

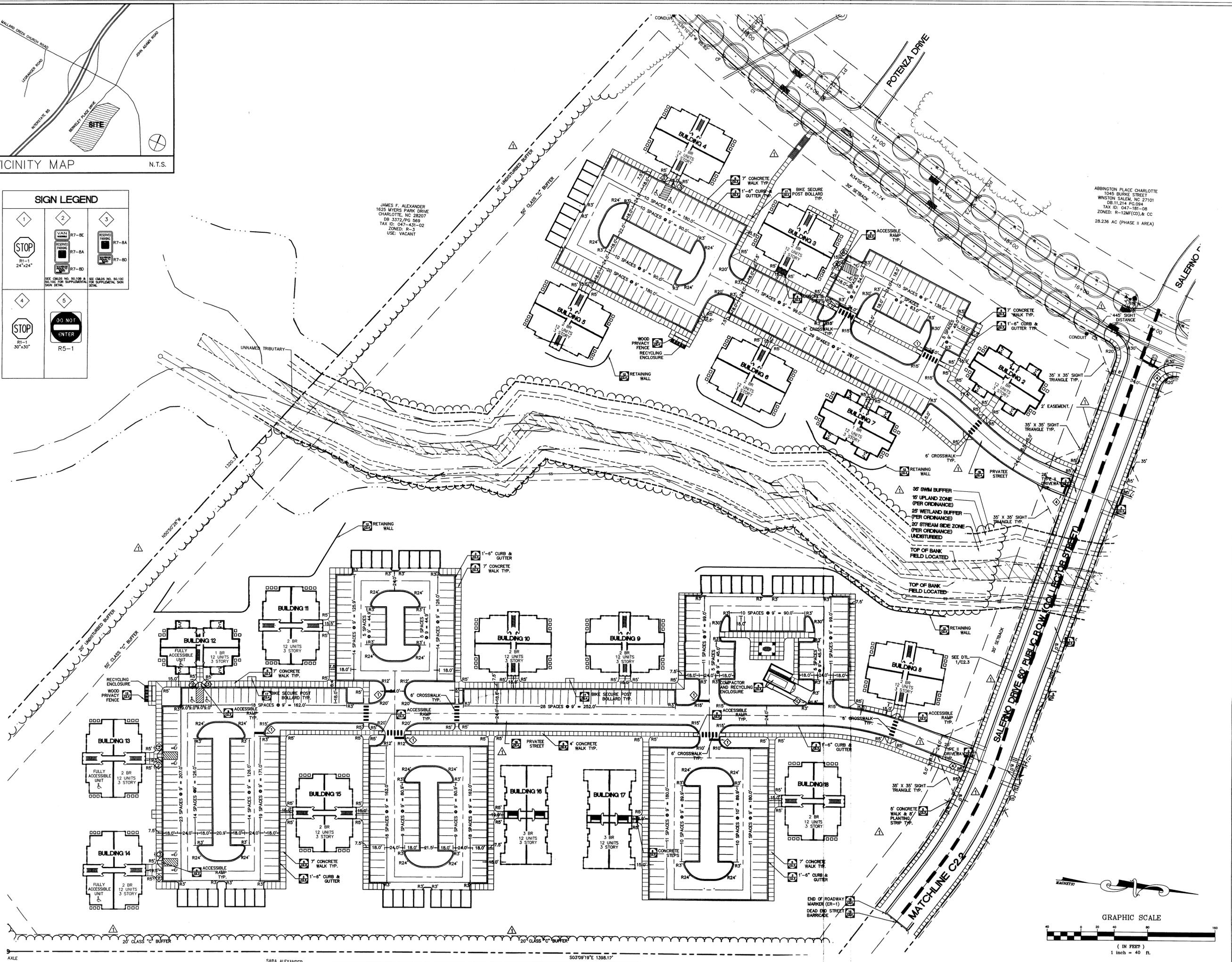
SIGN LEGEND

1 STOP R1-1 24"x24"	2 VAN R7-8E RESERVED R7-8A PARKING R7-8D	3 PARKING R7-8A R7-8D
4 STOP R1-1 30"x30"	5 DO NOT ENTER R5-1	

SEE CHAS. NO. 80.108 & 80.109 FOR SUPPLEMENTAL SIGN DATA.

JAMES F. ALEXANDER
1625 MYERS PARK DRIVE
CHARLOTTE, NC 28207
DB 3372/PG 569
TAX ID: 047-431-02
ZONED: R-3
USE: VACANT

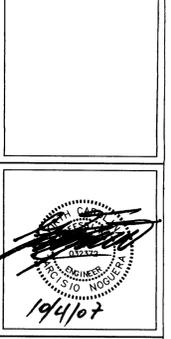
ABBINGTON PLACE CHARLOTTE
1045 BURKE STREET
WINSTON SALEM, NC 27101
DB 11,214 PG 094
TAX ID: 047-181-08
ZONED: R-12M(CD), & CC
28.236 AC (PHASE II AREA)



REVISED PER CITY OF CHARLOTTE COMMENTS

NO.	DATE	DESCRIPTION
1	8/17/07	

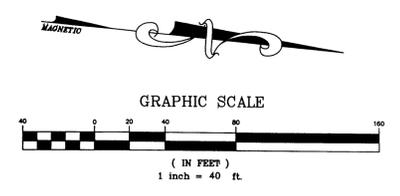
200 S COLLEGE ST
SUITE 720
CHARLOTTE, NC 28202
P 704.334.7925
www.stewart-eng.com

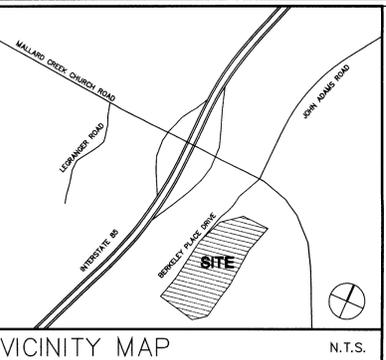


ASHLEY COURT APARTMENTS
DEVELOPED BY: ASHLEY COURT APARTMENTS, LLC
CHARLOTTE, NORTH CAROLINA

ENLARGED SITE PLAN

DATE: 5-31-07
SCALE: HORIZ: 1" = 40'
JOB NO. X6031.00
SHEET: C2.1

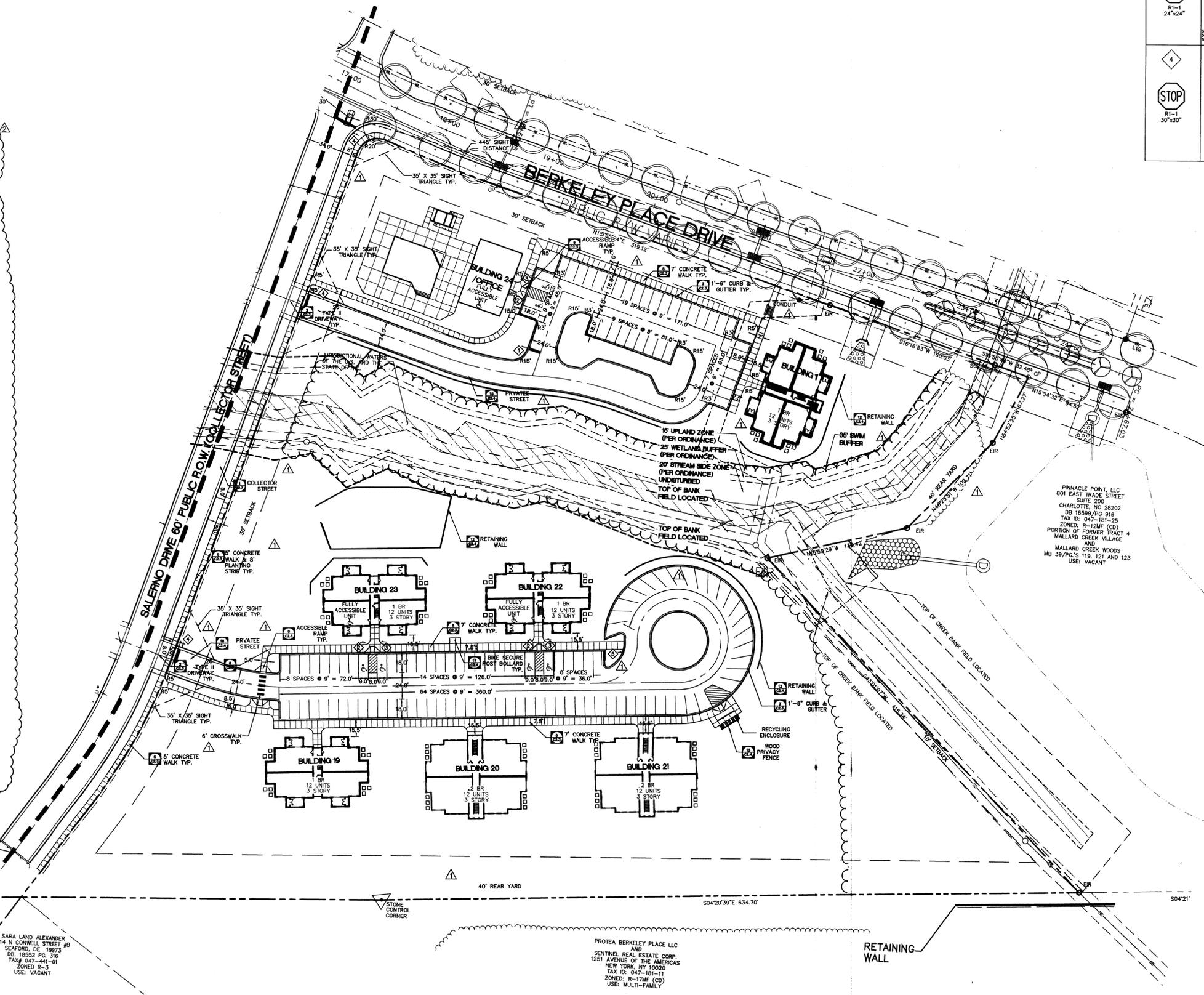




- SITE NOTES**
- BOUNDARY AND SURVEY INFORMATION TAKEN FROM A SURVEY PREPARED BY STEWART, DATED 3-13-07. REFER TO SHEET C1.0 FOR INFORMATION REGARDING SURVEY.
 - ALL DIMENSIONS ARE TO FACE OF CURB, EDGE OF PAVEMENT, OR FACE OF BUILDING UNLESS OTHERWISE NOTED.
 - ALL IMPROVEMENTS AND CONSTRUCTION SHALL BE IN ACCORDANCE WITH STATE AND LOCAL STANDARDS.
 - SIGHT TRIANGLES SHOWN ARE THE MINIMUM REQUIRED.
 - ANY DISCREPANCIES FOUND IN THE FIELD SHALL BE CALLED TO THE ATTENTION OF THE OWNER PRIOR TO PROCEEDING WITH THE WORK.
 - PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THAT ALL REQUIRED PERMITS AND APPROVALS HAVE BEEN OBTAINED FROM ALL REGULATORY AUTHORITIES.
 - THE CONTRACTOR SHALL CONTACT ALL OWNERS OF EASEMENTS, UTILITIES, AND RIGHT-OF-WAYS, PUBLIC AND PRIVATE, PRIOR TO WORKING IN THESE AREAS.
 - CONTRACTOR SHALL MAINTAIN THE SITE IN A MANNER SUCH THAT WORKMAN AND THE GENERAL PUBLIC SHALL BE PROTECTED FROM INJURY.
 - DO NOT SCALE DRAWING FOR ACTUAL DIMENSIONS, AS IT IS A REPRODUCTION AND SUBJECT TO DISTORTION.
 - REFER TO ARCHITECTURAL PLANS FOR ACTUAL BUILDING DIMENSIONS.
 - FOR EXTERIOR LIGHTING, REFER TO ELECTRICAL PLANS.
 - CONTRACTOR SHALL FURNISH AND INSTALL ALL PAVEMENT MARKINGS AS SHOWN ON THE PLANS.
 - ALL SIGNS, PAVEMENT MARKINGS, AND OTHER TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 - ALL PAVEMENT MARKINGS SHALL BE FOUR (4) INCH WHITE LINES UNLESS INDICATED OTHERWISE ON THE DRAWINGS.
 - DEVELOPER WILL PROVIDE STREET SIGNS PER C.M.D.S. #50.05. (9" SIGNS ONLY)
 - ALL ROAD IMPROVEMENTS AT BERKELEY PLACE DRIVE TO BE COORDINATE WITH ENGINEERING DEPARTMENT PRIOR TO CONSTRUCTION.
 - THE DEVELOPER SHALL MAINTAIN EACH STREAM, CREEK, OR BACKWASH CHANNEL IN AN UNSTRUCTURED STATE AND SHALL REMOVE FROM THE CHANNEL AND BANKS OF THE STREAM ALL DEBRIS, LOGS, TIMBER, JUNK, AND OTHER ACCUMULATIONS.
 - ANY CONSTRUCTION OR USE WITHIN THE FUTURE CONDITIONS FLOOD FRINGE LINE IS SUBJECT TO THE RESTRICTIONS IMPOSED BY THE FLOODWAY REGULATIONS OF THE CITY OF CHARLOTTE AND MECKLENBURG COUNTY.
 - THE PURPOSE OF THE STORM DRAINAGE EASEMENT (SDE) IS TO PROVIDE STORM WATER CONVEYANCE AND ANY STRUCTURES AND/OR OBSTRUCTION TO STORM WATER FLOW IS PROHIBITED.
 - THE CITY OF CHARLOTTE ENGINEERING DEPARTMENT HAS NOT REVIEWED THE STRUCTURAL STABILITY OF ANY RETAINING WALLS ON THE SITE AND DOES NOT ASSUME RESPONSIBILITY FOR THEM.
 - NON-STANDARD ITEMS IN THE RIGHT-OF-WAY REQUIRE A RIGHT-OF-WAY ENCROACHMENT AGREEMENT WITH CHARLOTTE DEPARTMENT OF TRANSPORTATION PRIOR TO INSTALLATION.
 - CURB AND GUTTER SHOWN ON PLANS ALONG BERKELEY PLACE DRIVE MAY BE ADJUSTED BASED UPON FIELD STAKING BY CITY ENGINEERING. ASSOCIATED STORM DRAINAGE MAY ALSO REQUIRE MODIFICATION BASED UPON FIELD CONDITIONS.
 - PRIOR TO PLAT RECORDATION, OFFSITE R/W AND/OR CONSTRUCTION EASEMENTS ARE REQUIRED TO BE OBTAINED ACCORDING TO THE GUIDELINES OF THE OFFSITE R/W ACQUISITION PROCESS. THESE NEEDED R/W AND CONSTRUCTION LIMITS ARE CLEARLY SHOWN ON THE ROADWAY IMPROVEMENT PLAN.
 - THE DEVELOPER SHALL CONTACT THE CHARLOTTE DEPARTMENT OF TRANSPORTATION, GUS JORDI (704-336-7086) TO IDENTIFY ANY CONFLICTS WITH TRAFFIC SIGNALIZATION EQUIPMENT. 60-90 DAYS WILL BE REQUIRED TO COORDINATE RELOCATION. DEVELOPER SHALL BE RESPONSIBLE FOR ALL RELATED RELOCATION COSTS AND/OR ANY REPAIR COST CAUSED BY THE CONTRACTOR/DEVELOPER.
 - REFER TO SHEET C3.0 FOR GRADING AND DRAINAGE.
 - ANY BUILDING WITHIN THE 100+1 STORMWATER ELEVATION LINE IS SUBJECT TO THE RESTRICTIONS OF THE CITY OF CHARLOTTE/MECKLENBURG COUNTY SUBDIVISION ORDINANCE, SECTION 7.200.6.
 - ALL OPENINGS (E.G. DOORS, WINDOWS, VENTS) IN STRUCTURES SHOULD BE LOCATED A MINIMUM OF ONE FOOT ABOVE THE ADJACENT FINISHED GROUND SURFACE (APPLIES TO LOTS WHICH MAY EXPERIENCE SIGNIFICANT OVERLAND FLOW NOT CONSIDERED IN THE 100+1 FLOOD ANALYSIS).
 - "AS-BUILT" DRAWINGS AND PLANS OF THE STORM DRAINAGE SYSTEM, INCLUDING DESIGNED DITCHES, MUST BE SUBMITTED PRIOR TO SUBDIVISION FINAL INSPECTION TO THE CITY/COUNTY ENGINEERING DEPARTMENT IN ACCORDANCE WITH THE CITY/COUNTY SUBDIVISION ORDINANCE.
 - PRIOR TO INSTALLATION, PE SEALED SHOP DRAWINGS OF UNDERGROUND DETENTION SYSTEMS MUST BE FURNISHED TO CITY OF CHARLOTTE OF APPROVAL.
 - PRIOR TO CO. SURVEYOR SEALED AS-BUILT DRAWINGS OF UNDERGROUND DETENTION SYSTEMS MUST BE PROVIDED.
 - COORDINATE ALL CURB AND STREET GRADES IN INTERSECTION WITH INSPECTOR.
 - IN ROLLING AND HILLY TERRAINS, SWEEPING OF THE STONE BASE AND/OR APPLICATION OF A TACK COAT MAY BE REQUIRED NEAR INTERSECTIONS. THESE REQUIREMENTS WILL BE ESTABLISHED BY THE INSPECTOR AND BASED ON FIELD CONDITIONS.
 - THE PURPOSE OF THE STORM DRAINAGE EASEMENT (SDE) IS TO PROVIDE STORM WATER CONVEYANCE. BUILDINGS ARE NOT PERMITTED IN THE EASEMENT AREA. ANY OTHER OBJECTS WHICH IMPEDE STORM WATER FLOW OR SYSTEM MAINTENANCE ARE ALSO PROHIBITED.

MATCH LINE C2.1

SARA LAND ALEXANDER
214 N CONNELL STREET #B
SEAFORD, DE 19973
DR. 18582 PG. 316
TAX# 047-441-01
ZONED R-3
USE: VACANT



40' REAR YARD

STONE CONTROL CORNER

PROTEA BERKELEY PLACE LLC AND SENTINEL REAL ESTATE CORP.
1281 AVENUE OF THE AMERICAS
NEW YORK, NY 10020
TAX ID: 047-181-11
ZONED: R-7MF (GD)
USE: MULTI-FAMILY

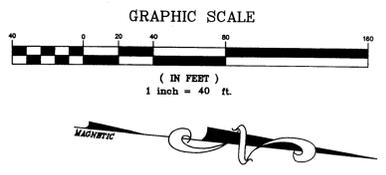
RETAINING WALL

SIGN LEGEND

1 STOP R1-1 24"x24"	2 RESERVED PARKING R7-BE R7-BA R7-BD	3 RESERVED PARKING R7-BA R7-BD
4 STOP R1-1 30"x30"	5 DO NOT ENTER R5-1	

SEE C.M.D.S. NO. 50.108 & 50.109 FOR SUPPLEMENTAL SIGN DETAIL.
SEE C.M.D.S. NO. 50.108 FOR SUPPLEMENTAL SIGN DETAIL.

PINNACLE POINT, LLC
801 EAST TRADE STREET
SUITE 200
CHARLOTTE, NC 28202
DB 165889 PG 916
TAX ID: 047-181-25
ZONED: R-12MF (CD)
PORTION OF FORMER TRACT 4
MALLARD CREEK VILLAGE
AND
MALLARD CREEK WOODS
MB 39/PG'S 119, 121 AND 123
USE: VACANT

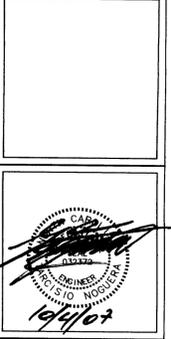


REVISED PER CITY OF CHARLOTTE COMMENTS

NO.	DATE	DESCRIPTION
1	8/17/07	

200 S COLLEGE ST
SUITE 720
CHARLOTTE, NC 28202
T 704.334.7925
F 704.334.7926
www.stewart-eng.com

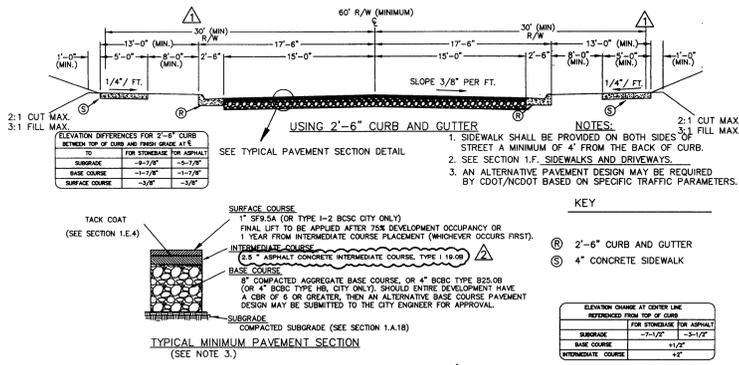
STEWART



ASHLEY COURT APARTMENTS
DEVELOPED BY: ASHLEY COURT APARTMENTS, LLC
CHARLOTTE, NORTH CAROLINA

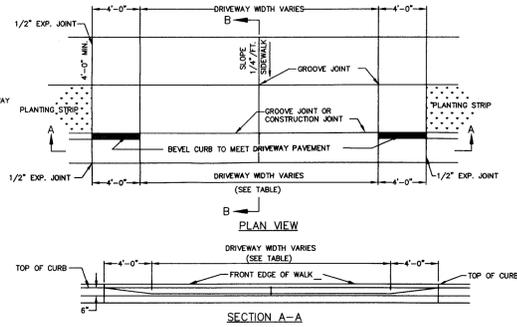
ENLARGED SITE PLAN

DATE:	5-31-07
SCALE:	HORIZ: 1" = 40'
JOB NO.	X6031.00
SHEET:	C2.2

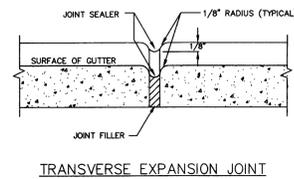


1 SALERNO DRIVE STREET TYPICAL SECTIONS
N.T.S. CMLDS STD. DTL. 11.06(MODIFIED)

- NOTES:**
- ALL CONCRETE TO BE 3600 P.S.I. COMPRESSIVE STRENGTH.
 - AT ALL DRIVEWAYS, SIDEWALKS TO BE REMOVED TO THE NEAREST JOINT BEYOND NEW CONSTRUCTION OR CUT WITH A SAW AND REMOVED. SAW CUT OR JOINT TO BE PERPENDICULAR TO EDGE OF EXISTING PAVEMENT. SEE ST. NO. 10.17 FOR JOINT DETAIL.
 - ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDDT REQUIREMENTS. SORT DISTANCE AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
 - "A" BREAKOVER SHALL BE .8% OR LESS (A = ALGEBRAIC DIFFERENCE).
 - PRIOR APPROVAL IS REQUIRED BY CDOT ON GRADES EXCEEDING WHAT ARE SHOWN.



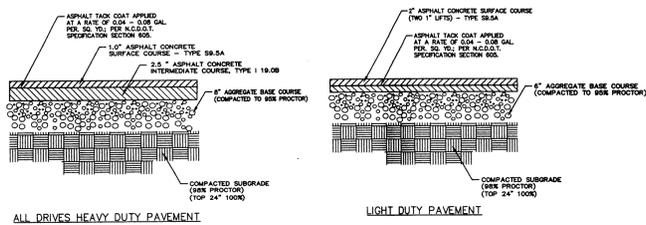
2 COMMERCIAL DROP CURB TYPE II DRIVEWAY WITH PLANTING STRIP (2'-6" CURB AND GUTTER)
N.T.S. CMLDS STD. DTL. 10.25B



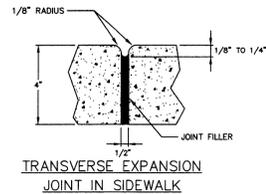
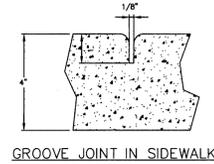
NOTES:

- CONTRACTION JOINTS SHALL BE SPACED AT 10-FOOT INTERVALS. FOR VALLEY GUTTER, A 10-FOOT SPACING MAY BE USED WHEN A MACHINE IS USED. JOINT SPACING MAY BE ALTERED BY THE CITY ENGINEER TO PREVENT UNCONTROLLED CRACKING.
- CONTRACTION JOINTS MAY BE INSTALLED BY THE USE OF TEMPLATES OR FORMED BY OTHER APPROVED METHODS. WHERE SUCH JOINTS ARE NOT FORMED BY TEMPLATES, A MINIMUM DEPTH OF 1 1/2" SHALL BE OBTAINED.
- ALL EXPANSION JOINTS SHALL BE SPACED AT 90-FOOT INTERVALS, AND ADJACENT TO ALL RIGID OBJECTS. JOINTS SHALL MATCH LOCATIONS WITH JOINTS IN ADJUTING SIDEWALK.
- CONCRETE COMPRESSIVE STRENGTH SHALL BE 3600 P.S.I. IN 28 DAYS.
- CURB SHALL BE DEPRESSED AT INTERSECTIONS TO PROVIDE FOR FUTURE ACCESSIBLE RAMPS.
- TOP 6" OF SUBGRADE BENEATH THE CURB AND GUTTER SHALL BE COMPACTED TO 100% STANDARD PROCTOR DENSITY.

3 CURB AND GUTTER
N.T.S. CMLDS STD. DTL. 10.17A

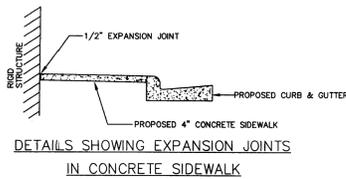


4 ASPHALT PAVEMENT SECTION
N.T.S.

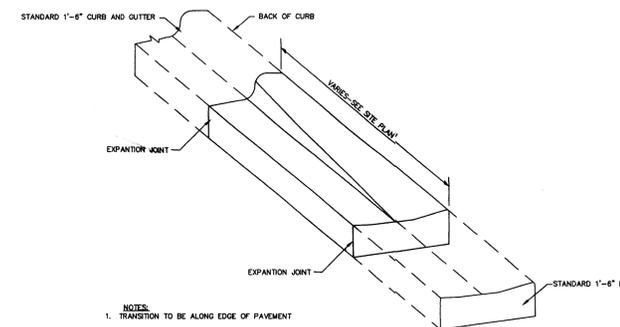


GENERAL NOTES:

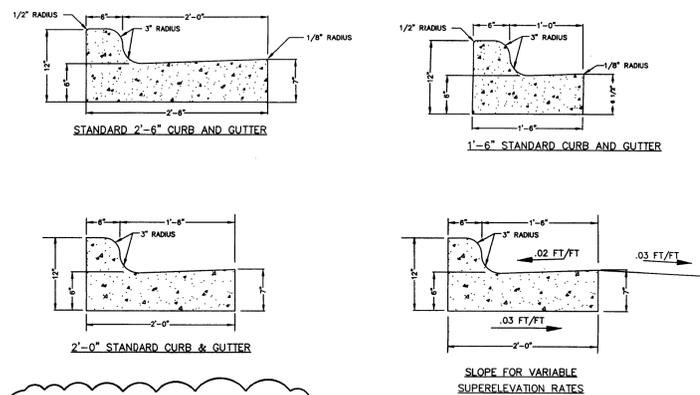
- A GROOVE JOINT 1" DEEP WITH 1/8" RADI SHALL BE REQUIRED IN THE CONCRETE SIDEWALK AT 5' INTERVALS. ONE 1/2" EXPANSION JOINT WILL BE REQUIRED AT 45' INTERVALS NOT TO EXCEED 50' AND MATCHING EXPANSION/CONSTRUCTION JOINT IN ADJACENT CURB. A SEALED 1/2" EXPANSION JOINT WILL BE REQUIRED WHERE THE SIDEWALK JOINS ANY RIGID STRUCTURE.
- SIDEWALK AT DRIVEWAY ENTRANCES TO BE 6" THICK.
- WIDTH OF SIDEWALK ON THOROUGHFARE STREETS SHALL BE A MINIMUM OF 5'. WIDTH OF SIDEWALKS IN THE CENTRAL BUSINESS DISTRICT WILL BE DETERMINED BY THE CDOT.
- WIDTH OF SIDEWALKS ON NON-THOROUGHFARE STREETS SHALL BE A MINIMUM OF 4'.
- SIDEWALK TO BE POURED TO END OF RADIUS AT INTERSECTING STREETS.
- CONCRETE COMPRESSIVE STRENGTH SHALL BE 3600 P.S.I. IN 28 DAYS.
- ZONING CONDITIONS MAY REQUIRE ADDITIONAL WIDTH SIDEWALKS WHICH SHALL SUPERSEDE THESE STANDARD DIMENSIONS SHOWN.



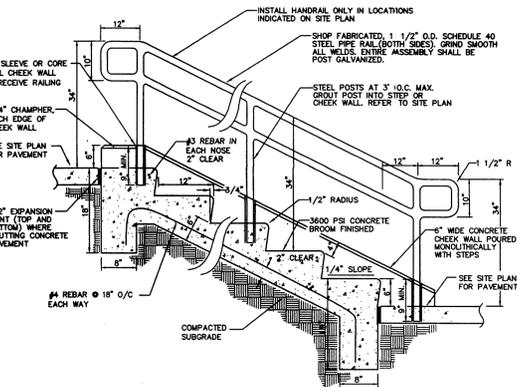
5 CONCRETE SIDEWALKS
N.T.S. CMLDS STD. DTL. 10.22



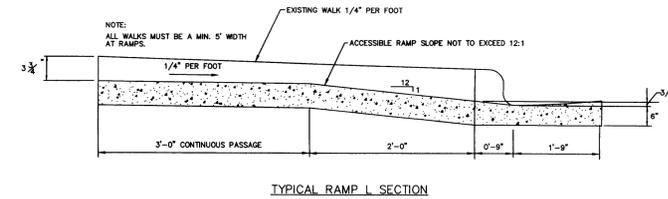
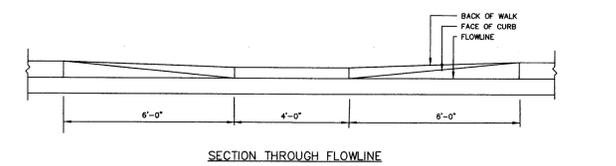
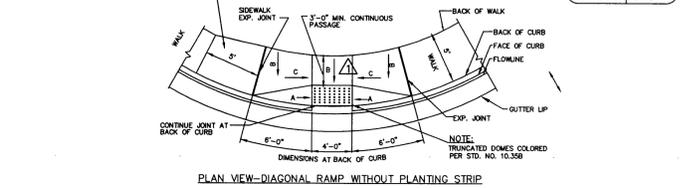
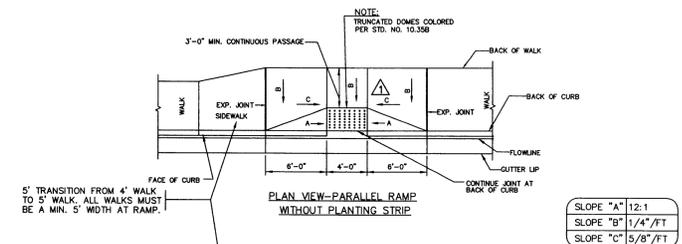
6 CURB TRANSITION FROM 1'-6" STANDARD CURB & GUTTER TO 1'-6" FLUSH CURB
N.T.S.



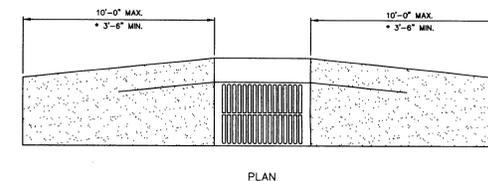
3A STANDARD CURB AND GUTTER
N.T.S. CMLDS STD. DTL. 10.17A



8 CONCRETE STEPS
N.T.S.



9 ACCESSIBLE RAMP STANDARD WITHOUT PLANTING STRIP 1'-6" CURB AND GUTTER
N.T.S. CMLDS STD. DTL. 10.32 (MODIFIED)



7 CATCH BASIN FRAME IN 1'-6" CURB & GUTTER
N.T.S. CMLDS STD. DTL. 10.29 (MODIFIED)

REVISION PER CITY OF CHARLOTTE COMMENTS
REVISED PER CITY OF CHARLOTTE COMMENTS

NO.	DATE	DESCRIPTION
1	8/17/07	
2	10/07	

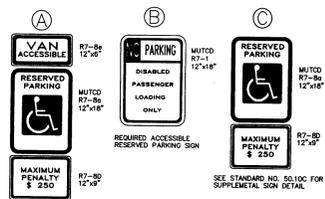
200 S COLLEGE ST
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STEWART

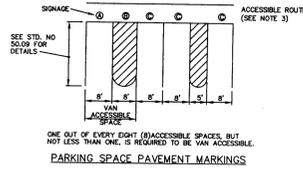
ASHLEY COURT APARTMENTS
DEVELOPED BY: ASHLEY COURT APARTMENTS, LLC
CHARLOTTE, NORTH CAROLINA

SITE DETAILS

DATE: 05.31.07
SCALE: N/A
JOB NO. X6031.00
SHEET: C2.3



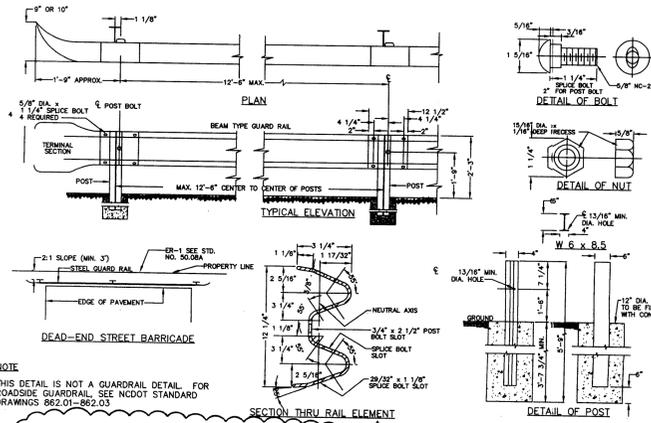
SEE STANDARD NO. 50.108 & 50.100 FOR SUPPLEMENTAL SIGN DETAIL.



- NOTES:
- ALL 12"x18" ACCESSIBLE SIGNS (R7-8a & R7-1) SHALL BE MOUNTED AT 7 FEET FROM GRADE TO BOTTOM EDGE OF SIGN FACE (NUTS). MOUNTING HEIGHT CAN BE REDUCED TO 5 FEET IF PLACED IN AN AREA BETWEEN SIDEWALK AND BUILDING FACE IN WHICH PEDESTRIANS ARE NOT EXPECTED TO USE.
 - REFER TO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) U.S. DEPARTMENT OF TRANSPORTATION AND NORTH CAROLINA DEPARTMENT OF TRANSPORTATION SUPPLEMENT.
 - IF ACCESSIBLE ROUTE IS A RAISED SIDEWALK AREA, THEN RAMPERS ARE REQUIRED AT LOADING ZONE AREA.

11 ACCESSIBLE PARKING & SIGNAGE STANDARDS
N.T.S.

CLDS STD. DTL. 50.10A(MODIFIED)



NOTE
THIS DETAIL IS NOT A GUARDRAIL DETAIL FOR ROADSIDE GUARDRAIL. SEE NCDOT STANDARD DRAWINGS 962.01-962.03

14 DEAD END STREET BARRICADE
N.T.S.

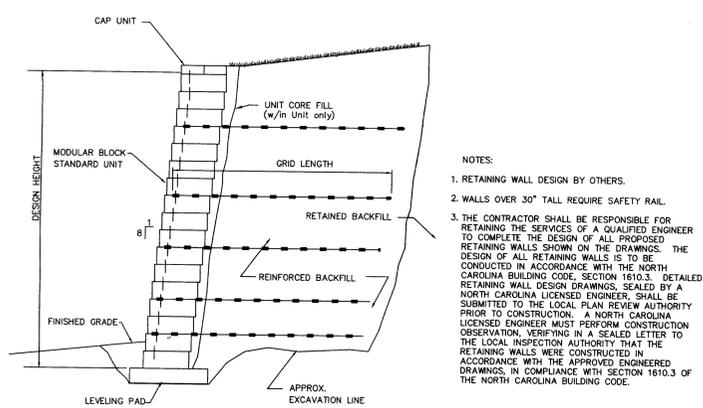
CLDS STD. DTL. 50.07A

GENERAL NOTES:

- STEEL BEAM TYPE GUARD RAILS SHALL BE INSTALLED AT THE END OF ALL DEAD-END STREETS, EXCEPT CUL-DE-SAC STREETS WHICH HAVE BEEN IMPROVED WITH A PERMANENT TURN-AROUND.
- FOR STREETS 26' IN WIDTH THE GUARD RAIL SHALL CONSIST OF TWO(2) 12'-6" SECTIONS OR ONE(1) 25' SECTION, THREE (3) STEEL POSTS, AND TWO (2) TERMINAL SECTIONS. FOR STREETS GREATER THAN 25' IN WIDTH THE GUARD RAIL SHALL SPAN THE ENTIRE WIDTH OF THE STREET.
- GUARD RAIL SHALL CONSIST OF RAIL ELEMENTS FABRICATED TO DEVELOP CONTINUOUS BEAM STRENGTH AND INSTALLED AS SHOWN.
- MINIMUM THICKNESS OF GUARD RAIL SHALL BE 12 GAGE U.S. STANDARD. THE RAIL ELEMENT INCLUDING SPLICES, SHALL HAVE A MINIMUM ULTIMATE TENSILE STRENGTH OF 80,000 LBS. GUARD RAIL PARTS FURNISHED SHALL BE INTERCHANGEABLE WITH SIMILAR PARTS REGARDLESS OF THE SOURCE OF MANUFACTURER. THE HOLES FOR CONNECTING BOLTS SHALL BE PUNCHED OR DRILLED, BURNING WILL NOT BE PERMITTED.
- THE GUARD, BOLTS, NUTS, STEEL POSTS, AND ALL OTHER METAL PARTS SHALL BE GALVANIZED TO CONFORM TO THE REQUIREMENTS FOR THE COATING CLASS, (2.50 OUNCES PER SQUARE FOOT) OF THE CURRENT SPECIFICATIONS FOR ZINC-COATED (GALVANIZED) IRON, AND STEEL SHEETS, COLS, AND CUT LENGTHS, IN ACCORDANCE WITH ASTM 123A.
- IF THE AVERAGE SPELTER COATING AS DETERMINED FROM THE REQUIRED SAMPLES IS LESS THAN TWO (2) OUNCES OF SPELTER PER SQUARE FOOT, OR IF ANY ONE SPECIMEN HAS LESS THAN 1.8 OUNCES OF SPELTER PER SQUARE FOOT OF DOUBLE EXPOSED SURFACE, THE SUCH AS BUSTERS, FLUX, AND UNCOATED SPOTS.
- THE GUARD RAIL SHALL BE INSPECTED TO DETERMINE THAT THE MATERIAL, DIMENSIONS, AND WORKMANSHIP ARE IN ACCORDANCE WITH THIS PLAN.
- WHERE A DEAD-END STREET REQUIRES GUARD RAIL, END OF ROADWAY MARKER SIGNS SHALL ALSO BE REQUIRED. (SEE STD. 50.08A & 50.08B) (ER-1).

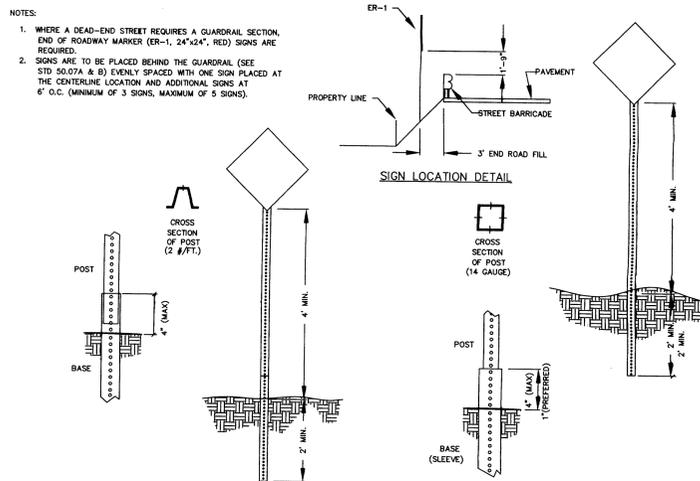
14 DEAD END STREET BARRICADE - GENERAL NOTES
N.T.S.

CLDS STD. DTL. 50.07B



- NOTES:
- RETAINING WALL DESIGN BY OTHERS.
 - WALLS OVER 30' TALL REQUIRE SAFETY RAIL.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR RETAINING THE SERVICES OF A QUALIFIED ENGINEER TO COMPLETE THE DESIGN OF ALL PROPOSED RETAINING WALLS SHOWN ON THE DRAWINGS. THE DESIGN OF ALL RETAINING WALLS IS TO BE CONDUCTED IN ACCORDANCE WITH THE NORTH CAROLINA BUILDING CODE, SECTION 1610.3. DETAILED RETAINING WALL DESIGN DRAWINGS, SEALED BY A NORTH CAROLINA LICENSED ENGINEER, SHALL BE SUBMITTED TO THE LOCAL PLAN REVIEW AUTHORITY PRIOR TO CONSTRUCTION. A NORTH CAROLINA LICENSED ENGINEER MUST PERFORM CONSTRUCTION OBSERVATION, VERIFYING IN A SEALED LETTER TO THE LOCAL INSPECTION AUTHORITY THAT THE RETAINING WALLS WERE CONSTRUCTED IN ACCORDANCE WITH THE APPROVED ENGINEERED DRAWINGS, IN COMPLIANCE WITH SECTION 1610.3 OF THE NORTH CAROLINA BUILDING CODE.

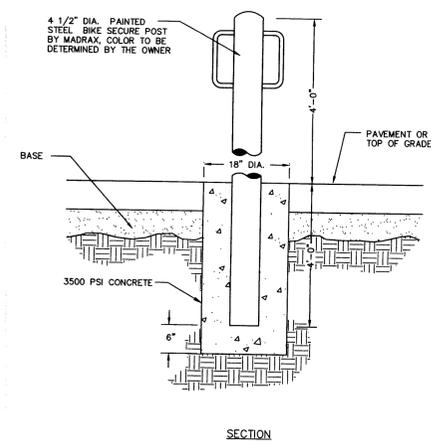
12 TYPICAL MODULAR BLOCK RETAINING WALL SECTION
N.T.S.



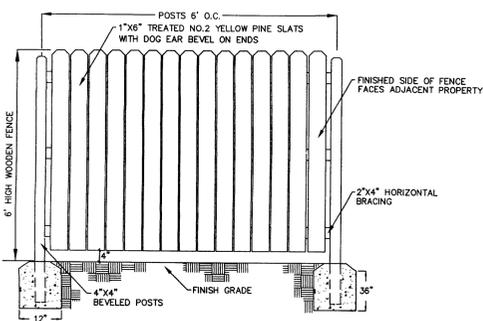
- NOTES:
- WHERE A DEAD-END STREET REQUIRES A GUARDRAIL SECTION, END OF ROADWAY MARKER (ER-1, 24"x24", RED) SIGNS ARE REQUIRED.
 - SIGNS ARE TO BE PLACED BEHIND THE GUARDRAIL (SEE STD 50.07A & B) EVENLY SPACED WITH ONE SIGN PLACED AT THE CENTERLINE LOCATION AND ADDITIONAL SIGNS AT 6' O.C. (MINIMUM OF 3 SIGNS, MAXIMUM OF 5 SIGNS).

15 END OF ROADWAY MARKER (ER-1)
N.T.S.

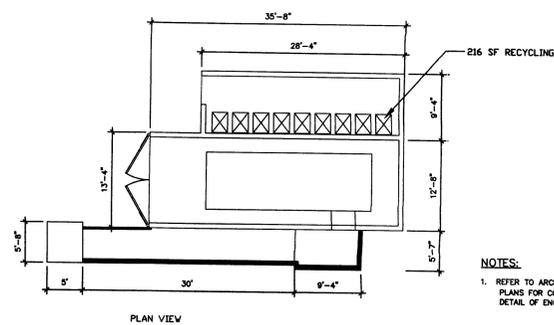
CMCLS STD. DTL. 50.08A



17 BIKE SECURE POST BOLLARD
N.T.S.



13 WOOD PRIVACY FENCE
N.T.S.

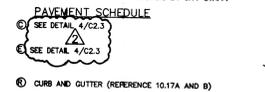


- NOTES:
- REFER TO ARCHITECTURAL PLANS FOR CONSTRUCTION DETAIL OF ENCLOSURE.

16 COMPACTOR AND RECYCLING ENCLOSURE
N.T.S.

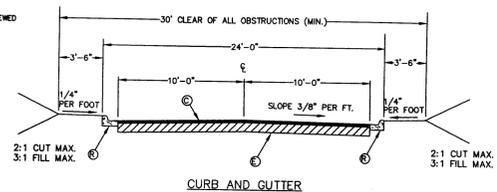
GUIDELINES FOR PRIVATE STREET DESIGN:

- INTERNAL STREET ALIGNMENT:
MAXIMUM GRADE: 10%
MINIMUM VERTICAL CURVE "X" VALUES: 10/20 (CREST/SAO)
MINIMUM HORIZONTAL CURVE CENTERLINE RADIUS: 50 FT.
- INTERSECTION WITH PUBLIC STREET:
SAME AS FOR PUBLIC STREET. SEE GENERAL NOTES, SECTION 1.8.2.
OR MAXIMUM GRADE WITHIN 40 FEET OF PUBLIC STREET INTERSECTION BEGINNING FROM EDGE OF PAVEMENT LINE.



NOTES:

- DETAILS SHOWN SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY STANDARDS.
- DITCH TYPE STREET REQUIRES APPROVAL OF CITY ENGINEER.
- MINIMUM CURB RADIUS ON INTERIOR DRIVES AND PARKING AREAS IS 10'



18 TYPICAL SECTIONS PRIVATE STREET
N.T.S.

CLDS STD. DTL. 11.13 (MODIFIED)

NO.	DATE	DESCRIPTION
1	8/17/07	REVISED PER CITY OF CHARLOTTE COMMENTS
2	10/4/07	REVISED PER CITY OF CHARLOTTE COMMENTS

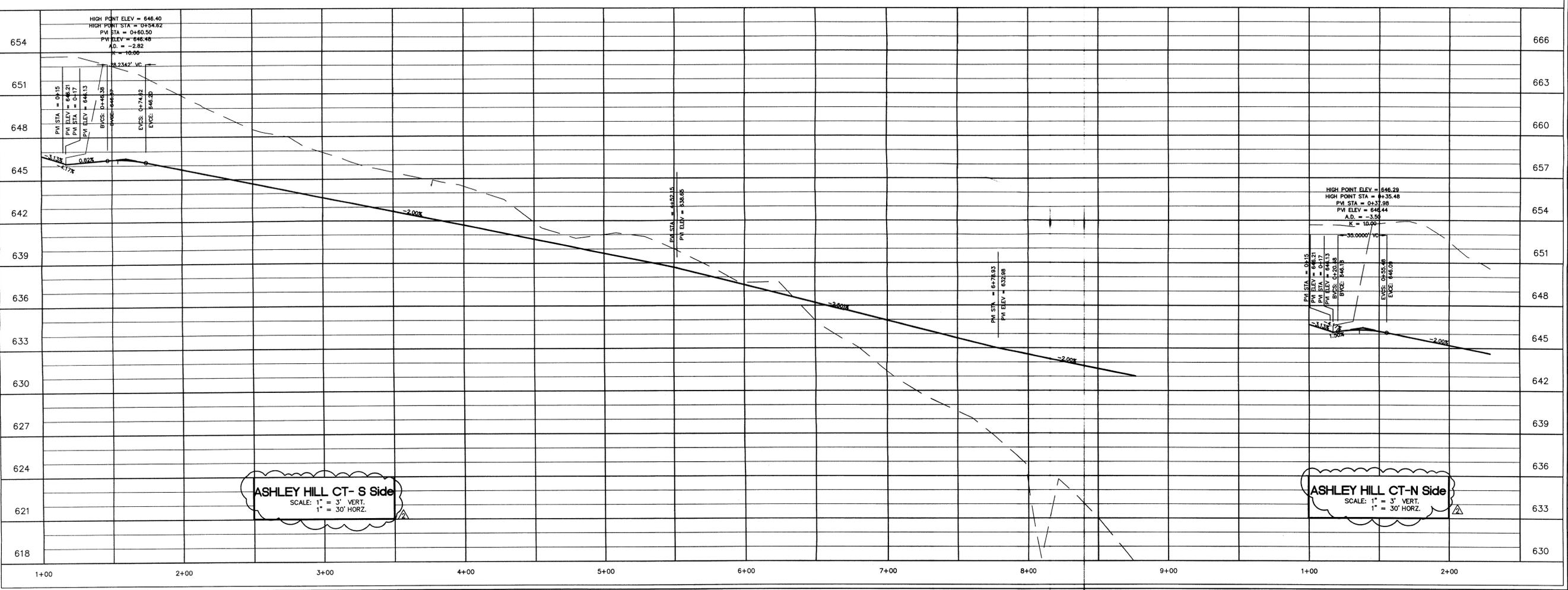
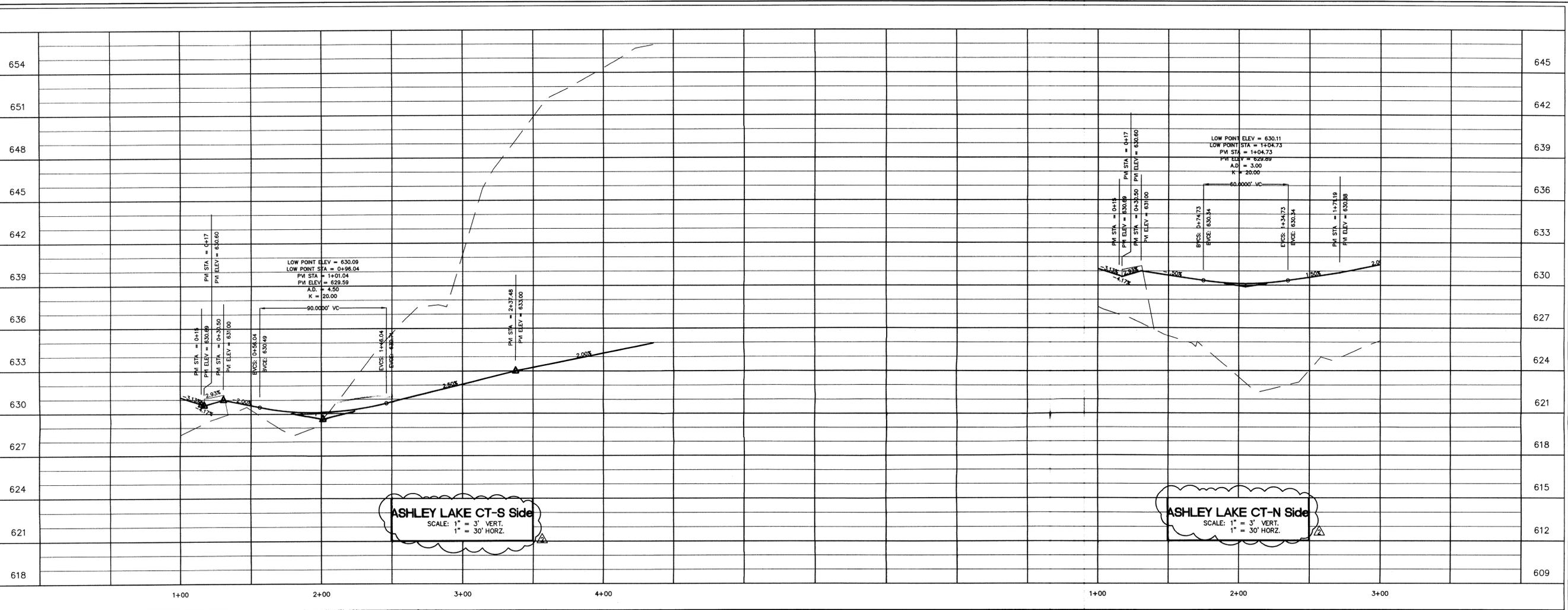
2015 COLLEGE ST
CHARLOTTE, NC 28202
T 704-334-7925
F 704-334-7926
www.stewarteng.com

STEWART

Professional Engineer
No. 332372
State of North Carolina
10/4/07

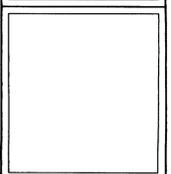
ASHLEY COURT APARTMENTS
DEVELOPED BY: ASHLEY COURT APARTMENTS, LLC
CHARLOTTE, NORTH CAROLINA

SITE DETAILS	
DATE:	05.31.07
SCALE:	N/A
JOB NO.:	X6031.00
SHEET:	C2.4



NO.	DATE	DESCRIPTION
1	01/17/07	REVISED PER CITY OF CHARLOTTE COMMENTS
2	10/04/07	REVISED PER CITY OF CHARLOTTE COMMENTS

200 S COLLEGE ST
SUITE 720
CHARLOTTE, NC 28202
P 704.334.7226
F 704.334.7226
www.stewart-eng.com



10/4/07

ASHLEY COURT APARTMENTS
DEVELOPED BY: ASHLEY COURT APARTMENTS, LLC
CHARLOTTE, NORTH CAROLINA

STREET PROFILES	
DATE:	05.31.07
SCALE:	1" = 30'
JOB NO.	X6031.00
SHEET:	C2.6

