

# Rezoning Transportation Analysis

Petition Number: 2019-007

General Location Identifier: 07501106

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## Revision Log:

Date	Description
02-13-2019	First Review

## General Review Information

The site is at the signalized intersection of Beatties Ford Road (major thoroughfare) and Gilbert Street (local) and is located in a wedge inside Route 4. The site is within the limits of the West End Land Use & Pedscape Plan. The site's location is served by two CATS bus routes (i.e.: #7 and #26).

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*CDOT's review of this rezoning petition is intended to ensure consistency with the Transportation Action Plan (TAP) which seeks to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.*

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*This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.*

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

To be provided prior to public hearing.

## Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	N/A	0	Tax Record
Entitlement with Current Zoning	Retail (0.51 ac of B-1)	5,100 sf	800	General Guidance from Planning
Proposed Zoning	Fast Food w/ Drive Thru	2,340 sf	1,100	Site Plan: 12-21-18

## Outstanding Issues

~~Strikeout~~ = Not an outstanding issue

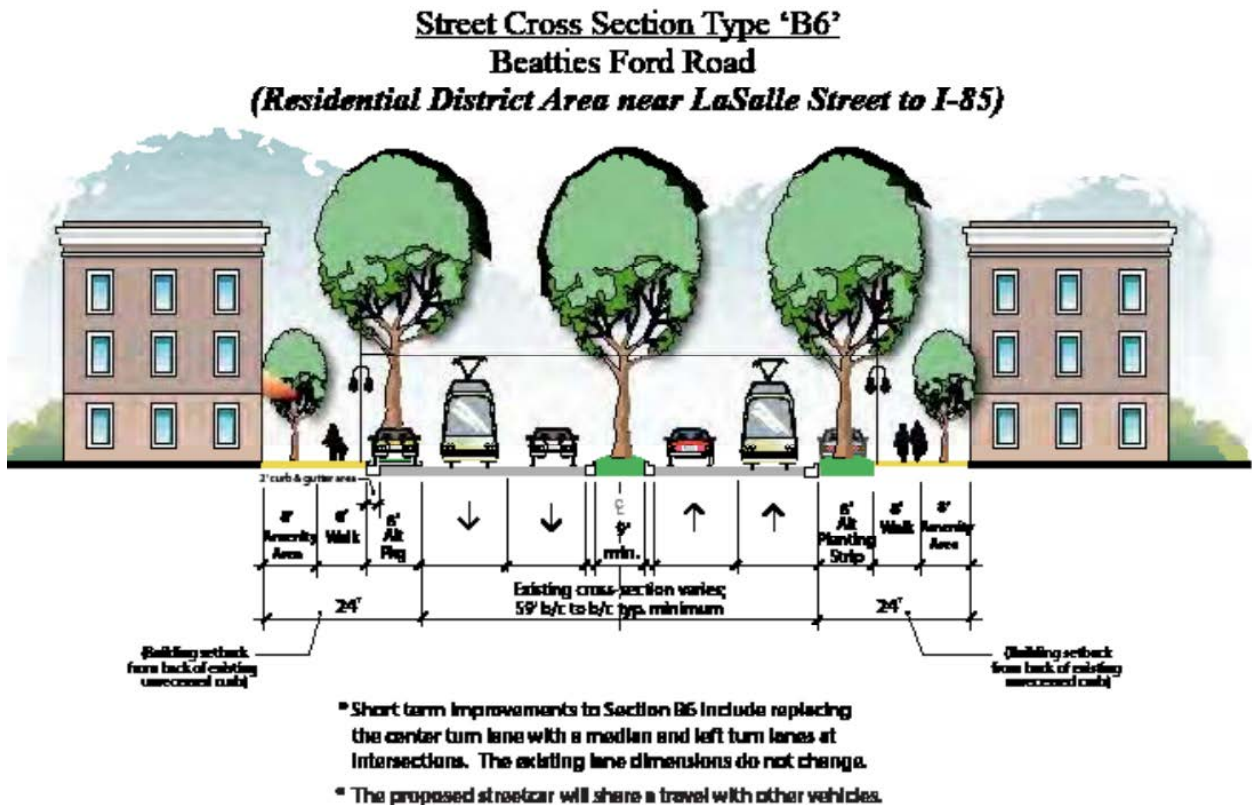
- Curbline:** The proposed zoning district has a setback measured from back of the existing or proposed future curbline.
  - Beatties Ford Road** - The location of the future back curb for Beatties Ford Road is 37.5' as measured from the street's existing centerline (note: this measurement includes an 8' buffered bike lane). The site plan shall commit to locating the curb and gutter along its frontage, labeling, and dimension it.
  - Gilbert Street** - The location of the future back of curb for Gilbert Street can remain as is.

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2. **Traffic Study:** A Traffic Impact Study/Transportation Technical Memorandum is not necessary for the complete review of this petition.
3. The petitioner shall revise the site plan and conditional note(s) to commit to dedicate the right-of-way for the below Beatties Ford Road streetscape plan, as depicted in the West End Land Use & Pedscape Plan and adopted by Charlotte City Council November 14, 2005.
  - a. Since 2005, CDOT's bicycle facility needs have expanded. The petitioner shall reserve space to provide an 8' wide separated (i.e. buffered) bike lane along the site's Beatties Ford Rd. frontage in addition to the Street Cross Section "Type B6" between the back of curb and planting strip.
  - b. The 8-foot planting strip and 6-foot sidewalk shall be constructed on its future location. The site plan and conditional note(s) shall commit to the construction of the streetscape.
  - c. The petitioner should revise the site plan and conditional note(s) to commit to dedicate 61.5' right-of-way in fee simple along the site's Beatties Ford Rd. frontage as measured from the street's existing centerline.
  - d. The petitioner shall label revise the site plan to label and dimension future streetscape.



4. The petitioner should revise the site plan and conditional note(s) to commit to close all existing driveways and provide an 8' planting strip and 6' sidewalk along the site's Gilbert St. frontage.
  - a. One (1) full-access driveway on Gilbert St. will be allowed as close to the site's eastern property line as possible.
5. The petitioner should revise the site plan and conditional note(s) to commit to close all existing driveways and provide an 8-foot planting strip and 8-foot sidewalk along the site's Beatties Ford Road frontage.

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- a. One (1) right-in/right-out driveway on Beatties Ford Road will be allowed as close to the site's northern property line as possible.
  - b. The petitioner will need to implement a 4-foot wide raised concrete median approximately 100' long; as measured from the existing stop bar on Beatties Ford Rd. along the site's frontage to restrict the proposed driveway to right-in/right-out traffic movements. The driveway needs to be restricted due to the proximity to existing signalized intersection and reduce conflict points on Beatties Ford Road.
6. The petitioner should revise the site plan and conditional note(s) to commit to implement two (2) accessible ramps to meet current PROWAG standards. These two ramps are shown in the exhibit below. The site will generate pedestrian traffic and the need for pedestrians to cross the road. The new accessible ramps will serve the new pedestrian traffic being generated by the development.



7. The petitioner should revise the site plan to clearly depict and label the adjacent street's existing centerline, existing and proposed right-of-way lines, building setback lines, 8' planting strips and sidewalk widths.
8. The petitioner should revise the site plan to add a note specifying dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible.
9. The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.

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10. The site plan shall revise the site plan and conditional note(s) to commit to provide a 5-foot sidewalk connection from the building to Beatties Ford Road and Gilbert Street, as required by the ordinance. The location of the sidewalk connection should be placed in locations that minimize vehicular conflict.

## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.