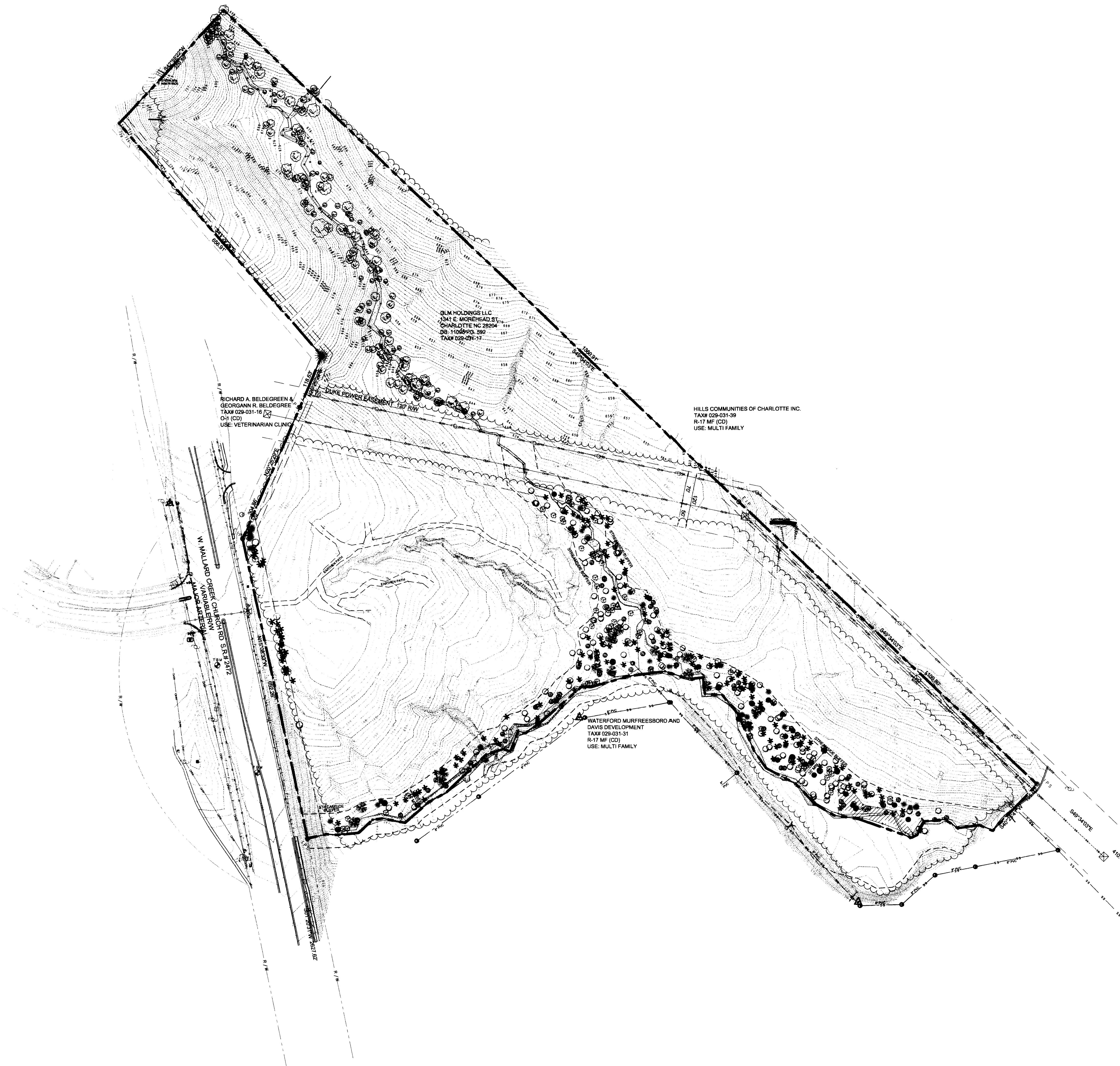
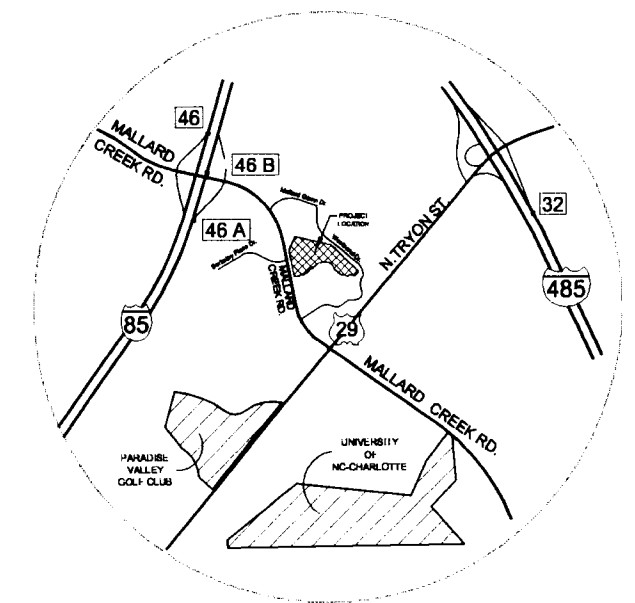


ALL SURVEY DATA COMPILED AND COORDINATED BY LANDDESIGN INC., (223 N. GRAHAM ST. CHARLOTTE, NORTH CAROLINA 28201).

NTS



223 N Graham Street Charlotte, NC 28202
V: 704.333.0325 F: 704.332.3246
www.LandDesign.com

10-09-07

Phillips Development & Realty, LLC., Charlotte North Carolina

EXISTING CONDITIONS

DATE: MARCH 27, 2007
DESIGNED BY: CCM
DRAWN BY: GPP/SKJ
CHECKED BY: CCM
Q.C. BY: CCM
SCALE: 1"=50'-0"
PROJECT #: 1006247

SHEET #: **L-1.0**

RICHARD A. BELDEGREEN &
GEORGANN R. BELDEGREE
TAX# 029-031-16
O-1 (CD)
USE: VETERINARIAN CLINIC

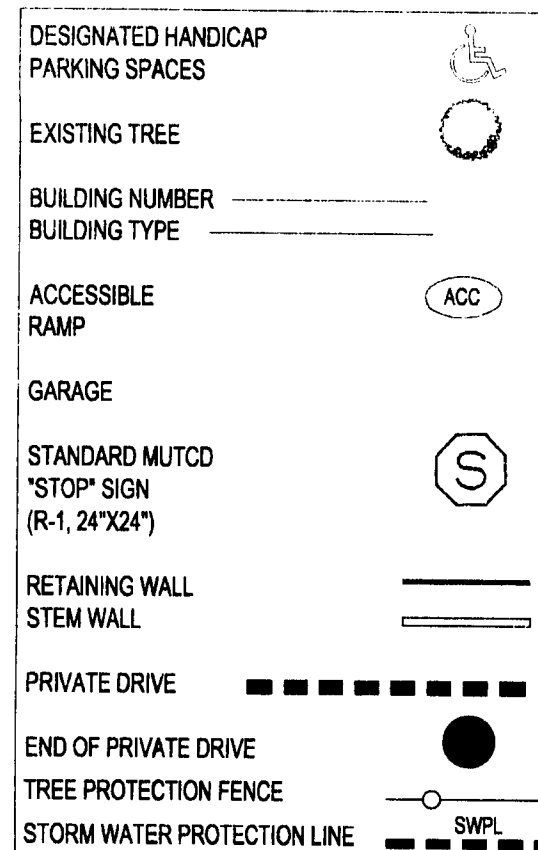
HILLS COMMUNITIES OF CHARLOTTE INC.
TAX# 029-031-39
R-17 MF (CD)
USE: MULTI FAMILY

WATERFORD MURFREESBORO AND
DAVIS DEVELOPMENT
TAX# 029-031-31
R-17 MF (CD)
USE: MULTI FAMILY

SITE DEVELOPMENT DATA

ZONING PETITION NUMBER	99-37C
TOTAL SITE AREA	1,315,954 SF / 30.21 AC
PHASE ONE	857,180 SF / 19.58 AC
PHASE TWO	458,774 SF / 10.53 AC
EXISTING ZONING	R-17 MF (CD)
TAX PARCEL ID	005501-17
PROPOSED PERMITTED USE	RESIDENTIAL APARTMENTS FOR RENT
TOTAL NUMBER OF EXISTING BUILDINGS	0
PROPOSED BUILDINGS	22 TOTAL (12 MULTIFAMILY + 10 GARAGES + 1 CLUBHOUSE)
DENSITY: 16.67 DU PER AC	328 UNITS (12 BUILDINGS)
EXISTING IMPERVIOUS COVERAGE	0 SF
PROPOSED IMPERVIOUS COVERAGE PHASE ONE	356,731 SF / 8.10%
PROPOSED BUILDING COVERAGE PHASE ONE	110,665 SF
ROAD & PARKING COVERAGE PHASE ONE	120,965 SF
PROPOSED IMPERVIOUS COVERAGE PHASE TWO	9,040 SF / 1.9%
PROPOSED BUILDING COVERAGE PHASE TWO	0 SF
ROAD & PARKING COVERAGE PHASE TWO	9,040 SF
PARKING SPACES REQUIRED	492 SPACES @ 1.5 SPACES PER UNIT
PARKING SPACES PROVIDED	502 SPACES @ 1.32 SPACES PER UNIT
STANDARD SPACES	411 STANDARD
COMPACT SPACES	19 @ 3.8%
HANDICAP SPACES REQUIRED	17 SPACE (TWO VAN ACCESSIBLE)
HANDICAP SPACES PROVIDED	20 SPACES (FOUR VAN ACCESSIBLE)
GARAGE SPACES PROVIDED	52 SPACES (4 HANDICAP)
BICYCLE PARKING REQUIRED	17 @ 1 SPACE PER 20 UNITS
BICYCLE PARKING PROVIDED	17 (REF 811.3.2)
PRIVATE STREET PARKING CALCULATIONS:	
BERKLEY PLACE DR. (409.78' ± 20' BY ALLOWABLE PARKING)	4 SPACES (59')
MINGLEWOOD DR. (1381.14' ± 880.5' ALLOWABLE PARKING)	75 SPACES (854.5')
RIPPLE WAY (114.45' ± 57.22' ALLOWABLE PARKING)	6 SPACES (51.0')
BUILDING HEIGHT:	
TYPE 100	57'-0" TO ROOF APEX (BUILDING 3/100, NO SPLIT, 48'-0" TO ROOF APEX)
TYPE 200	57'-9" TO ROOF APEX
TYPE 300	56'-9" TO ROOF APEX
TYPE 400	56'-7" TO ROOF APEX (BUILDING 1400, NO SPLIT, 45'-9" TO ROOF APEX)
SETBACK:	35'
SIDE YARD:	10'
REAR YARD:	40'
OPEN SPACE PHASE ONE	11.4 AC / 57.9%

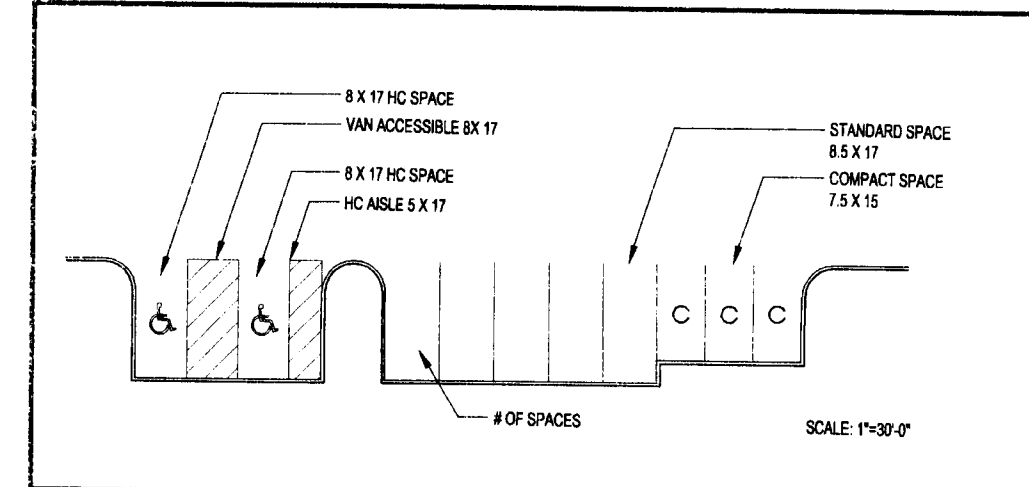
LEGEND



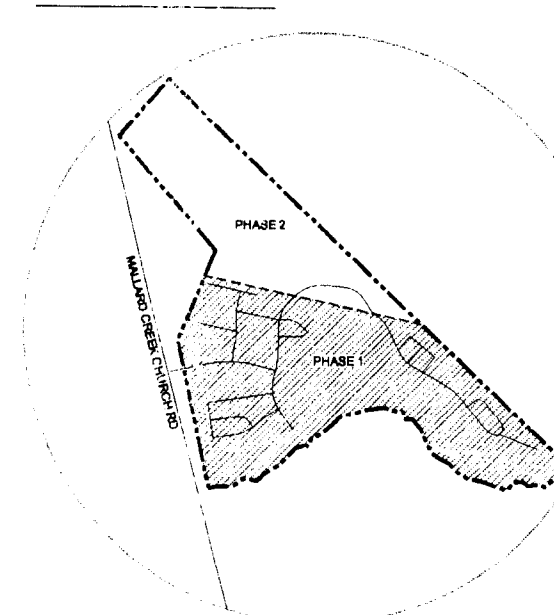
SOLID WASTE AND RECYCLING

SOLID WASTE REQUIRED:	(1) 8 C.Y. COMPACTOR PER 80 UNITS=28 C.Y.
RECYCLING REQUIRED:	(1) 144 S.F. RECYCLING STATION PER 80 UNITS = 720 S.F.
SOLID WASTE PROVIDED:	1.28 C.Y. COMPACTOR
RECYCLING AREA PROVIDED:	720 S.F. (5-144S.F. RECYCLING STATION)

PARKING LEGEND

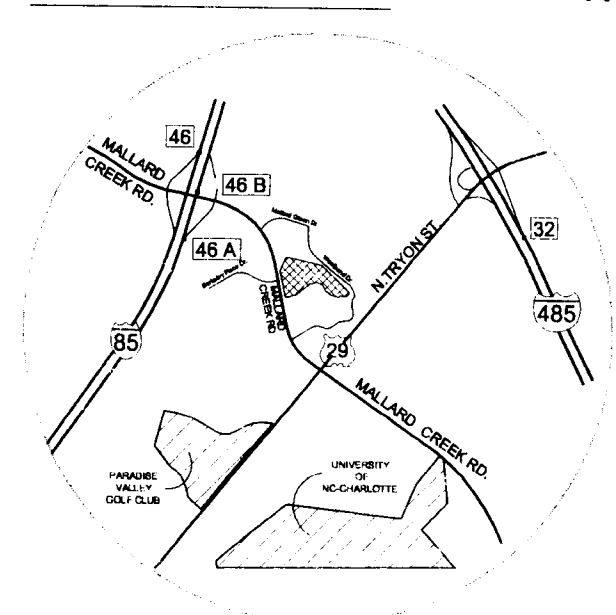


SITE KEY



NTS

VICINITY MAP



NTS

DATE: March 27th, 2023
DESIGNED BY: CCH
CHECKED BY: CCH
C.C.BY: CCH
PROJECT #: 100247

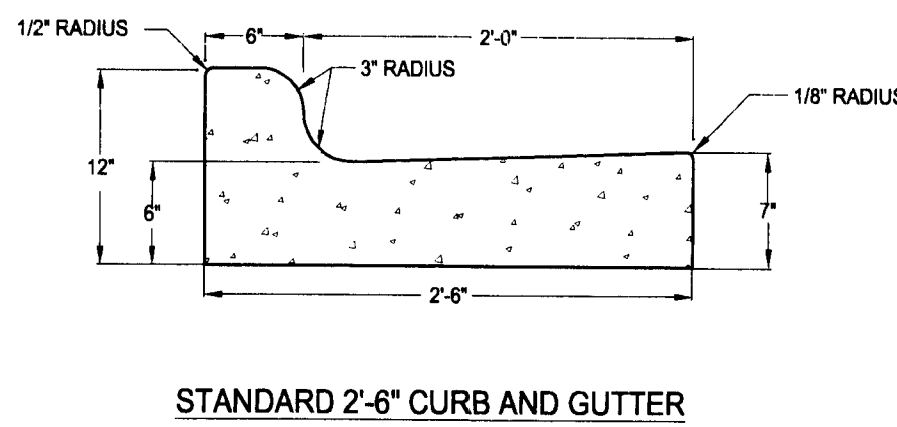
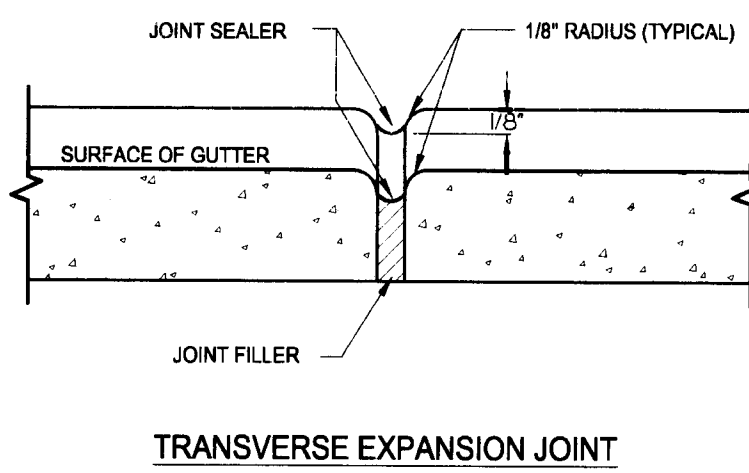
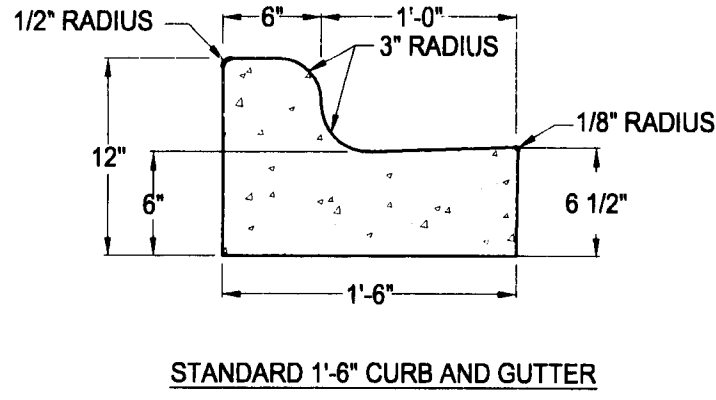
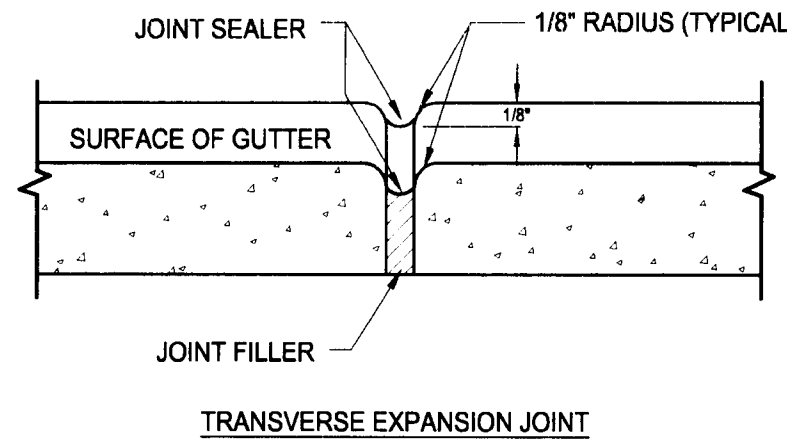
MALLARD CREEK
Residential Apartments
Phillips Development & Realty, LLC, Charlotte North Carolina
LAYOUT PLAN

LandDesign®
233 N Graham Street, Charlotte, NC 28202
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www.landdesign.com



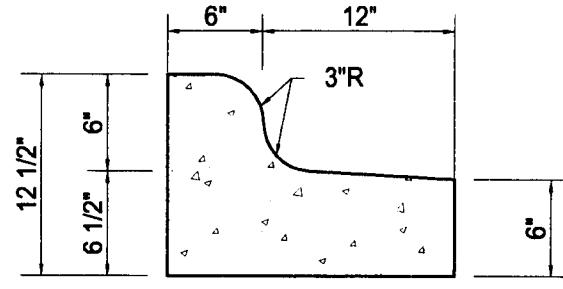
11-07-07

L-2.0



NOTES:

1. CONTRACTION JOINTS SHALL BE SPACED AT 10-FOOT INTERVALS. FOR VALLEY GUTTER, A 10-FOOT SPACING MAY BE USED WHEN A MACHINE IS USED. JOINT SPACING MAY BE ALTERED BY THE ENGINEER TO PREVENT UNCONTROLLED CRACKING.
2. CONTRACTION JOINTS MAY BE INSTALLED BY THE USE OF TEMPLATES OR FORMED BY OTHER APPROVED METHODS. WHERE SUCH JOINTS ARE NOT FORMED BY TEMPLATES, A MINIMUM DEPTH OF 1 1/2\"/>
3. ALL EXPANSION JOINTS SHALL BE SPACED AT 90-FOOT INTERVALS, AND ADJACENT TO ALL RIGID OBJECTS. JOINTS SHALL MATCH LOCATIONS WITH JOINTS IN ABUTTING SIDEWALK.
4. CONCRETE COMPRESSIVE STRENGTH SHALL BE 3600 P.S.I. IN 28 DAYS.
5. CURB SHALL BE DEPRESSED AT INTERSECTIONS TO PROVIDE FOR FUTURE ACCESSIBLE RAMPS.
6. TOP 6\"/>



TO BE USED WHEN PAVEMENT SLOPES AWAY FROM CURB OR AS SPECIFIED BY THE APPROPRIATE CITY OR COUNTY ENGINEERING DEPARTMENT. SEE GRADING PLANS FOR APPROPRIATE CONDITION.

NOTES:

1. CONTRACTION JOINTS SHALL BE SPACED AT 10-FOOT INTERVALS. FOR VALLEY GUTTER, A 10-FOOT SPACING MAY BE USED WHEN A MACHINE IS USED. JOINT SPACING MAY BE ALTERED BY THE ENGINEER TO PREVENT UNCONTROLLED CRACKING.
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5. CURB SHALL BE DEPRESSED AT INTERSECTIONS TO PROVIDE FOR FUTURE ACCESSIBLE RAMPS.
6. TOP 6\"/>

1 1'-6" CURB AND GUTTER
L3.0 SECTION

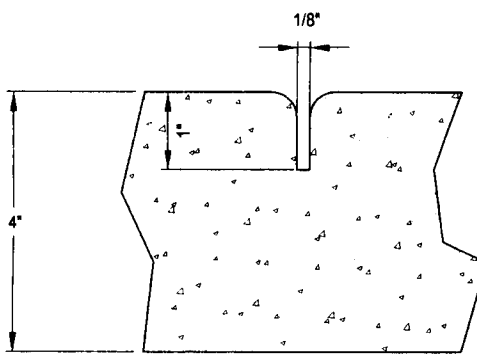
CITY STD# 10.17A
NOT TO SCALE

2 2'-6" CURB AND GUTTER
L3.0 SECTION

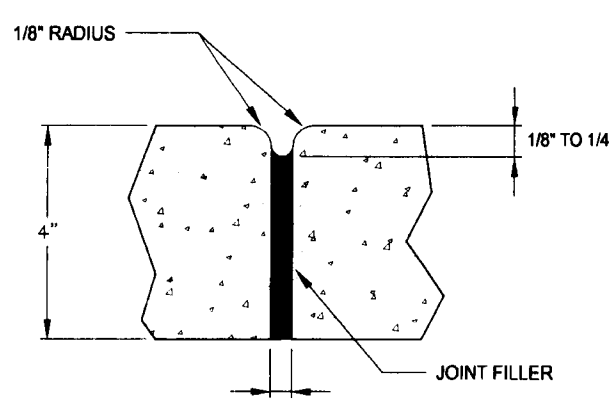
CITY STD# 10.17A
NOT TO SCALE

3 DETAIL NOT USED
L3.0

NOT TO SCALE



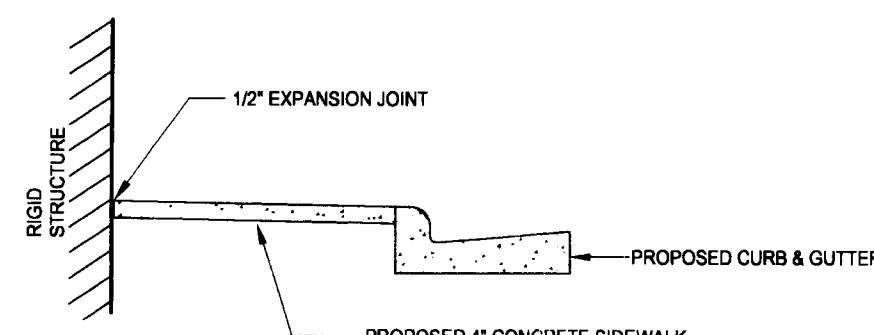
GROOVE JOINT IN SIDEWALK



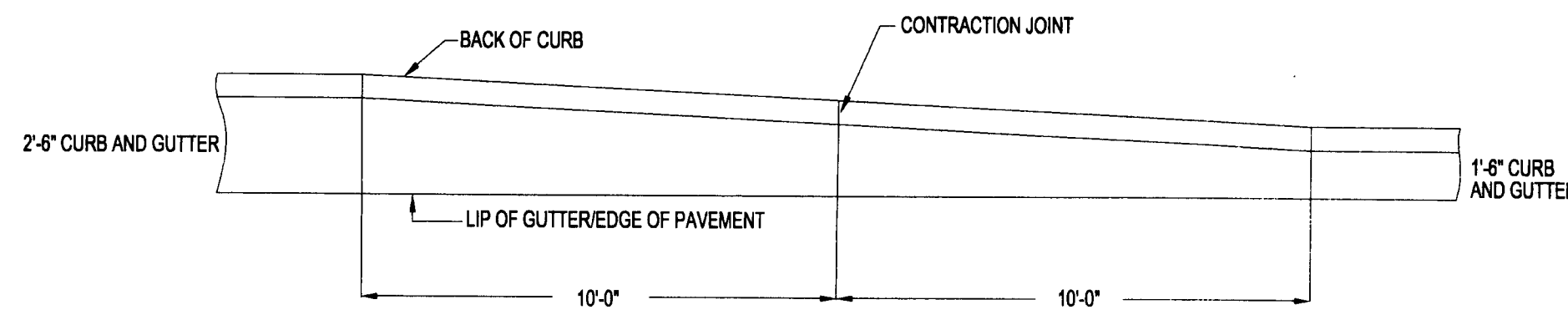
TRANSVERSE EXPANSION JOINT IN SIDEWALK

GENERAL NOTES:

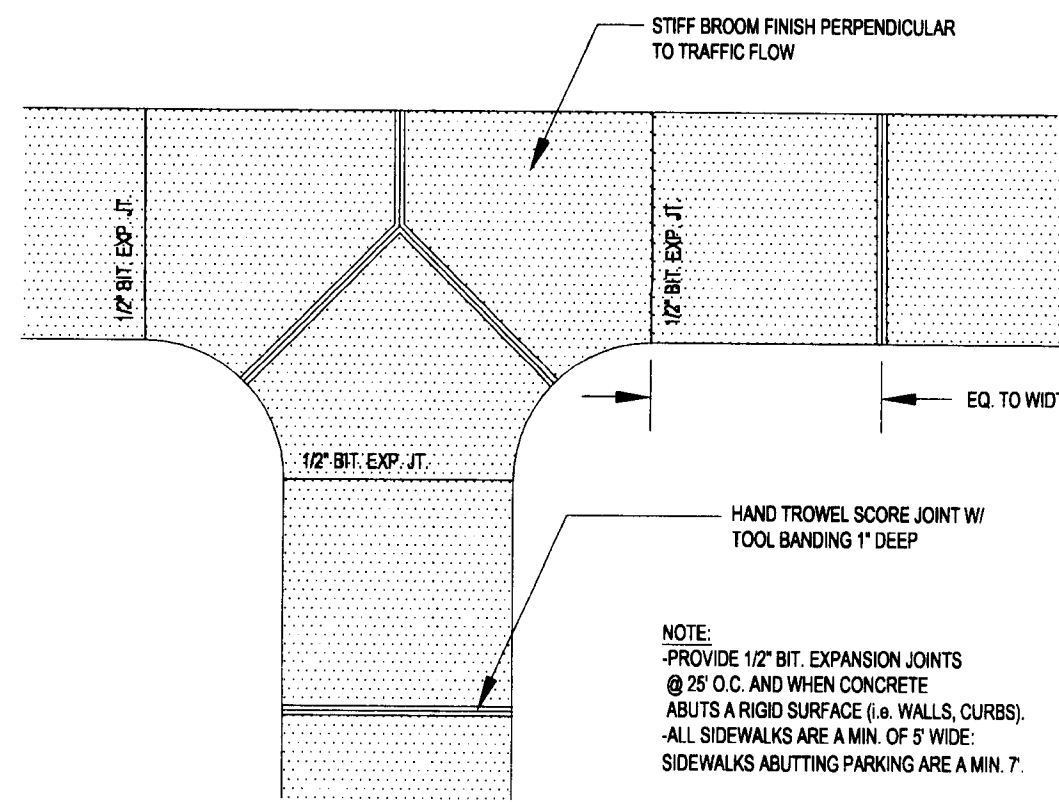
1. A GROOVE JOINT 1\"/>
2. SIDEWALK AT DRIVEWAY ENTRANCES TO BE 6\"/>
3. WIDTH OF SIDEWALK ON THOROUGHFARE STREETS SHALL BE A MINIMUM OF 5\"/>
4. WIDTH OF SIDEWALKS ON NON-THOROUGHFARE STREETS SHALL BE A MINIMUM OF 4\"/>
5. SIDEWALK TO BE POURED TO END OF RADIUS AT INTERSECTING STREETS.
6. CONCRETE COMPRESSIVE STRENGTH SHALL BE 3600 PSI. IN 28 DAYS.



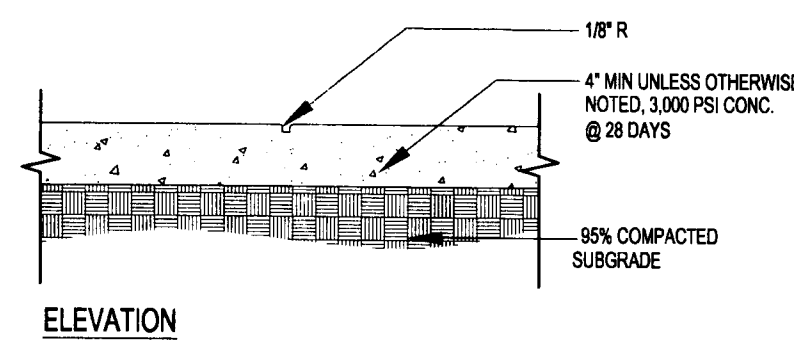
DETAILS SHOWING EXPANSION JOINTS IN CONCRETE SIDEWALK



- NOTES:
1. TRANSITION TO BE ALONG BACK OF CURB.



PLAN: TYPICAL SCORING PATTERN @ INTERSECTIONS



ELEVATION

4 EXPANSION JOINT
L3.0 SECTION

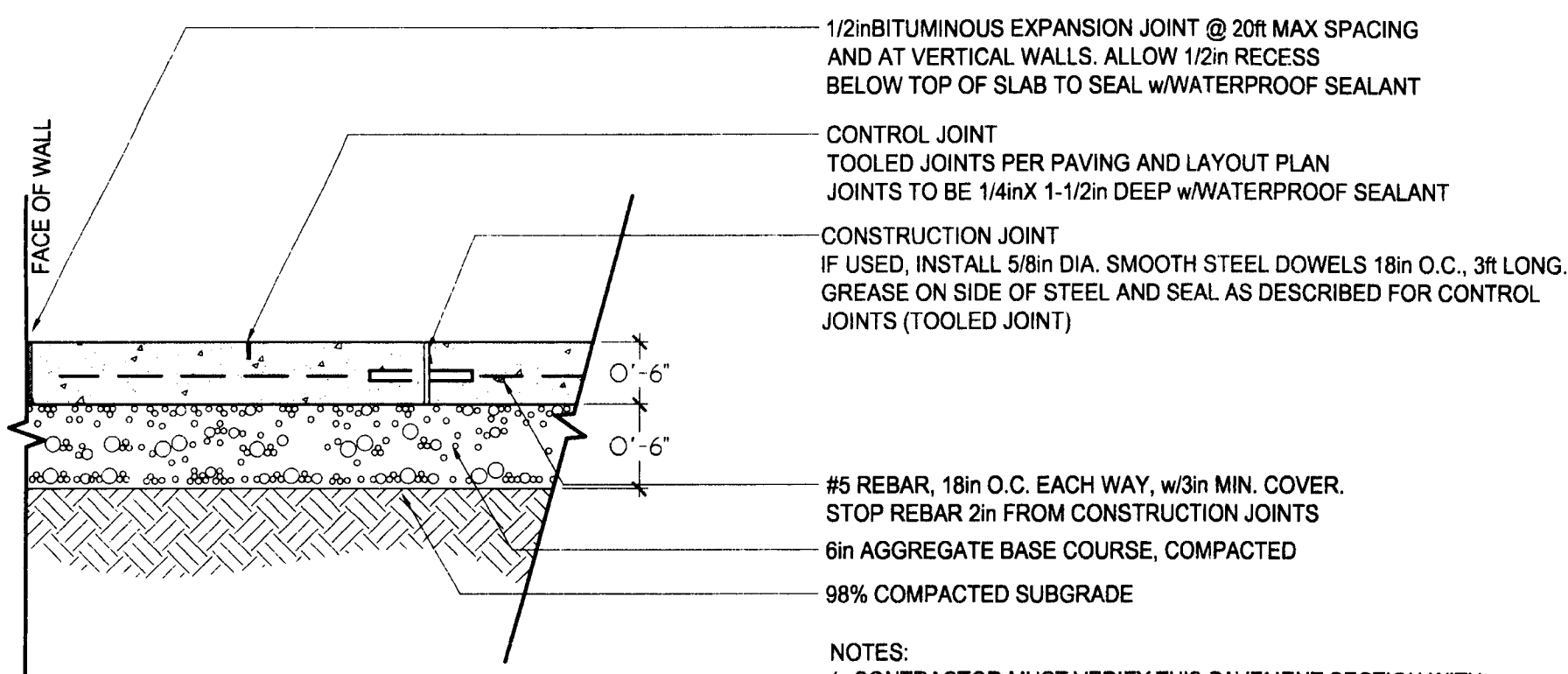
CITY STD# 10.22
NOT TO SCALE

5 CURB TRANSITION
L3.0 PLAN

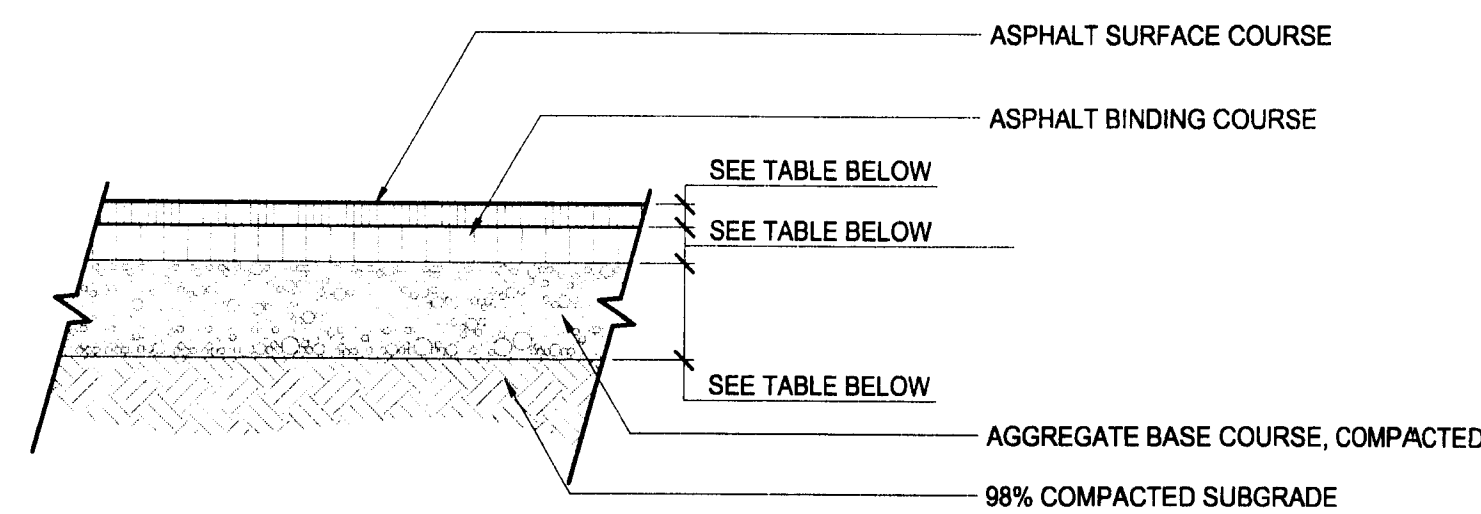
CITY STD# 10.20
NOT TO SCALE

6 CONCRETE SIDEWALK
L3.0 PLAN / ELEVATION

NOT TO SCALE



- NOTES:
1. CONTRACTOR MUST VERIFY THIS PAVEMENT SECTION WITH CONSULTING GEOTECH PRIOR TO INSTALLATION.
 2. SEE LAYOUT PLAN FOR CONTROL JOINT AND EXPANSION JOINT SPACING.
 3. CONCRETE SHALL BE 3600psi @ 28 DAYS.
 4. WATERPROOFING SEALANT TO MATCH CONCRETE COLOR



ASPHALT PAVING		
NOTE		
	REG	HEAVY
1. SURFACE COURSE	2"	2"
1-2 SURFACE COURSE		
2. BINDING COURSE	2"	3"
HB BINDING COURSE		
2. BASE COURSE	6"	8"
C.A.B.C.		

- NOTES:
1. GEOTECHNICAL ENGINEER SHALL REVIEW AND APPROVE ASPHALT PAVEMENT DESIGN PER ACTUAL SOIL CONDITIONS AND OWNERS LOADING REQUIREMENTS.
 2. ALL DRIVES SHALL BE HEAVY DUTY ASPHALT. ALL PARKING BAYS SHALL BE REGULAR DUTY ASPHALT

7 CONCRETE PAVING
L3.0 SECTION

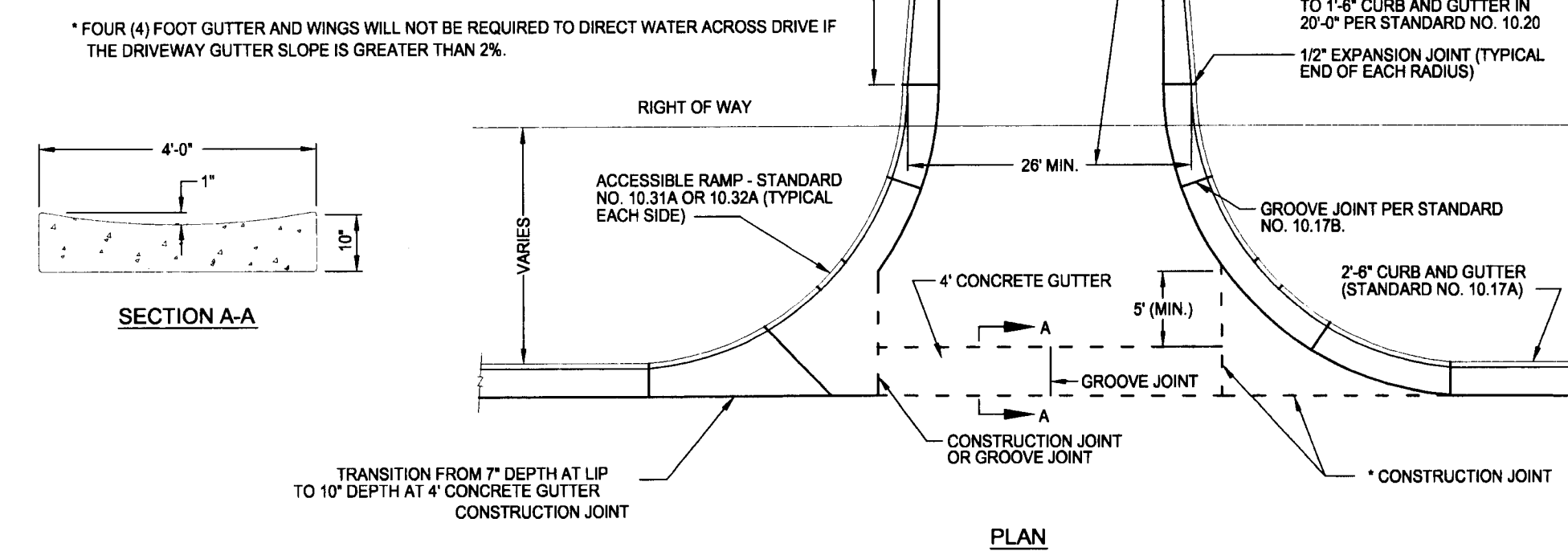
SCALE: NTS

8 REGULAR AND HEAVY DUTY ASPHALT PAVEMENT
L3.0 SECTION

SCALE: 3/4" = 1' - 0"

NOTES:

- WHERE A TYPE III DRIVEWAY IS APPROVED BY THE CHARLOTTE DEPARTMENT OF TRANSPORTATION, OR AT A LOCATION WHERE A TRAFFIC SIGNAL INSTALLATION IS PROPOSED BY CDDT BASED ON A TRAFFIC IMPACT/SECTIONAL WARRANT STUDY, A FULL DEPTH ASPHALT PAVEMENT (2" OF 5.5 S.B.C. AND 6" B-20 S.B.C.) IS REQUIRED. THIS PAVEMENT DESIGN IS REQUIRED IN THE DRIVEWAY EASEMENT (10'-0" FOOT MINIMUM) TO MAINTAIN DETECTOR COILS AND PAVEMENT MARKINGS. A TRAFFIC SIGNAL WILL BE INSTALLED ONLY IF CDDT DETERMINES THAT ONE IS NECESSARY BASED ON A TRAFFIC STUDY OF CURRENT CONDITIONS.
- A CONCRETE GUTTER IS TO BE USED EXCEPT AT EXISTING OR PROPOSED TRAFFIC SIGNAL LOCATIONS. AT THESE LOCATIONS ADDITIONAL DRAINAGE REQUIREMENTS WILL BE NECESSARY TO ELIMINATE THE NEED FOR GUTTER ACROSS THE DRIVEWAY CONNECTIONS.
- THE DRIVEWAY MUST RISE 6" FROM THE GUTTER LINE TO PREVENT RUNOFF FROM ENTERING DRIVEWAY.
- ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE, AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
- TWO (2) ACCESSIBLE RAMPS PER CURB RETURN REQUIRED AT SIGNALIZED INTERSECTIONS.
* FOUR (4) FOOT GUTTER AND WINGS WILL NOT BE REQUIRED TO DIRECT WATER ACROSS DRIVE IF THE DRIVEWAY GUTTER SLOPE IS GREATER THAN 2%.



1 TYPE III DRIVEWAY

L3.1 PLAN

CLDSM #10.28
NOT TO SCALE

2 PRIVATE INTERIOR CIRCULATION STREET

L3.1 SECTION

NOT TO SCALE

3 NCDOT WHEELCHAIR RAMP

L3.1 PLAN / ELEVATION

NOTE: FOR USE AT MALLARD CREEK CHURCH RD ONLY.

NOT TO SCALE

NOTES:

- CONSTRUCT THE WALKING SURFACE WITH SLIP RESISTANCE AND A 70% CONTRASTING COLOR TO THE SIDEWALK.
- CROSSWALK WIDTHS AND CONFIGURATION VARY BUT MUST CONFORM TO TRAFFIC DESIGN STANDARDS.
- NORTH CAROLINA GENERAL STATUTE 136-44.14 REQUIRES THAT ALL STREET CURBS BEING CONSTRUCTED OR RECONSTRUCTED FOR MAINTENANCE PURPOSES, TRAFFIC OPERATIONS, REPAIRS, CORRECTION OF UTILITIES OR ALTERED FOR ANY REASON AFTER SEPTEMBER 1, 1973, SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICALLY CHALLENGED AT ALL INTERSECTIONS WHERE BOTH CURB AND GUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER POINTS OF THE PEDESTRIAN FLOW.
IN ADDITION, SECTION 228 OF THE 1973 FEDERAL AID HIGHWAY SAFETY ACT REQUIRES PROVISION OF CURB RAMPS ON ANY CURB CONSTRUCTION AFTER JULY 1, 1978 WHETHER A SIDEWALK IS PROPOSED INITIALLY OR IS PLANNED FOR A FUTURE DATE.
THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990 EXTENDS TO INDIVIDUALS WITH DISABILITIES. COMPREHENSIVE CIVIL RIGHTS PROTECTIONS SIMILAR TO THOSE GRANTED TO PERSONS ON THE BASIS OF RACE, SEX, NATIONAL ORIGIN AND RELIGION UNDER THE CIVIL RIGHTS ACT OF 1964. THESE CURB RAMPS HAVE BEEN DESIGNED TO COMPLY WITH THE CURRENT ADA STANDARDS.
- PROVIDE WHEELCHAIR RAMPS AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATE WHEELCHAIR RAMPS AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANT, DROP INLETS, ETC. ARE PLACED. PLACE DUAL RAMPS WHEN RAMPS ARE INSTALLED TO PLACE NOT LESS THAN 2 FEET OF FULL HEIGHT CURB BETWEEN THE RAMPS. PLACE DUAL RAMPS AS NEARLY AS POSSIBLE TO THE TRAVEL LANE BEING CROSSED AS POSSIBLE.
- PAY FOR ALL VARIABLE DEPTH CONCRETE USED FOR CONSTRUCTION OF WHEELCHAIR RAMPS AS CONCRETE WHEELCHAIR RAMPS. (50 YDS.)
- PAY FOR ALL DEEPER CURBS AT WHEELCHAIR RAMPS AS THE TYPE CURB AND GUTTER USED ADJACENT TO DEEPER CURB. (14 FT.)
- SUCH PRICES AND PAYMENTS IS CONSIDERED FULL COMPENSATION FOR ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO SATISFACTORILY COMPLETE THE WORK.
- DO NOT EXCEED 0.08 (1/12.1) SLOPE ON THE WHEELCHAIR RAMP IN RELATIONSHIP TO THE GRADE OF THE STREET.
- CONSTRUCT WHEELCHAIR RAMPS 40" (3'-4") OR GREATER FOR DUAL RAMPS.
- USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
- PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB AND AS SHOWN ON STD. DWG. 848.01.
- PLACE THE INSIDE PEDESTRIAN CROSSWALK LINES NO CLOSER IN THE INTERSECTION BY BISECTING THE INTERSECTION RADIUS, WITH ALLOWANCE OF A 4" CLEAR JOINT IN THE VERTICAL CURB TRAVELING WHEN ONE RAMP IS INSTALLED. (SEE NOTE 17)
- COORDINATE THE CURB CUT AND THE PEDESTRIAN CROSSWALK LINES SO THE FLOOR OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES. PLACE DIAGONAL RAMPS WITH FLARED SIDES SO 24" OF FULL HEIGHT CURB FALLS WITHIN THE CROSSWALK MARKINGS ON EACH SIDE OF THE FLARES.
- CONSTRUCT THE PEDESTRIAN CROSSWALK A MINIMUM OF 6 FEET, A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRABLE.
- USE STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, WHERE IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEN MAY REQUIRE THE PLACEMENT OF THE STOP LINE TO BE PARALLEL TO THE INTERSECTING ROADWAY.
- TERMINATE PARKING A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
- PLACE ALL PAVEMENT MARKINGS IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.

4 NCDOT WHEELCHAIR RAMP

L3.1 NOTES

NOTE: FOR USE AT MALLARD CREEK CHURCH RD ONLY.

NOT TO SCALE

5 ACCESSIBLE PARKING AND SIGNAGE STANDARDS

L3.1

CLDSM #50.10A

NOT TO SCALE

6 ACCESSIBLE RAMP

L3.1 SECTION

CLDSM #10.32B

NOT TO SCALE

NOTES:

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7 ACCESSIBLE RAMP

L3.1 PLAN

CLDSM #10.34A
NOT TO SCALE

8 PAVEMENT MARKINGS

L3.1 PLAN

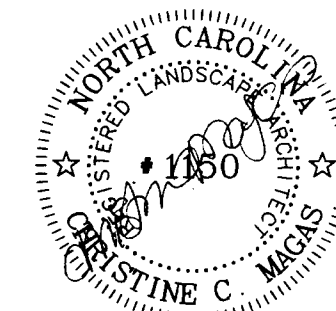
NOT TO SCALE

9 HANDRAIL

L3.1 SECTION

CLDSM #50.04A
NOT TO SCALE

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223 N Graham Street, Charlotte, NC 28202
704.334.1235
www.landdesign.com

10-09-07

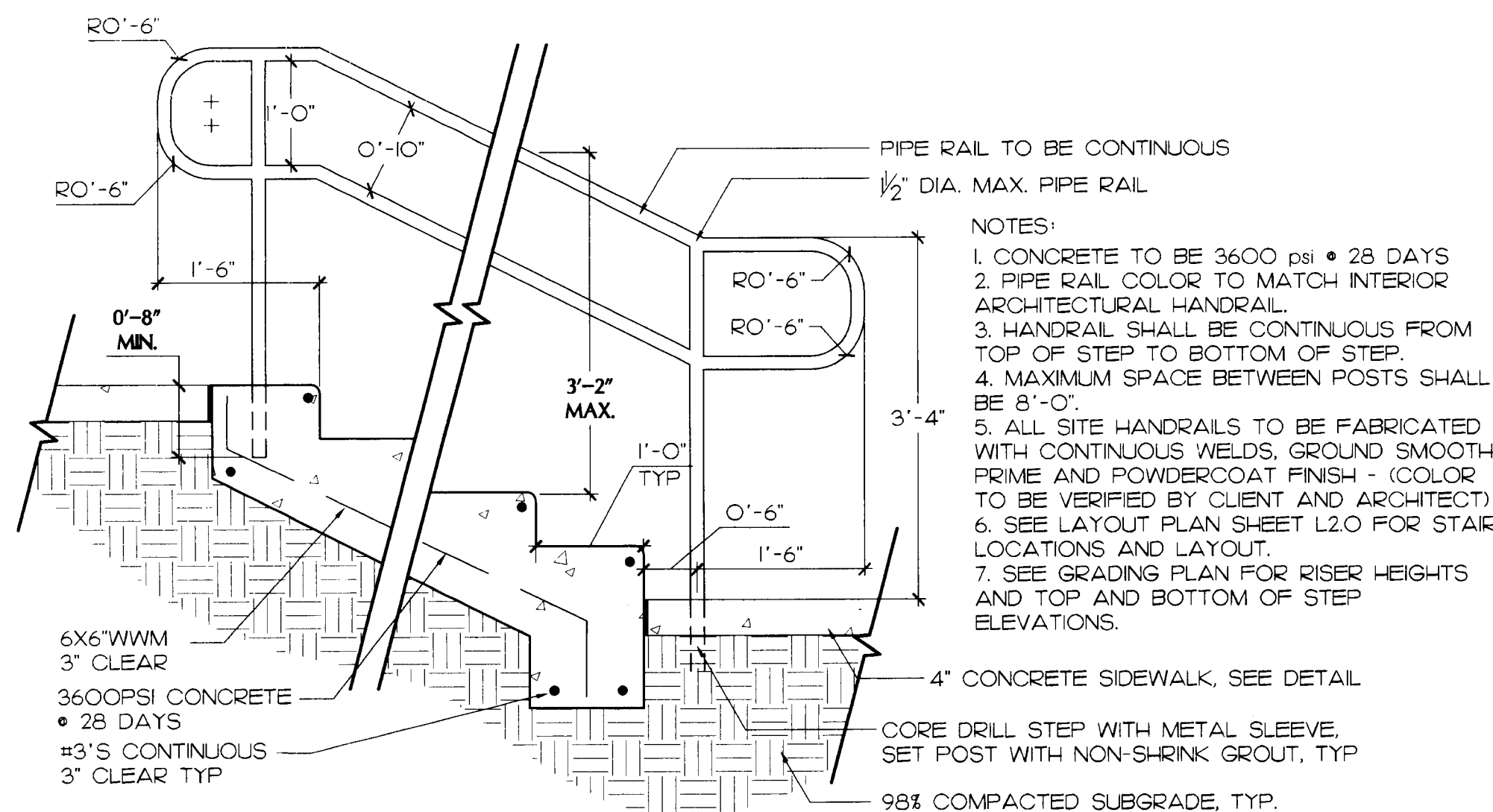
MALLARD CREEK
Residential Apartments

Phillips Development & Realty, LLC, Charlotte North Carolina

SITE DETAILS

REVISIONS:
1. 05/14/07 - CHPC/ENGINEERING REUBEN RITTAL
2. 07/17/07 - CHPC/ENGINEERING REUBEN RITTAL
3. 08/15/07 - CHPC/ENGINEERING REUBEN RITTAL
4. 09/15/07 - CHPC/ENGINEERING REUBEN RITTAL
5. 10/09/07 - CHPC/ENGINEERING REUBEN RITTALDATE: March 27th, 2007
DRAWN BY: GCH
CHECKED BY: GCH
SCALE: AS SHOWN
PROJECT #: 1006247
SHEET #

L-3.1



1 TYPICAL HANDRAIL w/STEPS
L3.2 SECTION

SCALE: 3/4" = 1'-0"

2 NOT USED
L3.2

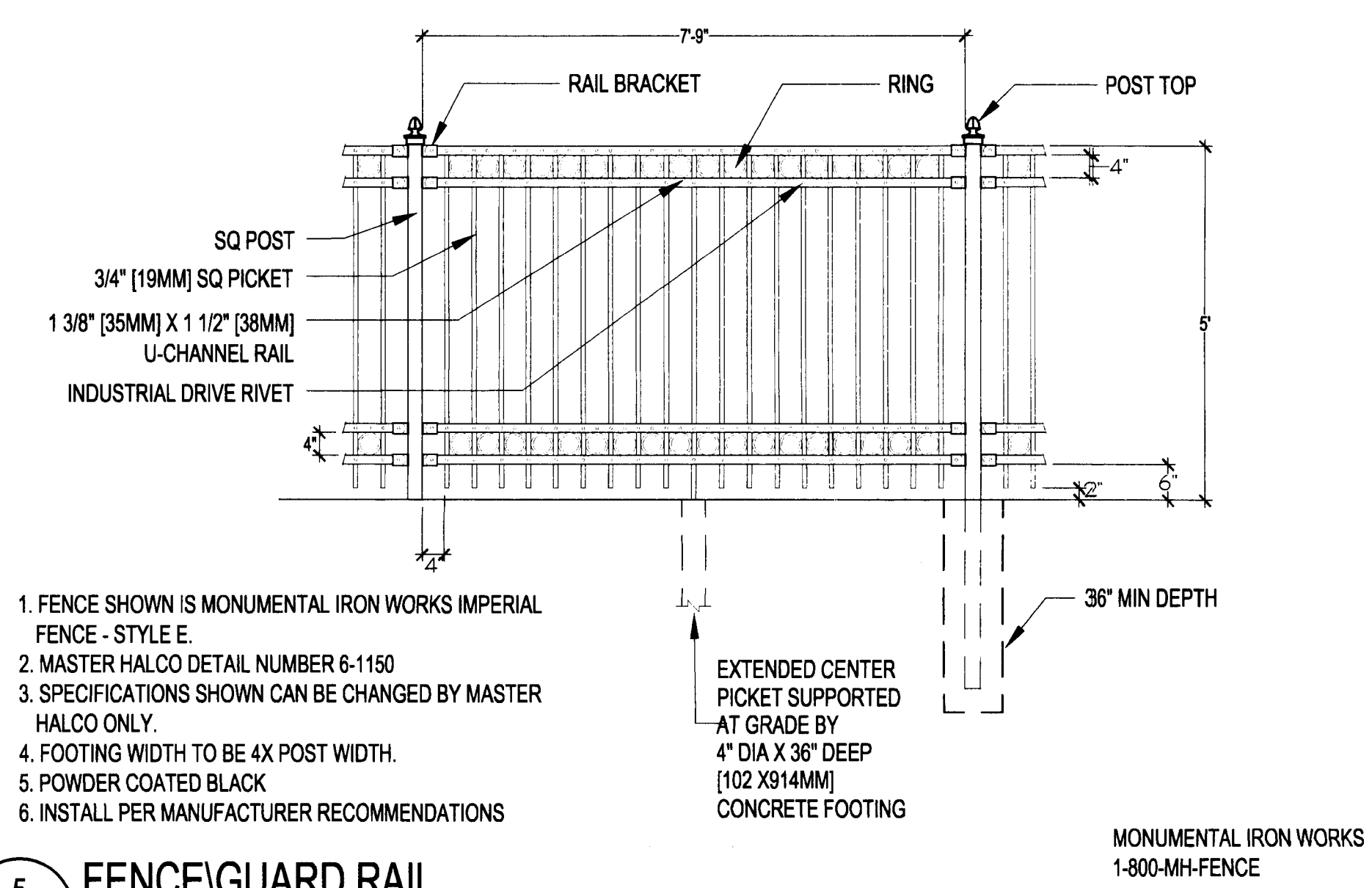
3 NOT USED
L3.2

WARRANTS		
HANDRAIL SHALL BE INSTALLED UNDER ANY OF THE FOLLOWING CIRCUMSTANCES IN BOTH NEW CONSTRUCTION AND IN RETROFITTING OR RECONSTRUCTION OF EXISTING ROADWAYS OR SITES:		
1. WHEN THE CULVERT-CROSSING DETAIL (STD. #10.36A-B) APPLIES.		
2. IF THERE IS A 2:1 OR STEEPER FILL SLOPE THAT IS 10 FEET OR TALLER THAT BEGINS WITHIN 5 FEET OF A SIDEWALK.		
3. IN ANY OF THE FOLLOWING COMBINATIONS OF DROPOFF AND OFFSET FROM SIDEWALK:		
a. 18" OR LARGER DROPOFF WITHIN 2 FEET OF THE EDGE OF THE SIDEWALK		
b. 36" OR LARGER DROPOFF WITHIN 4 FEET OF THE EDGE OF THE SIDEWALK		
c. 60" OR LARGER DROPOFF WITHIN 6 FEET OF THE EDGE OF THE SIDEWALK		
THESE CLEARANCES ASSUME THAT THE CROSS-SLOPE OF THE BERM BETWEEN THE SIDEWALK AND THE DROPOFF (PEDESTRIAN CLEAR ZONE) IS 6:1 OR FLATTER.		
4. AT THE TOP OF ANY DROPOFF WHERE PEDESTRIANS CAN REASONABLY BE EXPECTED IN THE VICINITY.		
5. AT THE DIRECTION OF CDOT OR ENGINEERING & PROPERTY MANAGEMENT STAFF BASED ON FIELD CONDITIONS.		
FOR PURPOSES OF THIS STANDARD, THE TERM "SIDEWALK" IS USED GENERICALLY AND SHALL MEAN ANY PATH OR SURFACE TO BE USED FOR BICYCLE AND/OR PEDESTRIAN TRANSPORTATION. EXAMPLES INCLUDE, BUT ARE NOT LIMITED TO, SIDEWALKS, BIKE PATHS, SHARED-USE PATHS, PEDESTRIAN PATHS, AND GREENWAYS.		
DEFINITIONS		
• DROPOFF — A SLOPE OF 2:1 OR STEEPER. EXAMPLES INCLUDE HEADWALLS, RETAINING WALLS, AND CULVERTS.		
• PEDESTRIAN CLEAR ZONE, — 10 FEET OF ANY COMBINATION OF SIDEWALK, SLOPE, AND SHOULDER SLOPED AT 6:1 OR FLATTER. SIDEWALK DOES NOT NEED TO BE PRESENT.		
CITY OF CHARLOTTE LAND DEVELOPMENT STANDARDS INCLUDES CHARLOTTE #71		HANDRAIL WARRANTS 50.04B

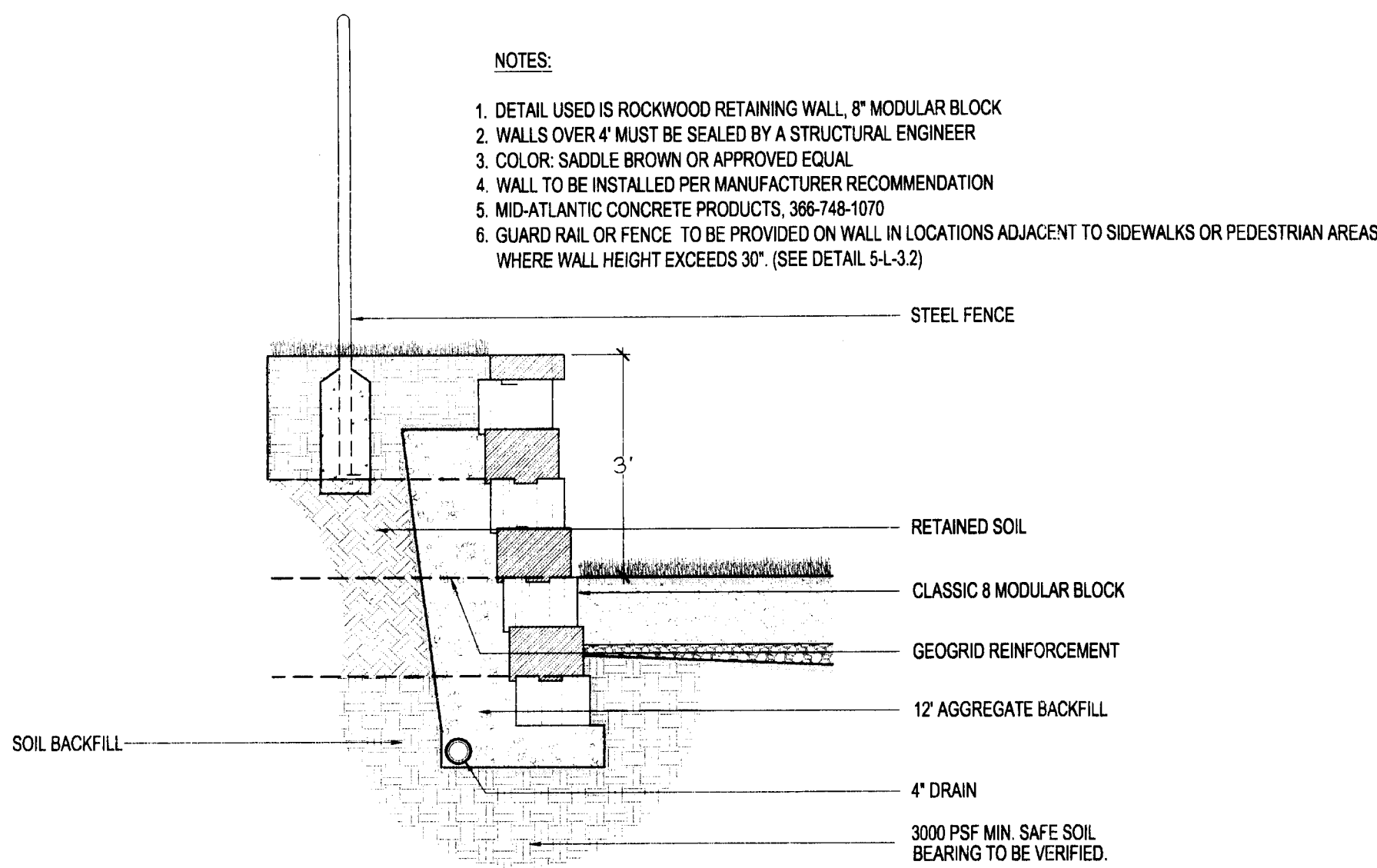
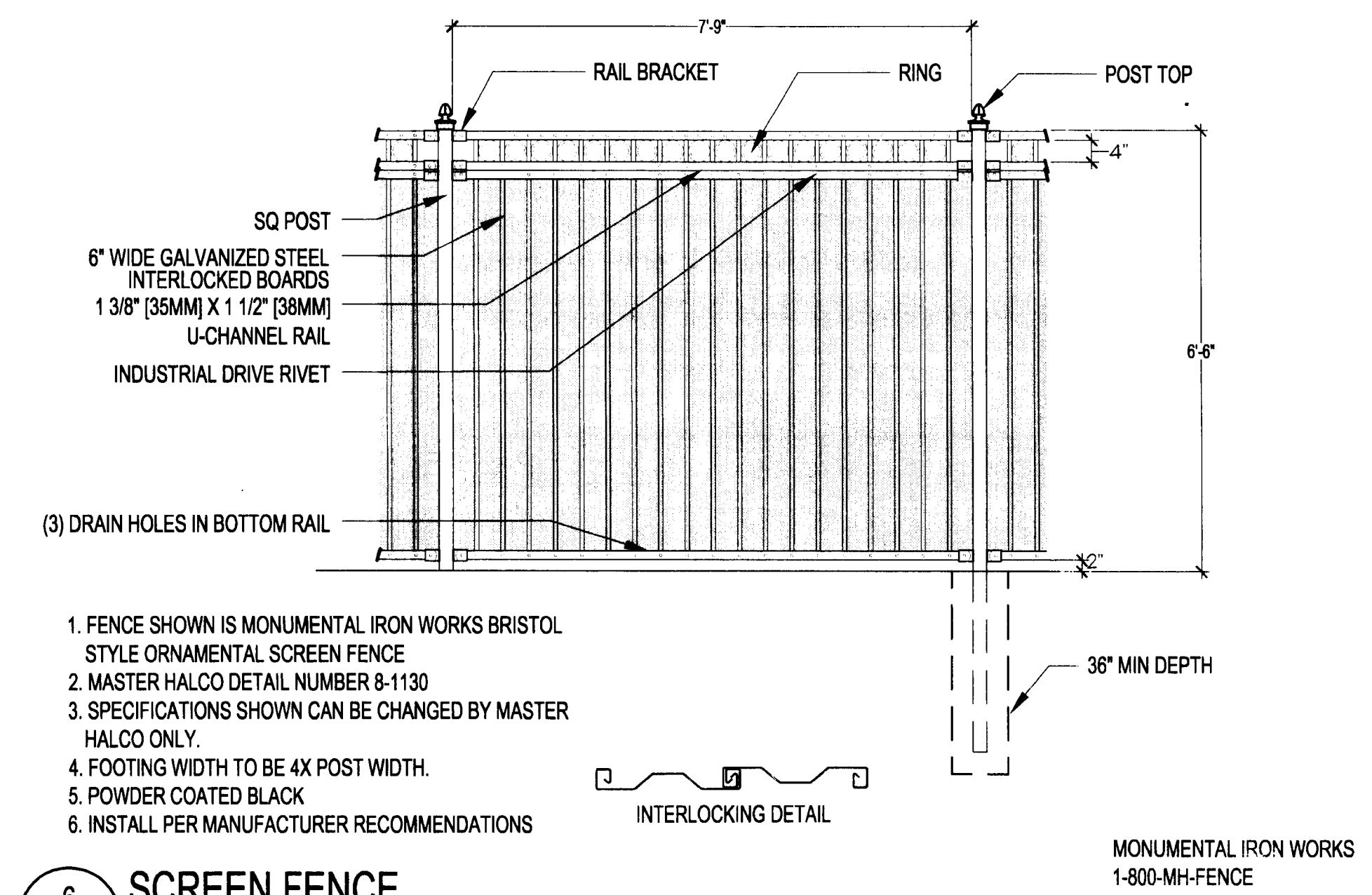
4 HANDRAIL
L3.2 NOTES

CMLDSM #50.04B
NOT TO SCALE

5 FENCE/GUARD RAIL
L3.2 ELEVATION



6 SCREEN FENCE
L3.2 ELEVATION



7 MODULAR RETAINING WALL
L3.2 ELEVATION

NOT TO SCALE

8 BIKE RACK
L3.2 ELEVATION

landscape forms

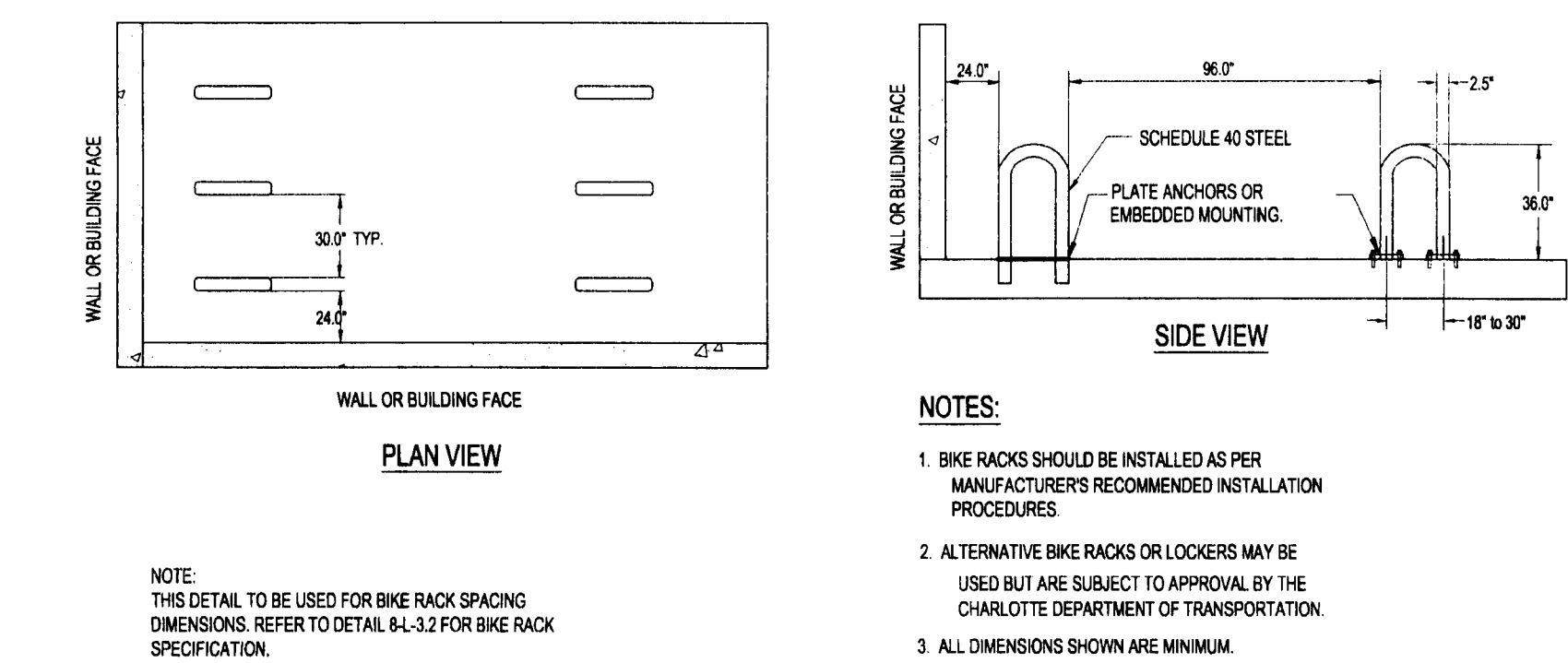
431 LAWSON AVE.
KALAMAZOO, MI 49008

PHONE: 800-551-3544
FAX: 269-381-3500

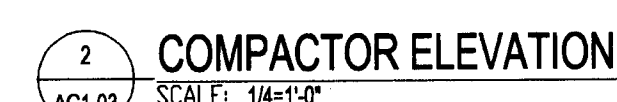
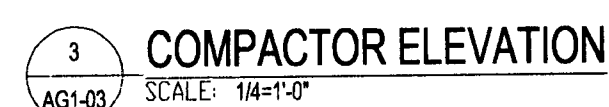
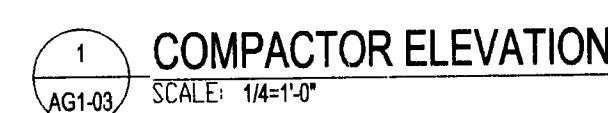
DESIGN GROUP: RING
RECYCLED POLYMER RACKS, IMMEDIATELY
PATENT PENDING
R/L: 10/2000
DRAWN BY: JMM
DIMENSIONS ARE IN INCHES (mm)

NOTE: BIKE RACKS SHALL MEET OR EXCEED CITY STD # 50.20

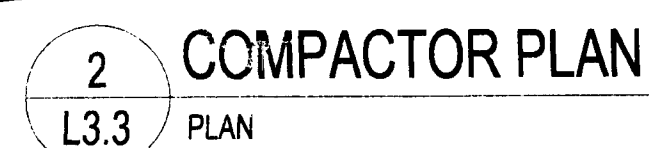
9 INVERTED "U" BIKE RACK
L3.2 ELEVATION



CITY STD# 50.20
NOT TO SCALE



NOTE:
COMPACTOR DETAILS PROVIDED/DESIGNED BY THE
PRESTON PARTNERSHIP. SEE ARCHITECTURAL
DRAWINGS FOR MORE INFORMATION.

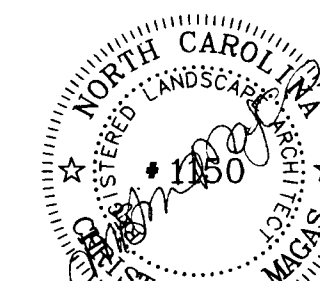


SOLID WASTE AND RECYCLING

SOLID WASTE REQUIRED:	(1) 8 C.Y. COMPACTOR PER 90 UNITS=29 C.Y.
RECYCLING REQUIRED:	(1) 144 S.F. RECYCLING STATION PER 80 UNITS = 720 S.F.
SOLID WASTE PROVIDED:	1 29 C.Y. COMPACTOR
RECYCLING AREA PROVIDED:	720 S.F. (5-144S.F. RECYCLING STATION)

NOTE:
PER CODE, THE DEVELOPER WILL PROVIDE AREA FOR POSSIBLE
FUTURE RECYCLING, IF BUILT. THIS LAYOUT WILL BE FOLLOWED

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10-09-07

MALLARD CREEK
Residential Apartments
Phillips Development & Realty, LLC, Charlotte North Carolina

REVISIONS:
06-14-07 - CMPC / ENGINEERING RESUBMITTAL
07-06-07-PER BUILDING STND. COMMENTS
08-15-07 - CMPC / ENGINEERING RESUBMITTAL
09-05-07 - CMPC / ENGINEERING RESUBMITTAL
10-09-07 - CMPC / ENGINEERING RESUBMITTAL

DATE: March 27th, 2007
DESIGNED BY: CCM
DRAWN BY: GPP/SKJ
CHECKED BY: CCM
Q.C. BY: CCM
SCALE: VARIES

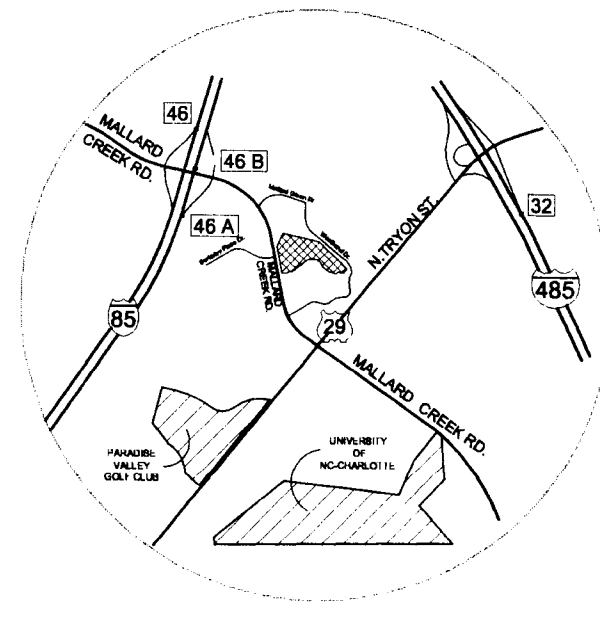
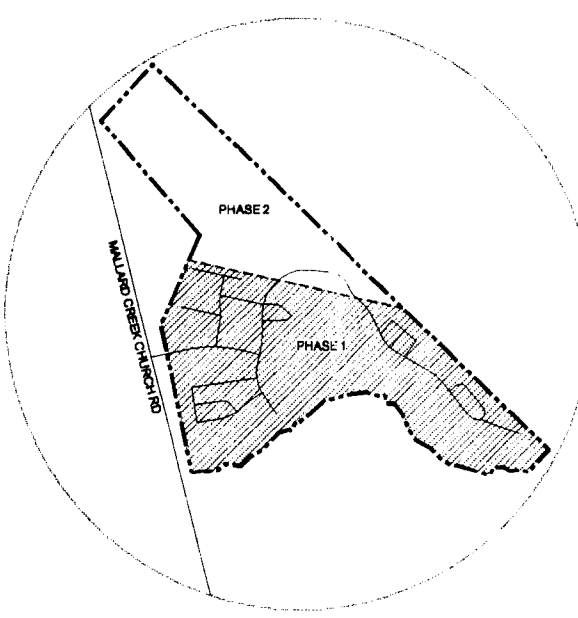
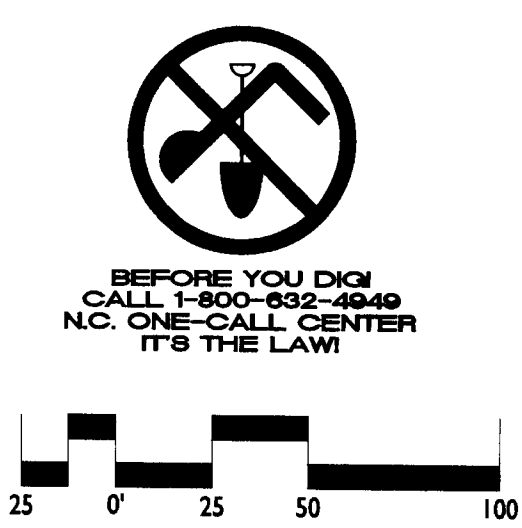
SHEET #: **L-3.3**



PLANTING SCHEDULE OUTLINE				
QTY	SYMBOL	LATIN NAME	COMMON NAME	SIZE
265	MYRC	MYRICA CERIFERA	SOUTHERN WAX MYRTLE	24" MIN.
150	ICBN	ILEX CORNUTA BURFORDI NANA	DWARF BURFORD HOLLY	24" MIN.
25	PRUO	PRUNUS X 'OKAME'	OKAME CHERRY	8" MIN.
47	ACER	ACER RUBRUM	RED MAPLE	2" CAL. MIN.
9	QUEA	QUERCUS ACUTISSIMA	SAWTOOTH OAK	2" CAL. MIN.

LANDSCAPE REQUIREMENTS:
AN EQUAL TO AT LEAST 10 % OF THE TOTAL IMPERVIOUS AREA IS REQUIRED FOR PARKING LOT LANDSCAPE PURPOSES.
1 TREE PER 10,000 SQ/FT OF IMPERVIOUS AREA IS REQUIRED.
TOTAL SITE IMPERVIOUS AREA = 365,771± (8.4AC) / 10,000 = 36.57
37 OVERSTORY TREES REQUIRED
39+ PROVIDED
ALL PARKING SPACES TO BE WITHIN 60' OF A TREE
-REQUIREMENT PROVIDED ON SITE
SCREENING OF ALL PARKING SPACES ALONG R.O.W. AND ADJOINING LAND USES WITH EVERGREEN SHRUBS 24" HT. MIN AT PLANTING WITH AN AVERAGE GROWTH RATE OF 5' HT. WITHIN 4 YEARS IS REQUIRED AND PROVIDED ON SITE.
ALONG MALLARD CREEK CHURCH ROAD 652.26 OF ROW FRONTAGE EXISTS.
652.26 = 16 TREES REQUIRED.
EXISTING TREES = 32
TREES PROVIDED = 9

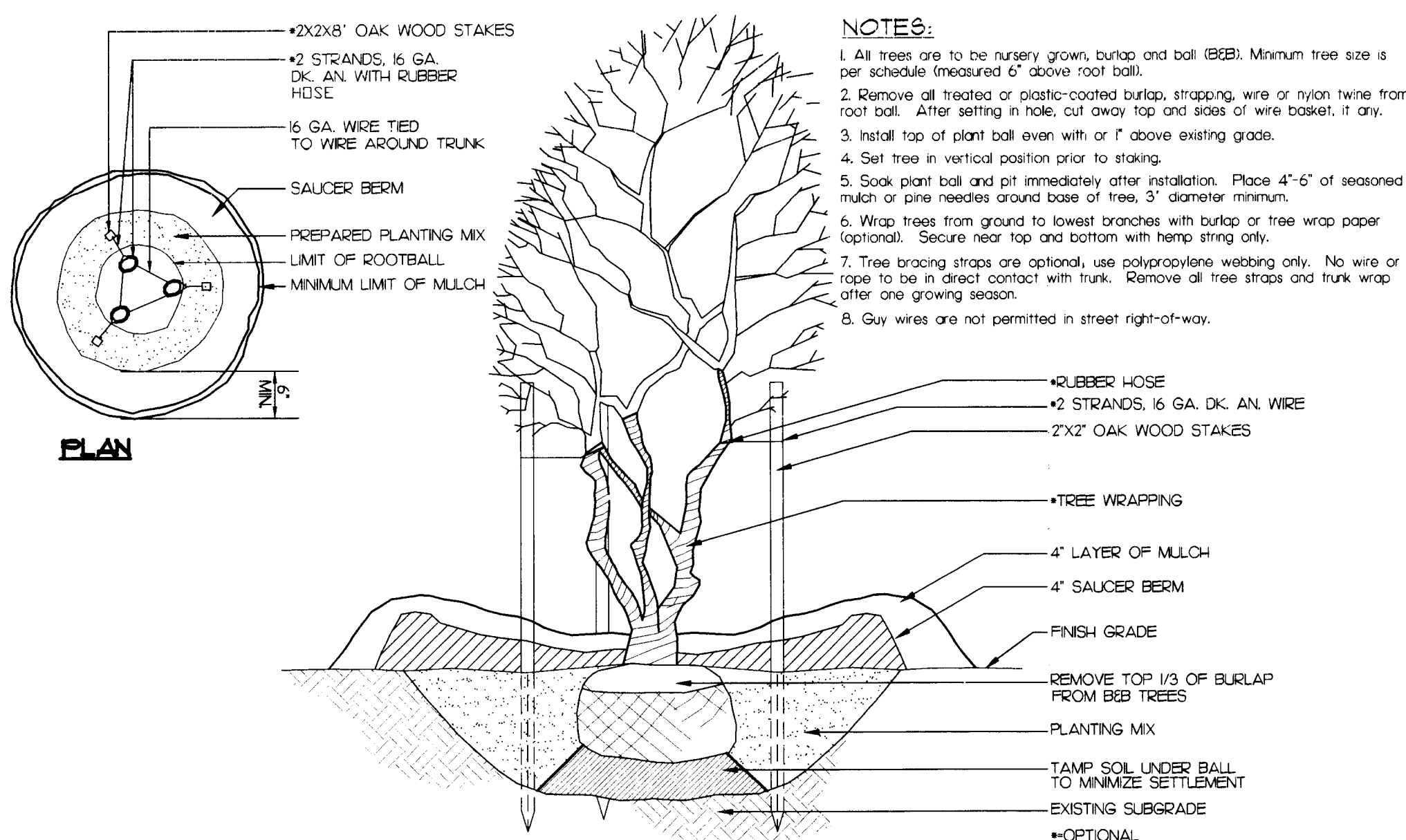
SITE KEY NTS VICINITY MAP NTS



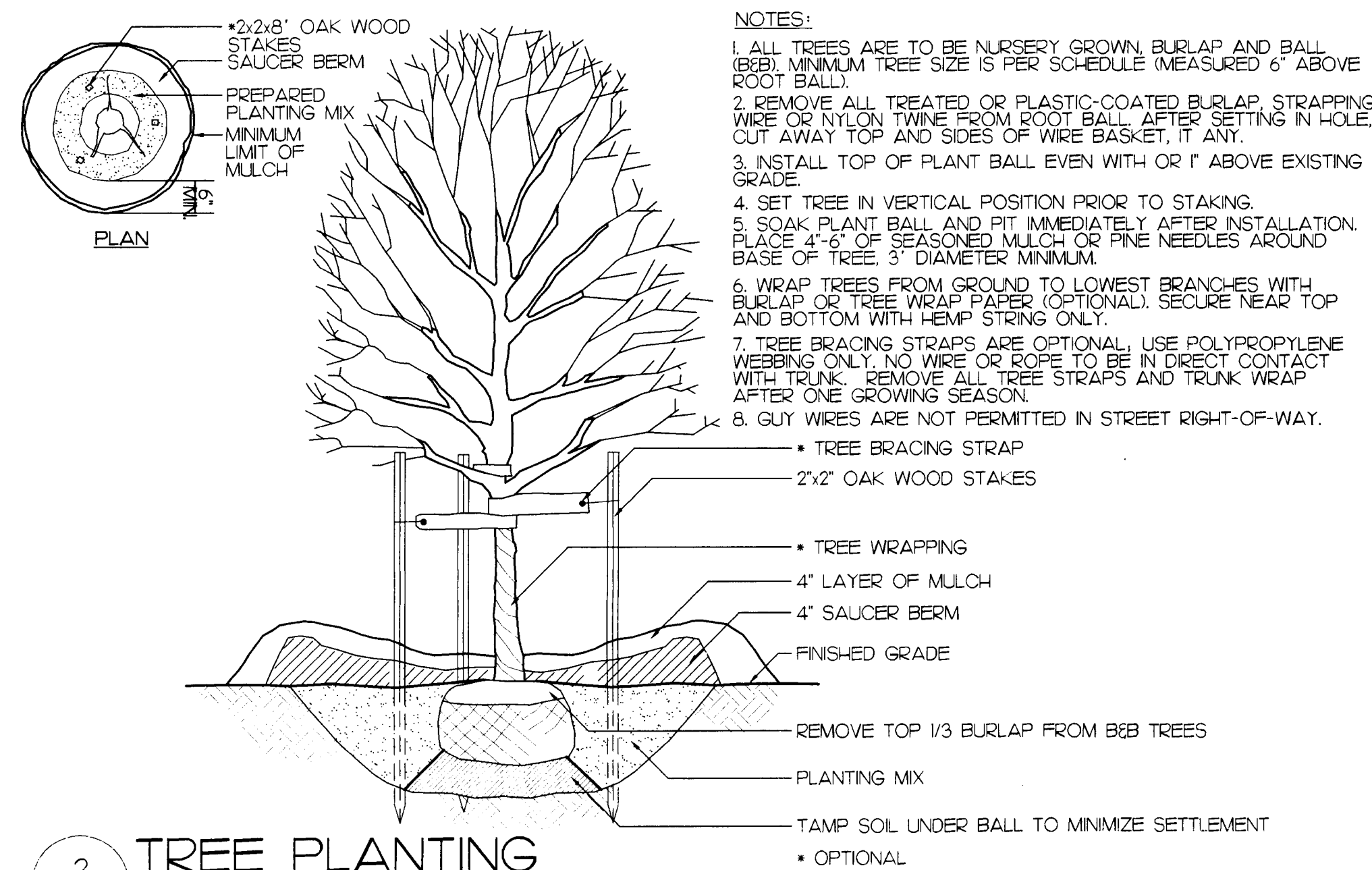
10-09-07

MALLARD CREEK
Residential Apartments
Phillips Development & Realty, LLC, Charlotte North Carolina
REQUIRED PLANTING PLAN

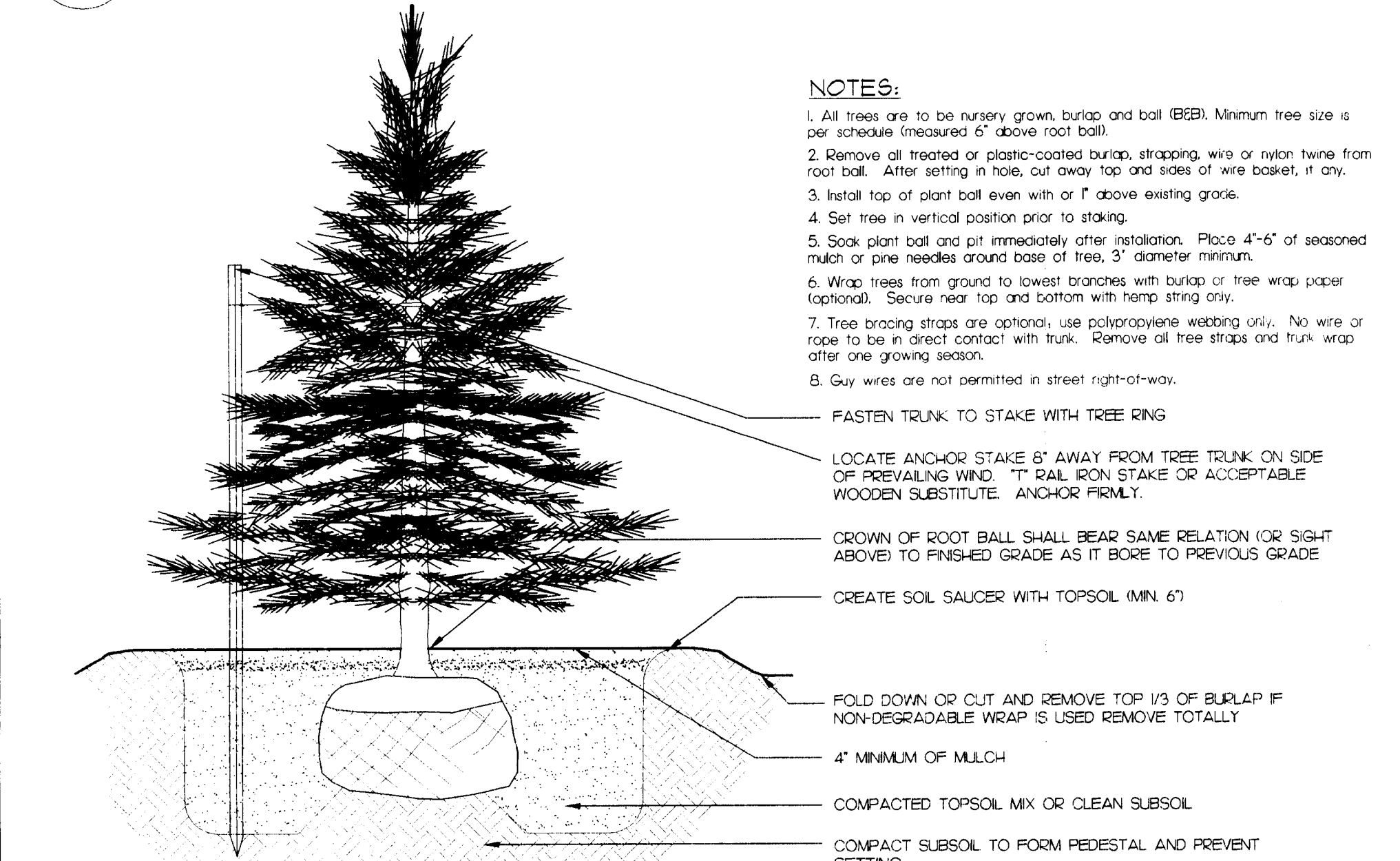
REVISIONS:
1. 06-14-07 CHRC / ENGINEERING RESUBMITTAL
2. 07-06-07 PHA & BUILDING STND. COMMENTS
3. 08-01-07 CHRC / ENGINEERING RESUBMITTAL
4. 09-05-07 CHRC / ENGINEERING RESUBMITTAL
5. 10-09-07 CHRC / ENGINEERING RESUBMITTAL
DATE: March 27th, 2007
DESIGNED BY: GPH
CHECKED BY: GPH
Q.C. BY: WOV
PROJECT #: 100647
SHEET #:
L-4.0



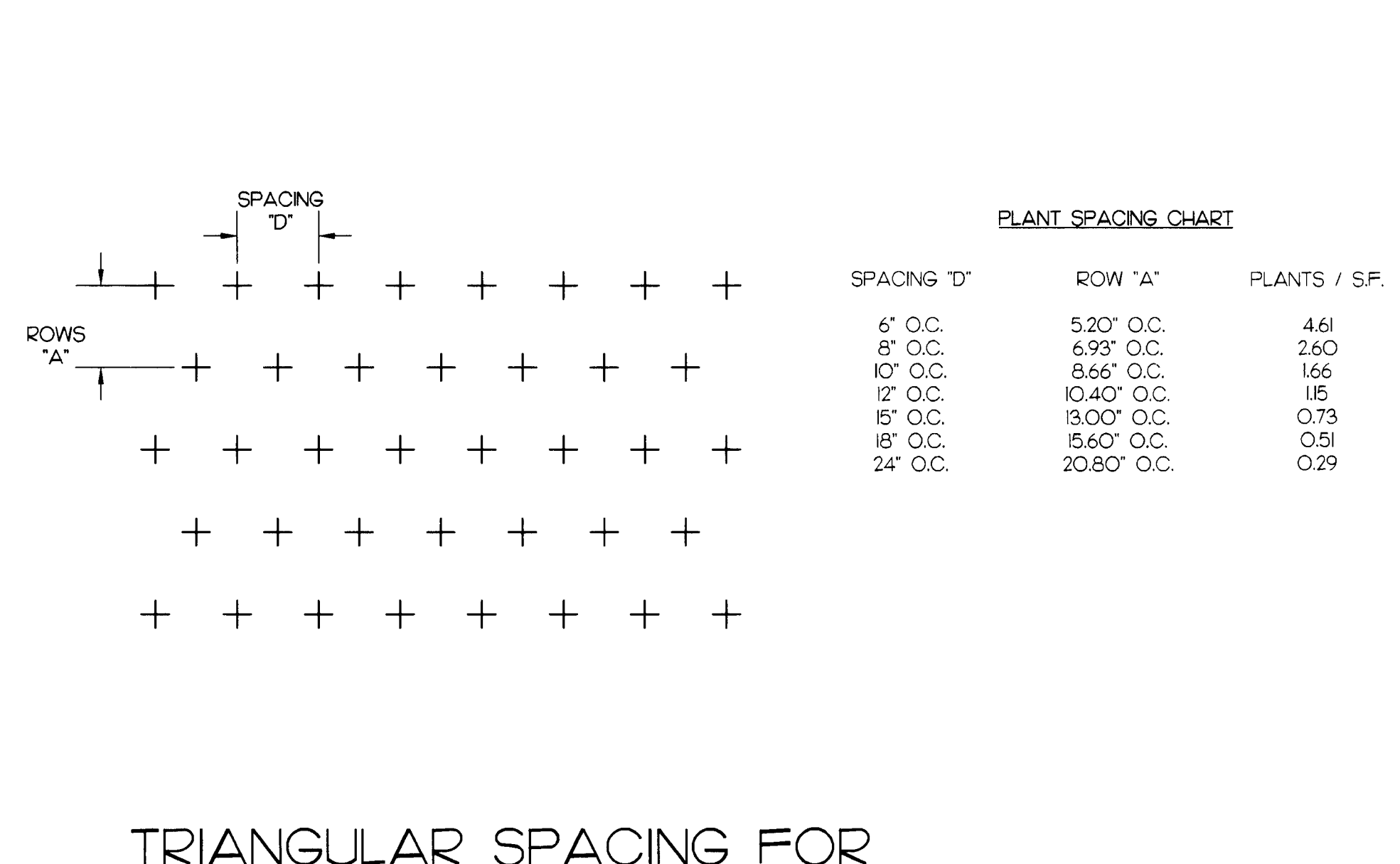
1 MULTI-STEM TREE
L6.0 NOT TO SCALE



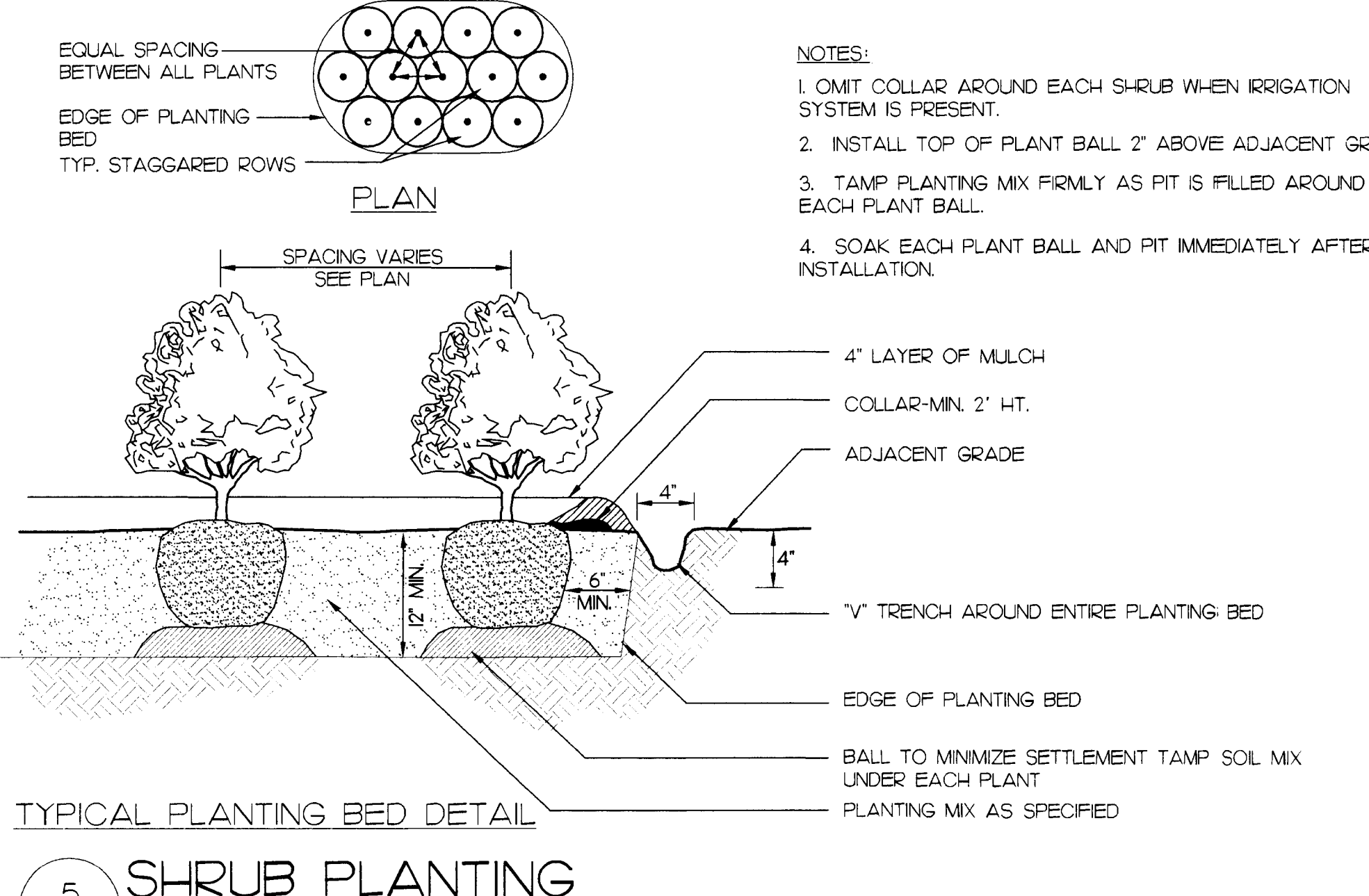
2 TREE PLANTING
L6.0 SECTION NOT TO SCALE



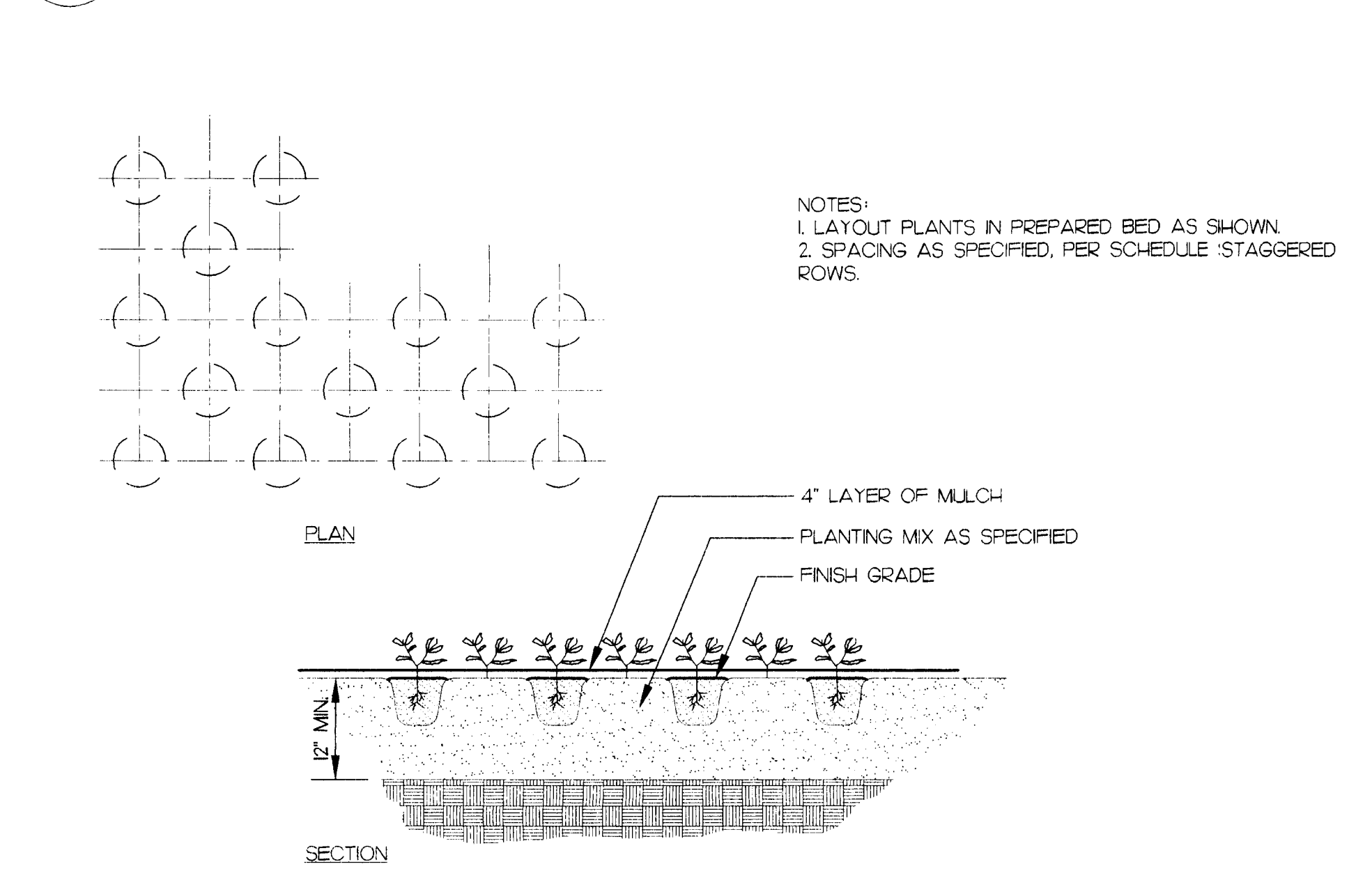
3 EVERGREEN TREE
L6.0 SECTION NOT TO SCALE



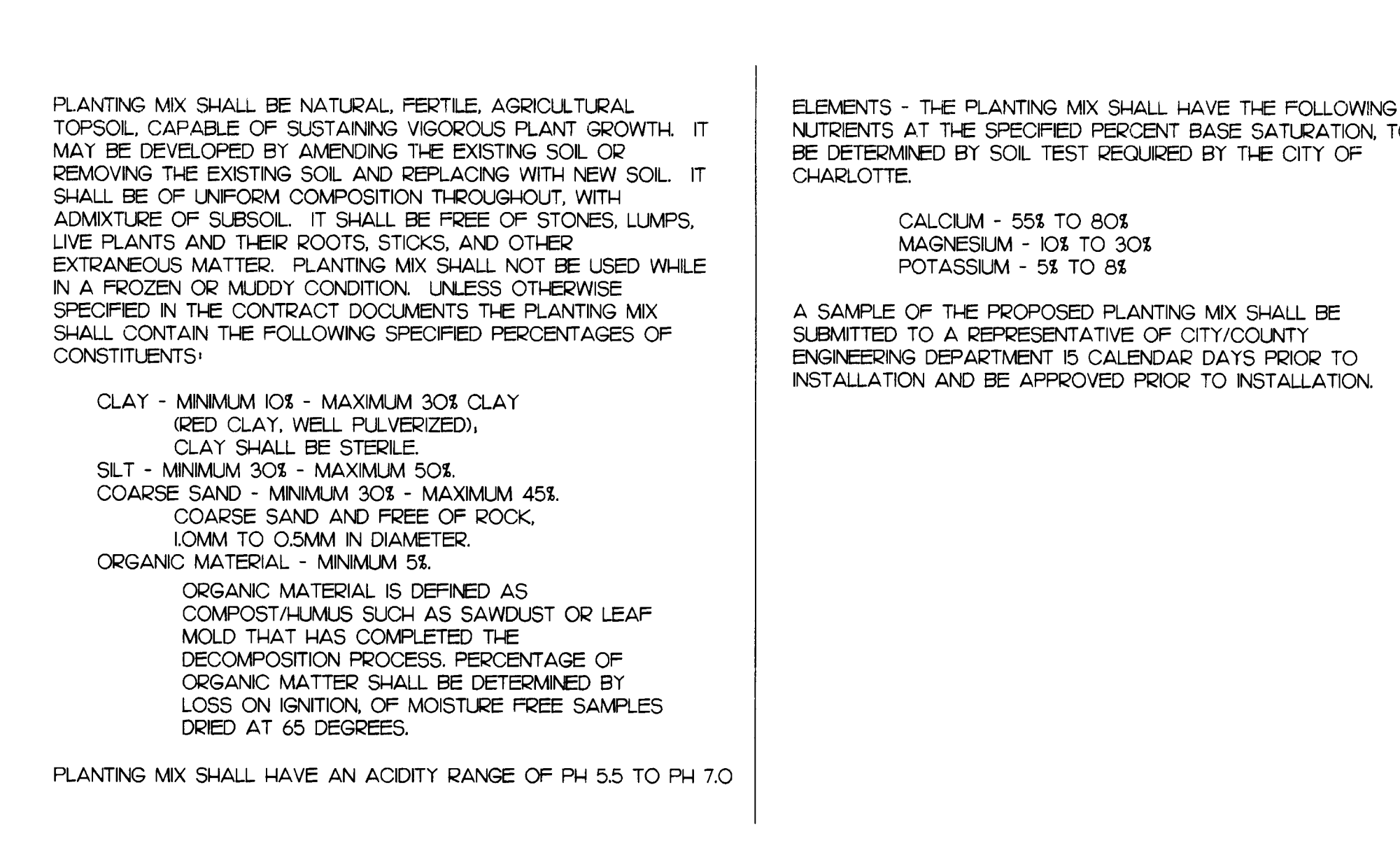
4 TRIANGULAR SPACING FOR SHRUBS AND GROUNDCOVERS
L6.0 PLAN NOT TO SCALE



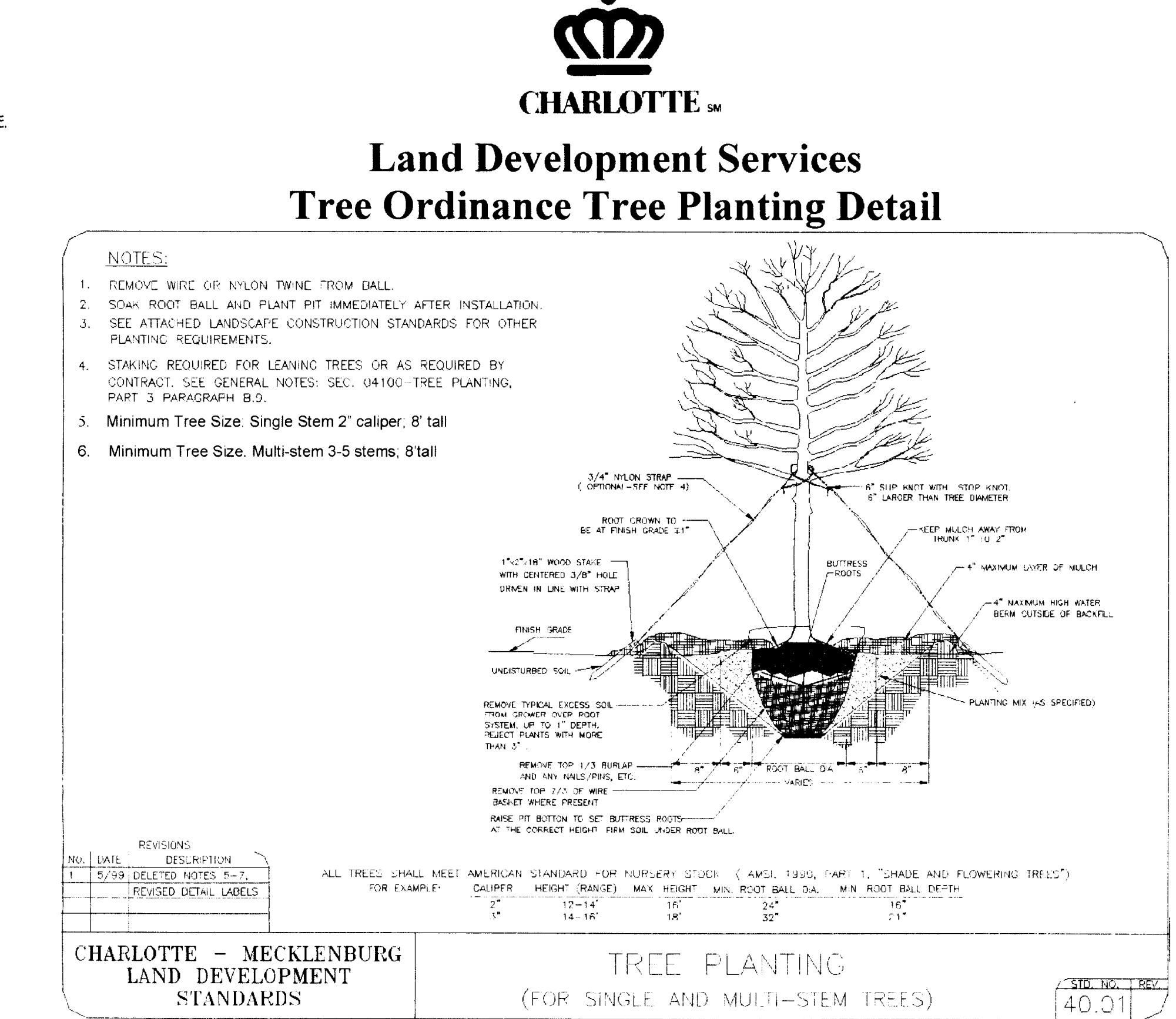
5 SHRUB PLANTING
L6.0 SECTION AND PLAN NOT TO SCALE



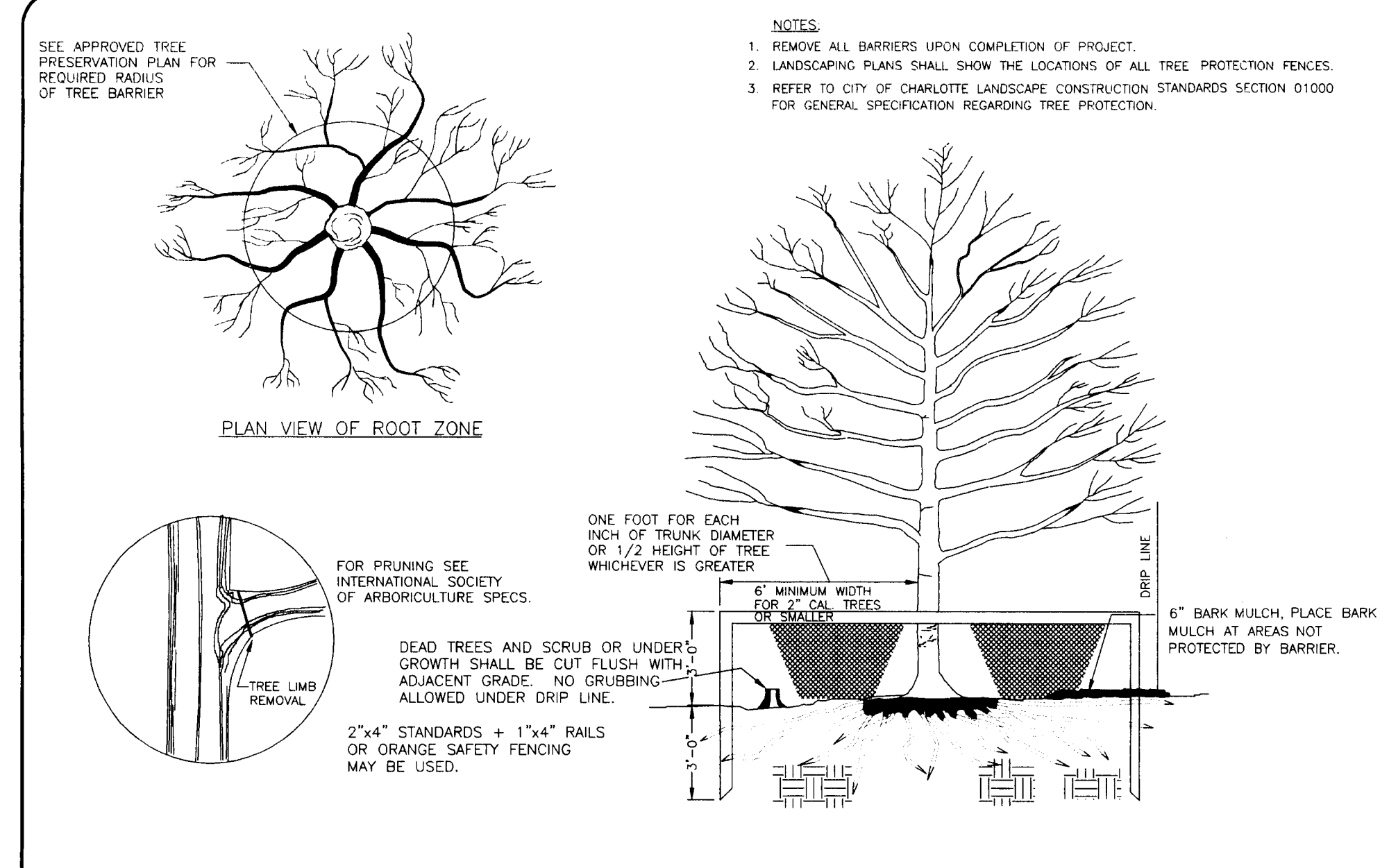
6 GROUNDCOVER PLANTING
L6.0 SECTION NOT TO SCALE



6 PLANTING MIX
L6.0 SPECIFICATION



7 TREE PLANTING DETAIL, CMLDS 40.01
L6.0 SECTION NOT TO SCALE



8 TREE PLANTING DETAIL, CMLDS 40.01
L6.0 SECTION NOT TO SCALE

CONDITIONAL NOTES - REZONING PETITION # 99-37C

GENERAL NOTES

1. The Site Plan consists of the Technical Data Sheet and the Illustrative Site Plan. The Technical Data Sheet contains all development notes and conditions regarding such things as vehicular access points, buffers, setback/yard dimensions, generalized building/parking/circulation areas, development intensity, etc. The Illustrative Site Plan depicts a development scenario based upon the conditions imposed by the Technical Data Sheet. However, other development scenarios are permitted as a matter of right which allow alternatives to the arrangement of building/parking/circulation areas, building footprints and shapes, etc. The illustrative subdivision may be changed as a matter of right provided the total number of dwelling units permitted on the property is not exceeded.
2. The proposed development shall comply with all applicable regulations pertaining to off-street parking, signage, buffers/landscaping, screening, drainage, tree ordinance, etc.

TRANSPORTATION

1. The number and type of vehicular access points to the site shall be limited to the points of ingress/egress as shown on the Technical Data Sheet. However, the locations may vary somewhat from that depicted. The location and design shall comply with all applicable standards and requirements of the Charlotte Mecklenburg Land Development Standards, the Mecklenburg County Department of Transportation, Charlotte Department of Transportation and the North Carolina Department of Transportation. The proposed median opening on US 29 will be located a minimum of 750 feet from the end of the US 29 median at Mallard Creek Church Road and constructed as a "protected left over."
2. The Petitioner/Developer shall provide a 5 foot sidewalk with an 8 foot planting strip provided the 5 foot sidewalk remains in the rights of way along the Property's Mallard Creek Church Road at the time such parcel is developed.
3. the Petitioner/Developer will dedicate right-of-way sufficient to provide a minimum of 50 feet from the centerline of Mallard Creek Church Road and US 29 to the Property line prior to commencement of development of any of the parcels.
4. The Petitioner/Developer shall add an additional lane along the Property's frontage along US 29. The Petitioner/Developer will complete necessary grading and install curb and gutter to Charlotte DOT standards and the City of Charlotte will pave the additional lane. The Petitioner/Developer will construct the proposed protected left over on US 29 as depicted on the Technical Data Sheet. All improvements to US 29 will conform to NCDOT requirements.
5. The Petitioner/Developer will install an additional lane along the Property's frontage along Mallard Creek Church Road extending from its intersection with US 29 through the first median cut access on Mallard Creek Church Road as depicted on the Technical Data Sheet.
6. The Petitioner/Developer will coordinate with the adjoining Robert F. and Carrie Alexander property to coordinate internal site connectivity from the Property to the existing median access point on US 29. This adjoining property is subject to Rezoning Petitions 99-04, 99-02 (C), and 99-13 (C). The Petitioner/Developer will coordinate and provide access through the Property to Mallard Creek Church Road for the benefit of the adjoining Alexander property. This joint access is shown conceptually on the Illustrative Site Plan. In the event such connectivity to US 29 can not be achieved, Petitioner/Developer will provide vehicular access between the B-1 parcel and the adjoining multi-family parcels, if necessary.
7. The Petitioner/Developer shall coordinate internal pedestrian walkways throughout the Property. This will include pedestrian access between the multi-family and the B-1 portion of the Property. A minimum of five (5) parking spaces will be reserved within the R-17 MF Parcel 3 close to the B-1 Pedestrian Access for Internal access to tie B-1 Parcel 1. petitioner will cooperate with the City Department of Transportation to provide a location for a transit stop along Mallard Creek Church Road in the approximate location depicted. The "transit friendly" internal walk way system will provide connections to the proposed transit station.
8. Prior to the issuance of a Certificate of Occupancy for any residential development within the multi-family portion of the Property, Petitioner will deposit or bond the sum of Twenty Thousand Dollars (\$20,000), which sum may be used by CDOT or NCDOT for Petitioner's 50% share of a traffic light which may be installed pursuant to a warrant issued by CCOT or NCDOT at one (1) of the two (2) median cuts in Mallard Creek Church Road along the Property. The deposit or bond will be refunded ten (10) years after issuance or deposit or in the event no warrant for a traffic light is issued.

BUILDING/ARCHITECTURE/USES

1. Maximum building heights for the R-17 MF portion of the property shall be limited to five (5) stories. Maximum heights for the B-1 portions of the property shall be limited to 35 feet in height.
2. The R-17 MF-Parcel 3 portion of the Property extending to the center line of the power line easement crossing the property will be used for the construction and operation of separate multi-family rental residential communities and may be subdivided in up to three (3) separate parcels. The business portion of the Property (Parcel 1) may be used for uses allowed in the B-1 district excluding hotel/motel uses and may be subdivided into up to four (4) separate parcels. The R-17 MF-Parcel 4 portion of the Property will be used for the construction and operation of up to 125 for sale multi-family homes and up to 300 beds for retirement or nursing home communities.

3. The multi-family building architecture adjacent to the Historic Alexander Homeplace and appurtenant structures will include the use of brick or stone which is compatible with the Alexander House.
4. Commercial buildings will be constructed of predominately brick, similar to the existing brick color of the Historic Alexander House, but may also incorporate the use of split face black, stucco-like materials.
5. No wall pak type lighting will be allowed on buildings.
6. Approximately 4.67 acres of the Property, comprising the Historic Alexander Homeplace and appurtenant structures will be reserved for preservation in conjunction with the Charlotte-Mecklenburg Historic Landmarks Commission. A Historic Preservation Agreement will be recorded against 2.82 acres within Parcel 2 to protect the existing structure. The Agreement will restrict changes to the physical structures, landscaping and entrance road, as well as require maintenance of the Alexander house and grounds. Adoptive re-uses allowed in the Homeplace will include only the uses allowed in the O-1 Zoning Classification those listed below.
 - 1) Social/recreational as an ancillary use for the adjoining multi-family property;
 - 2) Residential;
 - 3) Office.

BUFFERS/SETBACKS/SITE DESIGN

1. A Class "B" buffer shall be established as shown between the business zoning and the multi-family zoning and Class "C" buffers shall be established between: 1) the business zoning and the office zoning, and; 2) the office zoning and multi-family. Where there is insufficient natural vegetation to comply with minimum buffer standards, the buffer shall be improved with new landscaping in accordance with applicable standards. The required buffer width may be reduced by 25 % with the addition of a 6' screen fence per section 12.302 of the Mecklenburg County Zoning Ordinance. Reasonable efforts will be used to retain existing vegetation within these buffers.
2. Storm water detention shall not be located in setback or buffer areas. Trees which are 6" DBH or greater in caliper in any buffer areas will be left undisturbed.
3. Maximum height of exterior lighting in parking areas shall be 20 feet and will be shielded, if necessary, to prevent glare onto adjacent residential properties.
4. The Petitioner/Developer shall provide tree protection within the 35 foot setback along US 29 and Mallard Creek Church Road for trees which are 6" DBH or greater in caliper except to the extent necessary to accommodate ingress/egress to property, walls, fences and necessary utility lines.
5. The Petitioner/Developer shall provide a fire hydrant within 750 feet of the most remote building as a fire truck travels if such hydrant does not exist.
6. The Petitioner/Developer has eliminated or reduced the width of required buffers along the two (2) adjoining parcel(s) which are in rezoning: 1) Petition 99-02 (c) and 99-04 to R-12 MF (CD) and; 2) Petition 99-13 (c) to R-12 MF (CD). In the event the rezoning of either one (1) or both of the aforementioned petitions are denied, the appropriate buffers will be added.
7. Screening will conform to the applicable standards of Section 12.303 of the Zoning Ordinance All permanent garbage/trash disposal facilities (i.e. dumpsters or compactors if provided) will be screened with a solid enclosure with gates.
8. All utilities serving the site will be located underground.
9. Any above ground backflow preventers for this site shall be located outside of the setback or buffers.
10. The Petitioner/Developer has completed a Wetlands and Stream Analysis in conformance with Mecklenburg County Ordinance. The Wetlands and Stream Delineation have been surveyed and are shown on the Rezoning Plan. The Developer will preserve a minimum thirty-five (35) foot setback from the stream side and preserve substantially all of the existing wetlands area except as required for crossing to provide interconnectivity.
11. The following agencies must be contacted prior to construction regarding wetlands and water quality permits:

404/401 Permit	NCDEHNR - Raleigh office	919-733-1786
401/404 Permit	US Army Corp. of Engineers	704-271-4854

SIGNAGE

1. The Petitioner/Developer shall construct coordinated project entrance features which will be located at the main entrances on Mallard Creek Church Road and US 29 and at the intersection of Mallard Creek Church Road and US 29.
2. The Petitioner/Developer reserves the right under the "Sign Flex Option: to construct up to five (5) tenant identification signs along US 29 in the B-1 property. These monument signs shall not exceed eight (8) feet in height and seventy-five (75) square feet in signage area on each sign face. All signs will be coordinated as to architectural design as noted in the Illustrative Site Plan.
3. Signage will be permitted in accordance with applicable zoning standards.

LANDSCAPING

1. The Petitioner/Developer shall develop a master street scape program for the Property's frontage along US 29 and Mallard Creek Church Road as shown on the Illustrative Site Plan.
2. All landscape plans shall meet or exceed the City of Charlotte Tree Ordinance Requirements.

REVISIONS:

1.	05-14-07	CHFC / ENGINEERING RESUBMITTAL
2.	08-15-07	CHFC / ENGINEERING RESUBMITTAL
3.	08-15-07	CHFC / ENGINEERING RESUBMITTAL
4.	09-05-07	CHFC / ENGINEERING RESUBMITTAL
5.	10-09-07	CHFC / ENGINEERING RESUBMITTAL

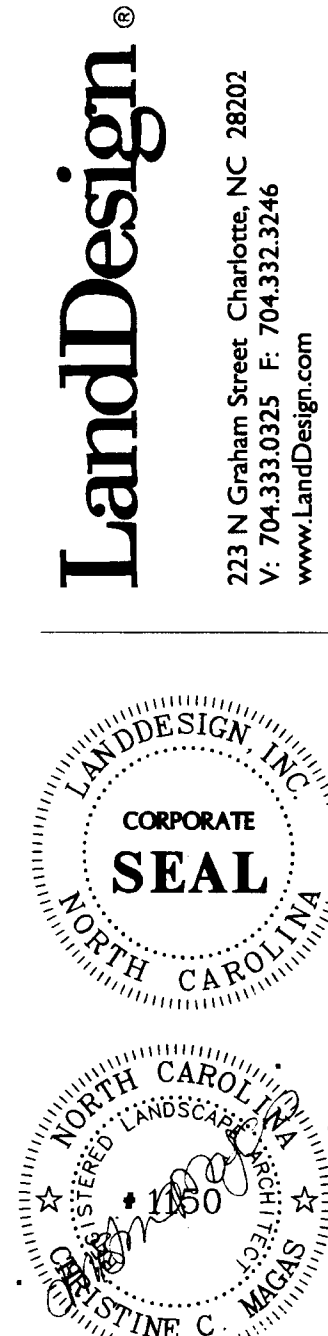
DATE: March 27th, 2007
DRAWN BY: GPPSKI
CHECKED BY: CCM
SCALE: NTS
PROJECT #: 1006247

SHEET #

MALLARD CREEK
Residential Apartments
Phillips Development & Realty, LLC, Charlotte North Carolina

DEVELOPMENT NOTES

L-6.0



10-09-07

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