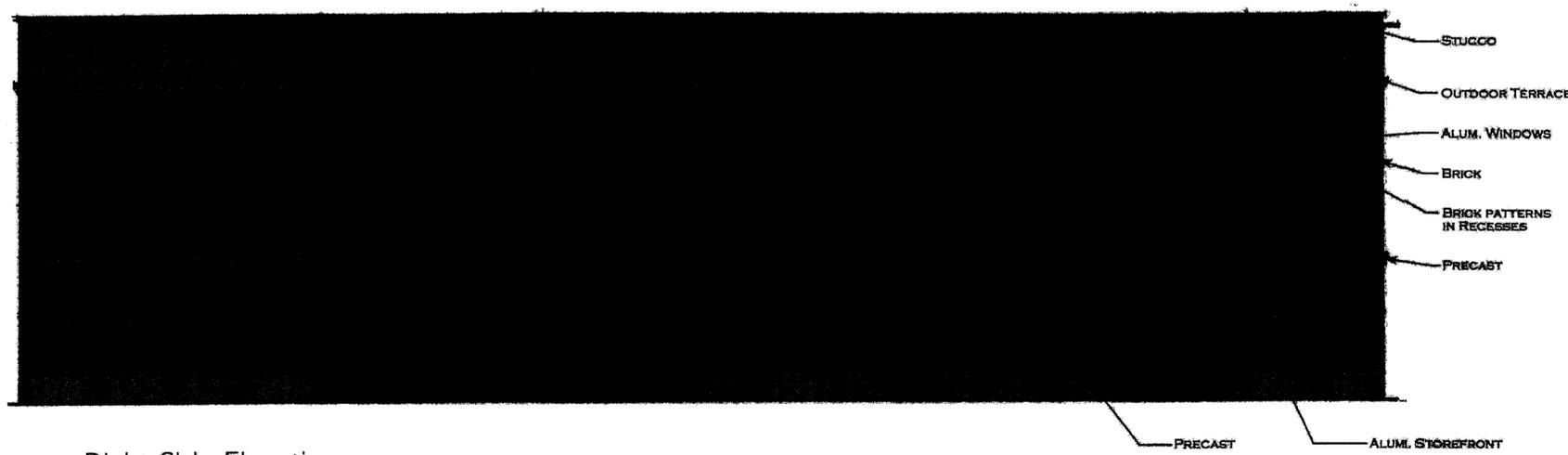
Front Left Elevation

Scale: 1/16" = 1'-0" (Facing W. Morehead & Radio Street)



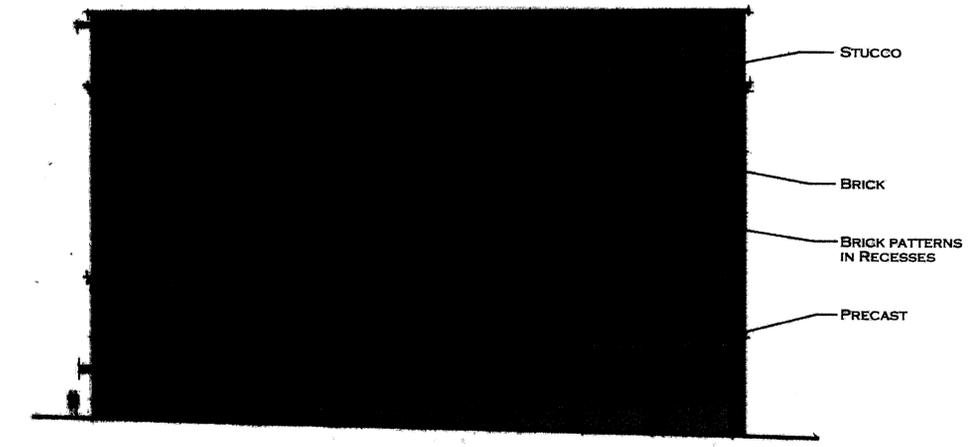
Front Elevation

Scale: 1/16" = 1'-0" (Facing W. Morehead Street)



Right Side Elevation

Scale: 1/16" = 1'-0" (Facing Radio Street, view from 77 Freeway)



Rear Elevation

Scale: 1/16" = 1'-0" (Facing existing parking area)