

**SITE DATA/SUMMARY:**

**OWNERS:** Park Selwyn LLC  
P.O. Box 6122  
Charlotte, NC 28201

**SITE ADDRESS:** PARK SELWYN APARTMENTS  
2000 EAST WOODLAWN ROAD  
CHARLOTTE, NC

**TOTAL ACRES:** 121 ACRES  
**PARK 1 REC DEDICATED AREA:** 0.44 ACRES  
**PARK SELWYN, LLC AREA:** 0.11 ACRES  
**TAX PARCEL #:** 17B-1B1-31

**EXISTING ZONING:** UR-2(CD) BY PARTITION # 99-034  
**PROPOSED ZONING:** UR-2(CD) 6PA

**EXISTING USES:** 16 MULTI-FAMILY RESIDENTIAL DWELLING UNITS  
**PROPOSED USES:** TO ADD EIGHT (8) MULTI-FAMILY RESIDENTIAL DWELLING UNITS TO THE SITE ± A TOTAL OF 24 MULTI-FAMILY RESIDENTIAL UNITS TOGETHER WITH ACCESSORY USES AS ALLOWED IN THE UR-2 ZONING DISTRICT WILL BE ALLOWED ON THE SITE

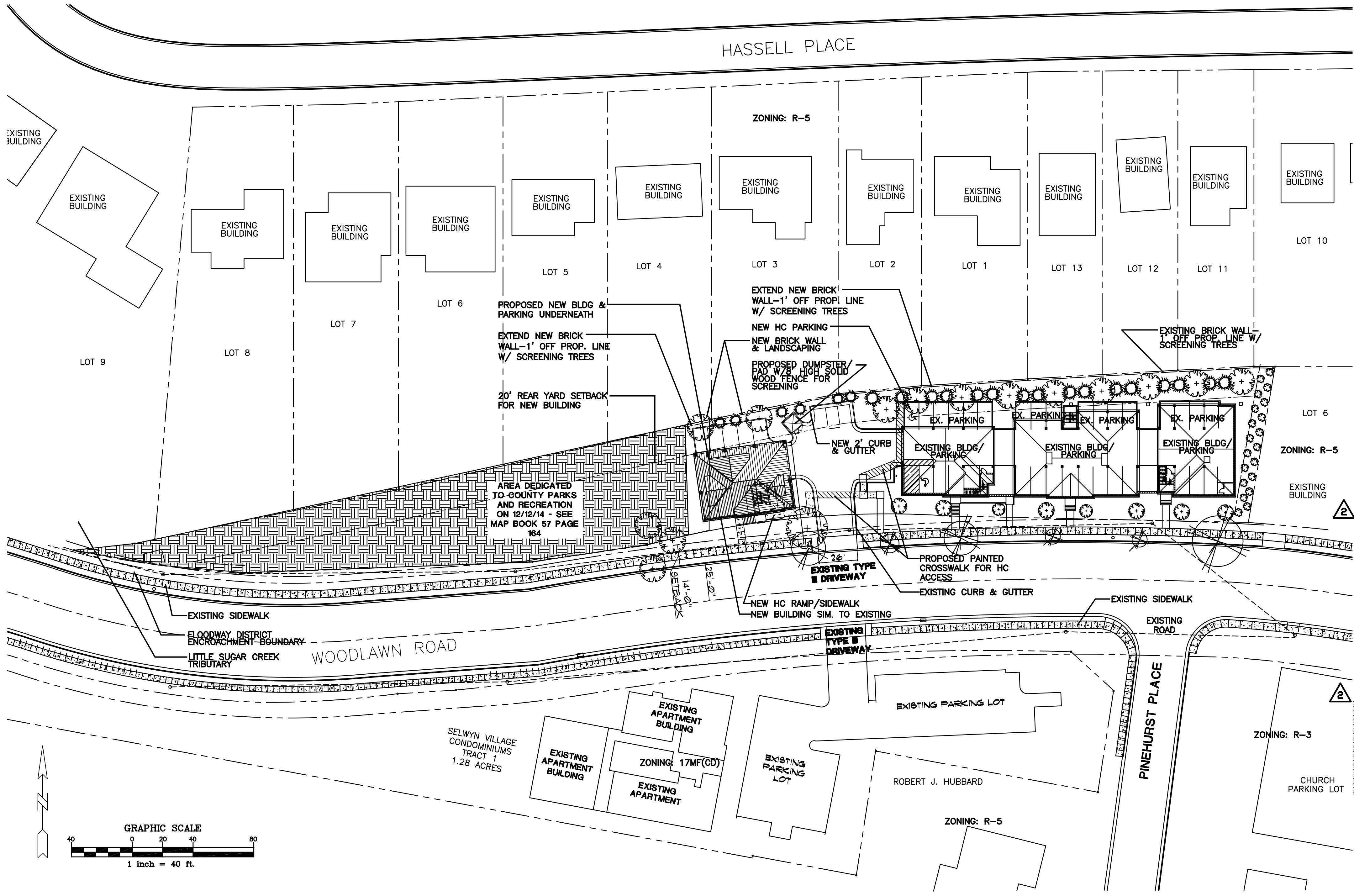
**MAXIMUM BUILDING HEIGHT:** AS ALLOWED BY THE ORDINANCE. HEIGHT WILL BE MEASURED BY THE ORDINANCE. THE NEW EIGHT (8) UNIT BUILDING WILL HAVE A THREE (3) STORY ELEVATION AS SEEN FROM WOODLAWN ROAD. THE SITE WILL NOT EXCEED A FLOOR AREA RATIO (FAR) OF 1.0. A MINIMUM OF 15 SPACES PER UNIT WILL BE PROVIDED.

**MAXIMUM FAR:** 1.0  
**PARKING:** 14 FT FROM BACK OF CURB  
**MINIMUM SETBACK:** 20 FEET  
**MINIMUM REAR YARD:** 5 FT  
**MINIMUM SIDE YARD:** 5 FT  
**SCREENING FOR PARKING:** 2'-6" H (EVERGREENS) MIN. 5' O.C.  
**SCREENING HEIGHT:** 6 FT. ABUTTING RESIDENTIAL AREA

**2 WOOD FENCE DETAIL**  
SP-1 NOT TO SCALE

**3 WOOD GATE DETAIL**  
SP-1 NOT TO SCALE

**4 SCREEN FENCE W/ MASONRY PIERS**  
SP-1 NOT TO SCALE



THE PLANNING DIRECTOR WILL DETERMINE IF SUCH MINOR MODIFICATIONS ARE ALLOWED PER THIS AMENDED PROCESS, AND IF IT IS DETERMINED THAT THE ALTERATION DOES NOT MEET THE CRITERIA DESCRIBED ABOVE, THE PETITIONER SHALL THEN FOLLOW THE ADMINISTRATIVE AMENDMENT PROCESS PER SECTION 6207 OF THE ORDINANCE. IN EACH INSTANCE, HOWEVER, SUBJECT TO THE PETITIONER'S APPEAL RIGHTS SET FORTH IN THE ORDINANCE.

1. GENERAL PROVISIONS:
  - a. SITE LOCATION: THESE DEVELOPMENT STANDARDS FORM A PART OF THE REZONING PLAN ASSOCIATED WITH THE REZONING PETITION FILED BY PARK SELWYN, LLC. (THE PETITIONER) TO ACCOMMODATE THE DEVELOPMENT OF 24 MULTI-FAMILY RESIDENTIAL DWELLING UNITS ON APPROXIMATELY ± 12 ACRES SITE LOCATED ON THE NORTH SIDE OF WOODLAWN ROAD BETWEEN SELWYN AVENUE AND BRANDYBINE ROAD (THE "SITE").
  - b. ZONING DISTRICTS/ORDINANCE: DEVELOPMENT OF THE SITE WILL BE GOVERNED BY THE REZONING PLAN AS WELL AS THE APPLICABLE PROVISIONS OF THE CITY OF CHARLOTTE ZONING ORDINANCE (THE "ORDINANCE"). UNLESS THE REZONING PLAN ESTABLISHES MORE STRINGENT STANDARDS THE REGULATIONS ESTABLISHED UNDER THE ORDINANCE FOR THE UR-2 ZONING CLASSIFICATION SHALL GOVERN.
  - c. GRAPHICS AND ALTERATIONS: THE SCHEMATIC DEPICTIONS OF THE USES, PARKING AREAS, SIDEWALKS, STRUCTURES AND BUILDINGS, DRIVEWAYS AND OTHER DEVELOPMENT MATTERS AND SITE ELEMENTS (COLLECTIVELY THE "DEVELOPMENT/SITE ELEMENTS") SET FORTH ON THE REZONING PLAN SHOULD BE REVIEWED IN CONJUNCTION WITH THE PROVISIONS OF THESE DEVELOPMENT STANDARDS. THE LAYOUT, LOCATIONS, SIZES AND FORMULATIONS OF THE DEVELOPMENT/SITE ELEMENTS DEPICTED ON THE REZONING PLAN ARE GRAPHIC REPRESENTATIONS OF THE DEVELOPMENT/SITE ELEMENTS PROPOSED. CHANGES TO THE REZONING PLAN NOT ANTICIPATED BY THE REZONING PLAN WILL BE REVIEWED AND APPROVED AS ALLOWED BY SECTION 6207 OF THE ORDINANCE.
2. PERMITTED USES & DEVELOPMENT AREA LIMITATION:
  - a. THE SITE MAY BE DEVELOPED WITH UP TO 24 RESIDENTIAL DWELLING UNITS (16 EXISTING DWELLING UNITS PLUS EIGHT (8) NEW UNITS) TOGETHER WITH ACCESSORY USES ALLOWED IN THE UR-2 ZONING DISTRICT.
  - b. SURFACE PARKING AREAS WILL NOT BE ALLOWED BETWEEN THE PROPOSED BUILDINGS AND WOODLAWN ROAD.
  - c. A 14 FOOT SETBACK WILL BE ESTABLISHED ALONG WOODLAWN ROAD. THE 14 FOOT SETBACK WILL BE MEASURED FROM THE BACK OF THE EXISTING CURB.
3. ACCESS AND TRANSPORTATION:
  - a. ACCESS TO THE SITE WILL BE FROM WOODLAWN ROAD VIA THE EXISTING DRIVEWAY.
  - b. THE PLACEMENT AND CONFIGURATION OF THE VEHICULAR ACCESS POINT IS SUBJECT TO ANY MINOR MODIFICATIONS REQUIRED TO ACCOMMODATE FINAL SITE DEVELOPMENT AND CONSTRUCTION PLANS AND TO ANY ADJUSTMENTS REQUIRED FOR APPROVAL BY CDOT IN ACCORDANCE WITH APPLICABLE PUBLISHED STANDARDS.
  - c. THE ALIGNMENT OF THE INTERNAL VEHICULAR CIRCULATION AND DRIVEWAYS MAY BE MODIFIED BY THE PETITIONER TO ACCOMMODATE CHANGES IN TRAFFIC PATTERNS, PARKING LAYOUTS AND ANY ADJUSTMENTS REQUIRED FOR APPROVAL BY CDOT IN ACCORDANCE WITH PUBLISHED STANDARDS.
4. ARCHITECTURAL STANDARDS:
  - a. THE PROPOSED BUILDING EXTERIOR FINISH SHALL BE A COMBINATION OF BRICK AND SIDING MATERIALS. THE OVERALL ARCHITECTURAL DESIGN OF THE NEW EIGHT (8) UNIT BUILDING WILL MATCH THE EXISTING BUILDINGS IN BOTH SCALE AND STYLE.
  - b. THE ATTACHED ARCHITECTURAL ELEVATIONS ARE INTENDED TO DEPICT THE GENERAL ARCHITECTURAL STYLE OF THE BUILDINGS. MINOR CHANGES AND ALTERATIONS OF DETAIL WHICH DO NOT CHANGE THE OVERALL ARCHITECTURAL THEME/STYLE ARE PERMITTED BASED UPON FINAL DESIGN/CONSTRUCTION DRAWINGS. THE BUILDINGS MAY EMPLOY COMPLEMENTARY DIFFERENCES IN COLOR PERTAINING TO BRICK, ROOF SHINGLES AND TRIM ACCENTS.
  - c. THE REAR BUILDING ELEVATION FACING THE REAR OF THE HASSELL PLACE RESIDENTS ADJOINING THE SITE SHALL CONSIST OF AN ALL MASONRY VENEER, WHICH MAY BE ACCENTED BY SIDING ON THE PERPENDICULAR AREAS NOT FACING THE HASSELL PLACE RESIDENTS.
  - d. ANY TRASH DUMPSTER LOCATED ON THE SITE SHALL BE LOCATED WITHIN AN ENCLOSURE WITH GATES.
  - e. STREETScape, BUFFERS, YARDS AND LANDSCAPING:
    - i. ALONG WOODLAWN ROAD THE EXISTING SIDEWALK AND PLANTING STRIP WILL BE MAINTAINED.
    - ii. THE REAR BUILDING SETBACK LINE SHALL BE A MINIMUM OF TWENTY FEET. THE SIDE YARD ADJACENT TO THE ROGERS PROPERTY (TAX PARCEL # 17B-1B1-36) SHALL BE AN AVERAGE OF 15 FEET.
    - iii. THE NEW BUILDING WILL BE LINKED TO THE SIDEWALK ALONG WOODLAWN ROAD VIA A NEW FIVE FOOT SIDEWALK. ADDITIONAL INTERNAL SIDEWALKS MAY BE PROVIDED; THE WIDTH OF ANY ADDITIONAL SIDEWALKS MAY BE LESS THAN FIVE FEET.
    - iv. SCREENING IN THE FORM OF A SIX (6) FOOT HIGH SCREENING WALL CONSTRUCTED OF BRICK SHALL BE PROVIDED ALONG THE REAR PROPERTY LINE AS SHOWN ON THE PLAN. THE EXISTING WALL WILL BE EXTENDED ALONG THE REAR PROPERTY LINE AS GENERALLY DEPICTED ON THE REZONING PLAN IMMEDIATELY FOLLOWING THE GRADING AND SITE WORK FOR THE NEW EIGHT (8) UNIT BUILDING AND PRIOR TO ANY ADDITIONAL CONSTRUCTION OR DEVELOPMENT BY THE PETITIONER.
  - f. THE PETITIONER WILL INSTALL ALONG THE REAR YARD OF THE NEW EIGHT (8) UNIT BUILDING, FOUR INCH (4") CALIBER, 18 FEET HIGH, LARGER MATURING TREES 30 FEET ON CENTER.
  - g. ABOVE GROUND BACKFLOW PREVENTERS WILL BE SCREENED FROM PUBLIC VIEW AND WILL BE LOCATED OUTSIDE OF THE REQUIRED SETBACKS.
5. ENVIRONMENTAL FEATURES:
  - a. THE PETITIONER SHALL COMPLY WITH THE CHARLOTTE CITY COUNCIL APPROVED AND ADOPTED POST CONSTRUCTION CONTROLS ORDINANCE.
  - b. THE SITE WILL COMPLY WITH THE TREE ORDINANCE.
6. SIGNAGE:
  - a. SIGNAGE AS ALLOWED BY THE ORDINANCE MAY BE PROVIDED.
7. LIGHTING:
  - a. ALL NEW LIGHTING SHALL BE FULL CUT-OFF TYPE LIGHTING FIXTURES EXCLUDING LOWER, DECORATIVE LIGHTING THAT MAY BE INSTALLED ALONG THE DRIVEWAYS, SIDEWALKS, AND PARKING AREAS.
  - b. DETACHED LIGHTING ON THE SITE WILL BE LIMITED TO 10 FEET IN HEIGHT. ALL OUTDOOR LIGHTING SHALL BE DESIGNED IN A FASHION TO DIRECT THE LIGHT AWAY FROM, OR SHIELD THE LIGHT FROM ADJOINING RESIDENTIAL AREAS.
8. GREENWAY DEDICATION:
  - a. AS INDICATED ON THE SITE PLAN, THE LOWER PORTION OF THE SITE HAS BEEN DEDICATED TO MECKLENBURG COUNTY PARKS AND RECREATION DEPARTMENT AS PASSIVE PUBLIC OPEN SPACE. THE AREA MAY BE IMPROVED BY THE ADDITION OF WALKING TRAILS, PICNIC TABLES, AND/OR OTHER SIMILAR FIXTURES. A RECORD MAP DEDICATING THIS AREA TO MECKLENBURG COUNTY PARKS AND RECREATION WAS RECORDED ON 12/12/14 IN MAP BOOK 51 PAGE 164.
9. HOURS OF CONSTRUCTION:
  - a. THE HOURS OF CONSTRUCTION WILL CONFORM TO APPLICABLE STATE AND LOCAL LAWS RESTRICTING CONSTRUCTION HOURS. ADDITIONALLY, NO EXTERIOR WORK OF ANY KIND WILL TAKE PLACE ON SUNDAYS. THERE WILL BE NO RESTRICTIONS ON TILING, PAINTING, OR SIMILAR INTERIOR WORK.
10. AMENDMENTS TO THE REZONING PLAN:
  - a. FUTURE AMENDMENTS TO THE REZONING PLAN (WHICH INCLUDES THESE DEVELOPMENT STANDARDS) MAY BE APPLIED FOR BY THE THEN OWNER OR OWNERS OF THE APPLICABLE DEVELOPMENT AREA PORTION OF THE SITE AFFECTED BY SUCH AMENDMENT IN ACCORDANCE WITH THE PROVISIONS OF CHAPTER 6 OF THE ORDINANCE.
11. BINDING EFFECT OF THE REZONING APPLICATION:
  - a. IF THIS REZONING PETITION IS APPROVED, ALL CONDITIONS APPLICABLE TO THE DEVELOPMENT OF THE SITE IMPOSED UNDER THE REZONING PLAN WILL, UNLESS AMENDED IN THE MANNER PROVIDED UNDER THE ORDINANCE, BE BINDING UPON AND INURE TO THE BENEFIT OF THE PETITIONER AND SUBSEQUENT OWNERS OF THE SITE AND THEIR RESPECTIVE HEIRS, DEVISEES, PERSONAL REPRESENTATIVES, SUCCESSORS IN INTEREST OR ASSIGNS.

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ARCHITECT'S PROJECT # 14097

**Project :**  
**PARK SELWYN PHASE II**  
FOR  
PARK SELWYN, LLC  
2003 E. WOODLAWN RD  
CHARLOTTE, NC

**Sheet Description :**  
**SITE PLAN**  
**REZONING PETITION**  
**NO. 078**

Seal

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Drawn By :  
GAMBILL

Checked By :  
CRAIG

Revisions :

1. REVISIONS PER COMMENTS

2. REVISIONS PER COMMENTS

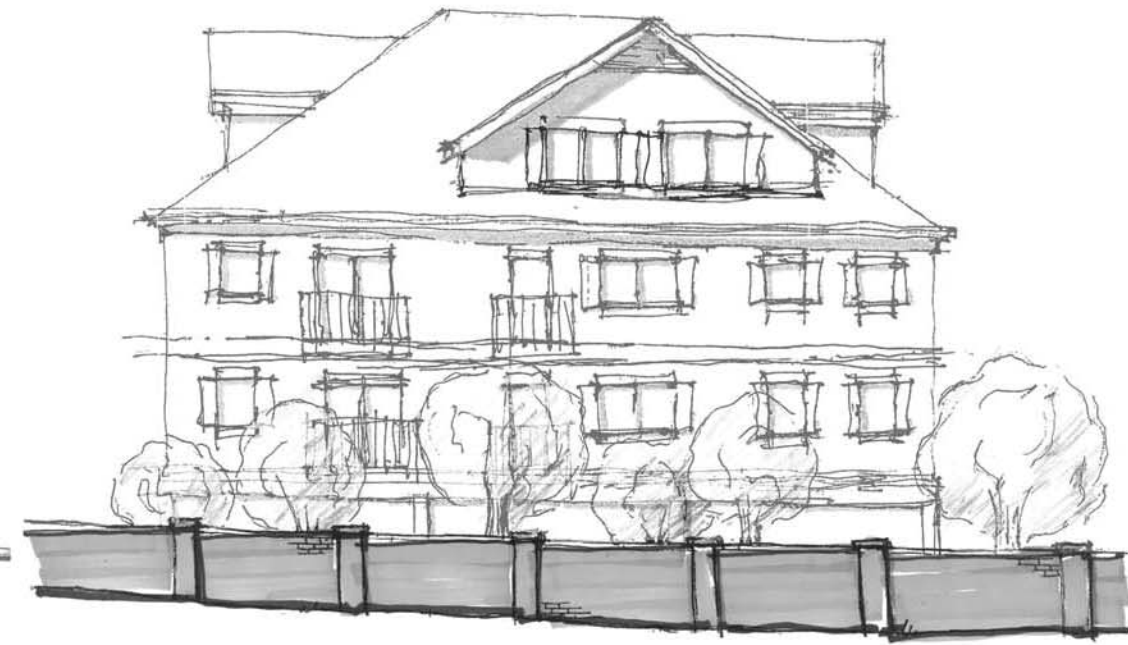
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SP-1





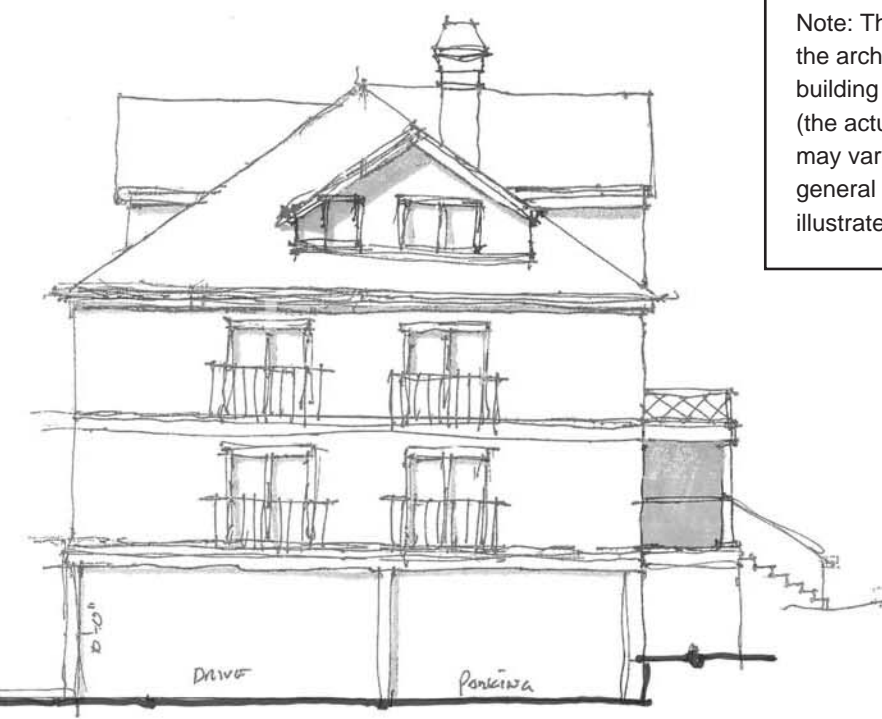
FRONT ELEVATION  
WOODLAWN ROAD



REAR ELEVATION



RIGHT ELEVATION



LEFT SIDE ELEVATION

Note: These elevations are provided to reflect the architectural style and quality of the building that may be constructed on the Site (the actual building constructed on the Site may vary from these illustrations as long as the general architectural concepts and intent illustrated is maintained).

