

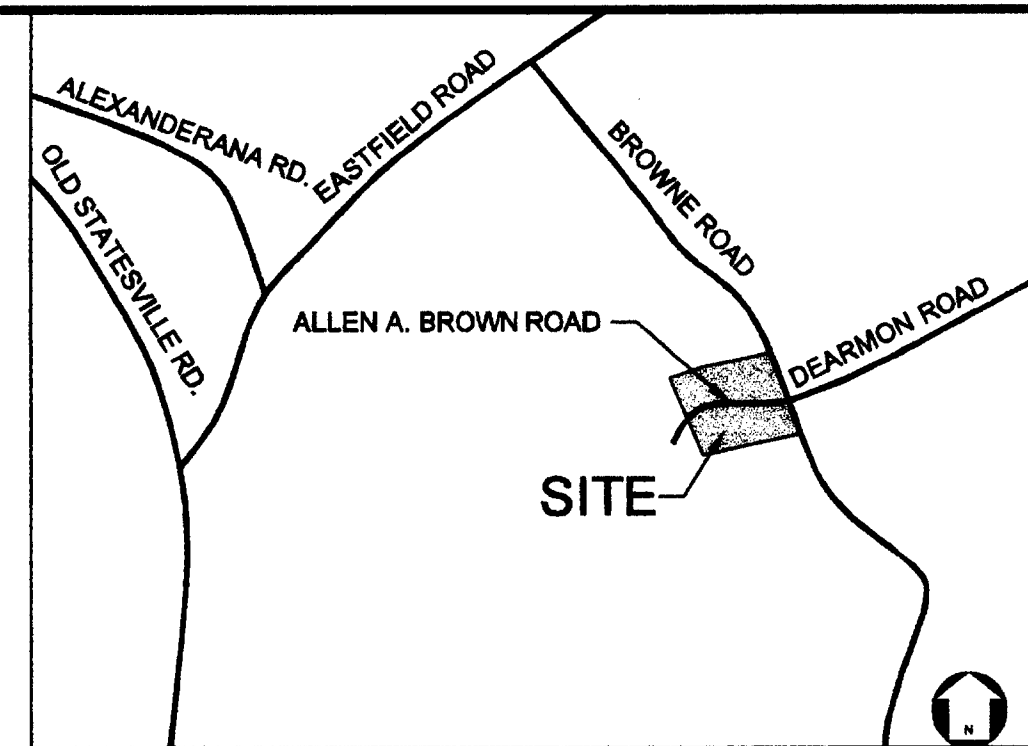
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- Diagram illustrating a method for removing a tree stump. The diagram shows a cross-section of a tree stump and its root system. A trench is dug around the stump, and the sides are lined with a 6" maximum layer of mulch. The trench is filled with water, and the water level is maintained at least 4" above the stump. The diagram also shows the stump being pulled out of the ground, with the root system exposed. The diagram is labeled with dimensions and materials:
- 6" SLIP KNIFE WITH STIFF SHAFT
 - 6" LANCED FROM TREE DIAMETER
 - 6" MAXIMUM LAYER OF MULCH
 - 4" MAXIMUM HIGH WATER (BEAM OUTSIDE OF SHOULDER)
 - PLANTING MIX (AS SPECIFIED)
 - ROOTS
 - NEED ROOT BALL
 - ROOT BALL DIA.
 - INCHES

TAX PARCEL NO.	027-17-106	027-17-109 (PARTIAL)
	027-17-138	027-17-137
	027-17-138	
PROPERTY OWNER:	YOUNGS BOND, LLC	
ZONING:	R-3	
MINIMUM LOT WIDTH:	70'	
MINIMUM SETBACK:	20' (TREE ORDINANCE INCENTIVE)	
MINIMUM SIDE YARD:	3' (TREE ORDINANCE INCENTIVE)	
MINIMUM REAR YARD:	5.000 S.F. (MIN. REQ'D)	
MINIMUM LOT AREA:	9,015 S.F. (MIN. PROPOSED)	
	10,000 S.F. (AVG. REQ'D)	
	11,998 S.F. (AVG. PROPOSED)	
	1,000 S.F. (CHARGELOTTE)	
JURISDICTION:	17-18-AC	
PHASE ONE TOTAL AREA	3DUAC (51 DU)	
DENSITY ALLOWED:	2.85 DU/AC (49 DU)	
DENSITY PROPOSED:		

OVERALL SITE ACREAGE:	748,323 S.F.
MINUS EXT. ROAD ROW:	-56,665 S.F.
MINUS BROWNE ROAD ROW:	-11,817 S.F.
NET SITE ACREAGE:	679,841 S.F.
<hr/>	
TREE SAVE REQUIRED (10%):	67,984 S.F.
TREE SAVE PROVIDED:	68,527 S.F. (101%)

	FRONTAGE	TREES REQ'D.	TREES PROV.
ALLEN A. BROWN ROAD:	2180'	44 (50' O.C.)	45
MERIDALE CROSSING DR:	2070'	42 (50' O.C.)	42
EVERSFIELD LANE:	480'	10 (50' O.C.)	10

1. ALL TREES SHALL BE PLANTED PER CMLDS 40.01 AND 40.09.
2. ALL STRAPPING AND TOP 23 OF WIRE BASKET MUST BE CUT AWAY AND REMOVED FROM THE ROOT BALL PRIOR TO CROWN PLANTING FIT. REMOVE TOP 15% OF THE BURLAP FROM ROOT BALL.
3. LARGE MATURING TREES MAY NOT BE PLANTED WHERE THERE IS AN OBSTACLE OR TRANSITION LINE EXIST. IF TREES CONFLICT WITH POWER LINES OR SIGNS, CALL THE URBAN FORESTER BEFORE PLANTING.
4. AVOID TREE-PLANTING LOCATIONS TO AVOID UNDERGROUND UTILITIES.
5. NEW TREES SHALL BE A MINIMUM 2" CALIPER FOR STREET TREE REQUIREMENTS.
6. ALL PRESERVATION NOTES ON GRADING AND EROSION CONTROL SHEETS.
7. PROPOSED STREET TREES SHALL BE 2" CALIPER LAUREL OAKS SHALL BE SELECTED FROM THE CITY OF CHARLOTTE APPROVED PLANTING LIST. COORDINATE WITH OWNER.
8. STREET TREE SPACING: 40-50' O.C.
9. PROPOSED STREET TREES SHALL BE GENETIC CLONES (CULTIVARS), SELECTED FOR UNIFORMITY. TREES SHALL MEET REQUIREMENTS OF AMERICAN STANDARDS FOR NURSERY STOCK. THEIR OWNERS SHALL BE SINGLE-SPECIES WITH A STRONG CENTRAL LEADER.
10. STREET TREES PLANTED WITHIN THE INTERSECTION SIGHT TRIANGLE SHALL BE PLANTED AT LEAST 10' AWAY FROM BRANCHES BELOW 6' ABOVE ADJACENT GRADE.



The diagram illustrates the construction of a typical planting bed. The cross-section (left) shows a 1/2" thick extruded polystyrene (EPS) insulation board at the base, topped with a 3" layer of amended mulch. A 1/2" thick collar is placed over the mulch. The plan view (right) shows the layout of the planting bed, including the location of the plants, the placement of the EPS board, and the location of the plants. The plan view also shows the location of the plants, the placement of the EPS board, and the location of the plants.

REMOVE ANY BROKEN OR DEAD AND DISEASED LIMBS

SPACING VASES
SEE LANDSCAPE PLAN

TYP. COLLAR
MIN. 2" THK.

3" LAYER OF AMENDED MULCH

1/2" EPS EXTRUDING ENTIRE PLANTING BED

EDGE OF PLANTING BED

CUT ROOTS AS NECESSARY IN CONTAINED PLANTS THAT ARE POT-BOUND

PLANT MIX

TAMP SOIL MIX UNDER EACH PLANT BALL TO MINIMIZE SETTLEMENT

TYPICAL PLANTING BED DETAIL

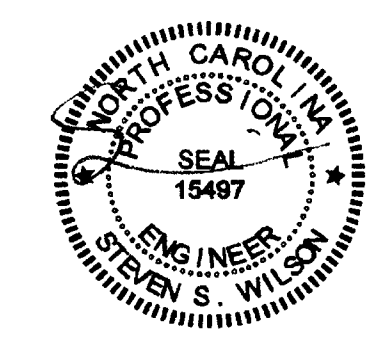
NOTES:

1. OMIT COLLAR AROUND EACH SHRUB WHEN IRRIGATION SYSTEM IS PRESENT
2. INSTALL TOP OF PLANT BALL 2" ABOVE ADJACENT GRADE.
3. TAMP PLANTING MIX FIRMLY AS PIT IS FILLED AROUND EACH PLANT BALL.
4. SOAK EACH PLANT BALL AND PIT IMMEDIATELY AFTER INSTALLATION.

NOT TO SCALE

DRAWING INDEX	
SHEET #	DESCRIPTION
L2.0	SITE PLAN
L3.0	GRADING
L3.1	STORM DRAINAGE AREAS
L3.2	SEDIMENT BASIN AREAS
L4.0	EROSION CONTROL INITIAL
L4.1	EROSION CONTROL FINAL
L5.0	ROAD PROFILES
L5.1	ROAD PROFILES
L6.0	DETAILS
L6.1	DETAILS

seals



Description	PER CITY REVIEW COMMENTS

REVISIONS: By SSW
 No. Date
 1 3-28-07

Project Manager: SSW
 Drawn By: MDL
 Checked By:

Date: 01-31-07
Project Number: 06019
Sheet Number:

L2.0

SHEET # 1 OF 1

L2.0 NOT TO SCALE

BOUNDARY, EXISTING UTILITIES, AND OTHER EXISTING FEATURES TAKEN FROM BOUNDARY AND TOPO SURVEY BY EAGLE ENGINEERING, NCRLS #11-5661) DATED 01/20/2011.

COORDINATE ALL CURB AND STREET GRADATIONS IN INTERSECTION WITH INSPECTOR. CITY ENGINEER AND INSPECTOR SHALL REVIEW AND APPROVE ALL GRADING PLANS. THE CITY OF CHARLOTTE ENGINEERING DEPARTMENT PRIOR TO CONSTRUCTION DEVELOPMENT WILL PROVIDE STREET SIGNS PER GCDMS 506.0 (6" SIGNS ONLY) NIGHT AND DAY SIGNS SHALL BE PROVIDED FOR ALL INTERSECTIONS.

PROHIBIT VEHICULAR ACCESS TO BROWNE ROAD FROM LOTS #1 & 2 IS PROHIBITED. IN ROLLING AND HILLY TERRAINS, SWEEPING OF THE STONE BASE AND ADJACENT AREAS OF A TACKLE MAT SHALL BE REQUIRED. THE SPECIFIC REQUIREMENTS WILL BE ESTABLISHED BY THE INSPECTOR AND BASED ON FIELD CONDITIONS.

APPROVAL OF THIS PLAN IS NOT AN AUTHORIZATION TO GRADE ADJACENT PROPERTIES, WHEN FIELD CONDITIONS WARRANT OFF-SITE GRADING, PERMISSION MUST BE OBTAINED FROM THE AFFECTED PROPERTY OWNERS.

IN ORDER TO ENSURE PROPER DRAINAGE, KEEP A MINIMUM OF 0.5% SLOPE ON THE CURB.

SURFACE DRAINAGE FACILITIES MAY BE REQUIRED IN THE STREET.

RIGHT-OF-WAY IF DEEMED NECESSARY.

CURB AND UTILITY SHALL BE MAINTAINED ALONG BROWNE ROAD MAY BE ADJUSTED BASED UPON FIELD STAKING BY CITY ENGINEERING. ASSUMPTIONS: STORED DRAINAGE MAY ALSO REQUIRE MAINTENANCE BASED UPON REPAIRS TO THE DRAINAGE.

THE CITY OF CHARLOTTE ENGINEERING DEPARTMENT (SDE) IS TO PROVIDE STORM WATER CONVEYANCE AND ANY STRUCTURES AND/OR OBSTRUCTION TO STORM WATER FLOW IS PROHIBITED.

EXISTING AND PROPOSED MANHOLE (HOLE) STORM DRAINAGE PIPE INSTALLED WITHIN EXISTING OR PROPOSED PUBLIC STREET RIGHT-OF-WAY MUST BE APPROVED BY THE CITY'S INSPECTOR PRIOR TO BE APPROVED BY THE CITY INSPECTOR PRIOR TO PLACEMENT OF THE MATERIAL WITHIN THE PUBLIC STREET RIGHT-OF-WAY.

THE DEVELOPER SHALL BE RESPONSIBLE FOR THE PROTECTION OF THE CHANNEL OF ANY EXISTING OR PROPOSED STRUCTURED STORM AND SHALL REMOVE FROM THE CHANNEL AND BANKS ANY BUILDING MATERIAL DEBRIS, LOGS, TIMBER, JUNK AND OTHER ACCUMULATIONS.

ANY STREAM WITHIN 100 FEET OF THE PROPERTY SHALL BE PROTECTED. SUBJECT TO THE CITY OF CHARLOTTE ENGINEERING DEPARTMENT (CITY ENGINEERING) SUBDIVISION ORDINANCE, SECTION 7.200.8.

ANY CONSTRUCTION OR USE OF THE PROPERTY AREAS DESIGNATED AS COMMUNITY ENCROACHMENT OR USE OF THE PROPERTY AREAS DESIGNATED AS COMMUNITY ENCROACHMENT ARE SUBJECT TO THE RESTRICTIONS IMPOSED BY THE FLOODWAY REGULATIONS OF THE CITY OF CHARLOTTE AND MECHANICAL.

STRUCTURES (E.G., DOORS, WINDOWS, VENTS) IN STRUCTURES BUILT ON LOT #15-20, 32-39 SHOULD BE LOCATED A MINIMUM OF ONE FOOT ABOVE THE ADJACENT FINISHED GROUND SURFACE (APPLICABLE TO THE 100-YEAR FLOOD ANALYSIS).

OVERLAND AND UNDERGROUND DRAINAGE SHALL BE CONSIDERED IN THE FLOOD ANALYSIS.

PE SEALED SHOP DRAWINGS FOR RETAINING WALL MUST BE SUBMITTED TO CITY ENGINEER PRIOR TO CONSTRUCTION.

THE CITY OF CHARLOTTE ENGINEERING DEPARTMENT OF THE STORM DRAINAGE SYSTEM, INCLUDING DESIGNED DITCHES, MUST BE SUBMITTED PRIOR TO SUBDIVISION FINAL INSPECTOR TO THE CITY/CITY ENGINEERING DEPARTMENT IN CONFORMANCE WITH THE CITY OF CHARLOTTE ENGINEERING DEPARTMENT SUBDIVISION ORDINANCE.

PRIOR TO INSTALLATION, PE SEALED SHOP DRAWINGS FOR UNDERGROUND DETENTION SYSTEMS MUST BE FURNISHED TO CITY OF CHARLOTTE ENGINEERING FOR APPROVAL.

PRIOR TO CO. SURVEYOR SEALED AS-BUILT DRAWINGS OF UNDERGROUND DETENTION SYSTEMS MUST BE PROVIDED.

NON-STANDARD DETENTION SYSTEMS (E.G., DETENTION SYSTEMS, ETC.) IN THE RIGHT-OF-WAY REQUIRE A RIGHT-OF-WAY ENCROACHMENT AGREEMENT WITH THE CITY/CITY ENGINEERING DEPARTMENT OF TRANSPORTATION/NORTH CAROLINA DEPARTMENT OF TRANSPORTATION.

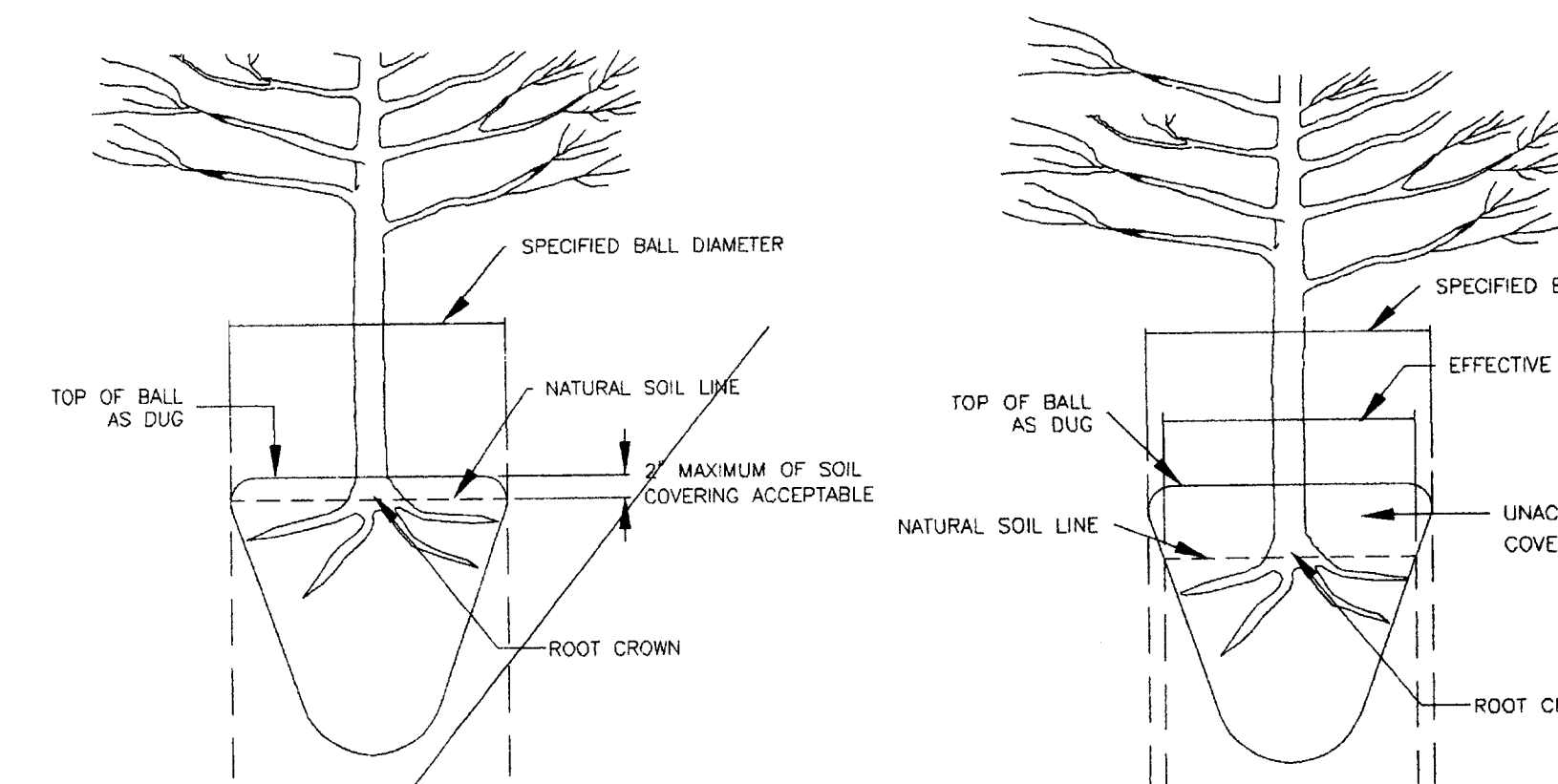
PRIOR TO PLOT RECORDATION, OFFSITE RW AND/OR CONSTRUCTION EASMENTS ARE REQUIRED TO BE OBTAINED ACCORDING TO THE GUIDELINES FOR CONSTRUCTION LIMITS ARE CLEARLY SHOWN ON THE ROADWAY IMPROVEMENT PLAN.

THE DEVELOPER SHALL CONTACT THE CHARLOTTE DEPARTMENT OF TRANSPORTATION PRIOR TO CONSTRUCTION TO DISCUSS ANY CONFLICTS WITH TRAFFIC SIGNALIZATION EQUIPMENT. 60-90 DAYS WILL BE REQUIRED TO COORDINATE ROAD EQUIPMENT. DEVELOPER SHALL BE RESPONSIBLE FOR ALL RELATED COSTS (NOT ASSIGNED TO THE CITY OF CHARLOTTE/DEVELOPER).

LOT DIMENSIONS ARE APPROXIMATE. REFERENCE RECORD FOR EXACT DIMENSIONS.

ON-SITE BURIAL PITS REQUIRE AN ON-SITE DETENTION LANDFILL PERMIT FROM THE ZONING ADMINISTRATOR.

THE DATE OF COMPLETION OF INFRASTRUCTURE: 24 MONTHS



NOTE:
A ROOT COLLAR EXCAVATION FOR ALL TREES SPECIFIED
WILL BE DONE BY THE CITY ARBORIST TO ENSURE THAT
TREES WERE NOT PLANTED/GROWN TOO DEEPLY AT SOURCE
(NURSERY). LANDSCAPE CONTRACTOR SHALL HAVE SUPPLIER
MARK GROUND LEVEL LINE ABOVE ROOT BALL. IF CITY ARBORIST
DETERMINES THAT THERE IS EXCESSIVE SOIL OVER THE ROOT
CROWN, THESE TREES WILL BE REJECTED.

THREE ROOT BALL CONDITION ON TREES FROM SWEETGUMS

NOTE:
ACCESS TO FOSTER PROPERTIES VIA EXIST
ALLEN BROWN ROAD IS TO BE MAINTAINED
AT ALL TIMES DURING CONSTRUCTION. AFTER
CONSTRUCTION AND ACCEPTANCE OF
RELOCATED ALLEN BROWN ROAD, EXISTING
ASPHALT TO BE REMOVED AND R/W
ABANDONED.

This Plan is A Final Design.
NOT Released For Construction
 Unless Initialed & Dated As
 Approved:
 APPROVED: _____
 Initials

