

Rezoning Transportation Analysis

Petition Number: 2019-124

General Location Identifier: 16105114, 16105113

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Revision Log:

Date	Description
09-27-19	First Review
11-12-19	Second Review

General Review Information

The site is on Monroe Road (major thoroughfare, state maintained) and is located in a corridor outside Route 4. The site is within the limits of the Independence Boulevard Area Plan.

Active Projects Near the Site:

- Monroe Road Streetscape
 - The project will develop a transformative community along Monroe Road from Briar Creek Road to Sharon Amity Road. Design and planning is currently taking place on this project. Phase I improvements in this project are to be implemented by April 2019.
- South Pedestrian and Bike Boulevard
 - The project will improve pedestrian and bicycle connectivity by providing a network paralleling Independence Boulevard to improve overall connectivity.
- Oakhurst-Amity Garden Street Connector
 - The project consists of realigning Shade Valley Road with Commodore Street and Monroe Road. The project will provide left-turn lanes on Monroe Road and a traffic signal at the intersection. In addition, the project will provide a roundabout at the intersection of Shade Valley Road and Oakhurst Commons Drive.
- Pierson Drive Extension
 - This project will extend the existing Pierson Drive at Wal-Mart on Independence Boulevard and connect it to Monroe Road. This connection will provide connectivity for neighborhoods on both sides of Independence Boulevard to increase mobility for transit user(s).
- Sharon Amity Sidewalk Improvements
 - This project will provide sidewalk on south side of N. Sharon Amity Road from Tangle Drive to Craig Avenue. The project length is .32 miles.
- Idlewild Road and Monroe Road Intersection
 - This project will identify improvements at the Idlewild Road and Monroe Road intersection to enhance conditions for all users including vehicles, pedestrians, bicyclists, and transit users. Improvements will include pavement widening to accommodate dual left-turn lanes at all intersection approaches incorporating bicycle lanes, pedestrian refuge islands, new sidewalks, and various improvements.

CDOT's review of this rezoning petition is intended to ensure consistency with the Transportation Action Plan (TAP) which seeks to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

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Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on a major thoroughfare. The petitioner has committed to improving street connectivity in the area by providing a public street and stub connection in accordance to the Subdivision Ordinance. CDOT is requesting the petitioner assist in the Monroe Road Streetscape Project to include a 10-foot shared use path on the Monroe Road frontage.

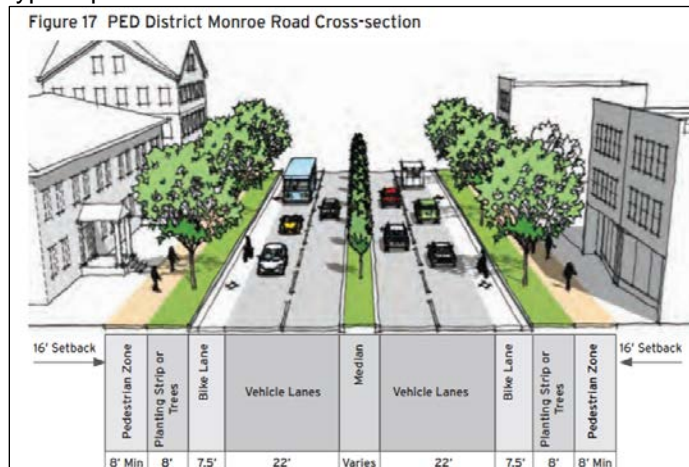
Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Office	14,540 sf	160	Tax Record
Entitlement with Current Zoning	Apartments (3.69 ac of R-17MF)	62 dwellings	560	General Guidance from Planning and RZ 1985-083
	Office	11,860 sf		
Proposed Zoning	Townhomes	75 dwellings	530	Site Plan: 11-08-19

Outstanding Issues

~~Strikeout~~ = Not an outstanding issue

- ~~**Curbline** The proposed zoning district has a setback measured from back of the existing or proposed future curbline.~~
 - ~~**Monroe Road:** The future location of curb and gutter is in its existing location.~~
- ~~**Traffic Study** A Traffic Impact Study is not necessary for the complete review of this petition.~~
- ~~**Comment Rescinded (Site Plan 11-12-19):** The petitioner should revise the site plan and conditional note(s) to commit to dedicate 45 feet right-of-way from the road centerline along Monroe Road (Parcel ID 161-05-113 and 161-05-114). The site plan should label and dimension the right-of-way from the road centerline.~~
- ~~The petitioner should revise the site plan and conditional note(s) to update the existing streetscape to comply with the Chapter 19 Ordinance. The petitioner is required to construct 8-foot planting strip, and 8-foot sidewalk along Monroe Road (Parcel ID 161-05-113 and 161-05-114). The Independence Boulevard Area Plan typical plan is shown below.~~

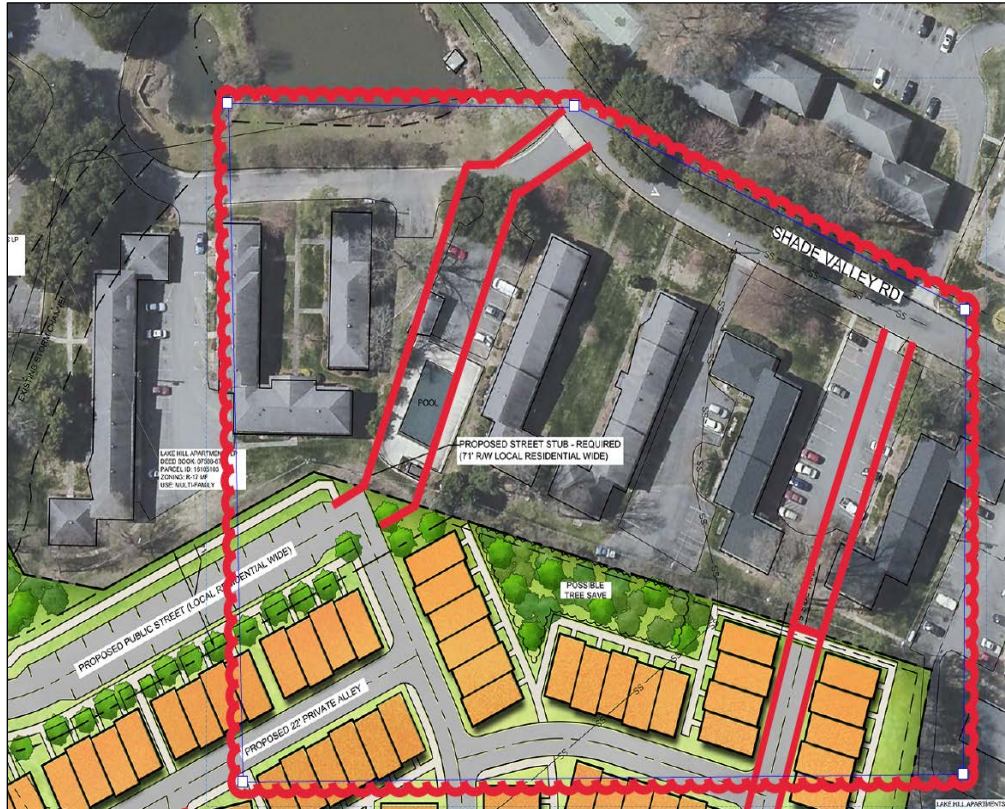


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5. CDOT is requesting the petitioner revise the site plan and conditional note(s) to commit to construct a 10-foot shared use path, at the back of curb, on Monroe Road property frontage (Parcel ID 161-05-113 and 161-05-114) to meet the City's Monroe Road Streetscape Project. The site plan should label and dimension both items from the back of curb and gutter and road centerline.
6. ~~The petitioner should revise the site plan to provide USDG typical sections for all private streets.~~
 - a. ~~In addition, provide a public access easement on the following private streets shown in the illustration below (shown in red) to accommodate future street connectivity.~~



- b. ~~The petitioner should reconfigure the internal street network to provide future street connectivity between Shade Valley Road and Eaton Road. See CDOT suggestions illustrated in red.~~

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7. NCDOT will require a left turn restriction during peak hours (7 a.m. to 9 a.m. and 4 p.m. to 6 p.m.) at the Monroe driveway allowing for right turns in, right turns out, and left turns into the site.
 - a. The petitioner should update the site plan and conditional note(s) committing to the installation of signage at the entrance of the proposed driveway access.

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8. The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.