

MARK	DATE	DESCRIPTION
1	6/22/12	ISSUED PER CITY OF CHARLOTTE COMMENTS
2	6/22/12	ISSUED PER CITY OF CHARLOTTE COMMENTS

PROFESSIONAL DEVELOPMENT GROUP, LLC
 8206-1200 PROVIDENCE ROAD #327
 CHARLOTTE, NC 28277

CITY OF CHARLOTTE, MECKLENBURG COUNTY, NORTH CAROLINA
 SKETCH PLAN FOR
BREWICK COMMONS

EMH&T
 EVANS, MEECHAM, HAMBLETON & TILTON, INC.
 ENGINEERS • SURVEYORS • PLANNERS • SCIENTISTS
 1000 S. COLLETTA AVENUE, SUITE 200
 CHARLOTTE, NC 28202
 PHONE: 704.546.0333 FAX: 704.546.0334
 emht.com

DATE	June 22, 2012
SCALE	As Noted
JOB NO.	20111544
SHEET	2/4

DEVELOPMENT DATA	
Site Acreage:	
Existing Total:	5.45± Acres (234,402 SF)
Proposed:	Lot A = 1.40± Acres (61,027 SF) Lot B = 1.17± Acres (51,002 SF) Lot C = 0.80± Acres (34,640 SF) Lot D = 1.09± Acres (47,688 SF) Right of Way = 0.99± Acres (43,070 SF) Total = 5.45± Acres (237,427 SF)
Tax Parcel Nos.:	201-09-104 201-09-114
Existing Zoning:	R-3
Proposed Zoning:	NS
Existing Use:	Single Family Residential
Proposed Use:	Lot A = Maximum 5,500 SF of all uses allowed within the NS District Lot B = Maximum 10,500 SF of all O-1 uses allowed within the NS District Lot C = Maximum 8,500 SF of all O-1 uses allowed within the NS District Lot D = Maximum 5,500 SF of all uses allowed within the NS District *SEE NOTE BELOW
Floor Area Ratio:	Lot A & D = As permitted by NS District Lot B & C = As permitted by O-1 District
Maximum Building Height:	40 Feet, not to exceed 2 stories
Parking Ratio:	Shall meet the requirements of Chapter 12, Part 2 "Off-Street and Loading," of the Zoning Ordinance. However, Lot D shall provide one space per 100SF of building if developed as a restaurant.
Tree Save Area:	Total Required = 194,357 SF (.15) = 29154 SF Lot A = 5,900 SF Lot B = 10,930 SF Lot C = 6,485 SF Lot D = 5,850 SF Total = 29,165 SF
TBR = To Be Removed	

NOTE: All dimensions are from face of curb unless otherwise noted.

NOTE: Locations of 5' sidewalk connection to public street walks are subject to change upon development.

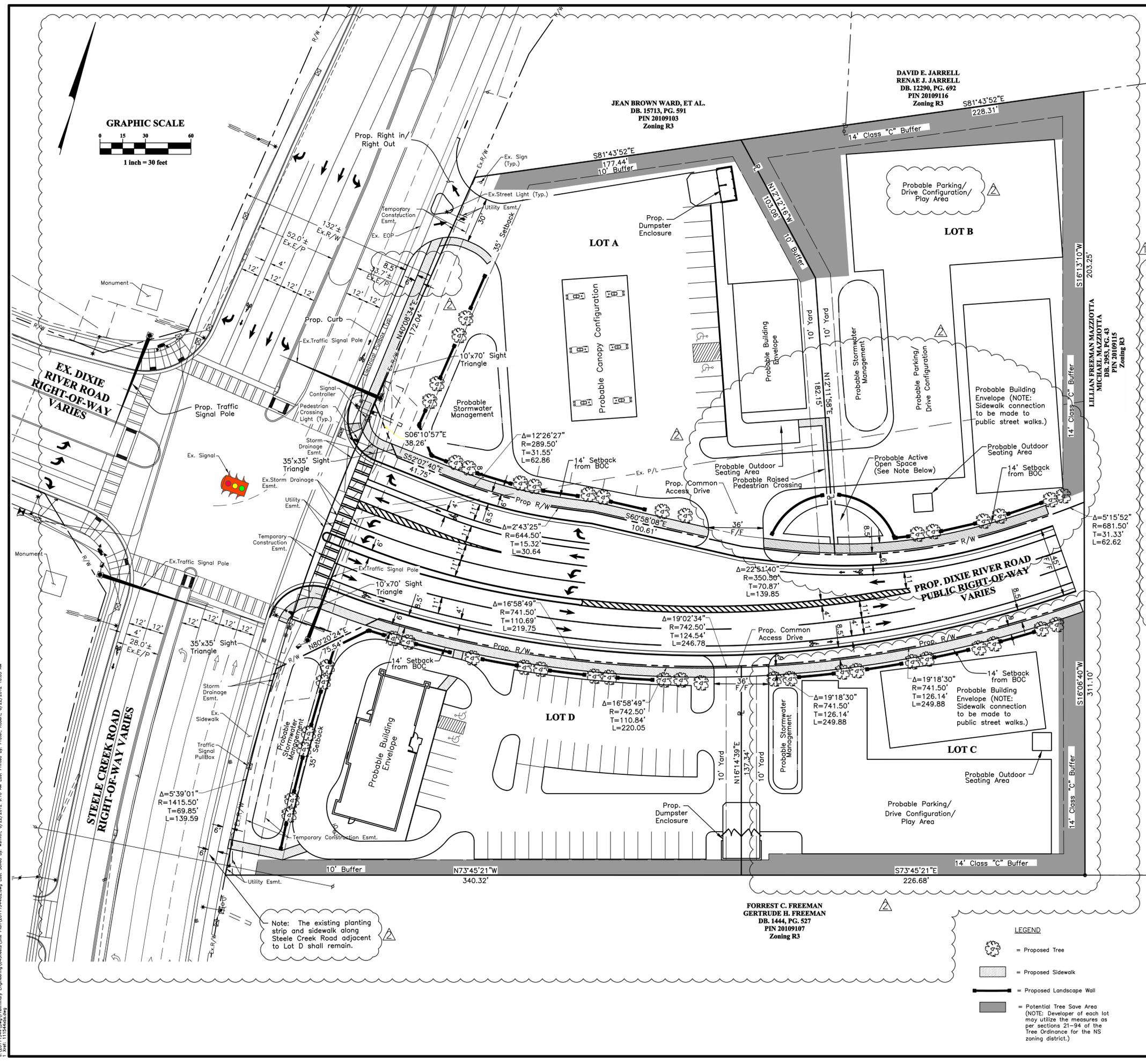
NOTE: Pedestrian crossings within parking/maneuvering areas shall be delineated by using stamped asphalt and/or pavers.

NOTE: Development square footage not utilized in Lots A and/or D may be transferred to Lots B and C. However, a maximum of 2,000 square feet may be transferred to Lot B and a maximum of 1,500 square feet may be transferred to Lot C.

NOTE: The Landscape Wall may be eliminated directly in front of the buildings located on Dixie River Road for Lots B and C.

NOTE: The "Active Open Space" provided shall be a minimum of 750 square feet. The "Active Open Space" will incorporate a low 3' brick landscape wall and may include hardscape areas consisting of brick pavers and/or stamped concrete, benches, tables with seating and bike rack as well as landscaping including ornamental trees, shrubs and plantings. The items listed shall not be considered all inclusive nor required. A detailed Landscape/Hardscape plan shall be included in the construction documents for City review and approval prior to development.

- LEGEND**
- = Proposed Tree
 - = Proposed Sidewalk
 - = Proposed Landscape Wall
 - = Potential Tree Save Area (NOTE: Developer of each lot may utilize the measures as per sections 21-94 of the Tree Ordinance for the NS zoning district.)



Note: The existing planting strip and sidewalk along Steele Creek Road adjacent to Lot D shall remain.

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GENERAL PROVISIONS

Future amendments to the conditional site plan development standards may be applied for by the then Owner of the parcel or parcels involved, in accordance with 6.207 of the Charlotte Zoning Ordinance.

PERMITTED USES

Lots A and D shall be allowed all uses within the NS Zoning District.

Lots B and C shall be allowed all O-1 uses allowed within the NS Zoning District as well as Neighborhood food and beverage service uses.

No residential use shall be allowed within any lot.

TRANSPORTATION

The total number of ingress/egress points to the site from existing and proposed thoroughfares that abut the site shall be limited to the number shown on the Plan. The exact locations may vary from those depicted based upon final design and location requirements as regulated by CDOT and NCDOT.

The Petitioner will construct and/or plat the New Dixie River Road to the eastern property line, abutting tax parcel 201-091-15, prior to the issuance of final certificate of occupancy for any of the uses allowed. The Right of Way shall be conveyed in fee simple fashion. Subdivision Approval for Dixie River Road is required prior to building permits being issued.

The Petitioner is responsible for all costs associated with the signal modifications related to the development at the intersection of Steele Creek Road and the future fourth leg of the intersection.

This is limited to modifications required for this development. Development along future portions of New Dixie River Road (not shown as part of this plan) that require signal modifications at this intersection will be the responsibility of the future development.

The roadway configuration shown for New Dixie River Road is the ultimate build-out upon development of the parcels to the east. The Petitioner will be responsible for implementing a right-turn lane, one through lane, one left-turn lane and one receiving lane (a total of four lanes) for the proposed Dixie River Road extension, including a wide median. The Dixie River Road transportation improvements shall be completed and approved by CDOT/NCDOT before the first building certificate of occupancy is issued. Final construction details will be discussed during the construction permitting process.

The proposed driveway connections to Steele Creek Road will require driveway permits to be submitted to CDOT and NCDOT for review and approval. The exact driveway locations and type/width of the driveways will be determined by CDOT during the driveway permit process. The locations of the driveways shown on the site plan are subject to change in order to align with driveways on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.

All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.

Any fence or wall constructed along or adjacent to any sidewalk or street right of way requires a certificate issued by CDOT.

A Right of Way Encroachment Agreement is required for the installation of any non standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right of way by a private individual, group, business or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning costs, submittal and liability insurance coverage requirements.

The improvements to Steele Creek Road adjacent to Lot A shall be the responsibility of said Lot and shall be completed prior to the issuance of the certificate of occupancy for Lot A.

ARCHITECTURAL STANDARDS

Each building facade exclusive of windows, doors and roofs will be constructed with a minimum of 50% brick, brick veneer, stone or simulated stone. Stucco or efis accents will be also allowed. No vinyl siding shall be used on this site except that vinyl may be used on soffits, windows, rails of fences, patios and stairs. Buildings shall provide residential characteristics. All buildings shall be designed so that the first floor along both Dixie River Road and Steele Creek Road will not have blank walls that exceed 20 continuous feet in length. Doors, windows, and/or a combination of design elements shall be used on the building facade to animate and enliven the streetscape.

Buildings located within Lots A and D shall follow the general guidelines of the elevations submitted with this rezoning petition. Modifications may be made upon staff approval.

Buildings located within Lots B and C shall be designed to be residential in character and have pitched roofs. An architectural elevation shall be submitted for Administrative Approval prior to submission for Mecklenburg County Building Permit.

The brick utilized for the buildings and landscape wall shall be similar in color throughout the development.

All dumpster and recycling areas will be enclosed by a solid wall with one side being a decorative wooden gate. The solid wall will utilize building materials similar to or identical to the building materials used on the adjoining principle use building.

No drive-through windows shall be permitted on Lots B and C.

STREETSCAPE AND LANDSCAPING

A 35 foot minimum setback measured from the existing right-of-way will be established along Steele Creek Road.

A 14' foot setback measured from the back of curb along New Dixie River Road shall be provided. An eight foot planting and six foot sidewalk will be installed within the setback. A minimum five foot wide planting strip behind the sidewalk will be provided and improved with plantings and a low wall. The wall shall be a minimum of 3' behind the sidewalk.

A sidewalk network will be provided within the site so that each building and use will be connected via a five (5) foot sidewalk to the public sidewalks along the abutting streets.

It shall be the responsibility of each individual Lot to install the sidewalk and decorative wall prior to certificate of occupancy for said Lot.

Lots A and D shall provide a minimum 10 foot wide buffer with 6 trees and 20 shrubs per 100 linear feet in accordance with Section 12.302(9) of the Zoning Ordinance where these lots abut residential zoning and along the proposed rear angled property line of Lot A labeled "N12'-12"-16"W - 103.08". In addition a 6 to 8 foot high wall/fence at least 8 feet from the property line with the plantings facing the abutting properties shall be within the 10 foot buffer.

Lots B and C shall provide a 14 foot Class "C" Buffer where the lots abut a residential zoning district.

The Buffers listed above may be removed if the zoning of the abutting property becomes a similar or more intensive zoning.

All landscaping and screening requirements shall meet the standards set forth by the City of Charlotte Zoning Ordinance.

The "Active Open Space" which is adjacent to Dixie River Road between Lots A and B shall be completed prior to the issuance of the certificate of occupancy for Lot A.

The Outdoor Seating Area located on Lot B shall be completed prior to the issuance of the certificate of occupancy for Lot B.

ENVIRONMENTAL FEATURES

The overall development shall provide 15 percent Tree Save Area. The locations and areas depicted shall not be considered final.

The areas depicted in the "Development Data" represent each Lots portion of Tree Save Area to be provided. If the individual Lot decides not to provide the allotted Tree Save Area measures as per Section 21-94 of the Tree Ordinance for the NS Zoning District may be taken.

Development on the site shall comply with the City of Charlotte Post Construction Controls Ordinance(PCCO).

FIRE PROTECTION

Fire protection shall meet the requirements set forth by the City of Charlotte Fire Department.

SIGNAGE

Wall signs will be allowed as prescribed by the Ordinances for the NS Zoning District, as applicable.

LIGHTING

All freestanding lighting and all exterior lighting on buildings will be fully shielded and full cut-off type fixtures downwardly directed. No "wall-pak" type lighting will be used but attached decorative lighting fixtures such as sconces may be allowed. The maximum height of any freestanding lighting shall not exceed 25 feet on lots.

PARKING

Parking may be provided by way of surface or structured parking facilities within the parking envelopes generally depicted on the Plan. Parking shall meet the requirement set forth by the city of Charlotte Zoning Ordinance.

No parking will be allowed between the building and Dixie River Road for Lot A. However, maneuvering within an access drive is permitted.

No parking and/or maneuvering shall be allowed between the building and Dixie River Road for Lot B. Any parking to the sides of the principal building for Lot B will be located a minimum of 30' behind the sidewalk with an outdoor seating/open space area located between the parking and the street.

No parking and/or maneuvering shall be allowed between the building and Dixie River Road for Lot C. Parking and driveways may be located to the sides of the principal building along the street frontage but may not exceed 50 percent of the lot width.

No parking will be permitted between Dixie River Road or Steele Creek Road and the principal building for Lot D. However, maneuvering for a drive-through is allowed between the building and streets.

SOLID WASTE MANAGEMENT PLAN

The Petitioner shall submit to the Mecklenburg County Solid Waste Department a Solid Waster Management Plan prior to initiating demolition and/or construction activities. The Solid Waste Management Plan will include, at a minimum, the procedures that will be used to recycle all clean wood, metal, and concrete generated during demolition and construction activities. Additionally, the Solid Waste Management Plan will specify that all land clearing and inert debris shall be taken to a properly permitted facility. The plan shall also include a requirement that a monthly reporting of all tonnage disposed and recycled will be made to the Mecklenburg County Solid Waste Program. The report shall include the identification and location of all facilities receiving disposed or recycled materials.

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MARK	DATE	DESCRIPTION
1	5/16/12	ISSUED PER CITY OF CHARLOTTE COMMONS
2	6/22/12	ISSUED PER CITY OF CHARLOTTE COMMONS

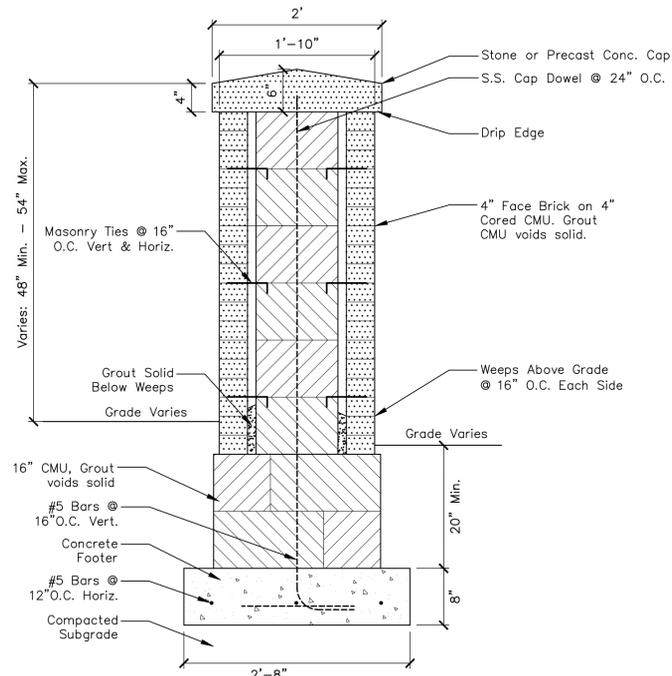
PROFESSIONAL DEVELOPMENT GROUP, LLC
8206-1200 PROVIDENCE ROAD #327
CHARLOTTE, NC 28277

CITY OF CHARLOTTE, MECKLENBURG COUNTY, NORTH CAROLINA
REZONING NOTES
FOR
BEREWICK COMMONS

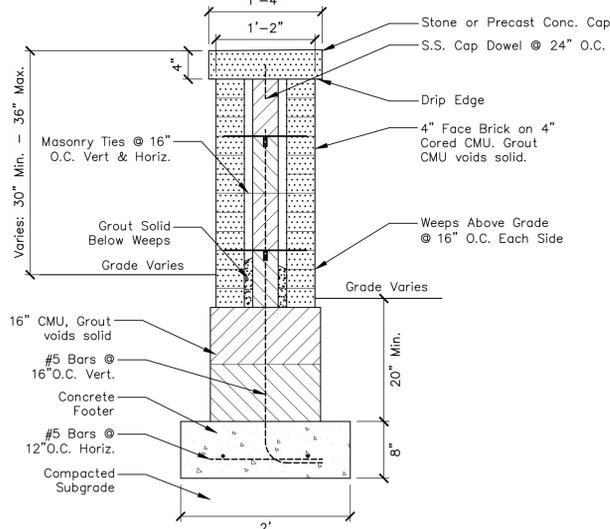
EMH&T
 Evans, Mechwart, Hambleton & Tilton, Inc.
 Engineers • Surveyors • Planners • Scientists
 4352 East W. 10th Street, Suite 200
 Charlotte, NC 28203
 Phone: 919.775.4500 Fax: 919.775.3548
 emht.com

DATE	June 22, 2012
SCALE	As Noted
JOB NO.	20111544
SHEET	3/4

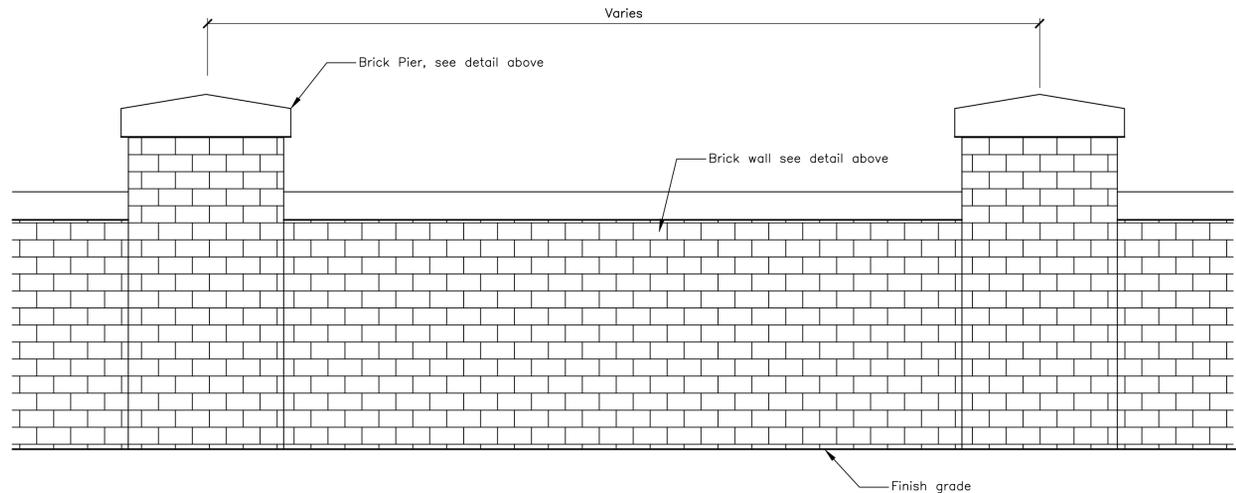




BRICK PIER
No Scale



BRICK WALL
No Scale

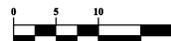


BRICK WALL ELEVATION
No Scale

GENERAL NOTES

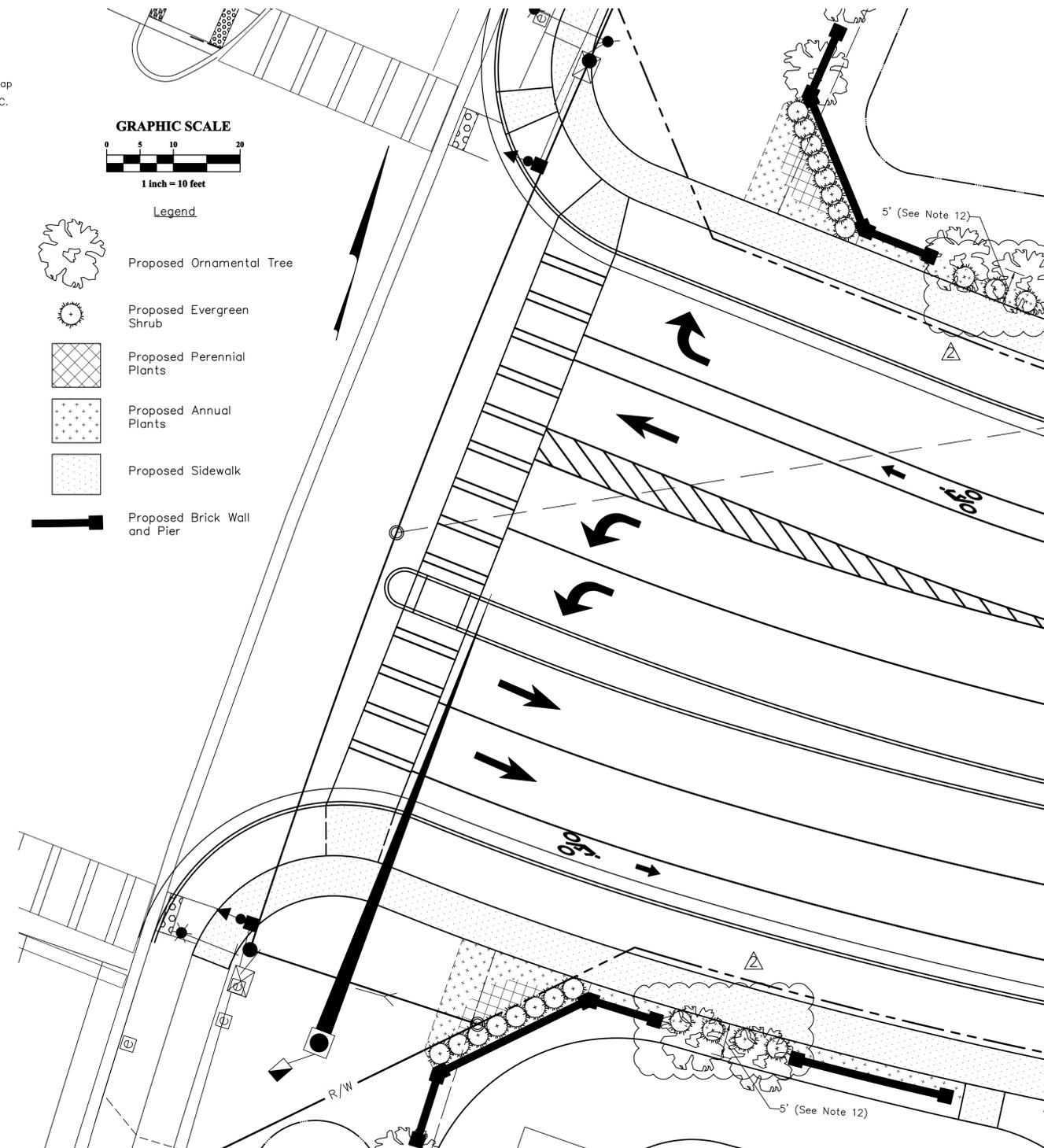
1. Prior to installation, the landscape contractor shall inspect the general site conditions and verify the sub-grade, elevations, utility locations and topsoil provided by general contractor. The landscape contractor shall notify the general contractor of any unsatisfactory conditions and work shall not proceed until such conditions have been corrected and are acceptable to the landscape contractor.
2. All plants shall meet or exceed standards set in the American Standard for Nursery Stock, ANSI Z60.1, 2004. All plants shall equal or exceed the measurements and sizes specified in the schedule.
3. All planting operations shall adhere to American Nursery & Landscape Association standards unless noted otherwise.
4. Confirm location of all utilities and subsurface drain lines prior to plant installation.
5. Contractor may slightly field adjust plant locations as necessary to avoid utilities. Finished planting beds shall be graded to provide positive drainage.
6. Mulch planting beds with shredded hardwood mulch of uniform dark brown color. It shall be free of twigs, leaves, disease, pest or other material unsightly or injurious to plants. Average applied thickness shall be 3" depth. Mulch hedges in a continuous bed.
7. All annuals to be selected from available seasonal stock. Submit annual planting plan to Owner for approval prior to installation.
8. Brick walls and planted areas shall be constructed by the associated property owner of each lot.
9. The brick type utilized for the walls shall be coordinated with the brick type used in the construction of the buildings for lots A through D.
10. All brick walls constructed shall be the same type, color, size and manufacturer throughout each lot.
11. The brick wall shown is subject to change pending staff approval.
12. There shall be a 5' wide planting area behind the sidewalk of Dixie River Road and along the location depicted along Steele Creek Road. This shall include a low wall separated by ornamental trees, shrubs and plantings at the base of the wall. The wall shall be a minimum of three feet behind the sidewalk.

GRAPHIC SCALE



Legend

- Proposed Ornamental Tree
- Proposed Evergreen Shrub
- Proposed Perennial Plants
- Proposed Annual Plants
- Proposed Sidewalk
- Proposed Brick Wall and Pier



Petition 2012-059

REVISIONS	
MARK	DATE
1	5/16/12
2	6/22/12

DESCRIPTION: REVISED PER CITY OF CHARLOTTE COMMENTS
REVISED PER CITY OF CHARLOTTE COMMENTS

PROFESSIONAL DEVELOPMENT GROUP, LLC
8206-1200 PROVIDENCE ROAD #327
CHARLOTTE, NC 28277

CITY OF CHARLOTTE, MECKLENBURG COUNTY, NORTH CAROLINA
LANDSCAPE DETAILS
FOR
BREWICK COMMONS

EMH
Evans, McSchwartz, Hambleton & Tilton, Inc.
Engineers • Surveyors • Planners • Scientists
1000 South Tryon Street, Suite 200
Charlotte, NC 28203
Phone: 704.548.0333 Fax: 704.548.0348
emh.com

DATE: **June 22, 2012**

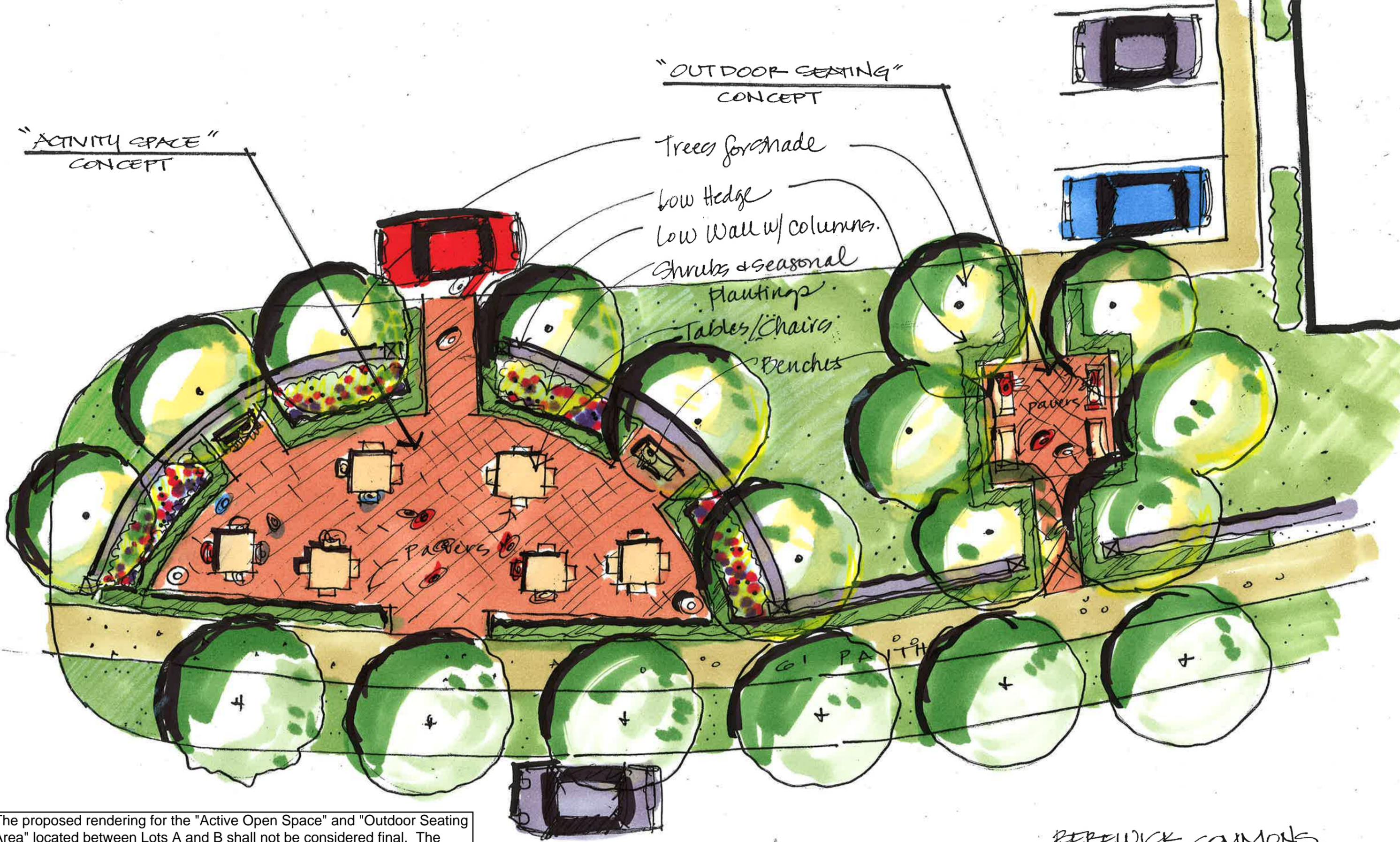
SCALE: **As Noted**

JOB NO.: **20111544**

SHEET: **4/4**

"ACTIVITY SPACE"
CONCEPT

"OUTDOOR SEATING"
CONCEPT



The proposed rendering for the "Active Open Space" and "Outdoor Seating Area" located between Lots A and B shall not be considered final. The provided is conceptual in nature. A detailed Landscape/Hardscape plan shall be included in the construction documents for City review and approval prior to development.

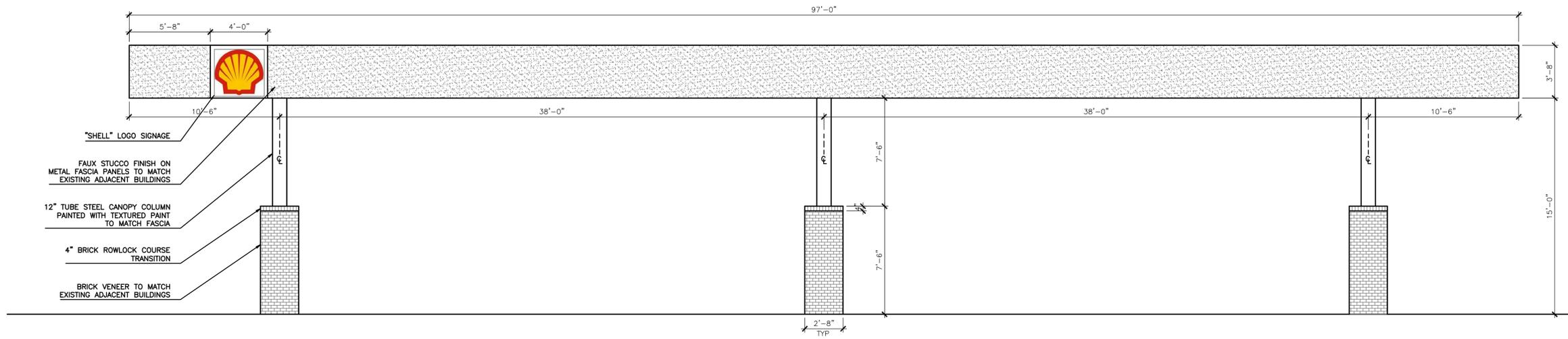
PEREWICK COMMONS.
6.21.12
EHL+T

Berewick Commons
Rezoning Petition 2012-059
Dixie River Road Perspective





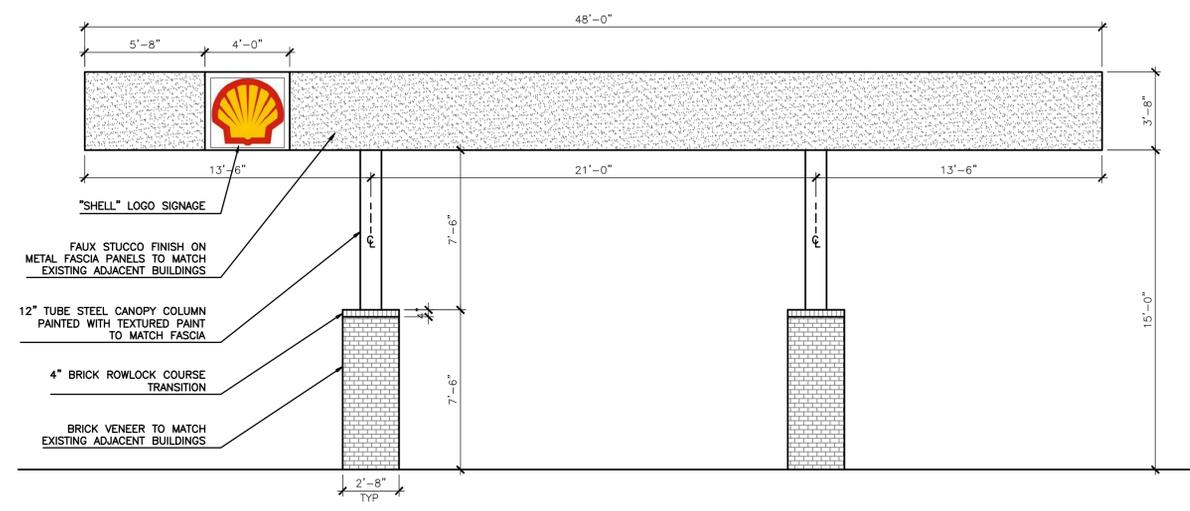
JOB NO: **116-11**
 PRELIMINARY DATES:
 CONTRACT DATE: **7/8/11**
 REVIEW DATES:
 8/9/11
 8/18/11
 8/22/11
 9/2/11
NOT FOR CONSTRUCTION



CANOPY FRONT ELEVATION

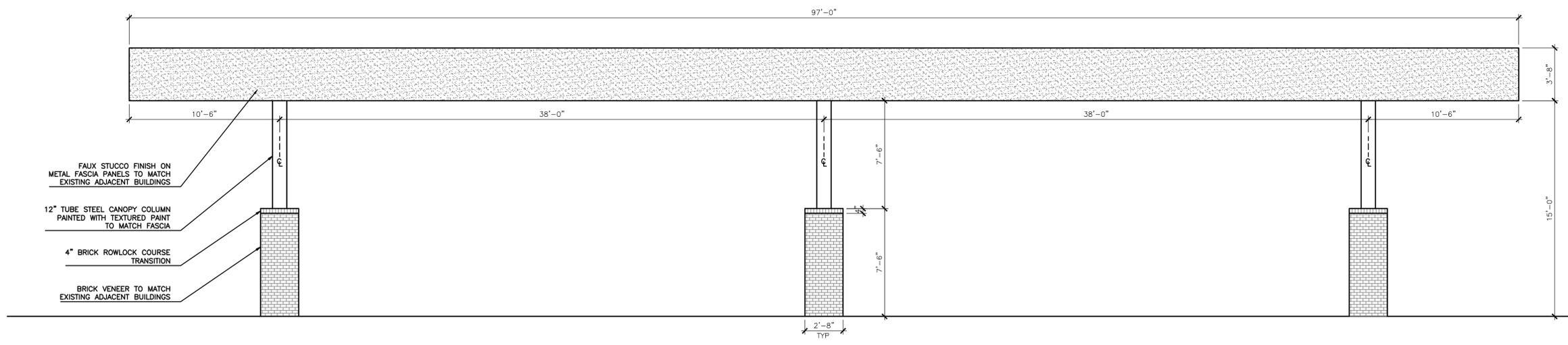
SCALE 1/4"=1'-0"

- NOTES**
1. ALL MATERIALS AND FINISHES TO BE INSTALLED PER THE MANUFACTURER'S WRITTEN INSTRUCTIONS.
 2. GENERAL CONTRACTOR TO VERIFY ALL EXTERIOR COLORS AND FINISHES WITH THE OWNER PRIOR TO CONSTRUCTION.
 3. REFER TO SHOP DRAWINGS BY CANOPY MANUFACTURER'S ENGINEER.
 4. REFER TO STRUCTURAL ENGINEERING DRAWINGS.



CANOPY LEFT/RIGHT SIDE ELEVATION

SCALE 1/4"=1'-0"



CANOPY REAR ELEVATION

SCALE 1/4"=1'-0"

LOT "A"