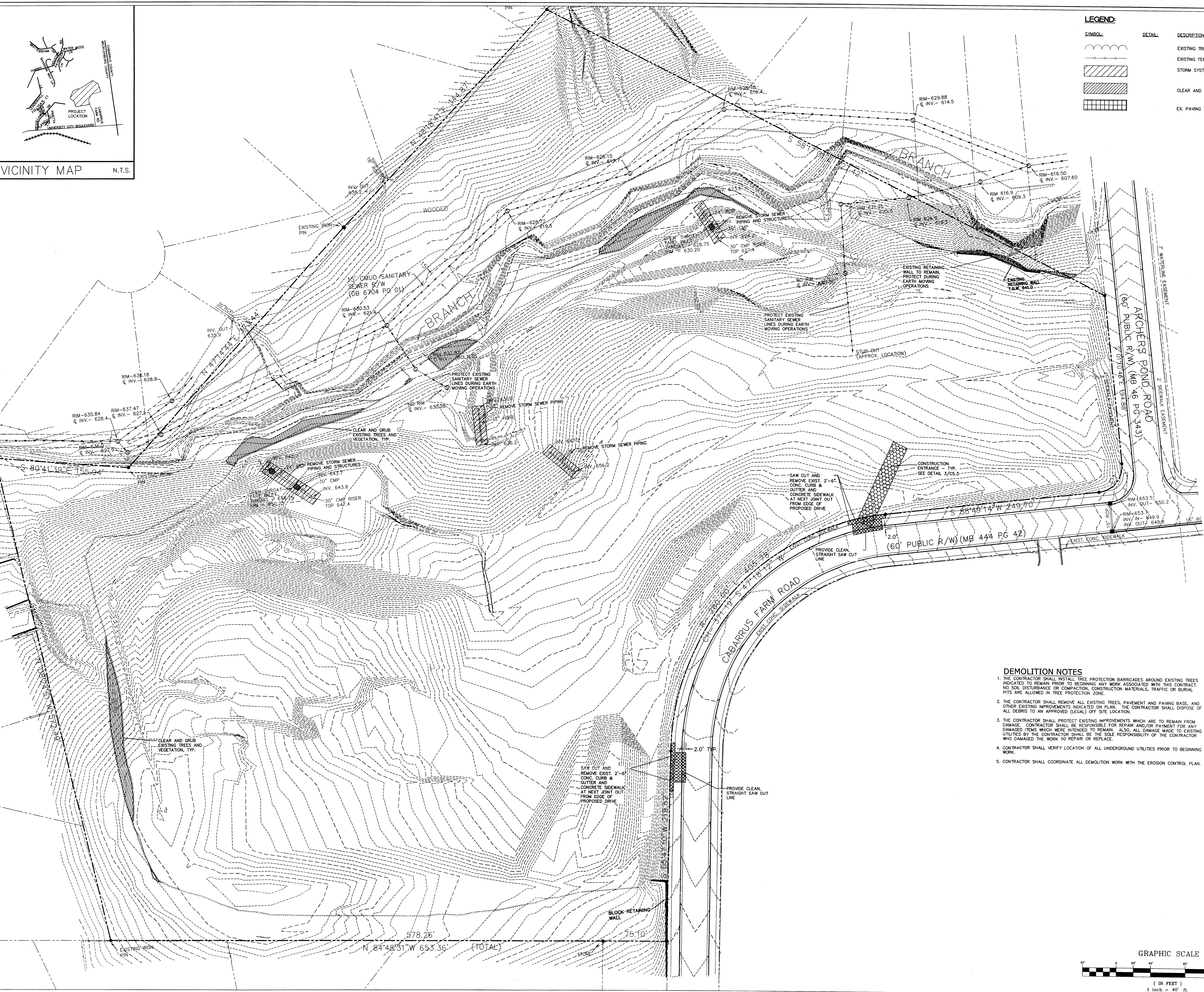


VICINITY MAP N.T.S.

LEGEND:

SYMBOL:	DETAIL:	DESCRIPTION:
		EXISTING TREE LINE
		STORM SYSTEM DEMOLITION
		CLEAR AND GRUB EX. TREES/VEGETATION
		EX. PAVING TO BE REMOVED



DEMOLITION NOTES

1. THE CONTRACTOR SHALL INSTALL TREE PROTECTION BARRICADES AROUND EXISTING TREES INDICATED TO REMAIN PRIOR TO BEGINNING ANY WORK ASSOCIATED WITH THIS CONTRACT. NO SOIL DISTURBANCE OR COMPACTION, CONSTRUCTION MATERIALS, TRAFFIC OR BURIAL PITS ARE ALLOWED IN TREE PROTECTION ZONE.
2. THE CONTRACTOR SHALL REMOVE ALL EXISTING TREES, PAVEMENT AND PAVING BASE, AND OTHER EXISTING IMPROVEMENTS INDICATED ON PLAN. THE CONTRACTOR SHALL DISPOSE OF ALL DEBRIS TO AN APPROVED (LEGAL) OFF SITE LOCATION.
3. THE CONTRACTOR SHALL PROTECT EXISTING IMPROVEMENTS WHICH ARE TO REMAIN FROM DAMAGE. CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR AND/OR PAYMENT FOR ANY DAMAGED ITEMS WHICH WERE INTENDED TO REMAIN. ALSO, ALL DAMAGE MADE TO EXISTING UTILITIES BY THE CONTRACTOR SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR WHO DAMAGED THE WORK TO REPAIR OR REPLACE.
4. CONTRACTOR SHALL VERIFY LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING WORK.
5. CONTRACTOR SHALL COORDINATE ALL DEMOLITION WORK WITH THE EROSION CONTROL PLAN.

PRELIMINARY PLANNED
MULTI-FAMILY SUBDIVISION
APPROVED
Charlotte-Mecklenburg Planning & Zoning Commission
7/11/03

THE VILLAGE AT BRIERFIELD TOWNHOMES
UNIVERSITY CITY BLVD (NC 49)
CHARLOTTE, NORTH CAROLINA

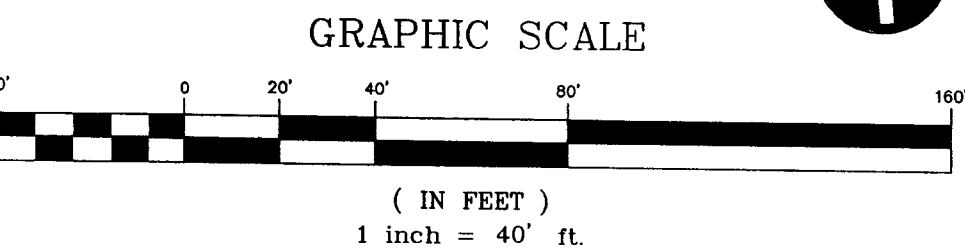
DEMOLITION
PLAN

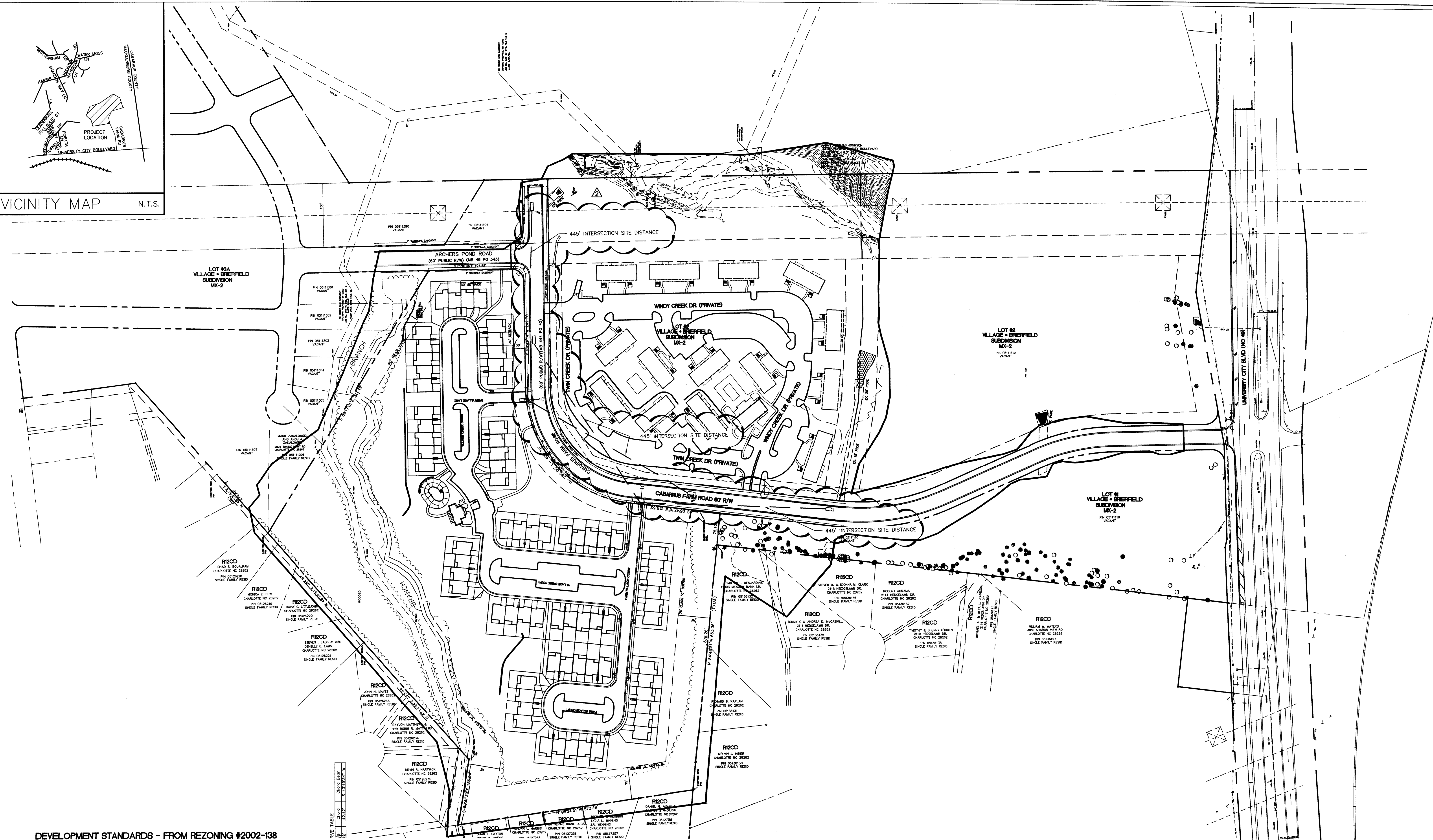
DATE: 04.04.08

SCALE: 1" = 40'

JOB NO. X7076.00

SHEET: C1.1





VICINITY MAP N.T.S.

DEVELOPMENT STANDARDS - FROM REZONING #2002-138

- A. PERMITTED USES
- NOT MORE THAN 440 ATTACHED MULTI-FAMILY UNITS MAY BE CONSTRUCTED WITHIN THE AREAS GENERALLY DEPICTED ON THIS TECHNICAL DATA SHEET AND SCHEMATICALLY REPRESENTED ON SHEET RZ-2 ON PARCELS A & B. ADDITIONALLY, ANY INCIDENTAL OR ACCESSORY USES PERMITTED BY RIGHT OR UNDER PRESCRIBED CONDITIONS WITHIN THE MX-2 ZONING DISTRICT SHALL BE ALLOWED.
- NOT MORE THAN 72 TOWNHOMES UNITS MAY BE CONSTRUCTED WITHIN THE AREA GENERALLY DEPICTED ON THIS TECHNICAL DATA SHEET AND SCHEMATICALLY REPRESENTED ON SHEET RZ-2 AS PARCEL C. ADDITIONALLY, ANY INCIDENTAL OR ACCESSORY USES PERMITTED BY RIGHT OR UNDER PRESCRIBED CONDITIONS WITHIN THE MX-2 ZONING DISTRICT SHALL BE ALLOWED.
- NOT MORE THAN 95 SINGLE FAMILY DETACHED DWELLING UNITS MAY BE CONSTRUCTED WITHIN THE AREA GENERALLY DEPICTED ON THIS TECHNICAL DATA SHEET AND SCHEMATICALLY REPRESENTED ON SHEET RZ-2 AS PARCEL D. ADDITIONALLY, ANY INCIDENTAL OR ACCESSORY USES PERMITTED BY RIGHT UNDER THE PRESCRIBED CONDITIONS WITHIN THE MX-2 ZONING DISTRICT SHALL BE ALLOWED.
- PARCEL A SHALL CONSIST OF THE FOLLOWING USES:
- 10,000 SF OFFICE
 - 10,000 SF INSTITUTIONAL/DAYCARE
 - 250 MULTI-FAMILY UNITS
- B. DEVELOPMENT REQUIREMENTS
1. GENERAL PROVISIONS
- (a) AT A MINIMUM, ALL APPLICABLE DEVELOPMENT STANDARDS ESTABLISHED UNDER THE ORDINANCE FOR THE MX-2 ZONING DISTRICT WILL BE SATISFIED IN CONNECTION WITH THE DEVELOPMENT TAKING PLACE ON THIS PROPERTY.
- (b) IN ADDITION, DEVELOPMENT OF THE SITE SHALL BE GOVERNED BY THESE DEVELOPMENT STANDARDS AND THE TECHNICAL DATA SHEET (DC-1).
- (c) THE BUILDING CONFIGURATIONS, PLACEMENTS AND SIZES AS WELL AS THE LOCATIONS OF ALL DEVELOPMENT SHOWN ON THE SCHEMATIC SITE PLAN (SHEET RZ-2) ARE SCHEMATIC IN NATURE AND MAY BE ALTERED OR MODIFIED DURING DESIGN DEVELOPMENT AND CONSTRUCTION DOCUMENT PHASES WITHIN THE MAXIMUM DEVELOPMENT LINES ESTABLISHED ON THE TECHNICAL DATA SHEET. FURTHERMORE, THE ACRESAGES FOR THE DAY CARE AND OFFICE USE PARCELS MAY BE ADJUSTED TO ACCOMMODATE WITH FINAL DEVELOPMENT PLANS AND TENANT NEEDS.
- (d) LANDSCAPED AREAS AND SIDEWALKS WITHIN THE SITE WILL BE PLANNED AND INSTALLED IN SEQUENCES WHICH ARE NEEDED TO THE DEVELOPMENT TAKING PLACE ON THE SITE.
- (e) THE HEIGHT OF THE PROPOSED MULTI-FAMILY BUILDINGS SHALL BE A MAXIMUM OF 3 STORIES AND 45 FEET IN HEIGHT.
- (f) THE OFFICE COMPONENT ASSOCIATED WITH PARCEL A SHALL BE LIMITED TO A MAXIMUM OF 10,000 SF. THE SPACE MAY BE ARRANGED AS DEPICTED OR THE BUILDINGS MAY BE COMBINED TO CREATE FEWER BUILDINGS, BUT IN NO EVENT SHALL THE 10,000 SF BE EXCEEDED. THE BUILDING HEIGHT SHALL BE A MAXIMUM OF 2 STORIES AND THE OFFICE BUILDINGS SHALL BE DESIGNED TO INCLUDE A PITCHED RESIDENTIAL STYLE ROOF.
2. SETBACKS, SIDE YARDS AND REAR YARDS
- (a) THE DEVELOPMENT OF THIS PROPERTY SHALL SATISFY THE BUFFER AND SETBACKS ESTABLISHED ON THIS TECHNICAL DATA SHEET AND THE STANDARDS ESTABLISHED UNDER PARAGRAPHS 2(b), (c), (d) AND (e) BELOW.
- (b) A 50 FOOT BUILDING AND PARKING SETBACK SHALL BE ESTABLISHED ALONG N.C. HIGHWAY 49. ALL EXISTING TREES EIGHT INCHES IN CALIPER AND GREATER SHALL BE PRESERVED EXCEPT IN AREAS WHICH FALL WITHIN THE ALLOUMENTS FOR UTILITY CONNECTIONS AND ACCESS POINTS TO THE SITE. IN AREAS WITHIN THE 50 FOOT SETBACK WHERE THERE ARE NO TREES EIGHT INCHES IN CALIPER OR GREATER, THE PETITIONER RESERVES THE RIGHT TO GRADE AND ADD NEW LANDSCAPING OR SETBACKS.
- (c) ALL DETACHED SINGLE FAMILY LOTS ASSOCIATED WITH PARCEL D WHICH BACK UP TO THE WESTERN PROPERTY LINE OF THIS SITE SHALL BE DEVELOPED IN ACCORDANCE WITH REGULATIONS APPLICABLE TO THE R-3 ZONING DISTRICT UNDER THE ORDINANCE, EXCEPT THAT THE REAR YARD FOR EACH LOT ASSOCIATED WITH PARCEL D MUST BE AT LEAST 55 FEET AND THE EXTERIOR 40 FEET OF THIS REAR YARD MAY NOT BE DISTURBED EXCEPT FOR THE REMOVAL BY HAND OF VINES, UNDERBRUSH AND TREES OF 1 1/2 INCHES OR LESS IN CALIPER SO THAT THE EXISTING VEGETATION IS LEFT IN ITS NATURAL CONDITION. DEED RESTRICTIONS SHALL BE IMPOSED BY THE DEVELOPER FOR EACH SUCH LOT WHICH PROHIBIT THE REMOVAL OF ANY VEGETATION FROM THE REAR 40 FEET OF THE LOT EXCEPT THE REMOVAL BY HAND OF VINES, UNDERBRUSH TREES OF 1 1/2 INCHES OR LESS IN CALIPER FOR THE PURPOSE OF IMPROVING THE OVERALL APPEARANCE OF NATURALLY VEGETATED AREAS.
- (d) THE REMAINDER OF THE DETACHED SINGLE FAMILY DEVELOPMENT ASSOCIATED WITH PARCEL D SHALL BE DEVELOPED TO A COMBINATION OF R-4, R-5, AND R-6 STANDARDS. (SEE RZ-2 FOR SPECIFIC LAYOUT)
- (e) ALL REAR YARDS DESCRIBED UNDER PARAGRAPH 2(c) ABOVE AND BUFFER AREAS SHALL BE CLEARLY MARKED AND PROTECTED PRIOR TO ANY LAND-DISTURBING ACTIVITY.
- (f) A 75 FOOT CLASS C BUFFER SHALL SEPARATE THE PROPOSED TOWNHOMES ON PARCEL C THAT ABUTT THE EXISTING SINGLE FAMILY SUBDIVISION ALONG THE WESTERN PROPERTY LINE.
- (g) THERE ARE NO EXISTING TREES OVER 8" CALIPER LOCATED IN THE FRONT 50 FOOT SETBACK ADJACENT TO NC HWY. 49.

3. BUFFER AREAS
- (a) BUFFER AREAS ESTABLISHED ON THIS TECHNICAL DATA SHEET SHALL CONFORM TO THE STANDARDS OF SECTION 12.302 OF THE ORDINANCE, SUBJECT, HOWEVER, TO THE PROVISIONS OF SECTION 12.304 THEREOF.
- (b) BUFFER AREAS SHALL REMAIN AS OPEN SPACE AND, SUBJECT TO THE PROVISIONS OF PARAGRAPHS (3) AND (4) BELOW, WILL REMAIN UNDISTURBED.
- (c) THE PETITIONER RESERVES THE RIGHT TO CLEAR, GRADE AND FILL WITHIN THE FIRST 20 INTERIOR FEET OF THE 75' CLASS "C" BUFFER AREAS AS ESTABLISHED ON THIS TECHNICAL DATA SHEET AND GRADE THE INTERIOR FEET OF CLASS "C" BUFFER THAT RUNS PARALLEL TO THE DUKE POWER R.O.W.
- (d) THE PETITIONER RESERVES THE RIGHT TO INSTALL UTILITIES AND PEDESTRIAN PATHS WITHIN BUFFER AREAS. HOWEVER, UTILITY INSTALLATIONS MAY ONLY CROSS BUFFER AREAS AT ANGLES MEASURED AT THE PROPERTY LINE WHICH ARE NOT LESS THAN 75 DEGREES.
- (e) NO BUILDINGS, PARKING SPACES, MANUEVERING AREAS OR STORM WATER DETENTION FACILITIES MAY BE PLACED WITHIN THE BUFFER AREAS.
- (f) WHERE EXISTING TREES AND NATURAL VEGETATION HAVE BEEN CLEARED TO ACCOMMODATE PEDESTRIAN SIDEWALKS OR PATHS OR THE INSTALLATION OF UTILITIES, THE CLEARED UNIMPROVED AREAS SHALL BE LANDSCAPED WITH TREES AND SHRUBS.
4. LANDSCAPED AREAS AND SCREENING: SCREENING SHALL CONFORM WITH THE STANDARDS AND TREATMENTS SPECIFIED IN SECTION 12.303 OF THE ORDINANCE.
5. ACCESS POINTS: VEHICULAR ACCESS TO THE SITE FROM NC HWY. 49 WILL BE LIMITED TO THE SINGULAR ACCESS POINT SHOWN ON THE TECHNICAL DATA SHEET AND WILL BE LOCATED IN THE GENERAL AREA DEPICTED. THE CONFIGURATION AND ULTIMATE LOCATION OF THE ACCESS POINTS WITHIN (E INTERNAL TO) THE SITE ARE SUBJECT TO ANY MINOR MODIFICATIONS REQUIRED TO ACCOMMODATE FINAL SITE AND ARCHITECTURAL CONSTRUCTION PLANS AND SCREENING. ALL DRIVEWAYS AND NEW PUBLIC STREETS SHALL BE SUBJECT TO APPROVAL BY DOT.
6. LIGHTING
- (a) A UNIFORM LIGHTING SYSTEM WILL BE EMPLOYED THROUGHOUT THE SITE.
- (b) ALL LIGHTING WITHIN THE SITE, EXCEPT STREET LIGHTS WHICH MAY BE ERCTED ALONG N.C. HIGHWAY 49 OR ALONG ANY OTHER PUBLIC STREET, SHALL BE DESIGNED SUCH THAT DIRECT ILLUMINATION DOES NOT EXTEND PAST INTERIOR PROPERTY LINES OF THE SITE.
- (c) ITEMS FOR CONSIDERATION WILL INCLUDE INTENSITY, CUT-OFF ANGLES, COLOR, ENERGY EFFICIENCY AND SHIELDING OF SOURCES OF LIGHT; THE INTENT BEING TO ELIMINATE GLARE TOWARDS ADJACENT STREETS AND PROPERTIES AND SHIELDING OF SOURCES OF LIGHT.
- (d) LIGHTING FIXTURES WILL NOT EXCEED 20 FEET IN HEIGHT, EXCEPT FOR STREET LIGHTS ALONG PUBLIC STREETS.
- (e) "MILK PANK" TYPE LIGHTING SHALL BE PROHIBITED.
7. PARKING:
- (a) THE PARKING SPACES DEPICTED ON THE SCHEMATIC SITE PLAN (SHEET RZ-2) MAY VARY IN LAYOUT AND LOCATION BUT, IN ALL EVENTS WILL BE SUFFICIENT TO SATISFY THE MINIMUM OFF-STREET PARKING STANDARDS ESTABLISHED UNDER THE ORDINANCE, INCLUDING SECTIONS 12.201(1) AND 12.303(2) OF THE ORDINANCE.
- (b) NO PARKING WILL BE PERMITTED WITHIN BUFFER AREAS OR SETBACK AREAS.
8. SIGNS:
- (a) A UNIFORM SIGNAGE AND GRAPHIC SYSTEM WILL BE EMPLOYED THROUGHOUT THE SITE WHICH WILL COMPLY WITH THE PROVISIONS OF SECTION 13 OF THE ORDINANCE.
- (b) ANY DETACHED IDENTIFICATION SIGNS FOR THE RETAIL/OFFICE AND INSTITUTIONAL/DAY CARE USES SHALL BE LIMITED TO A MAXIMUM HEIGHT OF 7 FEET AND A MAXIMUM SIGN AREA OF 50 SF.
- (c) DEVELOPER WILL PROVIDE STREET SIGNS PER CLOSING#003 (9" SIGNS ONLY)
9. FIRE PROTECTION: ADEQUATE FIRE PROTECTION IN THE FORM OF FIRE HYDRANTS WILL BE PROVIDED TO THE MECKLENBURG COUNTY FIRE MARSHAL'S SPECIFICATIONS.
10. DESIGN STANDARDS:
- (a) ALL PARKING WILL BE SCREENED FROM PUBLIC STREETS AND ABUTTING AND ADJOINING PROPERTIES IN ACCORDANCE WITH SECTION 12.303 OF THE ORDINANCE.
- (b) THE MULTI-FAMILY AND TOWNHOME COMPONENTS OF THE PROJECT SHALL BE DEVELOPED IN ACCORDANCE WITH SECTION 9.303 (19) OF THE ORDINANCE.
- (c) DUMPER AREAS WILL BE SCREENED IN ACCORDANCE WITH THE ORDINANCE BY BRICK WALLS OR FENCES NOT LESS THAN 6 FEET HIGH, OR BY A COMBINATION OF BRICK WALLS OR FENCES NOT LESS THAN 6 FEET HIGH AND A 75 FOOT CLASS C BUFFER.
- (d) THE PETITIONER/DEVELOPER SHALL COMPLY WITH A PROPOSED BICYCLE PARKING ORDINANCE (ANTICIPATED FOR ADOPTION IN MID 2003) AND AGREES TO PROVIDE BICYCLE PARKING IN ACCORDANCE WITH SAID ORDINANCE FOR ANY USE REQUESTING SUCH RESULTS REGARDLESS OF THE DATE OF THE ADOPTION OF THE ORDINANCE AND THE APPROVAL OF THE PROPOSED SITE PLAN AMENDMENT HEREIN.
- (e) AN INTERNAL RESTRICTION CONDITION SHALL CONVEY THE MULTI-FAMILY AREAS ON PARCELS A AND B AND BETWEEN THE MULTI-FAMILY AREA AND RETAIL AREA ON PARCEL A.

SITE DEVELOPMENT DATA

TOTAL SITE ACREAGE:	79.21
TOTAL SITE ACREAGE THIS PHASE:	18.44
TOTAL SITE ACREAGE TO DATE:	60.77
TOTAL # UNITS THIS PHASE:	72
DENSITY: # UNITS/ACRE THIS PHASE:	3.9
TOTAL # UNITS TO DATE:	258 (72 THIS PHASE, PARCEL 'C'; 186 VILLAGE AT BRIERFIELD PHASE II, PARCEL 'B')
DENSITY: # UNITS/ACRE TO DATE:	5.35
TOTAL # UNITS / PARCEL:	
PARCEL 'A'	OFFICE 10,000 SF
DAYCARE 10,000 SF	
MULTI-FAMILY 254 UNITS	
PARCEL 'B'	MULTI FAMILY 186 UNITS
PARCEL 'C'	TOWNHOMES 72 UNITS
PARCEL 'D'	SINGLE FAMILY 95 UNITS
OPEN SPACE THIS PHASE:	9.22+ ACRES (50%)
TOTAL OPEN SPACE TO DATE:	29.78 ACRES (61.7%)

200 S. COLLEGE ST.
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CHARLOTTE, NC 28202
F 704.334.7926
www.stewart-eng.com

STEWART

06-19-08

THE VILLAGE AT BRIERFIELD TOWNHOMES
UNIVERSITY CITY BLVD (NC 49)
CHARLOTTE, NORTH CAROLINA

OVERALL SITE PLAN

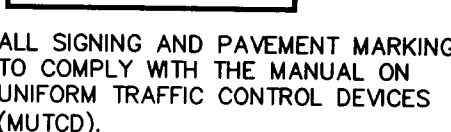
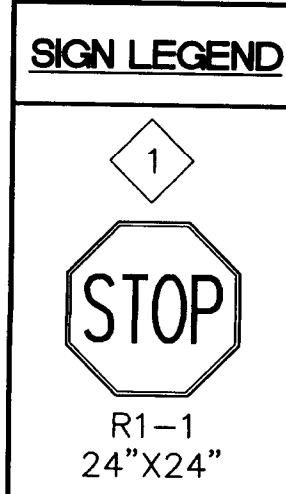
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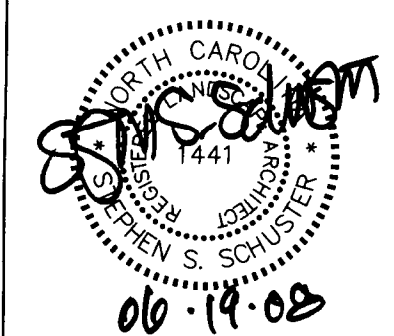
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SHEET: **C2.0**

NO.	DATE	DESCRIPTION
2	05/09/08	ADDED SIGHT DISTANCES PER PLANNING REVIEW
1	07/09/08	REVISED SITE PLAN AND UPDATED SURVEY PER MULTI-FAMILY REVIEW

[illegible]

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THE VILLAGE AT BRIERFIELD TOWNHOMES
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CHARLOTTE, NORTH CAROLINA

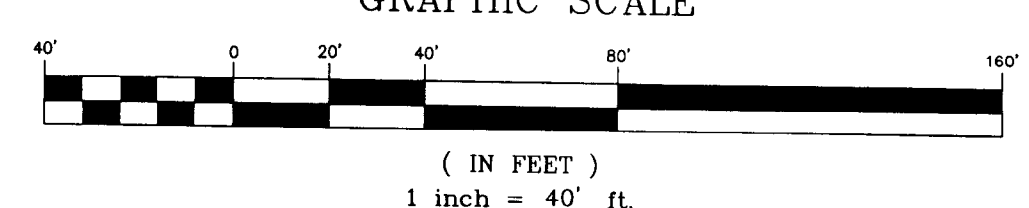
SITE
PLAN

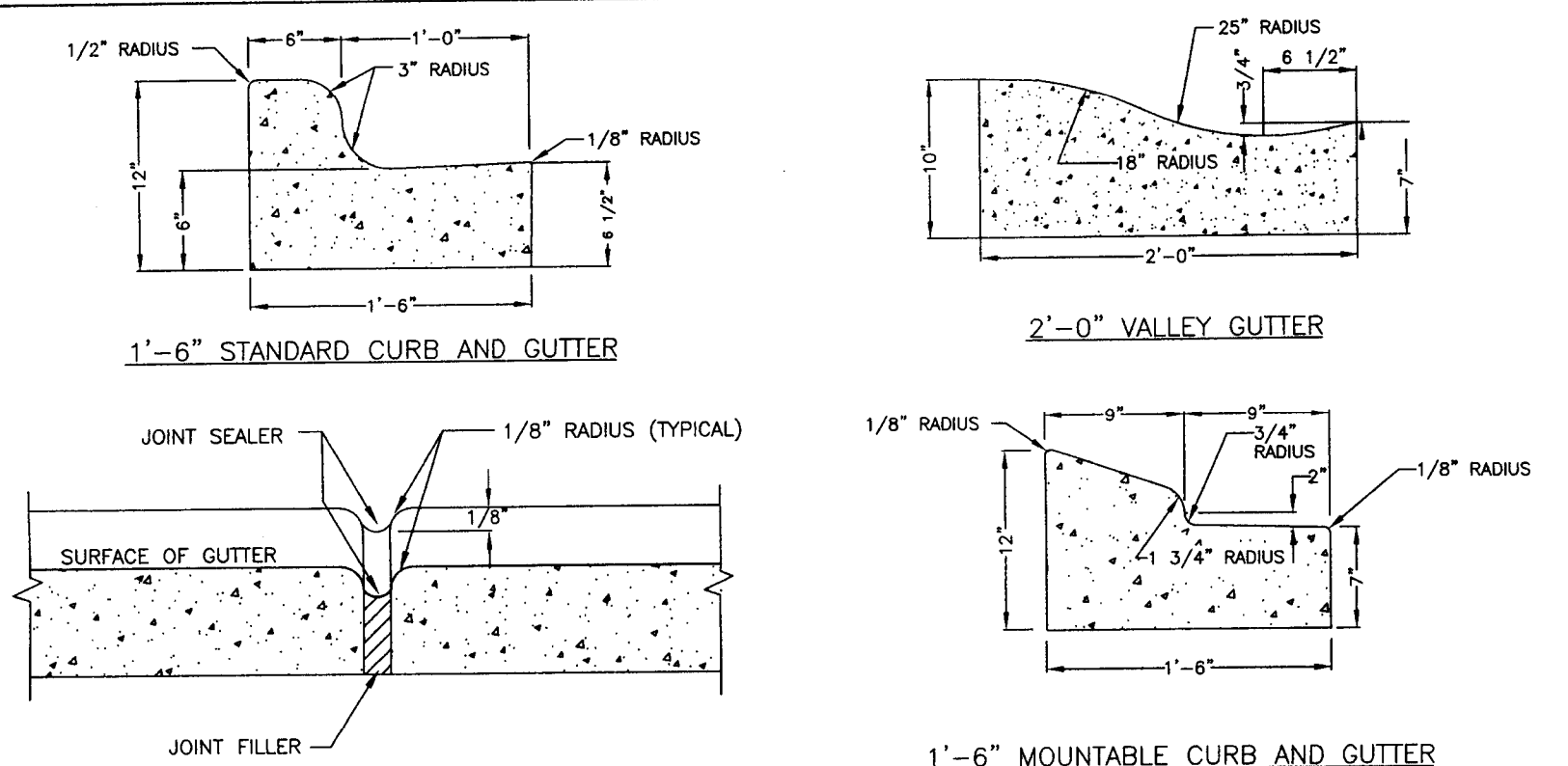
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SHEET: C2.1



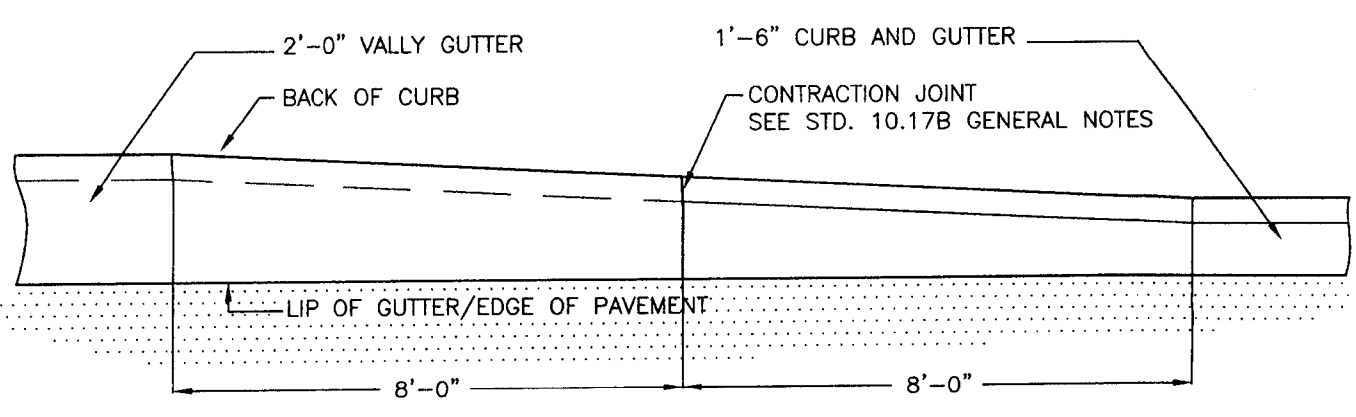


TRANSVERSE EXPANSION JOINT

NOTES:

- CONTRACTION JOINTS SHALL BE SPACED AT 10'-FOOT INTERVALS. FOR VALLEY GUTTER, A 10'-FOOT SPACING MAY BE USED WHEN A MACHINE IS USED. JOINT SPACING MAY BE ALTERED BY THE CITY ENGINEER TO PREVENT UNCONTROLLED CRACKING.
- CONTRACTION JOINTS MAY BE INSTALLED BY THE USE OF TEMPLATES OR FORMED BY OTHER APPROVED METHODS. WHERE SUCH JOINTS ARE NOT FORMED BY TEMPLATES, A MINIMUM DEPTH OF 1 1/2" SHALL BE OBTAINED.
- ALL EXPANSION JOINTS SHALL BE SPACED AT 90'-FOOT INTERVALS, AND ADJACENT TO ALL RIGID OBJECTS. JOINTS SHALL MATCH LOCATIONS WITH JOINTS IN ADJUTING SIDEWALK.
- CONCRETE COMPRESSIVE STRENGTH SHALL BE 3600 P.S.I. IN 28 DAYS.
- CURB SHALL BE DEPRESSED AT INTERSECTIONS TO PROVIDE FOR FUTURE ACCESSIBLE RAMPS.
- TOP 6" OF SUBGRADE BENEATH THE CURB AND GUTTER SHALL BE COMPACTED TO 100% STANDARD PROCTOR DENSITY.

1 CURB AND GUTTER
N.T.S. CLDS STD. DTL. 10.17A,B,C

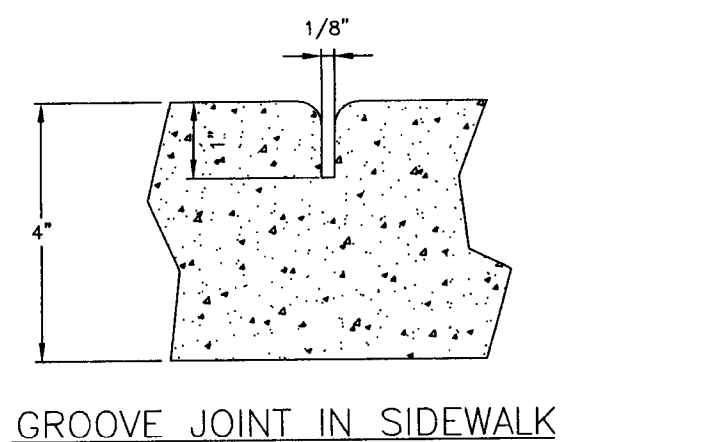


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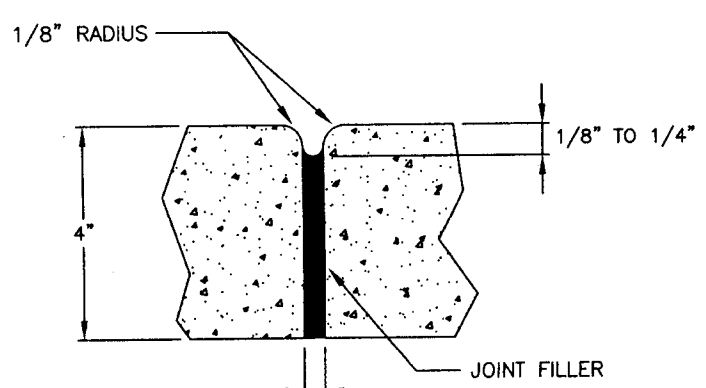
- TRANSITION TO BE ALONG BACK OF CURB.

2 CURB TRANSITION FROM 1'-6\"/>

2 N.T.S.



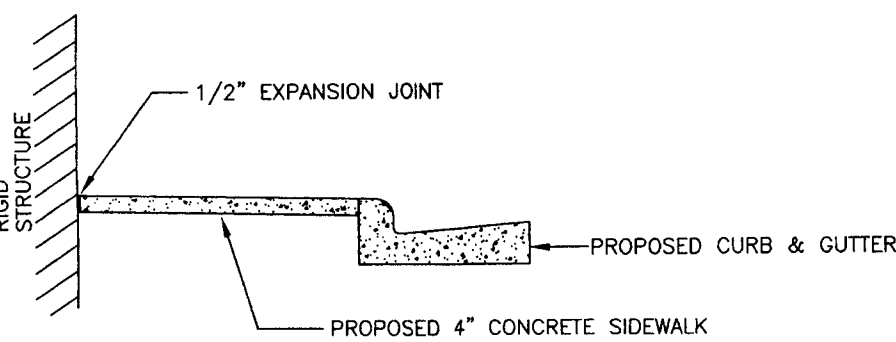
GROOVE JOINT IN SIDEWALK



TRANSVERSE EXPANSION JOINT IN SIDEWALK

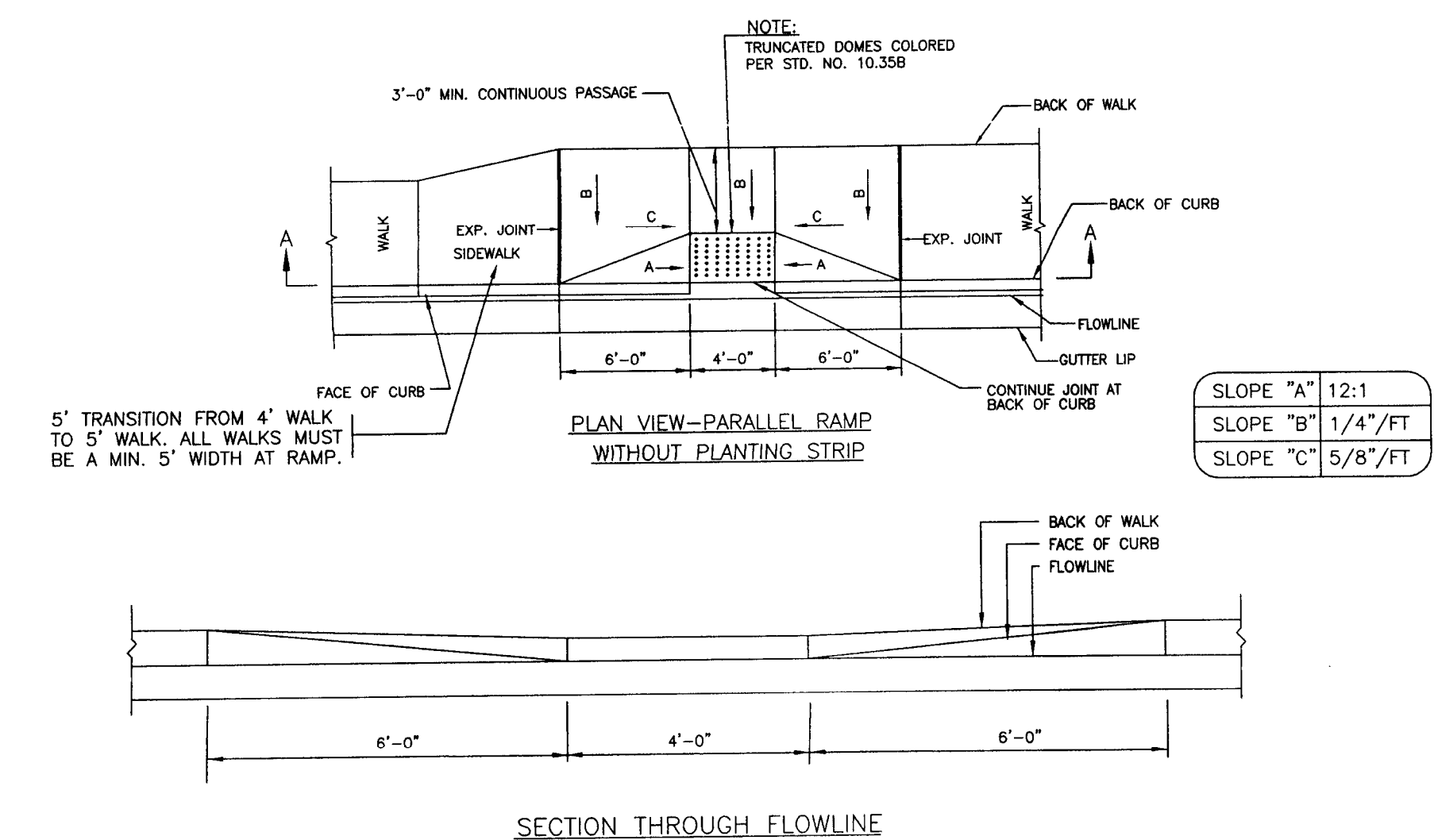
GENERAL NOTES:

- A GROOVE JOINT 1" DEEP WITH 1/8" RADI SHALL BE REQUIRED IN THE CONCRETE SIDEWALK AT 5' INTERVALS. ONE 1/2" EXPANSION JOINT WILL BE REQUIRED AT 45' INTERVALS NOT TO EXCEED 50' AND MATCHING EXPANSION/CONSTRUCTION JOINT IN ADJACENT CURB. A SEALED 1/2" EXPANSION JOINT WILL BE REQUIRED WHERE THE SIDEWALK JOINS ANY RIGID STRUCTURE.
- SIDEWALK AT DRIVEWAY ENTRANCES TO BE 6" THICK.
- WIDTH OF SIDEWALK ON THOROUGHFARE STREETS SHALL BE A MINIMUM OF 5'. WIDTH OF SIDEWALKS IN THE CENTRAL BUSINESS DISTRICT WILL BE DETERMINED BY THE CDDT.
- WIDTH OF SIDEWALKS ON NON-THOROUGHFARE STREETS SHALL BE A MINIMUM OF 4'.
- SIDEWALK TO BE POURED TO END OF RADIUS AT INTERSECTING STREETS.
- CONCRETE COMPRESSIVE STRENGTH SHALL BE 3600 PSI. IN 28 DAYS.
- ZONING CONDITIONS MAY REQUIRE ADDITIONAL WIDTH SIDEWALKS WHICH SHALL SUPERSEDE THESE STANDARD DIMENSIONS SHOWN.



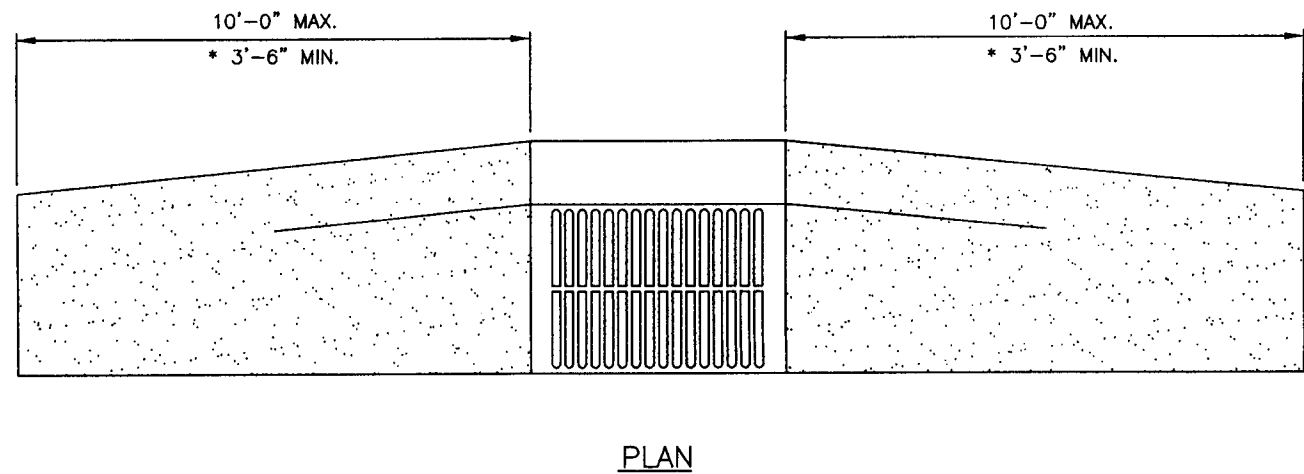
DETAILS SHOWING EXPANSION JOINTS IN CONCRETE SIDEWALK

3 CONCRETE SIDEWALKS
N.T.S. CLDS STD. DTL. 10.22



4 ACCESSIBLE RAMP STANDARD WITHOUT PLANTING STRIP (1'-6\"/>

4 N.T.S. CLDS STD. DTL. 10.32A



PLAN

NOTE:

- * TRANSITION FROM 2'-6" STANDARD CURB TO VALLEY CURB AT A DRAINAGE INLET ONLY.
- SEE STANDARD 10.19 FOR CROSS SECTION GEOMETRY.

5 CATCH BASIN FRAME IN VALEY GUTTER

N.T.S. CLDS STD. DTL. 10.29

NOTES:

- DETAILS SHOWN SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY STANDARDS.
- 2'-0" VALLEY GUTTER MAY BE USED UPON APPROVAL OF CITY ENGINEER.
- MINIMUM CURB RADIUS ON INTERIOR DRIVES AND PARKING AREAS IS 10'

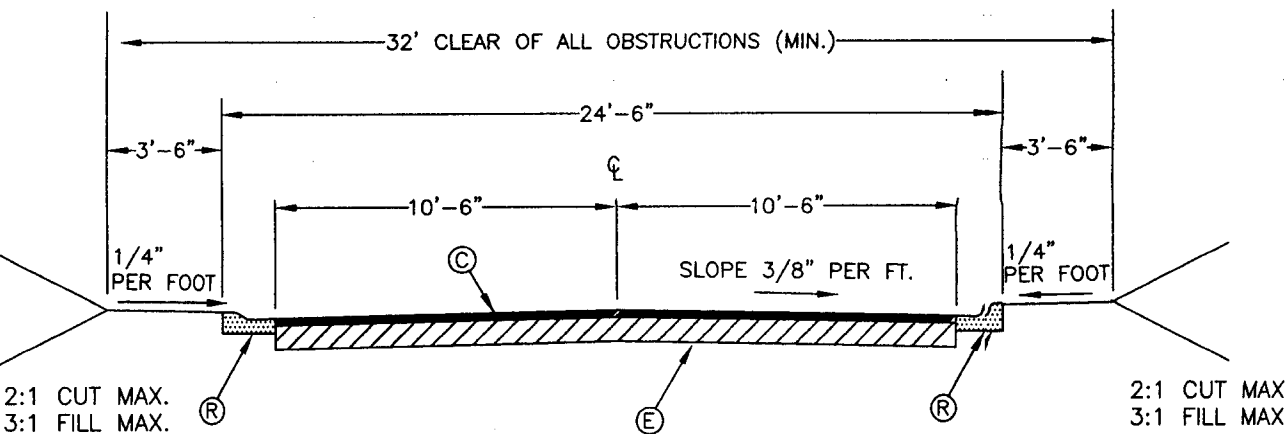
GUIDELINES FOR PRIVATE STREET DESIGN:

- INTERNAL STREET ALIGNMENT:
MAXIMUM GRADE: 10%
MINIMUM VERTICAL CURVE: 10/20 (CREST/SAH)
MINIMUM HORIZONTAL CURVE CENTERLINE RADIUS: 50 FT.
- INTERSECTION WITH PUBLIC STREET:
SAME AS FOR PUBLIC STREET. SEE GENERAL NOTES.
SECTION 10.2
5% MAXIMUM GRADE WITHIN 40 FEET OF PUBLIC STREET INTERSECTION BEGINNING FROM EDGE OF PAVEMENT LINE.

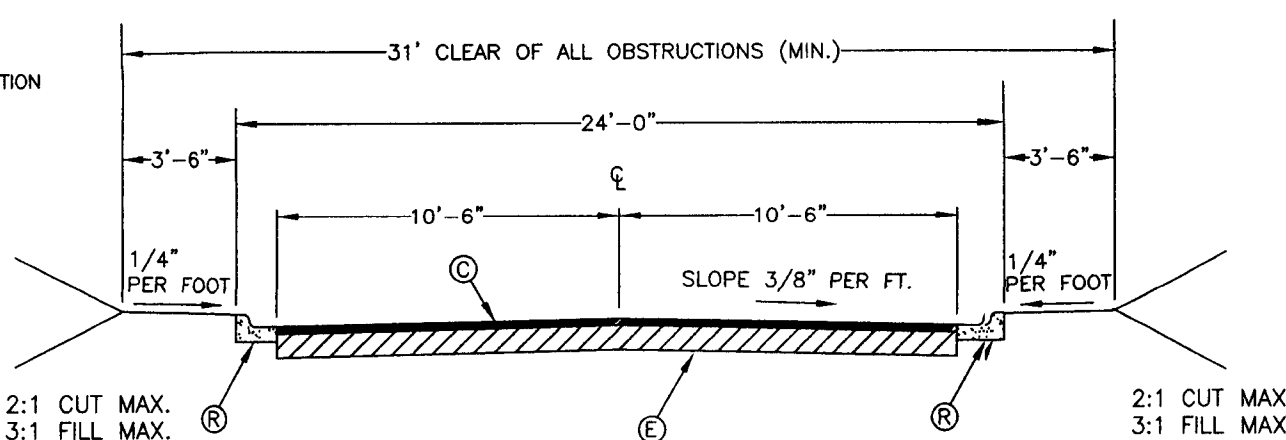
NOTE: VARIATIONS ON THESE GUIDELINES WILL BE REVIEWED ON A CASE BY CASE BASIS BY CITY STAFF.

PAVEMENT SCHEDULE

- 1.5" BITUMINOUS CONCRETE SURFACE COURSE, TYPE SF9.5A OR 1.5" BITUMINOUS CONCRETE SURFACE COURSE, TYPE I-2.
- 6" COMPACTED AGGREGATE BASE COURSE OR 4" BITUMINOUS CONCRETE BASE COURSE, TYPE B25.0B OR 4" BCBC TYPE HB.
- CURB AND GUTTER ON 24" VALLEY GUTTER, SEE SHI. C2.0 FOR ACTUAL CONDITIONS (REFERENCE 10.17A AND B).



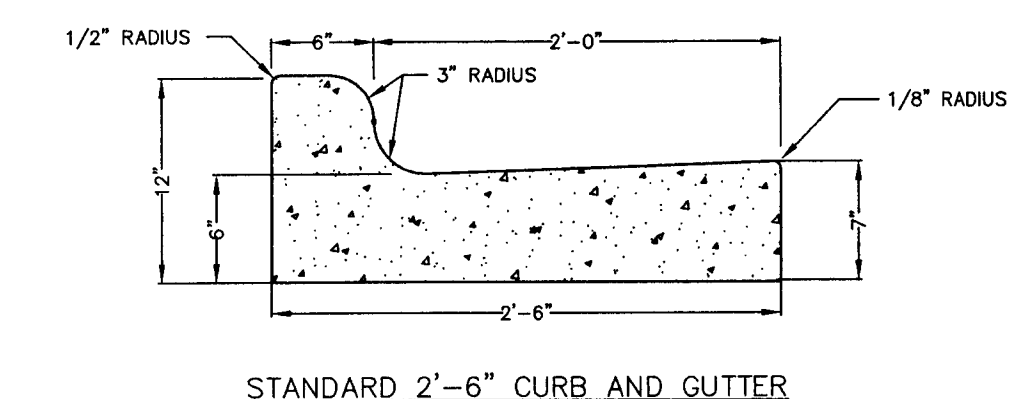
CURB AND GUTTER / VALLEY GUTTER



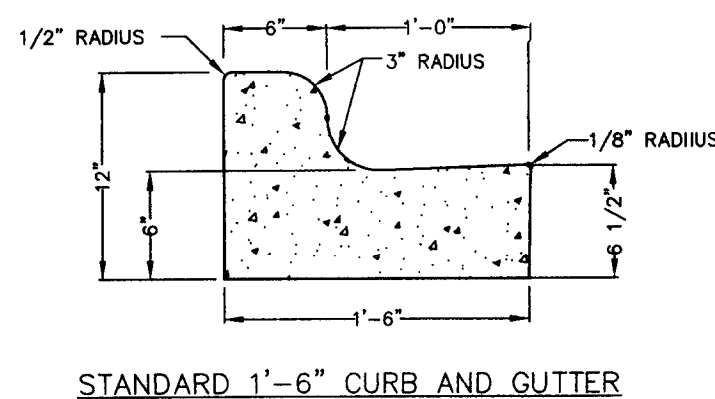
CURB AND GUTTER

6 TYPICAL SECTIONS PRIVATE STREET

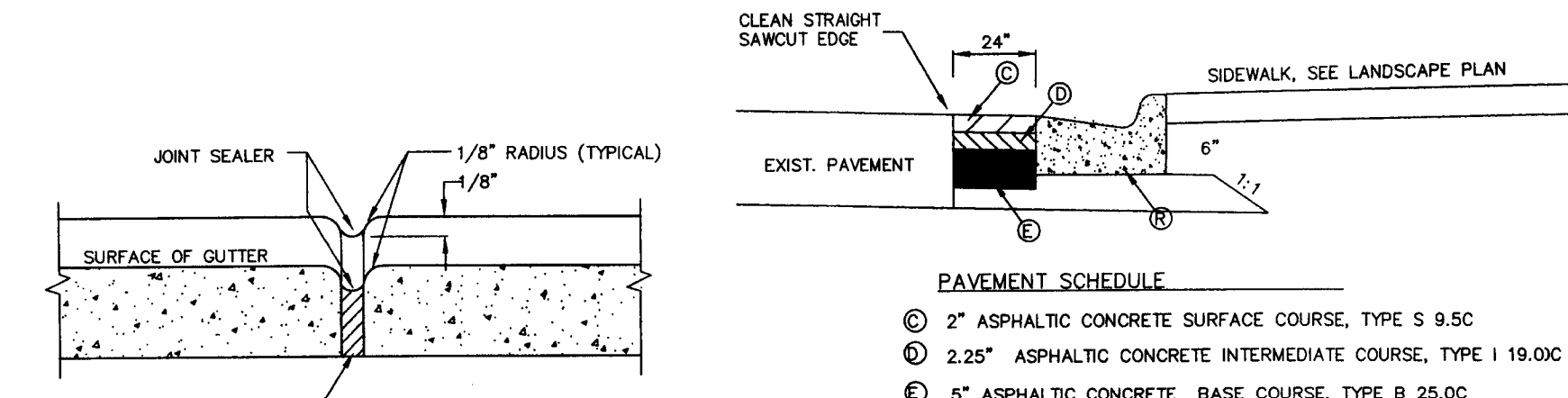
N.T.S. CLDS STD. DTL. 11.13 (MODIFIED)



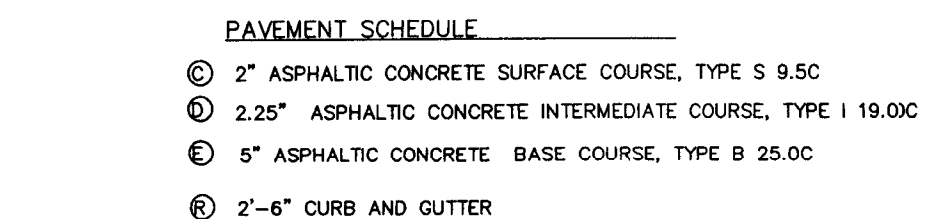
STANDARD 2'-6" CURB AND GUTTER



STANDARD 1'-6" CURB AND GUTTER



TRANSVERSE EXPANSION JOINT (E.J.)



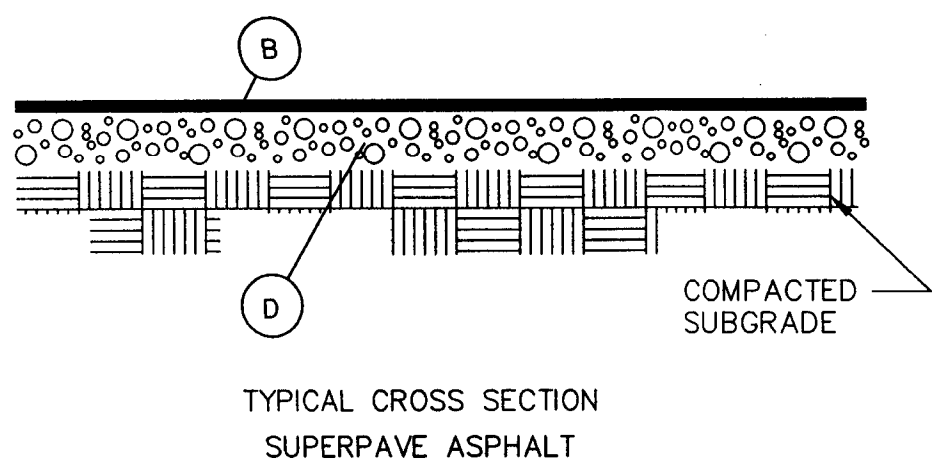
IMPROVEMENT REQUIREMENTS FOR NEW CURB AND GUTTER INSTALLATIONS

NOTES:

- CONTRACTION JOINTS SHALL BE SPACED AT 10'-FOOT INTERVALS. FOR VALLEY GUTTER, A 10'-FOOT SPACING MAY BE USED WHEN A MACHINE IS USED. JOINT SPACING MAY BE ALTERED BY THE ENGINEER TO PREVENT UNCONTROLLED CRACKING.
- CONTRACTION JOINTS MAY BE INSTALLED BY THE USE OF TEMPLATES OR FORMED BY OTHER APPROVED METHODS. WHERE SUCH JOINTS ARE NOT FORMED BY TEMPLATES, A MINIMUM DEPTH OF 1 1/2" SHALL BE OBTAINED.
- ALL EXPANSION JOINTS SHALL BE SPACED AT 90 FOOT INTERVALS, AND ADJACENT TO ALL RIGID OBJECTS. JOINTS SHALL MATCH LOCATIONS WITH JOINTS IN ADJUTING SIDEWALK.
- CONCRETE COMPRESSIVE STRENGTH SHALL BE 3600 P.S.I. IN 28 DAYS.
- TOP 6" OF SUBGRADE BENEATH THE CURB AND GUTTER SHALL BE COMPACTED TO 100% STANDARD PROCTOR DENSITY.

7 CURB AND GUTTER

N.T.S.



TYPICAL CROSS SECTION SUPERPAVE ASPHALT

PAVEMENT SCHEDULE

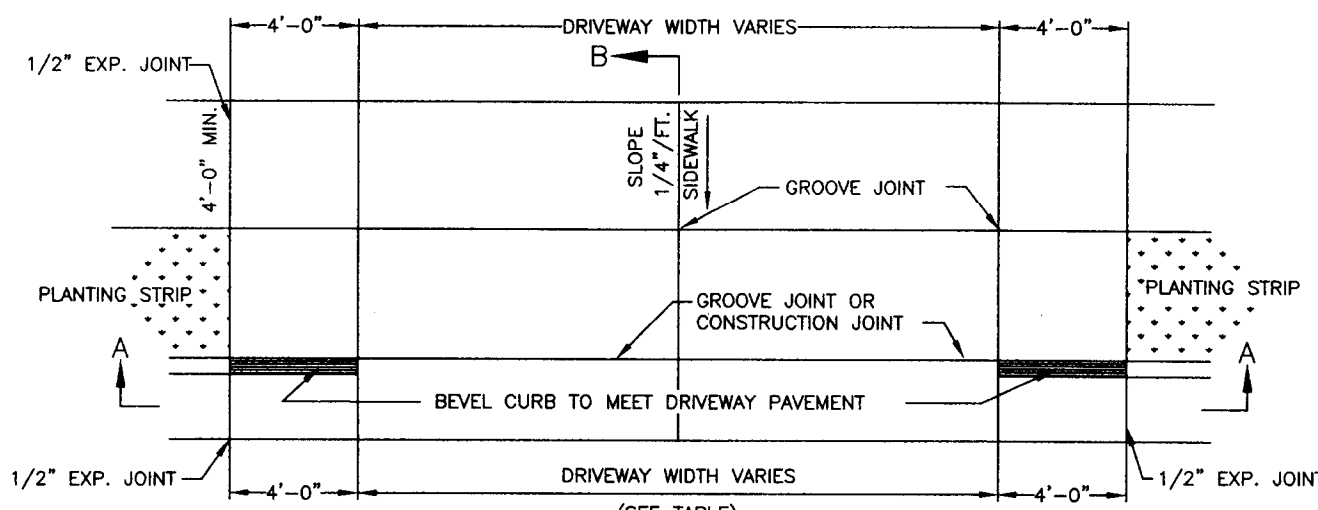
- B TYPE SF-9.5A, 1.5" MIN. THICKNESS
- D TYPE B-25.0B, 4" MIN. THICKNESS
- 8 ASPHALT PAVING SECTION

8 N.T.S.

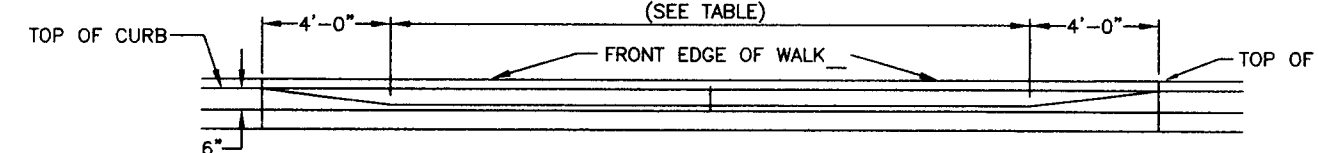
- NOTES:
- ALL CONCRETE TO BE 3600 P.S.I. COMPRESSIVE STRENGTH.
 - AT ALL DRIVEWAYS, SIDEWALKS TO BE REMOVED TO THE NEAREST JOINT BEYOND NEW CONSTRUCTION OR CUT WITH A SAW AND REMOVED. SAW CUT OR JOINT TO BE PERPENDICULAR TO EDGE OF EXISTING PAVEMENT. SEE ST. NO. 10.17 FOR JOINT DETAIL.
 - ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND HOOD REQUIREMENTS. SIGHT DISTANCE AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
 - 7" BREAKOVER SHALL BE 6% OR LESS (A = ALGEBRAIC DIFFERENCE).
 - PRIOR APPROVAL IS REQUIRED BY CDDT ON GRADES EXCEEDING WHAT ARE SHOWN.

DRIVEWAYS CLASSIFICATION		
TYPE DRIVEWAYS	MINIMUM	MAXIMUM
ONE-WAY TYPE II - COMMERCIAL	20'	30'
TWO-WAY TYPE II - COMMERCIAL	26'	50'

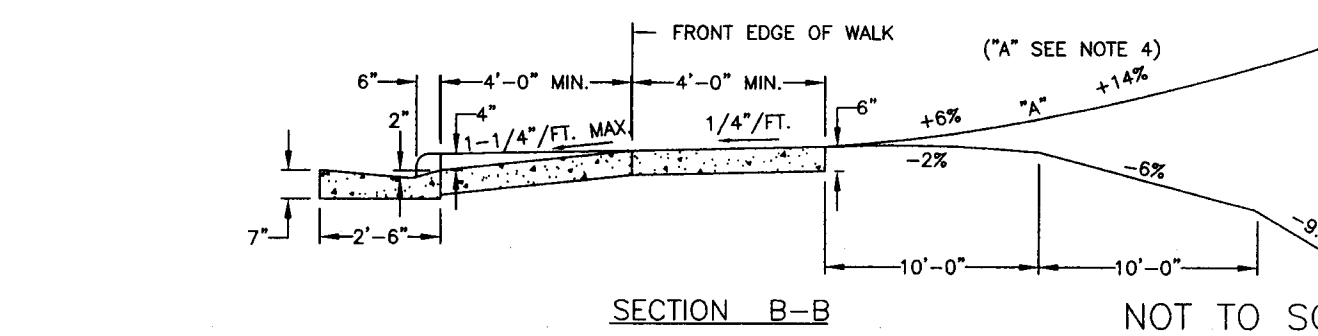
* NEED MORE THAN ONE CONTRACTION JOINT IN CENTER.



PLAN VIEW



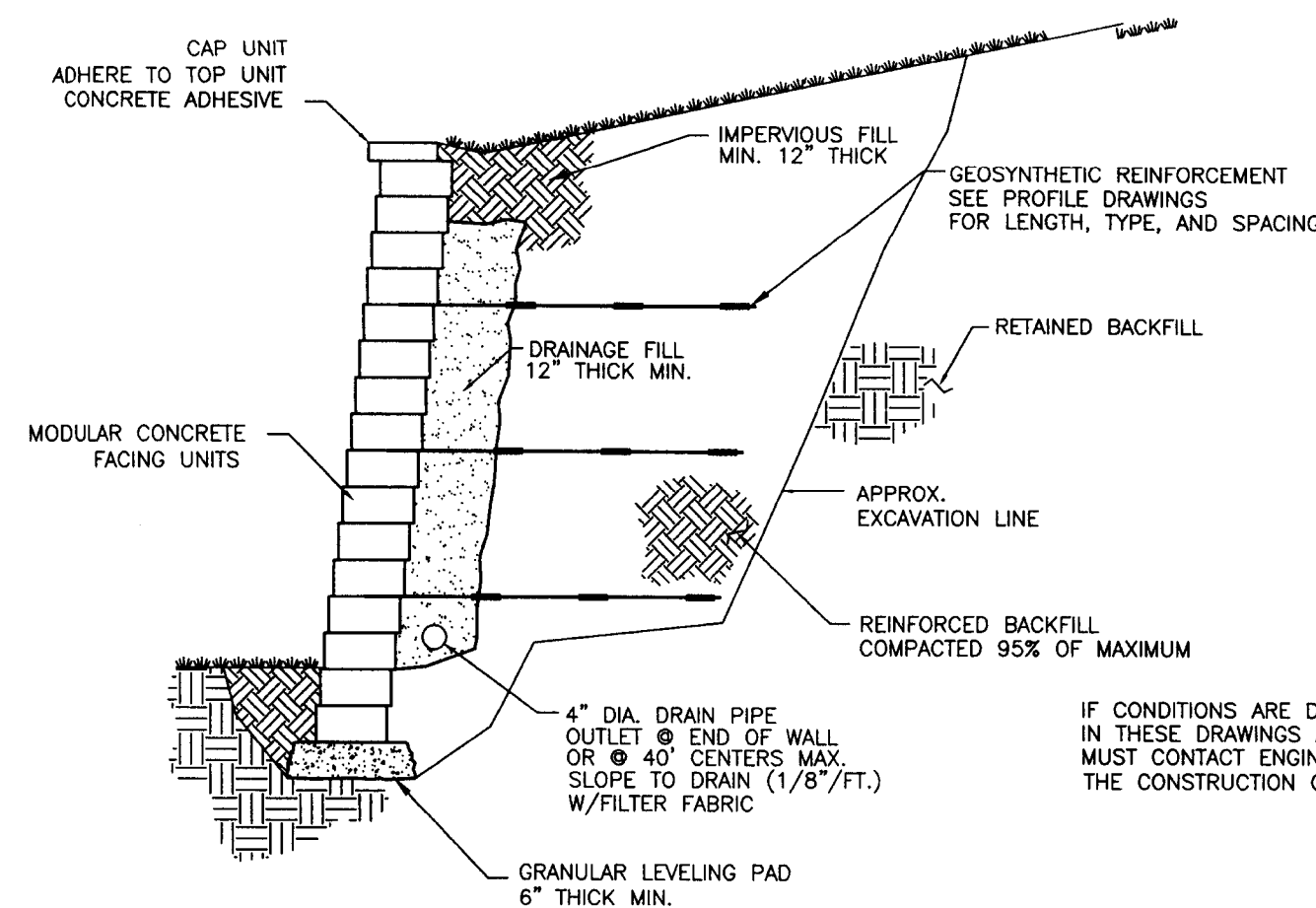
SECTION A-A



SECTION B-B

9 COMMERCIAL DROP CURB TYPE II DRIVEWAY WITH PLANTING STRIP (2'-6\"/>

N.T.S. CLDS STD. DTL. 10.25B



NOTES:

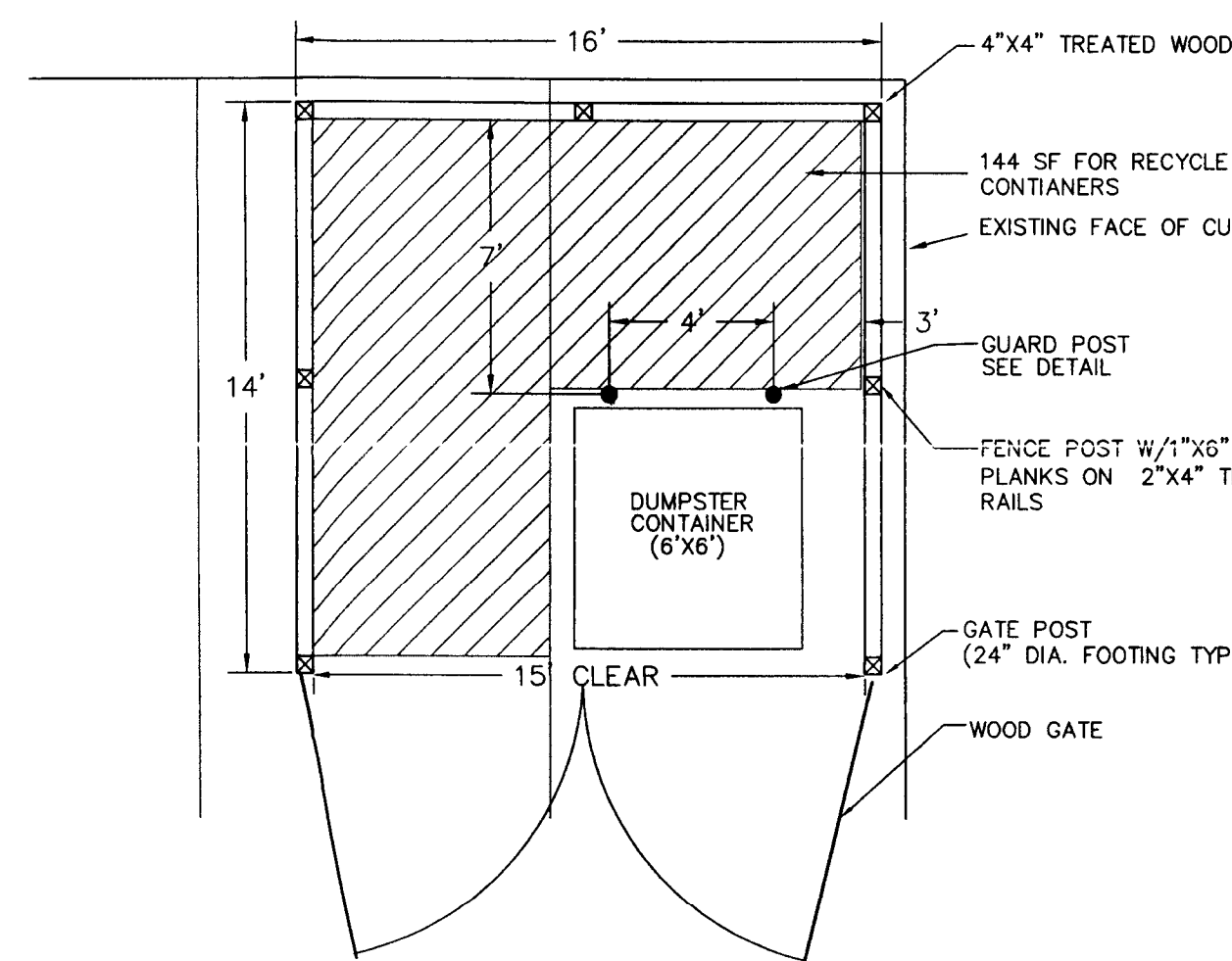
- DESIGN OF ALL RETAINING WALLS IS TO BE PER NC BUILDING CODE SECTION 1610.3.
- DETAILED RETAINING WALL DESIGN DRAWINGS, SEALED BY AN NC LICENSED ENGINEER, WILL BE SUBMITTED TO THE LOCAL AUTHORITY PRIOR TO CONSTRUCTION.
- AN NC LICENSED ENGINEER MUST PERFORM CONSTRUCTION OBSERVATION, VERIFYING IN A SEALED LETTER TO THE LOCAL AUTHORITY THAT RETAINING WALLS ARE CONSTRUCTED PER THE ENGINEERED DRAWINGS, IN COMPLIANCE WITH SECTION 1610.3 OF THE NC BUILDING CODE.

GENERAL NOTES

- STRIP VEGETATION AND ORGANIC SOIL FROM WALL AND GEOSYNTHETIC ALIGNMENT.
- BENCH CUT ALL EXCAVATED SLOPES.
- DO NOT OVER EXCAVATE UNLESS DIRECTED BY SITE SOIL ENGINEER TO REMOVE UNSUITABLE SOIL.
- SITE SOIL ENGINEER SHALL VERIFY FOUNDATION SOILS AS BEING COMPETENT FOR THE DESIGN STANDARDS AND PARAMETERS.
- BASE SHALL CONSIST OF COMPACTED COARSE SAND AND/OR GRAVEL, 6" THICK MIN.
- CONTRACTOR MAY OPT FOR A LEAN CONCRETE PAD. CONCRETE PAD SHALL BE UNREINFORCED, 1" TO 2" THICK MAXIMUM.
- MINIMUM EMBEDEDMENT OF WALL BELOW FINISH GRADE SHALL BE 12" FOR WALL HEIGHTS FROM 4 FT. AND UP, 6" BELOW 4 FT., UNLESS SHOWN DIRECTED OTHERWISE BY SOIL ENGINEER.
- FOLLOW APPLICABLE PROVISIONS OF THE MANUFACTURER'S INSTALLATION INSTRUCTIONS AND WRITTEN SPECIFICATIONS.
- DRAINAGE FILL SHALL BE INSTALLED BEHIND THE WALL TO 18" FROM THE TOP OF THE WALL. DRAINAGE FILL SHALL NOT EXTEND BELOW FINAL GRADE IN FRONT OF WALL.
- WHERE DRAIN PIPE IS USED, PROVIDE OUTLETS @ MAX. 40 FT ON CENTER.
- FOR UNITS TO BE EMBEDDED, COMPACT FILL IN FRONT OF UNITS AT THE SAME TIME BACKFILL BEHIND UNITS IS COMPACTED.
- COMPACTION TESTS SHALL BE TAKEN AS THE WALL IS INSTALLED. THE MINIMUM NUMBER OF TESTS SHALL BE DETERMINED BY THE SITE SOILS ENGINEER.
- COMPACTION SHALL BE TO 95% OF MAXIMUM STANDARD PROCTOR DENSITY (ASTM D-698).
- SEE ELEVATION FOR GEOSYNTHETIC TYPE, LENGTH AND LOCATION REQUIRED.
- AS DESIGNED BY MANUFACTURERS REP.
- GEOSYNTHETIC SHALL BE THE TYPE AND LENGTH AS SHOWN BY GEOTECH ENGINEER. FULL GEOSYNTHETIC FABRIC TIGHT PRIOR TO BACKFILLING.
- GEOSYNTHETIC SHALL BE PLACED WITH STONGEST DIRECTION PERPENDICULAR TO WALL. FOLLOW GEOSYNTHETIC MANUFACTURER'S INSTALLATION INSTRUCTIONS AND WRITTEN SPECIFICATIONS.
- THE RETAINING WALL SHALL BE ENGINEERED BY MANUFACTURERS REPRESENTATIVE.

10 MODULAR RETAINING WALL

N.T.S.



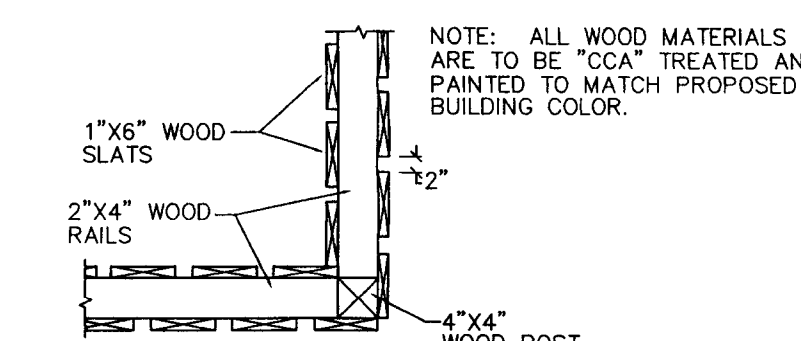
SCREEN FENCE LAYOUT AT DUMPSTER WITH RECYCLE AREA

N.T.S.

- NOTES:
- DUMPSTER AND RECYCLE ENCLOSURE ONLY TO BE INSTALLED IF ROLL SERVICE IS NOT PROVIDED - SEE SHEET C2.0 FOR LOCATIONS
 - DUMPSTER ENCLOSURE FOR TWO DUMPSTERS SHALL ONLY BE 8' X 16' - ALL OTHER INFORMATION REMAINS THE SAME

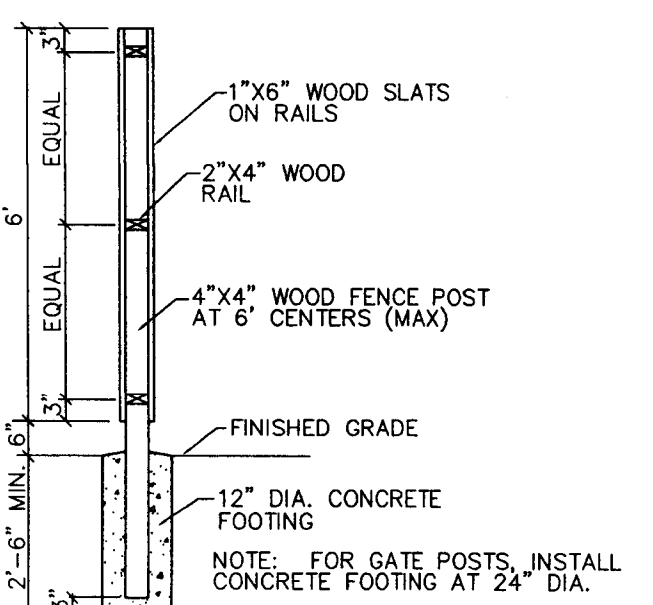
11 DUMPSTER PAD/SCREEN FENCE DETAIL

N.T.S.



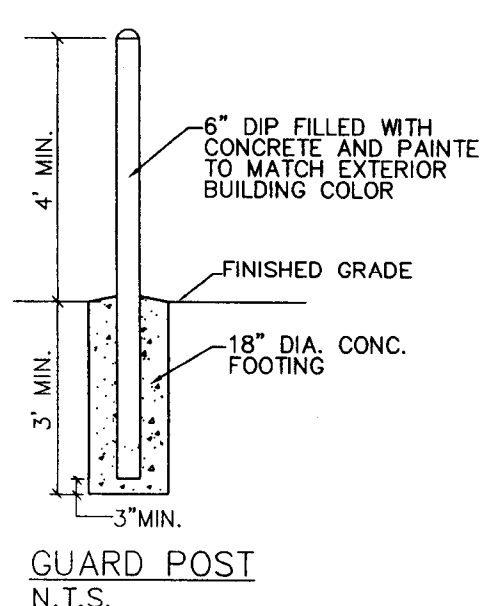
SCREENING FENCE CORNER DETAIL

N.T.S.



SCREENING FENCE POST SECTION

N.T.S.



GUARD POST

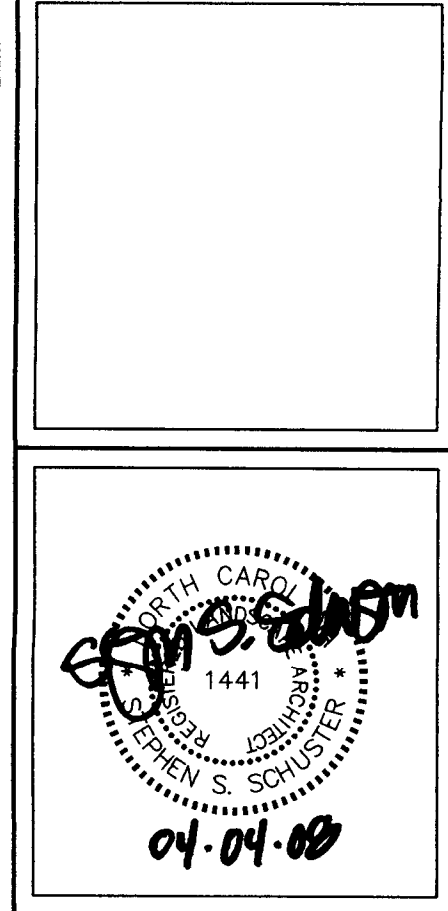
N.T.S.

REVISIONS		DATE	DESCRIPTION
1		04/04/08	REVISED PER MULTI-FAMILY REVIEW

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THE VILLAGE AT BRIERFIELD TOWNHOMES
UNIVERSITY CITY BLVD (NC 49)
CHARLOTTE, NORTH CAROLINA

SITE DETAILS

DATE: 12.05.07

SCALE: N.T.S.

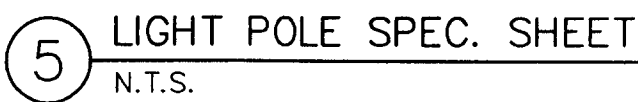
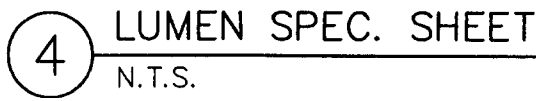
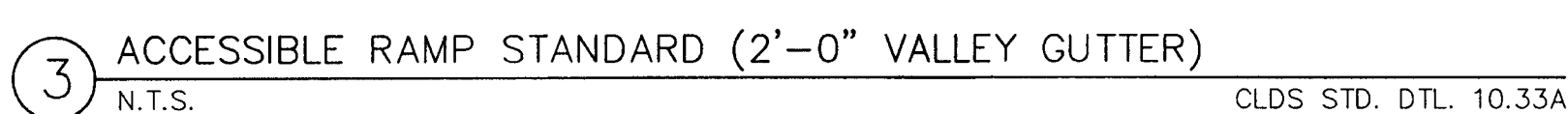
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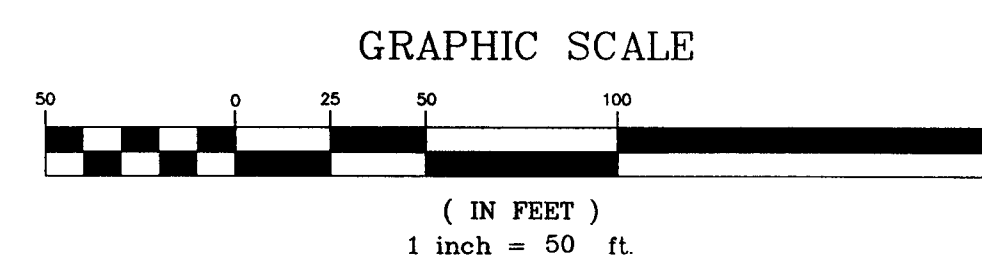
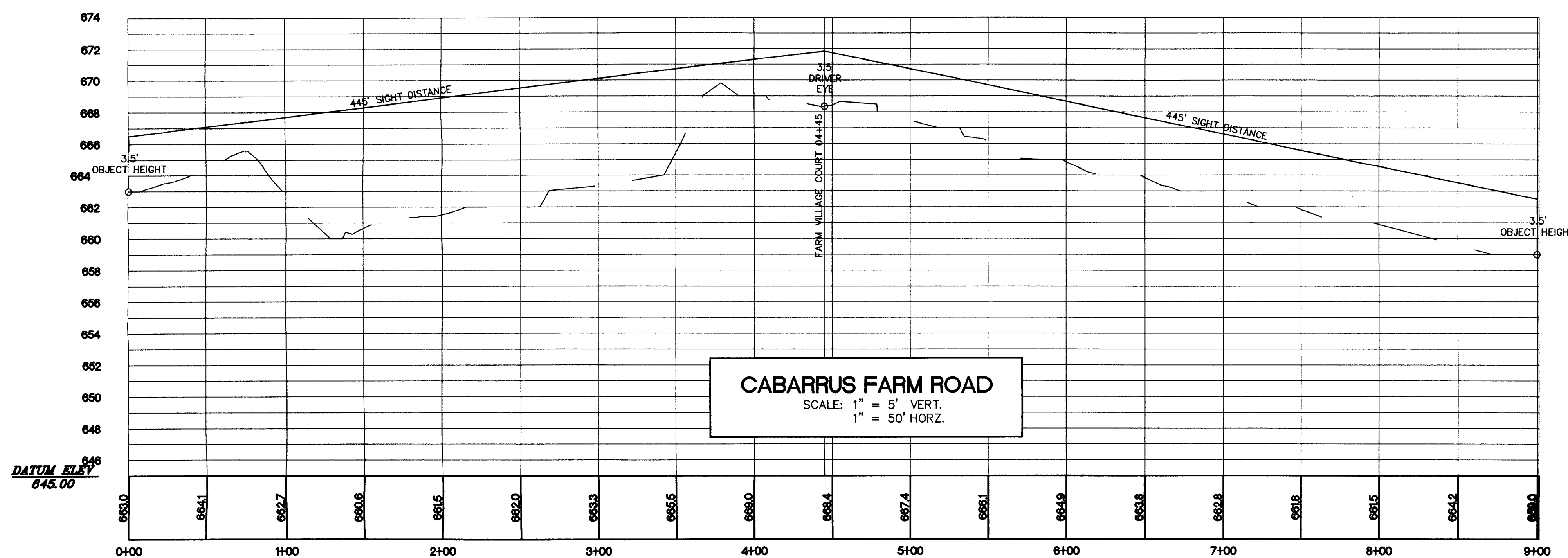
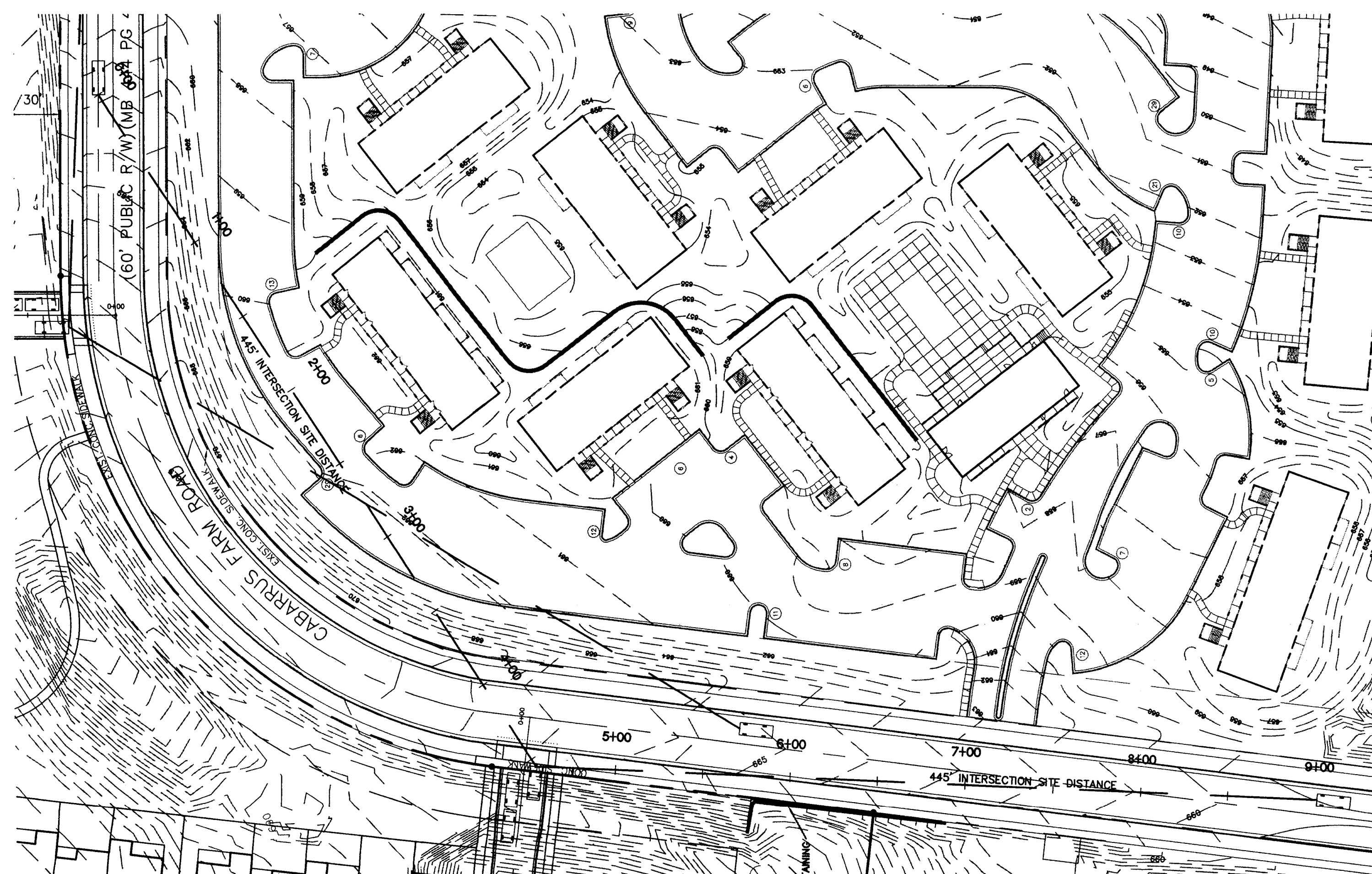
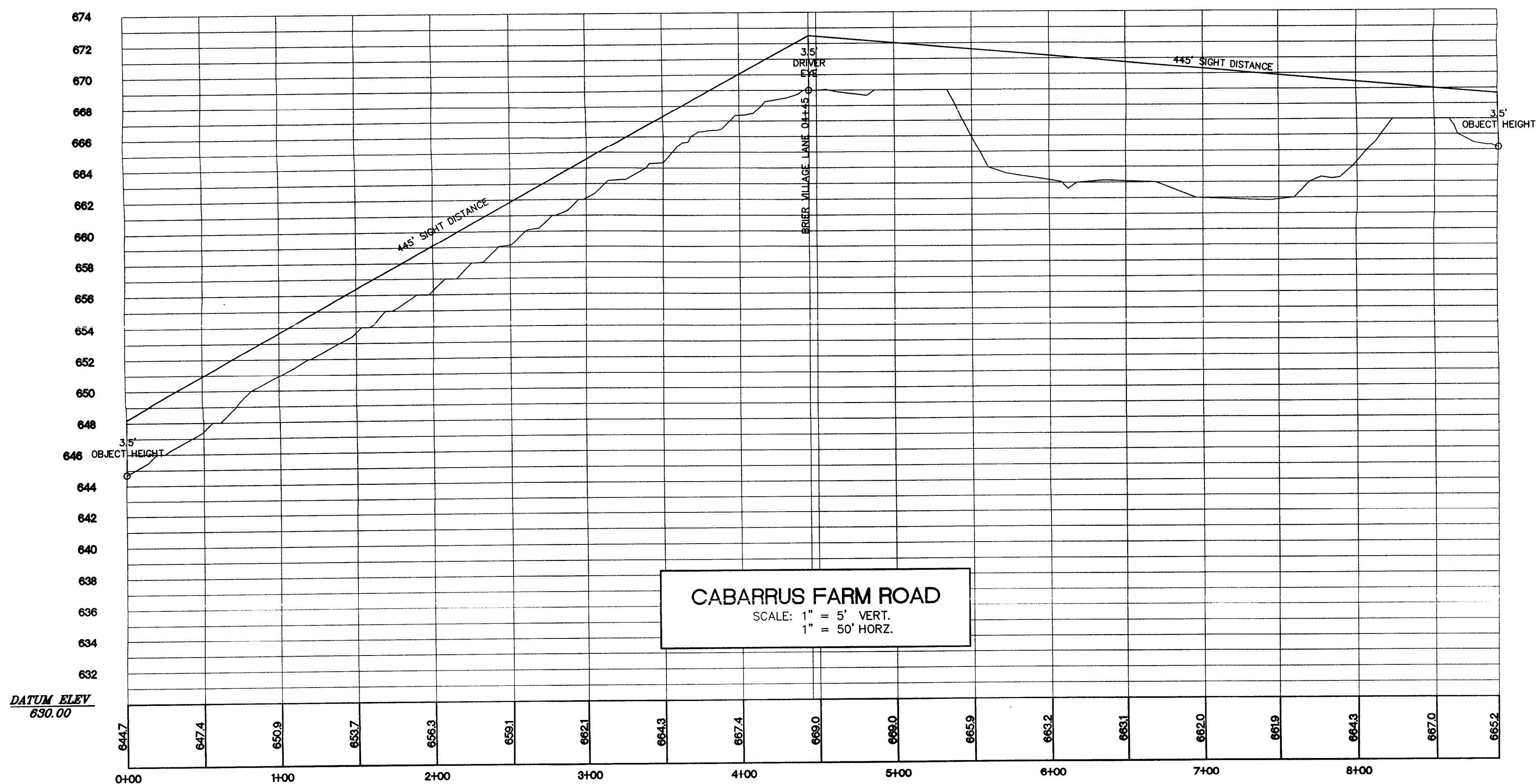
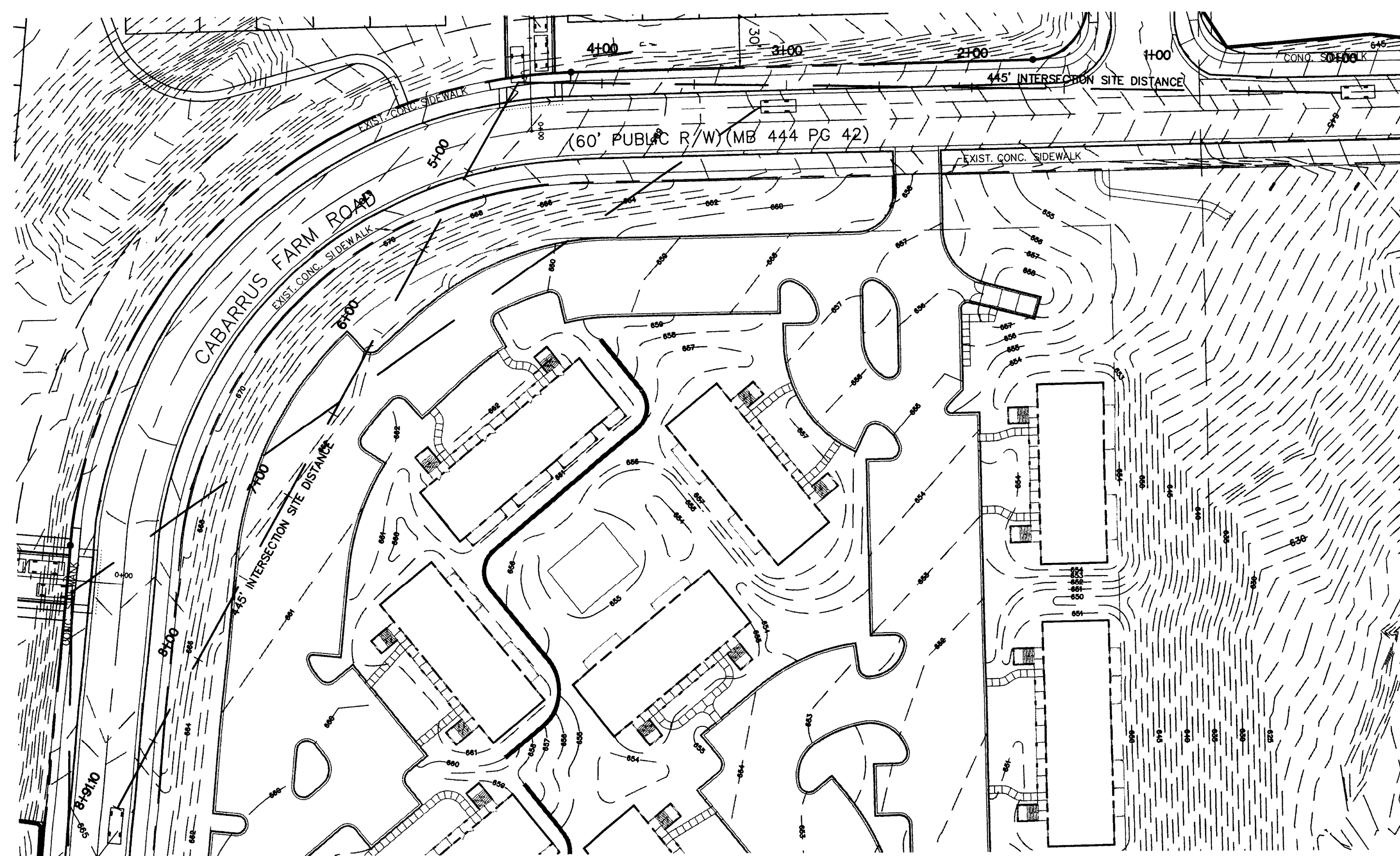

SHEET: C2.2



- NOTES:**
1. DETECTABLE WARNINGS SHALL BE OF THE PAVER OR MAT TYPE WITH ADHESIVE PER MANUFACTURER SPECIFICATIONS.
 2. WIDTH OF DETECTABLE WARNING AREA SHALL BE A MINIMUM 5 FEET AND VARY WITH THE WIDTH OF THE TRAVEL LANE.
 3. LENGTH OF DETECTABLE WARNING AREA SHALL BE 2 FEET REGARDLESS OF SECTION WIDTH.
 4. DETECTABLE WARNING AREA CAN BE SQUARE WHERE USED IN A CURB RADIUS.
 5. DETECTABLE WARNING DIMES SHALL BE ALIGNED ON A SQUARE GRID. THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DIMES.
 6. DETECTABLE WARNING AREA SHALL BE COLORED BLACK IN ALL LOCATIONS EXCEPT ON TRYON STREET MAIL, WHERE FRENCH GRAY IS TO BE USED.
 7. IF PAVERS ARE TO BE USED, PAVERS SHALL BE 6" THICK AND CAST FROM 5000 PSI CONCRETE.
 8. IF MATS ARE TO BE USED, EDGES SHALL BE BEVELED TO ELIMINATE TRIP HAZARD.

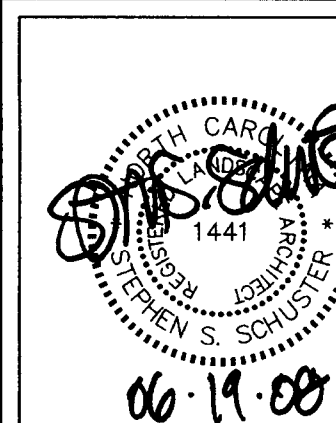
2 TRUNCATED DOMES PLAN AND CROSS-SECTION
N.T.S.



[illegible]

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THE VILLAGE AT BRIERFIELD TOWNHOMES
UNIVERSITY CITY BLVD (NC 49)
CHARLOTTE, NORTH CAROLINA

SITE DISTANCE PROFILE

DATE: 06.19.08

SCALE: 1" = 50'

JOB NO. X7076.00

SHEET: C2.4