


Charlotte Department of Transportation

Memorandum

Date: June 2, 2015

To: Tammie Keplinger
Charlotte-Mecklenburg Planning Department

From: Michael A. Davis, PE 
Development Services Division

Subject: Rezoning Petition 15-087: Approximately 72.2 acres located on the east side of Providence Road between Interstate 485 and Providence Country Club Drive.

CDOT has completed a review of the subject petition in order to ensure consistency with the Transportation Action Plan (TAP). The TAP seeks to ensure that Charlotte's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users. Based on our review of the petition, we offer the following comments.

Vehicle Trip Generation

Under the existing zoning, this site could generate 2,100 trips per day. Under the proposed zoning the site could generate approximately 8,100 trips per day. As such, CDOT has required the petitioner to perform a Traffic Impact Study (TIS) for the proposed development. The study has yet to be submitted to CDOT for review. We will have specific comments for the subject TIS in subsequent rezoning memos.

CDOT requests the following changes to the rezoning plan:

1. CDOT request the petitioner realign the intersection of the approved Golf Links Dr with the proposed Golf Links Dr extension as shown in Figure 1 below.

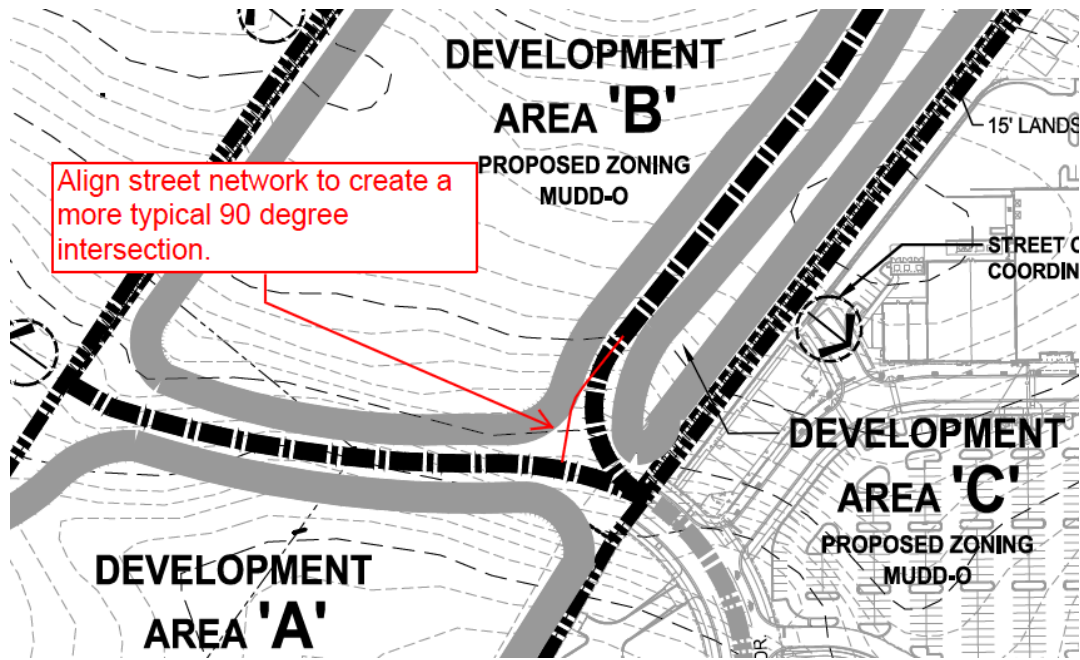


Figure 1

2. CDOT requests that the petitioner align the proposed side street roadway connections to Ardrey Kell Rd Extension as shown in Figure 2 below:

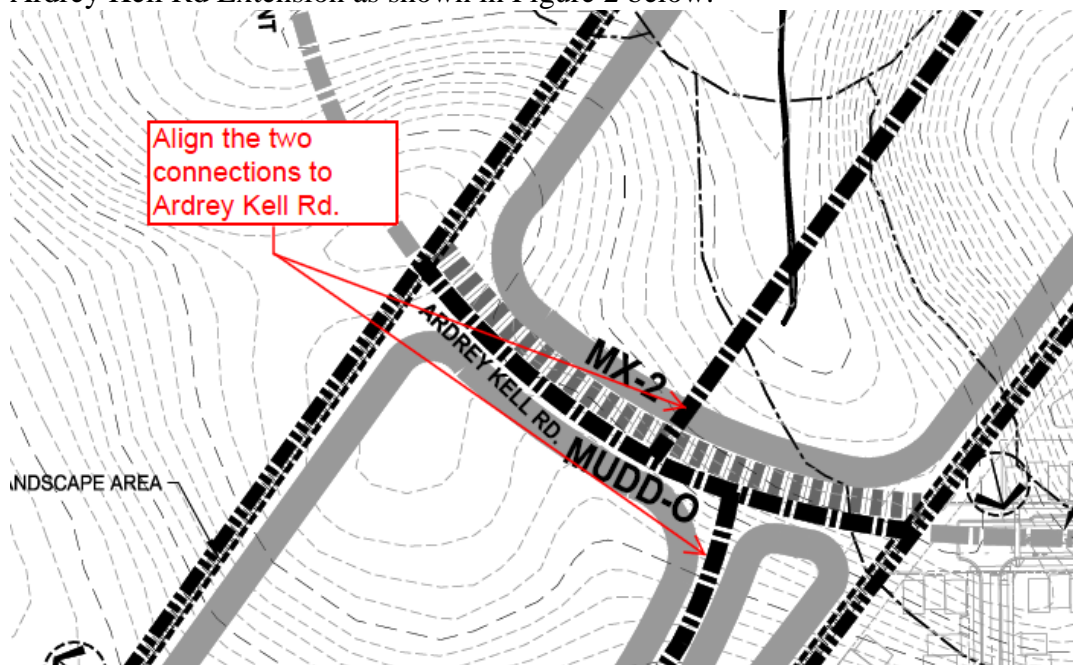


Figure 2

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte. CDOT has determined that a left-turn lane is necessary to serve the traffic using the proposed public street/private driveway connection(s) for this site. The engineering design and construction of the left-turn lane is the responsibility of the owner, and shall be performed by a professional engineer registered in the State of North Carolina who has roadway-design experience. CDOT will only approve the proposed public street/private driveway connection(s) provided that a left-turn lane is constructed on Ardrey Kell Rd., Golf Links, and all other public roads as needed. We recommend the rezoning plan reflect the design of this required left-turn lane prior to submittal/approval of the public street/private driveway connection(s). The left-turn lane needs to be designed using NCDOT standards with a minimum 150 feet of storage. This roadway improvement is required to meet the traffic demands of the proposed development.
2. Adequate sight triangles must be reserved at the existing/proposed street entrances. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrances. Such items should be identified on the site plan.
3. The proposed driveway connections to Ardrey Kell Rd. and Golf Links Dr. will require a driveway permits to be submitted to CDOT and the North Carolina Department of Transportation for review and approval. The exact driveway locations and type/width of the driveways will be determined by CDOT during the driveway permit process. The locations of the driveways shown on the site plan are subject to change in order to align with driveways on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.

We recommend the Petitioner work directly with Mr. Brett Canipe with NCDOT regarding any anticipated request.

Tammie Keplinger

June 2, 2015

Page 4 of 4

1. The developer/petitioner is recommended to meet with NCDOT early in the development process to identify any issues that they may have. Please contact Brett Canipe with NCDOT at 980-523-0002.

If we can be of further assistance, please advise.

K. Hedrick

cc: S. Correll
Rezoning File