



Know what's below.
Call before you dig.

- CONTRACTOR IS FULLY RESPONSIBLE FOR CONTACTING APPROPRIATE PARTIES AND ASSURING THAT EXISTING UTILITIES ARE LOCATED PRIOR TO CONSTRUCTION.
- CONTRACTOR IS RESPONSIBLE FOR PLACING BARRICADES USING FLAG MEN, ETC. AS NECESSARY TO INSURE SAFETY TO THE PUBLIC.
- ALL PAVEMENT CUTS, CONCRETE OR ASPHALT, ARE TO BE REPLACED ACCORDING TO STANDARDS OF THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION AND CHARLOTTE-MECKLENBURG UTILITIES SPECIFICATIONS.

1A TRENCHING STANDARDS PART



FINAL
APPROVAL

http://development.charmeck.org

ENGINEERING

PCO / DETENTION / DRAINAGE PLAN

APPROVED

By Brendan Smith at 11:08 am, Apr 08, 2016

EROSION CONTROL

NOTE: SCHEDULE PRE-CONSTRUCTION MEETING AT LEAST 48 HRS. PRIOR TO ANY LAND DISTURBING ACTIVITY USING THE ONLINE FORM AT <http://development.charmeck.org>

APPROVED

URBAN FORESTRY

TREE ORDINANCE

APPROVED

CDOT

APPROVED

accessory uses, as allowed in the R-17MF zoning district building # 3 that backs up to of the building facing Clem's Court, which will be limited to and Clementine Court which basement (three stories) on the ends of the building and three (3) stories over the top of the building, the basement will be oriented toward Clem's Creek.

--Parking: 1.5 spaces per unit minimum.

--Open Space: A minimum of 15% of the Site will be established as tree save/open space areas as defined by the Ordinance.

1. General Provisions:

- These Development Standards form a part of the Rezoning Site Plan associated with the Rezoning Petition filed by GCI Acquisitions, LLC to accommodate development of multi-family dwellings on an approximately 16.20 acre site located at 15640 Lancaster Hwy. (the "Site").
- Development of the Site will be governed by the attached Rezoning Site Plan and these Development Standards (collectively referred to as the "Rezoning Plan") as well as the applicable provisions of the City of Charlotte Zoning Ordinance (the "Ordinance"). Unless the Rezoning Plan establishes more stringent standards, the regulations established under the Ordinance for the R-17MF zoning district classification shall govern development taking place on the Site.
- The development depicted on the Rezoning Plan is schematic in nature, and except as otherwise specified in these Development Standards, is intended to describe the possible arrangements of uses and site elements but won't exceed stipulations above. The depictions of the uses, parking areas, structures and buildings, and other site elements set forth on the Rezoning Plan should be reviewed in conjunction with the provisions of these Development Standards. Consequently, except as otherwise expressly specified on the Rezoning Site Plan and in these Development Standards, the ultimate layout of the development proposed; the exact alignments of streets and points of access; the numbers, the size, configuration and placements of buildings and parking areas; and the depictions of other site elements on the Rezoning Plan as well as any schematic building elevations are preliminary graphic representations of the types and quality of development proposed. They may, therefore, be altered or modified during design development and construction document phases within the maximum building/parking envelope generally depicted on the Rezoning Plan as long as the changes maintain the general building and parking orientation and character of the development generally depicted on the Rezoning Site Plan. Changes to the Rezoning Plan will be reviewed and approved as allowed by Section 6.207(2).

- Parking layouts for surface and structured parking may be modified to accommodate final building locations and parking spaces may be located within the development area boundaries to the extent permitted by the Ordinance. Sidewalks generally depicted on the Rezoning Plan are intended to reflect the general pedestrian circulation for development on the Site but the specific locations of such sidewalks may be subject to variations that do not materially change the design intent generally depicted on the Rezoning Plan.

- Notwithstanding the number of buildings shown on the Rezoning Plan, the total number of principal buildings to be developed on the Site shall not exceed nine (9). Accessory buildings and structures located on the Site shall not be considered in any limitation on the number of buildings on the Site.

2. Permitted Uses & Development Area Limitation:

- The Site may be developed with up to 248 multi-family dwelling units together with accessory uses allowed in the R-17MF zoning district.
- Surface parking areas will not be allowed between the proposed buildings and the extension of Landing Place Lane (internal public street) and Lancaster Highway. Parking areas may be located to the side of buildings that front on Landing Place Lane or Lancaster Highway.

- Access:**
Access to the Site will be from Lancaster Hwy, and from the extension of Landing Place Lane as generally depicted on the Rezoning Plan. Landing Place will be extended as a public street in compliance with the Subdivision regulations.

- The Petitioner will contribute to the Charlotte Department of Transportation (CDOT) or NCDOT up to half the cost but not to exceed \$50,000 for the installation of a traffic signal at the intersection of Landing Place Lane and Lancaster Highway when the signal is warranted and the funds are requested by CDOT or NCDOT. This commitment to provide funds toward the future signalization of the intersection of Landing Place Lane and Lancaster Highway is valid for a period of ten (10) years from the date of approval of this Petition.

- Prior to the issuance of a building permit for the Site the Petitioner will commission a Signal Warrant Analysis for the intersection of Landing Place Lane and Lancaster Highway. The Petitioner will work with CDOT on the preparation of the Signal Warrant Analysis and will submit the result of the Signal Warrant Analysis to CDOT for review and for its use.

- As part of the extension of Landing Place Lane the Petitioner will install a choker lane within Landing Place Lane as generally depicted on the Rezoning Plan. The final location and design of the choker lane will be determined by CDOT and the Planning Department during the subdivision review process.

- The intersection of Landing Place Lane and Lancaster Hwy. will be design so that the eastbound intersection approach for Landing Place is aligned with the westbound approach of Clem's Branch Drive as to not affect operations of a future traffic signal. The Petitioner will be responsible for any of the modifications required to properly align existing Clem's Branch Drive with the extension of Landing Place Lane.

- The Petitioner will work with the City of Charlotte to provide cross-walks along the extension of Landing Place Lane. If allowed by the City of Charlotte the proposed cross-walks will be designed with stamped and colored asphalt as generally illustrated on the Rezoning Plan.

- The Petitioner will construct a northbound left turn lane from Lancaster Hwy. to the extension of Landing Place Lane per NCDOT and CDOT standards.

- The Petitioner will provide a southbound right-turn lane along Lancaster Hwy. into the extension of Landing Place Lane if required by NCDOT.

- If curb and gutter is required along Lancaster Hwy. the face of the curb will be located approximately 37 feet from the center line of the road.

- The Petitioner will dedicate to the City of Charlotte 50 feet of right-of-way from the center line of Lancaster Highway. This right-of-way dedication will occur at the time the right-of-way for Landing Place is recorded.

- The placements and configurations of vehicular access points are subject to any minor modifications, as approved by CDOT, required to accommodate final site and construction plans and designs and to any adjustments required for approval by the CDOT. This includes adjustments required for approval by CDOT in accordance with published standards.

- Architectural Standards:**
 - The building materials used on the principle buildings constructed on Site will be a combination of the following: brick, stone, precast stone, precast concrete, textured stone, corrugated siding (south or hand-plank), stone, and/or wood. At least 35% of the exterior of each building, exclusive of windows, doors and roofs, will be constructed of brick, stone, synthetic stone, precast stone or precast concrete. Vinyl as a building material may only be used on windows and soffits.

[Revision]
PRELIMINARY PLANNED
MULTI-FAMILY SUBDIVISION
APPROVAL
CHARLOTTE-MECKLENBURG PLANNING DEPARTMENT
By Brent Wilkinson 4/8/16

*Storm revisions along Lancaster Highway

- The attached illustrative building elevation is included to reflect an architectural style and a quality of the building that may be constructed on the Site (the actual building constructed on the Site may vary from this illustration).

- The Petitioner will provide as part of the developments' club house amenity package an indoor basketball half-court.

- The Petitioner will construct as part of the development of the Site three (3) buildings that will include garages as generally depicted on the Rezoning Plan. In addition the Petitioner will construct three (3) detached garage structures throughout the Site as generally depicted on the Rezoning Plan. The location of the detached garage structures indicated on the Rezoning Plan may be modified during the design development and construction document phases.

- Meter banks will be well be screened.

- HVAC and related mechanical equipment will be screened from public view at grade.

- The compactor area and recycling area provided will be enclosed by a solid wall with one side being a decorative gate. The wall used to enclose the compactor will be architecturally compatible with the building materials and colors used on the building. Evergreen landscape materials will also be used to screen and enhance the appearance of the compactor and recycling area.

- Streetscape, Buffers and Landscaping:**
 - A 30 foot setback and tree save area will be provided along Lancaster Highway. Utility lines, roadway improvements, sidewalks and grading for these improvements will be allowed to occur within this tree save area.

- The Petitioner will provide a 27 foot setback along the extension of Landing Place Lane as measured from the back of curb.

- The Petitioner will provide eight (8) foot planting strips and six (6) foot sidewalks along the Site's frontage on Lancaster Highway and on both sides of the extension of Landing Place Lane as generally depicted on the Rezoning Site Plan.

- Along the Site's internal parking areas, the Petitioner will provide a sidewalk and cross-walk network that links the buildings on the Site with one another and links the buildings to the sidewalks along the abutting public streets. The minimum width for this internal sidewalk will be five (5) feet.

- Screening requirements of the Ordinance will be met.

- Above ground backflow preventers will be screened from public view and will be located outside of the required setbacks.

- As indicated on the Rezoning Site Plan a 50 foot vegetated buffer will be provided where the Site abuts existing single-family homes. The outer 30 feet of this buffer will be undisturbed and any existing trees and shrubs in this 30 foot area will be preserved. The inner 20 feet of this buffer will be landscaped with additional shrubs and trees. At a minimum an additional 14 trees and 20 evergreen shrubs per 100 linear feet will be planted within the 50 foot buffer, 35% of the new trees planted will be evergreen trees. At a minimum the combination of existing vegetation, the new vegetation to be planted and the proposed fence when taken together must meet or exceed the requirements of a class B buffer.

- A six (6) foot solid wooden fence will be installed within the 50 foot buffer. The fence will be located at the edge of the undisturbed portion of the buffer as generally depicted on the Rezoning Plan.

- Environmental Features:**
 - The Petitioner shall comply with the Charlotte City Council approved and adopted Post Construction Controls Ordinance.

- The location, size and type of storm water management systems depicted on the Rezoning Plan are subject to review and approval as part of the full development plan submittal and are not implicitly approved with this rezoning. Adjustments may be necessary in order to accommodate actual storm water treatment requirements and natural site discharge points.

- The Site will comply with the Tree Ordinance. Tree save areas are generally depicted on the Rezoning Plan but may be relocated to other portions of the Site during the design development and construction phases.

- Open Space/Tree Save Areas/Greenway Conveyance:**
 - Open space/tree save areas equaling 15% of the Site area will be provided. The open space/tree save areas depicted on the Rezoning Plan may be relocated to other locations on the Site. The area of the Site dedicated to County Parks and Recreation as for a greenway may be used to meet this requirement.

- As generally depicted on the Rezoning Site Plan a playground for small children will be provided.

- The Petitioner will initiate the procedure to convey, to County Parks and Recreation the area generally depicted on the Rezoning Site Plan along Clem's Branch, this area will be conveyed to County Parks and Recreation prior to the issuance of a certificate of occupancy for the last building completed on the south side of Landing Place Lane.

- Signage:**
 - Signage as allowed by the Ordinance will be provided.

- The Petitioner will provide an entry feature/signage for the Providence Pointe neighborhood where Landing Place Lane enters this existing neighborhood. This entry feature/signage must be installed prior to the issuance of a certificate of occupancy for the last building on the Site.

- Lighting:**
 - All new lighting shall be full cut-off type lighting fixtures excluding lower, decorative lighting that may be installed along the driveways, sidewalks, and parking areas.

- Detached lighting on the Site will be limited to 20 feet in height.

- No "wall pak" lighting will be allowed, however architectural lighting on building facades, such as sconces, will be permitted.

- The Petitioner will provide pedestrian scale lighting along the extension of Landing Place Lane.

- Other:**
 - Prior to the decision by the City Council on this Rezoning Petition #2012-048, the Petitioner and the owner of the property covered by Rezoning Petition No. 2000-02(C) will file an Administrative Amendment for Rezoning Petition No. 2000-02(C) that will reduce the allowed number of units on that Petition by 100 units. The Administrative Amendment will indicate that if Petition No. 2012-048 is approved by the City Council the reduction by 100 units in the number of allowed units by Rezoning Petition No. 2000-02(C) will be binding and may not be added back to the Petition through the Administrative Amendment Process. If Petition No. 2012-048 is not approved by the City Council the Administrative Site Plan Amendment will be null and void.

- Amendments to the Rezoning Plan:**
 - Future amendments to the Rezoning Plan (which includes these Development Standards) may be applied for by the then Owner or Owners of the applicable Development Area portion of the Site affected by such amendment in accordance with the provisions of Chapter 6 of the Ordinance.

- Binding Effect of the Rezoning Approval:**
 - If this Rezoning Petition is approved, all conditions applicable to the development of the Site imposed under the Rezoning Plan will, unless amended in the manner provided under the Ordinance, be binding upon and inure to the benefit of the Petitioner and subsequent owners of the Site and their respective heirs, devisees, personal representatives, successors in interest or assigns.

LANCASTER APARTMENTS

CHARLOTTE, NORTH CAROLINA
GCI RESIDENTIAL

5406 ETTA BURKE COURT SUITE 101
RALEIGH, NORTH CAROLINA

CONSTRUCTION DOCUMENTS

GENERAL NOTES:

- SURVEY INFORMATION OBTAINED BY CAROLINA SURVEYORS, DATED 11/07/2012.
- ARCHITECTURAL DIMENSIONS TO BE VERIFIED BY SURVEYOR. REFER TO ARCHITECTURAL PLANS FOR EXACT BUILDING DIMENSIONS.
- SURVEYOR SHALL REPORT ANY ENCROACHMENTS OR DISCREPANCIES GENERATED BY THE SITE PLAN AGAINST ANY SITE DEVELOPMENT REQUIREMENTS SPECIFIED BY THE SITE PLAN TO THE OWNER, LANDSCAPE ARCHITECT OR CIVIL ENGINEER PRIOR TO ANY ACTUAL CONSTRUCTION.
- COORDINATE ALL CURB AND STREET GRADES IN INTERSECTION WITH INSPECTOR.
- ALL ROAD IMPROVEMENTS AT LANCASTER HIGHWAY ARE TO BE COORDINATED WITH THE CITY OF CHARLOTTE ENGINEERING DEPARTMENT AND NCDOT PRIOR TO CONSTRUCTION.
- DEVELOPER WILL PROVIDE STREET SIGNS PER CMLDS# 50.05 (9" SIGNS ONLY)
- SIGHT TRIANGLES SHOWN ARE THE MINIMUM REQUIRED.
- IN ROLLING AND HILLY TERRAINS, SWEEPING OF THE STONE BASE AND/OR APPLICATION OF A TACK COAT MAY BE REQUIRED NEAR INTERSECTIONS. THESE REQUIREMENTS WILL BE ESTABLISHED BY THE INSPECTOR AND BASED ON FIELD CONDITIONS.
- APPROVAL OF THIS PLAN IS NOT AN AUTHORIZATION TO GRADE ADJACENT PROPERTIES. WHEN FIELD CONDITIONS WARRANT OFF-SITE GRADING, PERMISSION MUST BE OBTAINED FROM THE AFFECTED PROPERTY OWNERS.
- IN ORDER TO ENSURE PROPER DRAINAGE, KEEP A MINIMUM OF 0.5% SLOPE ON THE CURB.
- SUBSURFACE DRAINAGE FACILITIES MAY BE REQUIRED IN THE STREET RIGHT-OF-WAY IF DEEMED NECESSARY BY THE INSPECTOR.
- CURB AND GUTTER SHOWN ON PLANS ALONG LANCASTER HIGHWAY AND LANDING PLACE LANE ROAD MAY BE ADJUSTED BASED UPON FIELD STAKING BY CITY ENGINEERING. ASSOCIATED STORM DRAINAGE MAY ALSO REQUIRE MODIFICATION BASED UPON FIELD CONDITIONS.
- THE PURPOSE OF THE STORM DRAINAGE EASEMENT (SDE) IS TO PROVIDE STORM WATER CONVEYANCE AND ANY STRUCTURES AND/OR OBSTRUCTION TO STORM WATER FLOW IS PROHIBITED.
- HIGH-DENSITY POLYETHYLENE (HDPE) STORM DRAINAGE PIPE INSTALLED WITHIN EXISTING OR PROPOSED PUBLIC STREET RIGHT-OF-WAY MUST BE APPROVED BY THE CITY'S INSPECTOR PRIOR TO ANY BACKFILL BEING PLACED. BACKFILL MATERIAL MUST BE APPROVED BY THE CITY INSPECTOR PRIOR TO PLACEMENT OF THE MATERIAL WITHIN THE PUBLIC STREET RIGHT-OF-WAY.
- THE DEVELOPER SHALL MAINTAIN EACH STREAM, CREEK, OR BACKWASH CHANNEL IN AN UNOBSTRUCTED STATE AND SHALL REMOVE FROM THE CHANNEL AND BANKS OF THE STREAM ALL DEBRIS, LOGS, TIMBER, JUNK AND OTHER ACCUMULATIONS.
- PE SEALED SHOP DRAWINGS FOR RETAINING WALL MUST BE SUBMITTED TO CITY ENGINEER PRIOR TO CONSTRUCTION.
- "AS-BUILT" DRAWINGS AND PLANS OF THE STORM DRAINAGE SYSTEM, INCLUDING DESIGNED DITCHES, MUST BE SUBMITTED PRIOR TO SUBDIVISION FINAL INSPECTION TO THE CITY/COUNTY ENGINEERING DEPARTMENT IN ACCORDANCE WITH THE CITY/COUNTY SUBDIVISION ORDINANCE.
- PRIOR TO INSTALLATION, PE SEALED SHOP DRAWINGS FOR UNDERGROUND DETENTION SYSTEMS MUST BE FURNISHED TO CITY OF CHARLOTTE ENGINEERING FOR APPROVAL.
- PRIOR TO CO, SURVEYOR SEALED AS-BUILT DRAWINGS OF UNDERGROUND DETENTION SYSTEMS MUST BE PROVIDED.
- NONSTANDARD ITEMS (IE: PAVERS, IRRIGATION SYSTEMS, ETC.) IN THE RIGHT-OF-WAY REQUIRE A RIGHT-OF-WAY ENCROACHMENT AGREEMENT WITH THE (CHARLOTTE DEPARTMENT OF TRANSPORTATION/NORTH CAROLINA DEPARTMENT OF TRANSPORTATION) BEFORE INSTALLATION.
- PRIOR TO PLAT RECORDEATION, OFFSITE R/W AND/OR CONSTRUCTION EASEMENTS ARE REQUIRED TO BE OBTAINED ACCORDING TO THE GUIDELINES OF THE "OFFSITE R/W ACQUISITION PROCESS". THESE NEEDED R/W AND CONSTRUCTION LIMITS ARE CLEARLY SHOWN ON THE ROADWAY IMPROVEMENT PLAN.
- PIPE SYSTEMS AND/OR CHANNELS LOCATED WITHIN PUBLIC DRAINAGE EASEMENTS ARE THE MAINTENANCE RESPONSIBILITY OF THE INDIVIDUAL PROPERTY OWNER.
- ALL CONSTRUCTION TO CONFORM TO CHARLOTTE / MECKLENBURG LAND DEVELOPMENT STANDARDS.
- FIRE HYDRANT LOCATION SHALL BE REVIEWED AND APPROVED BY THE COUNTY FIRE MARSHALL DURING THE BUILDING PERMIT PROCESS.

SITE ACCESSIBILITY NOTES

- ALL BUILDINGS WITHIN THE BOUNDARY OF THIS SITE, UNLESS OTHERWISE STATED AS EXEMPT, SHALL HAVE "ACCESSIBLE ROUTES" AS REQUIRED THAT CONFORM TO THE REQUIREMENTS OF "THE AMERICANS WITH DISABILITIES ACT", "FAIR HOUSING STANDARDS", ICC ANSI A 117.1-2009 AND GOVERNING STATE HANDICAP CODE STANDARDS.

THESE STANDARDS FOR ACCESSIBILITY INCLUDE, BUT ARE NOT LIMITED TO:

- MAXIMUM WALK SLOPE = 1:20
MAXIMUM RAMP SLOPE = 1:12 WITH RAILING
MAXIMUM CROSS SLOPE = 1/4" PER FOOT (2%)
ALL WALKS TO BE BROOM FINISHED CONCRETE UNLESS OTHERWISE SPECIFIED ON THESE DRAWINGS
MAXIMUM ACCESSIBLE ROUTE LENGTH (NC) = 200'
- THE PAVEMENT SLOPE WITHIN ACCESSIBLE PARKING SPACES SHALL NOT EXCEED 2% IN ALL DIRECTIONS.
- CONTRACTOR TO CONTACT DESIGN RESOURCE GROUP FOR ANY VARYING SITE CONDITIONS OR DISCREPANCIES AFFECTING SITE ACCESSIBILITY REQUIREMENTS.

ADDITIONAL ACCESSIBLE ROUTE NOTES FOR ACCESSIBILITY COMPLIANCE WITH ICC/ANSI A117.1-2009, AND 2010 ADA STANDARDS.

- PROTRUDING OBJECTS ON CIRCULATION PATHS WITHIN COMMON USE AREAS TO COMPLY WITH ANSI 2009 SECTION 307, INCLUDING:
 - PROTRUSION LIMITS: OBJECTS WITH LEADING EDGES MORE THAN 27" AND NOT MORE THAN 80" ABOVE THE FLOOR SHALL PROTRUDE 4" MAXIMUM HORIZONTALLY INTO THE CIRCULATION PATH. EXCEPTIONS INCLUDE HANDRAILS, DOOR CLOSERS, AND DOOR STOPS.
 - POST MOUNTED OBJECTS: OBJECTS ON POSTS OR PYLONS SHALL BE PERMITTED TO OVERHANG 4" MAXIMUM WHERE MORE THAN 27" AND NOT MORE THAN 80" ABOVE THE FLOOR. OBJECTS ON MULTIPLE POSTS OR PYLONS WHERE THE CLEAR DISTANCE BETWEEN THE POSTS OR PYLONS IS GREATER THAN 12" SHALL HAVE THE LOWEST EDGE OF SUCH OBJECT EITHER 27" MAXIMUM OR 80" MINIMUM ABOVE THE FLOOR.
 - REDUCED VERTICAL CLEARANCE: GUARDRAILS OR OTHER BARRIERS SHALL BE PROVIDED WHERE OBJECT PROTRUSION IS BEYOND THE LIMITS ALLOWED BY SECTIONS 307.2 AND 307.3, AND WHERE THE VERTICAL CLEARANCE IS LESS THAN 80" ABOVE THE FLOOR. THE LEADING EDGE OF SUCH GUARDRAIL OR BARRIER SHALL BE 27" MAXIMUM ABOVE THE FLOOR.
- PROTRUDING OBJECTS ON CIRCULATION PATHS WITHIN PUBLIC USE AREAS TO COMPLY WITH 2010 ADA SECTION 307, INCLUDING:
 - 2010 ADA SECTION 307.2: OBJECTS PROJECTING FROM WALLS
 - 2010 ADA SECTION 307.4: MINIMUM CLEAR HEADROOM
- FLOOR AND GROUND SURFACES IN COMMON USE AREAS SHALL BE STABLE, FIRM, AND SLIP RESISTANT AND SHALL COMPLY WITH ANSI 2009 SECTIONS 302.
- FLOOR AND GROUND SURFACES ALONG ACCESSIBLE ROUTES AND IN ACCESSIBLE ROOMS AND SPACES WITH PUBLIC USE AREAS SHALL BE STABLE, FIRM, AND SLIP-RESISTANT AND SHALL COMPLY WITH 2010 ADA SECTION 302.
- CHANGES IN LEVEL IN COMMON USE AREAS SHALL COMPLY WITH ANSI 2009 SECTION 303, INCLUDING:
 - ANSI 2009 SECTION 303.2: VERTICAL
 - ANSI 2009 SECTION 303.3: BEVELED
 - ANSI 2009 SECTION 303.4: RAMPS
- CHANGES IN LEVEL ALONG ACCESSIBLE ROUTES AND IN ACCESSIBLE ROOMS AND SPACES WITH PUBLIC USE AREAS SHALL COMPLY WITH 2010 ADA SECTION 303, INCLUDING:
 - 2010 ADA SECTION 303.2: VERTICAL
 - 2010 ADA SECTION 303.3: BEVELED
 - 2010 ADA SECTION 303.4: RAMPS

SITE DATA

TAX MAP NO:
F-17 MF (CO)
REZONING PETITION NO:
2012-048
SITE AREA:
16.153 AC.
PROPOSED USE:
APARTMENTS FOR RENT
PROPOSED UNITS:
248 UNITS
DENSITY:
15.35 DU/A
PARKING REQUIRED:
268 REGULAR SPACES
PARKING PROVIDED:

223-031-12 & 223-031-98
1.5 SP/UNIT = 372 SPACES
268 REGULAR SPACES
53 COMPACT SPACES (14R)
15 ACCESSIBLE SPACES
3 VAN ACCESSIBLE SPACES
1 GARAGE ACCESSIBLE SPACE
16 ON STREET SPACES
18 TANDEM SPACES
16 DETACHED GARAGE SPACES
18 ATTACHED GARAGE SPACES
408 TOTAL SPACES (1.65 SPACE/UNIT)

BIKE PARKING REQUIRED:

BIKE PARKING PROVIDED:

OPEN SPACE REQUIRED:

OPEN SPACE PROVIDED:

SETBACK:

SIDE YARD:

REAR YARD:

BUILDING SEPARATION

BUILDING HEIGHT (MAXIMUM):

BUILDING HEIGHT PROPOSED:

IMPERVIOUS AREA:

TRASH & RECYCLING PICK UP REQUIRED:

TRASH & RECYCLING PICK UP PROVIDED:

(1) 30 CY. COMPACTOR
576 SQ FT RECYCLING STATIONS

ESTIMATED TIME OF COMPLETION: FALL 2014

ALL UTILITIES WILL BE UNDERGROUND WITH AN EXCEPTION OF BACKFLOW PREVENTORS AND HVAC UNITS WHICH WILL BE SCREENED FROM RIGHT-OF-WAY AND ADJACENT PROPERTIES. CONTACT SOLID WASTE SERVICES ADMINISTRATOR TO SET UP COLLECTION SERVICES WHEN NEEDED. * FIRE DEPARTMENT - SEE NOTES, CALCULATIONS, AND FIRE HYDRANT INFORMATION ON SHEETS C2.0 & C5.0

SHEET SCHEDULE	
C0.0	COVER SHEET
C1.0	SURVEY
C1.1	DEMOLITION PLAN
C2.0	SITE PLAN
C2.1	VEHICLE MANEUVERING PLAN
C2.2	ACCESSIBLE ROUTE PLAN
C3.0	EROSION CONTROL PHASE 1
C3.1	EROSION CONTROL PHASE 2
C4.0	GRADING PLAN - SOUTHWEST
C4.1	GRADING PLAN - SOUTHEAST
C4.2	GRADING PLAN - NORTH
C4.3	STORM DRAINAGE PLAN
C4.4	STORM DRAINAGE AREA PLAN
C4.5	STORMWATER BMP DETAILS
C4.6	STORMWATER MANAGEMENT PLAN
C5.0	WATER AND SEWER PLAN
C5.1	PRIVATE SEWER PLAN AND PROFILE
C5.2	PRIVATE SEWER PLAN AND PROFILE
C6.0	REQUIRED PLANTING PLAN
C7.0	LANCASTER HIGHWAY PLAN AND PROFILE
C7.1	PAVEMENT MARKING PLAN
C7.2	LANDING PLACE LANE PLAN & PROFILE
C7.3	GREYTHORNE DRIVE PLAN & PROFILE
C7.4	ISD PLAN/PROFILE - LANCASTER HIGHWAY
C7.5	ISD PLAN/PROFILE - LANDING PLACE LANE
C7.6	ISD PLAN/PROFILE - LANDING PLACE LANE
C7.7	LANCASTER HWY. CROSS-SECTIONS
C7.8	LANCASTER HWY. CROSS-SECTIONS
C7.9	LANCASTER HWY. CROSS-SECTIONS
C7.10	LANCASTER HWY. CROSS-SECTIONS
C7.11	LANCASTER HWY. CROSS-SECTIONS
C7.12	LANCASTER HWY. CROSS-SECTIONS
C7.13	LANCASTER HWY. CROSS-SECTIONS
C7.14	LANCASTER HWY. CROSS-SECTIONS
C7.15	LANCASTER HWY. CROSS-SECTIONS
C7.16	LANCASTER HWY. CROSS-SECTIONS
C7.17	LANDING CONTROL PLAN PHASE I
C7.18	TRAFFIC CONTROL PLAN PHASE II
C8.0	SITE DETAILS
C8.1	SITE DETAILS

CONSULTANTS:

DEVELOPER: GCI RESIDENTIAL
5406 ETTA BURKE COURT
SUITE 101
RALEIGH, NORTH CAROLINA 27606
919.851.0661

ARCHITECT: CLINE DESIGN
1500 CAMDEN ROAD
SUITE 108
CHARLOTTE, NC 28203
704.333.7272

LANDSCAPE ARCH.: DESIGN RESOURCE GROUP, PA
2459 WILKINSON BOULEVARD
SUITE 200
CHARLOTTE, NC 28208
704.343.0608

CIVIL ENGINEER: DESIGN RESOURCE GROUP, PA
2459 WILKINSON BOULEVARD
SUITE 200
CHARLOTTE, NC 28208
704.343.0608

SURVEYOR: CAROLINA SURVEYORS, INC
P.O. BOX 267
PINEVILLE, NC 28134
704.899.7601

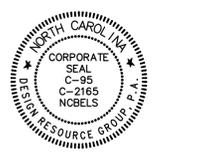
C8.2	SITE DETAILS
C8.3	SITE DETAILS
C8.4	SITE DETAILS
C9.0	EROSION CONTROL DETAILS
C9.1	EROSION & STORM DETAILS
C9.2	STORM & UTILITY DETAILS
C9.3	UTILITY DETAILS
RZ1.0	REZONING PLAN - TECHNICAL DATA SHEET
RZ2.0	REZONING PLAN - SCHEMATIC SITE PLAN
AS.01	REZONING PLAN - BUILDING ELEVATION
AS.02	REZONING PLAN - BUILDING ELEVATION
AS.03	REZONING PLAN - BUILDING ELEVATION
A2.01	ARCH. ELEVATION
A2.02	ARCH. ELEVATION
A2.03	ARCH. ELEVATION



design resource group

- landscape architecture
- civil engineering
- urban design
- land planning
- traffic engineering
- transportation planning

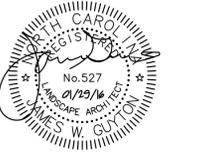
2459 wilkinson boulevard, suite 200
 charlotte, nc 28208
 p 704.343.0608 f 704.358.3093
 www.drgrp.com



LANCASTER APARTMENTS
 CHARLOTTE, NORTH CAROLINA

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 5406 ETTA BURKE COURT, SUITE 101
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 919.851.0661

CONSTRUCTION DOCUMENTS



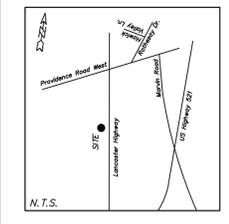
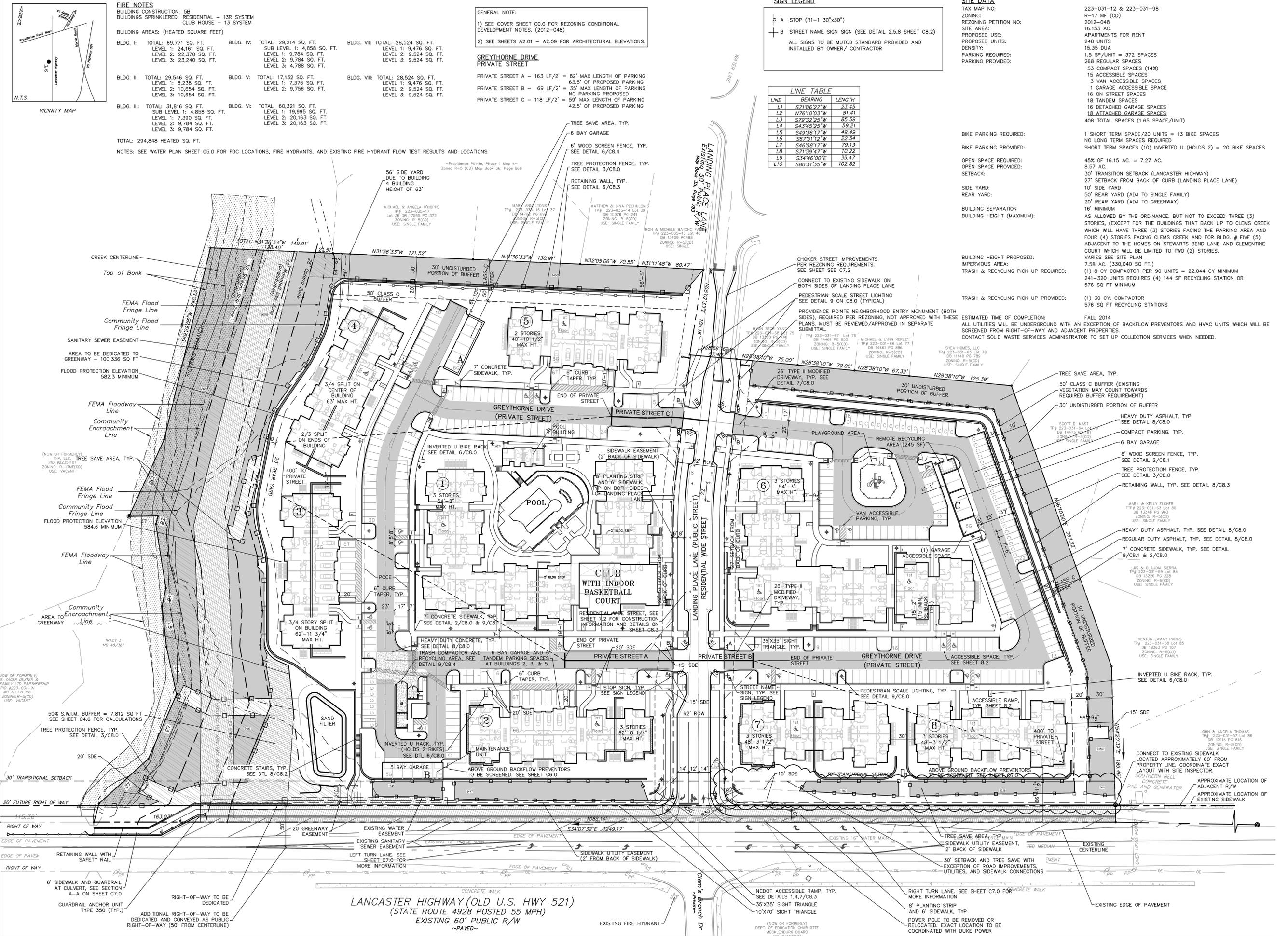
SCALE: 1"=50'

PROJECT #: 253-008
 DRAWN BY: CC
 CHECKED BY: SK

SITE PLAN

- JULY 23, 2013
- REVISIONS:
- 10/01/13 - PER CITY REVIEW
 - 11/15/13 - PER CITY REVIEW COMMENT
 - 01/03/14 - REVISED STREET NAME
 - 06/23/14 - EXCEPT TRASH COMPACTOR AND DUMPISTER PAD
 - 01/29/16 - RTAP

C2.0



FIRE NOTES
 BUILDING CONSTRUCTION: 5B
 BUILDINGS SPRINKLERED: RESIDENTIAL - 13R SYSTEM
 CLUB HOUSE - 13 SYSTEM

BUILDING AREAS: (HEATED SQUARE FEET)

BLDG. I: TOTAL: 69,771 SQ. FT. LEVEL 1: 24,161 SQ. FT. LEVEL 2: 22,370 SQ. FT. LEVEL 3: 23,240 SQ. FT.	BLDG. IV: TOTAL: 29,214 SQ. FT. LEVEL 1: 4,858 SQ. FT. LEVEL 2: 9,784 SQ. FT. LEVEL 3: 4,788 SQ. FT.	BLDG. VII: TOTAL: 28,524 SQ. FT. LEVEL 1: 9,476 SQ. FT. LEVEL 2: 9,524 SQ. FT. LEVEL 3: 9,524 SQ. FT.
BLDG. II: TOTAL: 29,546 SQ. FT. LEVEL 1: 8,238 SQ. FT. LEVEL 2: 10,654 SQ. FT. LEVEL 3: 10,654 SQ. FT.	BLDG. V: TOTAL: 17,132 SQ. FT. LEVEL 1: 7,376 SQ. FT. LEVEL 2: 9,756 SQ. FT.	BLDG. VIII: TOTAL: 28,524 SQ. FT. LEVEL 1: 9,476 SQ. FT. LEVEL 2: 9,524 SQ. FT. LEVEL 3: 9,524 SQ. FT.
BLDG. III: TOTAL: 31,816 SQ. FT. SUB LEVEL 1: 4,858 SQ. FT. LEVEL 1: 7,390 SQ. FT. LEVEL 2: 9,784 SQ. FT. LEVEL 3: 9,784 SQ. FT.	BLDG. VI: TOTAL: 60,321 SQ. FT. LEVEL 1: 19,995 SQ. FT. LEVEL 2: 20,163 SQ. FT. LEVEL 3: 20,163 SQ. FT.	

TOTAL: 294,848 HEATED SQ. FT.

NOTES: SEE WATER PLAN SHEET C5.0 FOR FDC LOCATIONS, FIRE HYDRANTS, AND EXISTING FIRE HYDRANT FLOW TEST RESULTS AND LOCATIONS.

GENERAL NOTE:

- 1) SEE COVER SHEET C0.0 FOR REZONING CONDITIONAL DEVELOPMENT NOTES. (2012-048)
- 2) SEE SHEETS A2.01 - A2.09 FOR ARCHITECTURAL ELEVATIONS.

GREYTHORNE DRIVE PRIVATE STREET

PRIVATE STREET A - 163 LF/2' = 82' MAX LENGTH OF PARKING 63.5' OF PROPOSED PARKING
 PRIVATE STREET B - 69 LF/2' = 35' MAX LENGTH OF PARKING NO PARKING PROPOSED
 PRIVATE STREET C - 118 LF/2' = 59' MAX LENGTH OF PARKING 42.5' OF PROPOSED PARKING

SIGN LEGEND

A	STOP (R1-1 30"x30")
B	STREET NAME SIGN (SEE DETAIL 2,5,8 SHEET C8.2)

ALL SIGNS TO BE MUTCD STANDARD PROVIDED AND INSTALLED BY OWNER/ CONTRACTOR

LINE TABLE

LINE	BEARING	LENGTH
L1	S71°36'27"W	23.45
L2	N76°10'03"W	81.41
L3	S79°32'25"W	85.59
L4	S43°45'25"W	59.21
L5	S49°36'17"W	49.49
L6	S67°51'12"W	22.54
L7	S46°58'17"W	79.13
L8	S71°39'47"W	10.22
L9	S34°46'00"E	35.47
L10	S80°31'35"W	102.82

SITE DATA

TAX MAP NO: 223-031-12 & 223-031-98
 ZONING: R-17 MF (CD)
 REZONING PETITION NO: 2012-048
 SITE AREA: 16,153 AC.
 PROPOSED UNITS: 248 UNITS
 DENSITY: 15.35 DUA
 1.5 SP/UNIT = 372 SPACES
 268 REGULAR SPACES
 53 COMPACT SPACES (14%)
 15 ACCESSIBLE SPACES
 3 VAN ACCESSIBLE SPACES
 1 GARAGE ACCESSIBLE SPACE
 16 ON STREET SPACES
 18 TANDEM SPACES
 16 DETACHED GARAGE SPACES
 18 ATTACHED GARAGE SPACES
 408 TOTAL SPACES (1.65 SPACE/UNIT)

BIKE PARKING REQUIRED: 1 SHORT TERM SPACE/20 UNITS = 13 BIKE SPACES
 NO LONG TERM SPACES REQUIRED
 SHORT TERM SPACES (10) INVERTED U (HOLDS 2) = 20 BIKE SPACES

BIKE PARKING PROVIDED: 45% OF 16.15 AC. = 7.27 AC.
 8.57 AC.
 30' TRANSITION SETBACK (LANCASTER HIGHWAY)
 27' SETBACK FROM BACK OF CURB (LANDING PLACE LANE)
 10' SIDE YARD
 50' REAR YARD (ADJ TO SINGLE FAMILY)
 20' REAR YARD (ADJ TO GREENWAY)
 16' MINIMUM

AS ALLOWED BY THE ORDINANCE, BUT NOT TO EXCEED THREE (3) STORIES, (EXCEPT FOR THE BUILDINGS THAT BACK UP TO CLEMS CREEK WHICH WILL HAVE THREE (3) STORIES FACING THE PARKING AREA AND FOUR (4) STORIES FACING CLEMS CREEK AND FOR BLDG. # FIVE (5) ADJACENT TO THE HOMES ON STEWARTS BEND LANE AND CLEMENTINE COURT WHICH WILL BE LIMITED TO TWO (2) STORIES.

BUILDING HEIGHT PROPOSED: 7.58 AC. (330,040 SQ. FT.)
 IMPERVIOUS AREA: 241-320 UNITS REQUIRES (4) 144 SF RECYCLING STATION OR 576 SQ FT MINIMUM

TRASH & RECYCLING PICK UP PROVIDED: (1) 30 CY. COMPACTOR
 576 SQ FT RECYCLING STATIONS

ESTIMATED TIME OF COMPLETION: FALL 2014
 ALL UTILITIES WILL BE UNDERGROUND WITH AN EXCEPTION OF BACKFLOW PREVENTORS AND HVAC UNITS WHICH WILL BE SCREENED FROM VIEW OF ADJACENT PROPERTIES.
 CONTACT SOLID WASTE SERVICES ADMINISTRATOR TO SET UP COLLECTION SERVICES WHEN NEEDED.