



# Elizabeth Area Plan

Charlotte-Mecklenburg Planning Department



Adopted by  
Charlotte City Council  
November 14, 2011



# Acknowledgements

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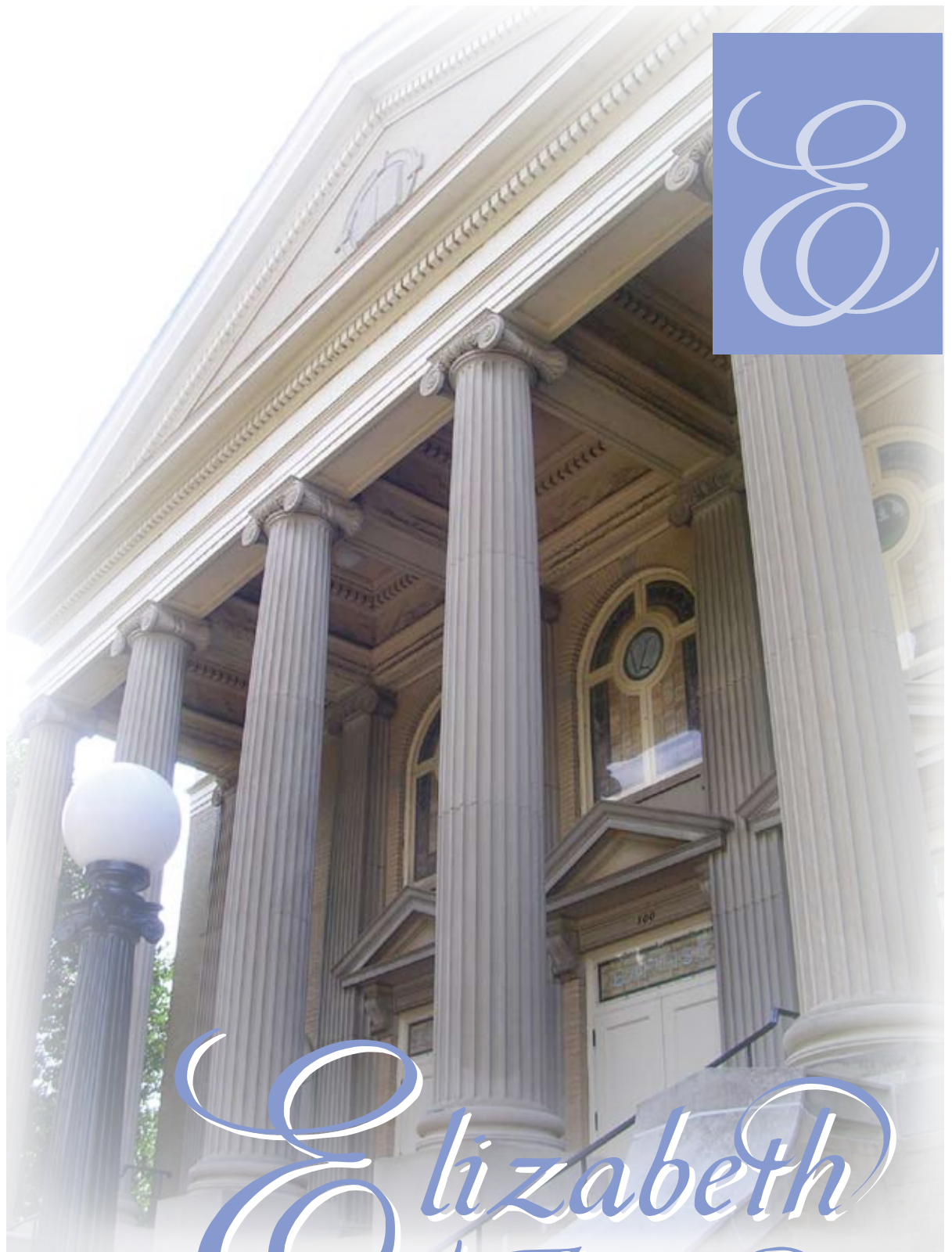
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**Elizabeth Area Market Analysis**

available at Elizabeth Area Plan web page at  
[www.charmeck.org](http://www.charmeck.org)





## EXECUTIVE SUMMARY



## Executive Summary

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### Purpose

This document defines the vision and land use policies for the plan area and provides specific direction to guide the preservation of historic neighborhoods and future growth and development, consistent with the *Centers, Corridors and Wedges Growth Framework* (adopted August 2010).

### Plan Area

The plan area comprises approximately 630 acres and is bounded by I-277 and Independence Boulevard on the north, the Seaboard Coastal Railroad on the east, Briar Creek on the south, and Randolph Road/Fourth Street, South Laurel Avenue, and Providence Road on the west.

The area north of the plan boundary between Independence Boulevard and Central Avenue/East Tenth Street, including Sunnyside, has historically been considered part of the Elizabeth neighborhood. However, it is not within the boundaries of this area plan. Three other plans provide land use and planning guidance for this area: the *Belmont Area Plan* (2003), the *Plaza Central Pedscape Plan* (2004), and the *Sunnyside Pedscape and Land Use Plan* (2003). The Elizabeth plan area also encompasses Crescent Heights, a neighborhood not considered a part of Elizabeth.

(See **Map 1**, *Area Plan Boundary* on page 3 for the plan area boundaries as well as the traditional Elizabeth neighborhood boundaries.)

### Key Opportunities and Challenges

An understanding of existing conditions, including feedback received from the public during the planning process, reveals both opportunities to build upon and challenges to consider. Key opportunities and challenges, highlighted below, were discussed at the first public meeting and formed the basis for the vision statement and, ultimately, the plan policies.

#### Opportunities

- Eclectic Character and Mix of Uses
- Parks, Recreation, and Open Spaces
- Distinctive Architectural Design
- Accessibility
- Natural Features

#### Challenges

- Development Pressure
- Memorial Stadium Site
- Institutional Expansion
- Traffic
- Aging Infrastructure



### Vision Statement

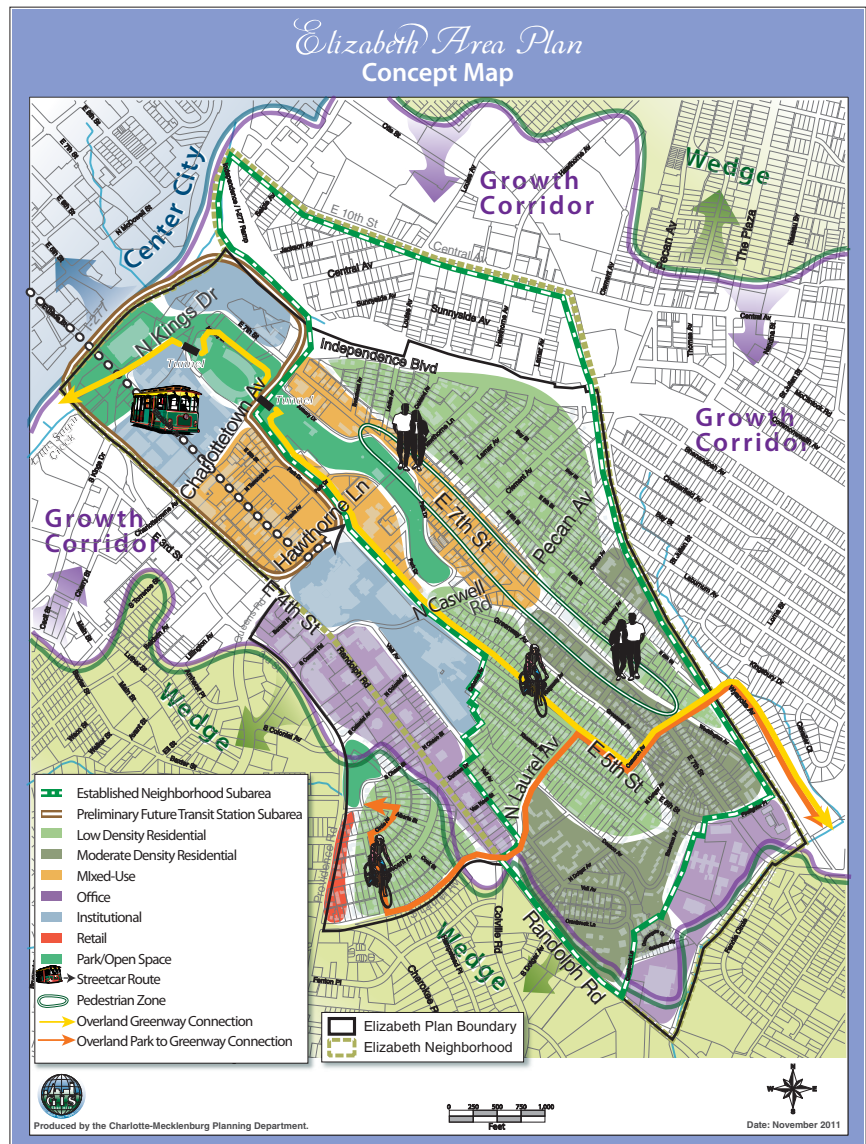
Elizabeth is a community that embraces its urban vitality, preserving its historic heritage and diversity of residents, businesses, and institutions. The area is a finely-woven tapestry that delicately balances the buildings, open spaces, landscapes, and pedestrian scale. Elizabeth will strive to accommodate area-contextual housing types for various income levels. Well-conceived infill development, important to the ongoing vitality of the area, will be encouraged and balanced with neighborhood preservation. There should be broad community participation in planning and development decisions that affect the fabric of the community.



## Neighborhood Organizations' Priorities

Elizabeth residents have a long history of being involved and engaged in matters affecting their neighborhood. Residents strive to be proactive in ensuring that the neighborhood is protected from incompatible development and that the historic character is protected, while acknowledging inevitable growth and change. During the area plan development, representatives from the Historic Elizabeth Neighborhood Foundation (HENF) and the Elizabeth Community Association (ECA) were very involved in the process. The following are priorities the neighborhood has identified as being key to the area plan:

- Protect, save, and enhance Independence Park.
- Integrate the proposed redevelopment of Elizabeth Avenue.
- Preserve residential areas.
- Demand pedestrian-oriented development along East Seventh Street.
- Create transit options.



## Overall Plan Concept

The area plan provides numerous specific policies to help achieve the desired future. The overall concept is illustrated in the *Concept Map* (above).

- Maintain and build upon the character of the established, historic neighborhood.
- Independence Park, and other area parks, greenways, and open spaces, should be preserved, protected, and enhanced.
- Create a more inviting pedestrian environment.
- Recognize the importance of institutional and office uses.
- Continue the transition to more intense, mixed use development near future transit stations and selected streetcar stops.

## Summary of Goals and Policies

### Land Use

The plan contains a number of policies related to Land Use and Community Design. The plan area lies mostly within a Growth Corridor as an Established Neighborhood Subarea, as defined by the City's *Centers, Corridors and Wedges Growth Framework*. Established Neighborhood subareas are those existing, primarily low density residential communities located within a Growth Corridor. These neighborhoods should be maintained and enhanced and, in general, protected from more intense development.



#### ***Land Use Goal***

*Maintain and preserve the area's predominant historic residential character and parks and open spaces, balanced with well-crafted and appropriately scaled businesses, health care facilities, and educational institutions.*

---

### **Land Use Policies**

The **Land Use** policies are summarized as follows and illustrated in **Map 4, Future Land Use** on page 18. Please see the Concept Plan for a listing of all the plan policies and a more detailed discussion of each.

#### **A. Established Neighborhood Subarea**

- Maintain the existing, well-established, primarily low-density residential character where single family residential is the predominant land use.
- Recognize East Seventh Street as an important vehicular corridor, but with a more pedestrian-friendly character and streetscape and a mixture of residential, office, and retail uses.
- Support low or moderate density non-residential development, primarily along East Seventh Street, East Fifth Street (north of Caswell Road), North Caswell Road, and Park Drive.
- Support the incorporation of existing buildings into new development or redevelopment, particularly existing structures with historical, architectural, or aesthetic value.
- Support the enhancement, protection, and preservation of existing parks and open spaces.

#### **B. Preliminary Future Transit Station Subarea**

- Support existing and future Park/Open Space uses and encourage connections to the various available modes of transit and the surrounding neighborhood.
- Support well-designed pedestrian-friendly uses, including a mix of complementary moderate to high intensity residential, office, retail/entertainment, and civic uses within easy walking distance of future streetcar stops and rapid transit stations.

#### **C. General Corridor Area**

- Support Institutional uses for existing hospital, medical office, and associated parking uses.
- Support Office uses along Randolph Road and adjacent to Briar Creek.

#### **D. Wedge Area**

- Continue the current land use pattern of predominantly low density residential uses, with limited moderate density multi-family.
- Ensure that non-residential uses along the edges do not encroach further into the residential area.



## Community Design

The **Community Design** policies help ensure that new development complements the existing or desired character of the Elizabeth community. While design policies alone do not dictate land use or zoning, they are used to strengthen how various land uses fit together. These policies consider not only the built environment, but also consider how people move through and amongst those spaces as pedestrians, bicyclists, or automobile drivers.



### ***Community Design Goal***

*Plan new development that respects the scale, design, and proximity of adjacent buildings and land uses. Good architectural and site design and quality construction materials should be utilized to ensure the long-term sustainability and economic vitality of all development.*

---

### **Community Design Policies**

The **Community Design** policies are summarized as follows:

- Design infill residential buildings in harmony with the scale, size, massing, and materials of the adjacent neighborhood.
- Minimize impacts to the tree canopy.
- Encourage green site design and building practices.
- Encourage architectural design that complements the scale, style, character, and materials of surrounding structures that contribute to the neighborhood character.
- Buildings immediately adjacent to single-family residential areas should “step down” to 40’ or less along shared boundaries.
- Design non-residential buildings to face the primary street with a minimized setback wherever possible.
- Integrate functional open space into new development.



*East Seventh Street should continue to be an important vehicular corridor, but with a more pedestrian-friendly character and streetscape.*



*This residential infill building on Vail Avenue respects the scale and character of the surrounding neighborhood.*

## Transportation



### **Transportation Goal**

*Support and develop a connected, affordable, balanced transportation system that encourages walking. It should safely and sensibly accommodate pedestrians, bicyclists, and transit users while allowing for neighborhood-friendly automobile speeds for motorists and business patrons traveling to and through Elizabeth.*

---

### **Transportation Policies**

The **Transportation** policies provide guidance that will both protect established neighborhood areas and enhance commercial districts within Elizabeth. They address both proposed new streets and enhancements to existing streets, to make them more pedestrian and bicycle friendly. These improvements, summarized below, will likely be accomplished as new development occurs.

- Enhance the intersection of East Seventh Street at Pecan Avenue and North Caswell Road to make it safer for pedestrians.
- Provide additional pedestrian crossings of East Seventh Street.
- Provide street connections to support new development and eliminate gaps in the sidewalk system to promote vehicular and pedestrian connectivity.
- Provide alternative bicycle accommodations along or parallel to all thoroughfares.
- Recognize the future Center City Streetcar and coordinate planning around stops within the plan area.

## Infrastructure and Public Facilities



### **Infrastructure and Public Facilities Goal**

*Provide and maintain public facilities such as parks, greenways, recreational facilities, and infrastructure in excellent condition and emphasize environmental and economic sustainability in the design and planning of new or updated public facilities and infrastructure.*

---

### **Infrastructure and Public Facilities Policies**

The **Infrastructure and Public Facilities** policies are intended to enhance, preserve, and protect the area's existing public facilities, and to encourage infrastructure that serves today's requirements while incorporating innovative practices to meet future needs.

- Independence Park, as the geographic and symbolic heart of the Elizabeth neighborhood, should be preserved, protected, and enhanced.
- Complete primary and overland greenway connections to enhance pedestrian and bicycle connectivity.
- Maintain and enhance existing public parks and recreation facilities to ensure they are in good condition for community use and encourage the incorporation of public art as an integral components in park facilities.
- Maintain water and sewer service, enhancing the capacity as needed, and encourage water conservation techniques.

## Natural Environment



### **Natural Environment Goal**

*Preserve and protect the natural environment, parks, tree canopy, open spaces, and natural resources. Encourage measures that will help ensure the long-term sustainability of the tree canopy in Independence Park and other public spaces. Support expansion of the greenway network, enhancement and improvement of existing greenways, and connections between greenways and to neighborhood centers. Promote measures to minimize the adverse affects of new development on air and water quality, and encourage the use of eco-friendly building technologies and renewable resources.*

---

### **Natural Environment Policies**

The **Natural Environment** policies are intended to protect the area's natural resources while permitting well-planned development that can preserve and enhance the natural environment.

- Minimize impacts to the existing tree canopy and help ensure its long-term sustainability.
- Maintain a high level of connectivity to help reduce vehicle miles travelled and improve air quality.
- Encourage the use of low impact design and innovative and eco-friendly storm water management practices, including rain barrels, rain gardens, and green roofs.



*The intersection of North Caswell Road and East Seventh Street is challenging for pedestrians. A few simple improvements could make it safer for crossing.*



*This asphalt parking lot near the rose garden in Independence Park could be restored to parkland. Additional on-street parking nearby would help offset the loss of capacity.*



## Key Implementation Strategies

The Implementation Guide identifies strategies to implement the policies of the Concept Plan, outlining a number of projects for each of the plan components. However, it is not adopted by elected officials and is not a commitment to implement the projects. These strategies are both short term, such as corrective rezonings or minor intersection improvements, and long term, such as coordinating with future streetcar and rapid transit projects. Many will be implemented by the private sector as development occurs.

Key implementation items include:

- Using the land use and design recommendations to guide and evaluate development proposals.
- Enhancing the intersection of East Seventh Street at Pecan Avenue and Caswell Road with high-visibility crosswalks, new curb ramps, and re-timed signals.
- Providing additional pedestrian crossings of East Seventh Street.
- Removing the surface parking area adjacent to the Independence Park rose garden.
- Maintaining the Hawthorne Recreation Center for public use.
- Establishing a pedestrian/bicycle connection from Laurel Avenue to Bascom Street.
- Supporting environmental education to reduce the amount of pollutants entering area streams and creeks.
- Establish an annual meeting to include key neighborhood organizations, institutions, and community leaders, in order to keep channels of communication open and facilitate discussions about issues affecting the neighborhood.

Since conditions evolve over time, staff will update the Implementation Guide to recognize changes and reflect progress.





# *Elizabeth Area Plan*

Part I: CONCEPT PLAN

## Part I Concept Plan

### Plan Context

#### Purpose

This document establishes a vision for the plan area and provides policy direction to guide future growth and development consistent with the *Centers, Corridors, and Wedges Growth Framework*. Once adopted, this plan will:

- Update existing adopted plans for the area, including the *1993 Central District Plan*;
- Refine the boundaries for any portion of an Activity Center, Growth Corridor, and/or Wedge included in the plan area;
- Address key land use, transportation, community design, and development issues identified through the planning process;
- Provide guidance for future land use and infrastructure decisions to insure that Elizabeth's unique and diverse urban environment is preserved and maintained;
- Function as the official streetscape plan.

#### Plan Boundaries

The plan area comprises approximately 630 acres and is bounded by I-277 and Independence Boulevard on the north, the Seaboard Coastal Railroad on the east, Briar Creek on the south, and Randolph Road/Fourth Street, South Laurel Avenue, and Providence Road on the west (see **Map 1, Area Plan Boundary**, page 3).

The traditionally accepted boundaries of the Elizabeth *neighborhood* are slightly different from the boundaries of the *plan area*. The area north of the plan boundary between Independence Boulevard and Central Avenue/East Tenth Street, including Sunnyside, is considered part of the Elizabeth neighborhood. However, it is not within the boundaries of this area plan. Instead, land use

and planning guidance for this area is provided by the following adopted plans: *Belmont Area Plan (2003)*, *Plaza Central Pedscape Plan (2004)*, and *Sunnyside Pedscape and Land Use Plan (2003)*. The Elizabeth plan area also encompasses the Crescent Heights neighborhood, a geography not considered to be part of Elizabeth. Maps in this area plan will depict the boundary of the plan area as well as the traditionally accepted boundary of the Elizabeth neighborhood.

Most of the plan area lies within the boundaries of the Southeast **Growth Corridor** described in the *Centers, Corridors, and Wedges Growth Framework*. The Crescent Heights neighborhood is in a Wedge. Although most of the plan area is within the Growth Corridor, a significant portion is within the Established Neighborhood Subarea. This well-established, primarily low-density residential area, where the predominant land use is single family residential, should remain little-changed and will enjoy protection from more intensive land uses.

**Map 2, Centers, Corridors and Wedges**, page 4, illustrates the boundaries of the Growth Corridor, the Established Neighborhood subarea, the preliminary future Transit Station Subarea, the Wedge area, and the areas of the Elizabeth neighborhood covered by previously adopted plans.

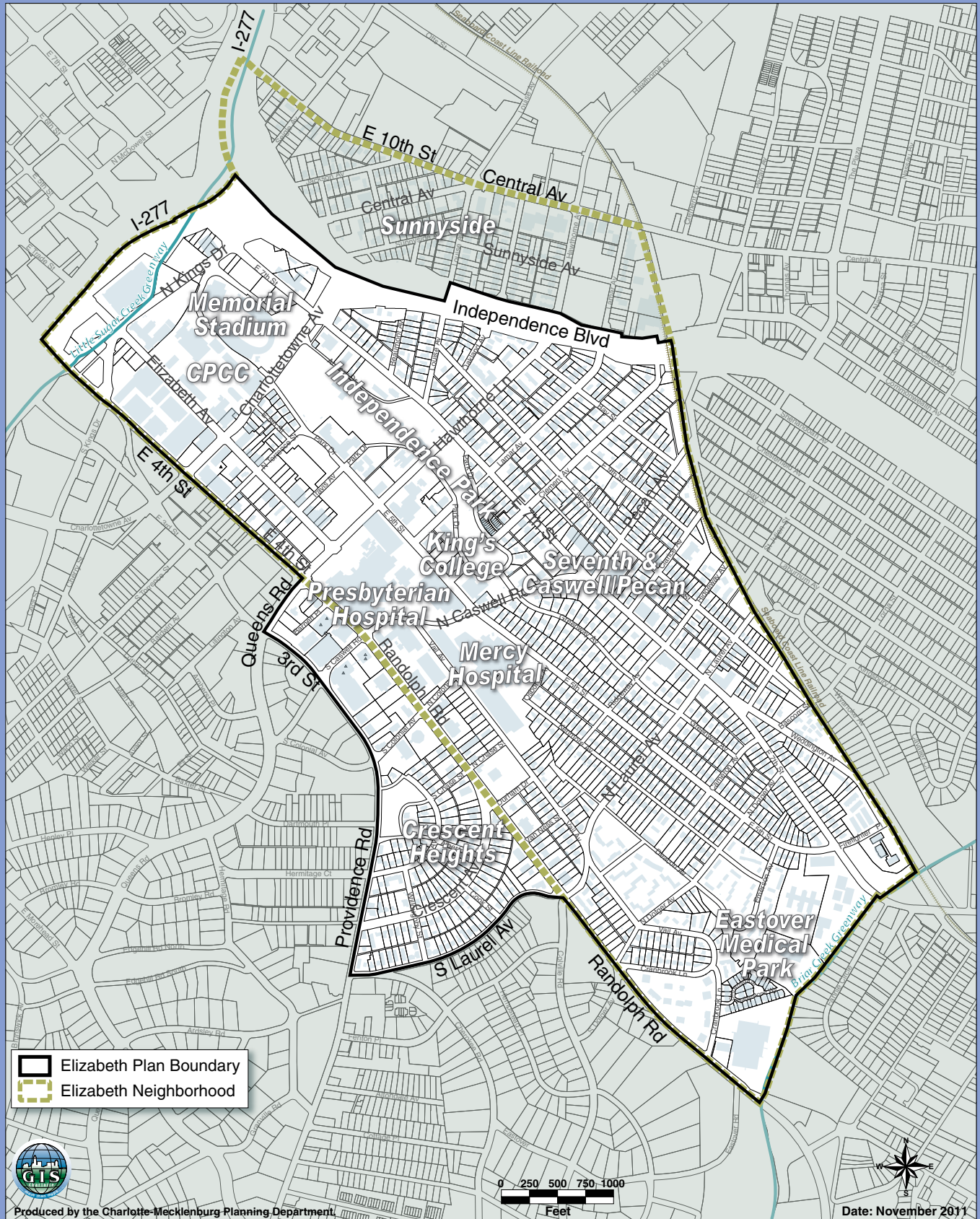




# Elizabeth Area Plan

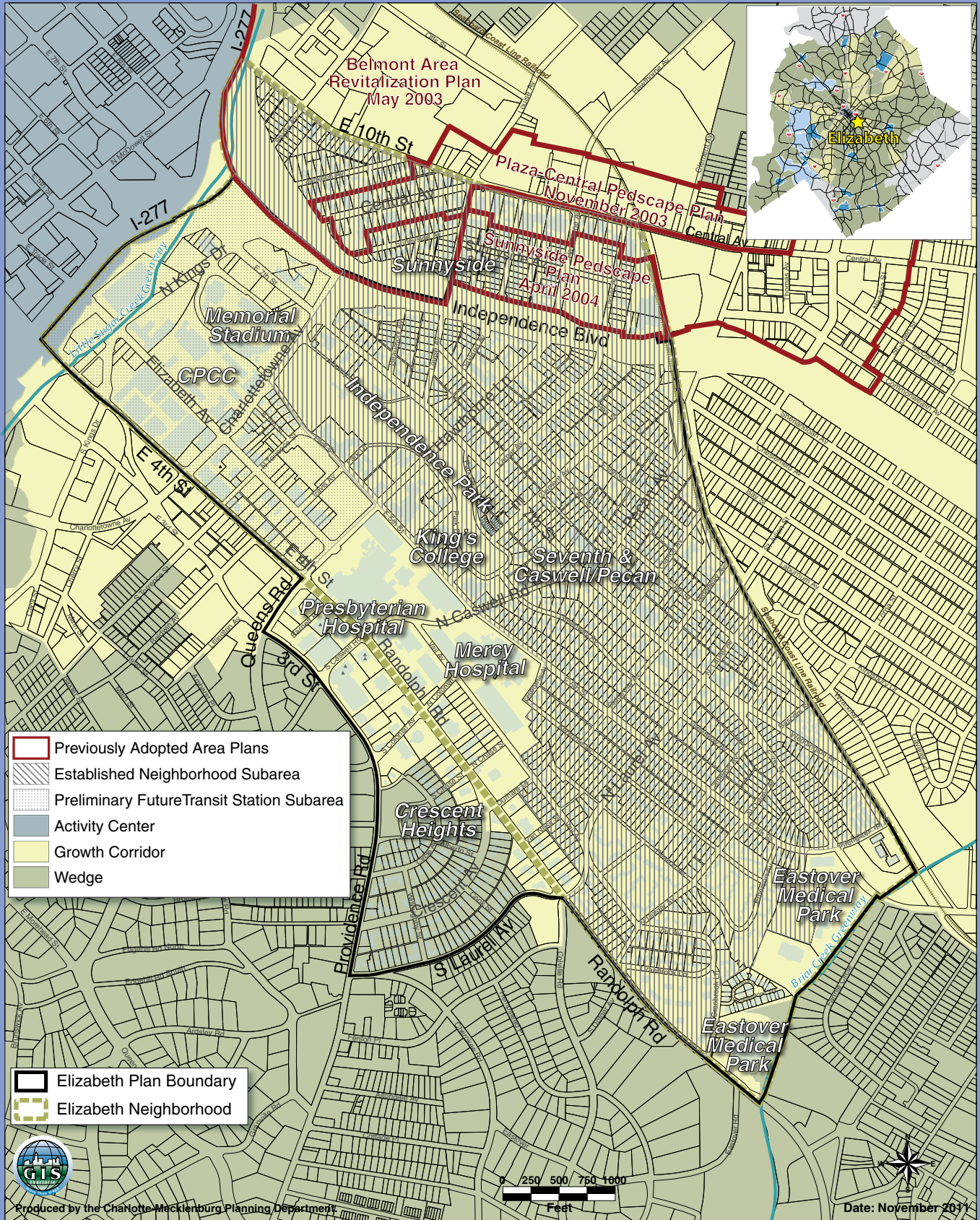
## Area Plan Boundary

Map 1



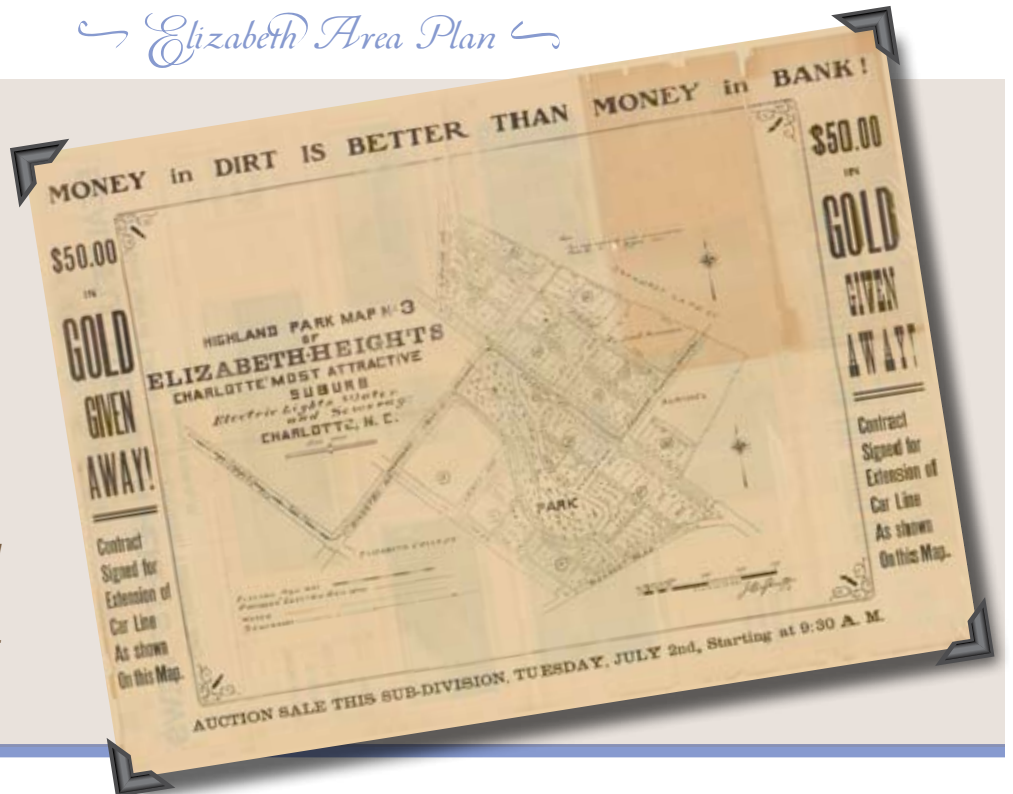


# Elizabeth Area Plan Centers, Corridors and Wedges



## Elizabeth Area Plan

*This 1907 advertisement for a sale of lots in the Elizabeth Heights subdivision shows a planned streetcar extension along Elizabeth Avenue onto Kingston Avenue (now Hawthorne Lane), then onto Seventh Street and Clement Avenue. Most of the streets, alleys, and lots depicted on this map are still in place today, more than one hundred years later.*



## History of Elizabeth Development

In the last decades of the nineteenth century, Charlotte underwent a period of stunning growth as the textile industry boomed in the Carolina Piedmont. After the end of the Civil War, industrialists from the North looked at the region's combination of vigorous cotton production, available labor, and railroads and began to invest heavily in the region by building numerous cotton mills to take advantage of this combination of assets. Although this happened throughout the South, few towns were as well positioned to attract the interest of these textile magnates as Charlotte.

From 1880 through 1930, Charlotte's population doubled every ten years, creating a huge demand for new housing for both the blue collar mill workers and the professional workers in the mills and other businesses. By the dawn of the 1900's, Charlotte had begun to develop a ring of suburbs on its outskirts, including Dilworth and Myers Park, connected to the center of town by a growing streetcar system. The next wave of residential development shifted toward the north side of town, filling in the area between the town of Charlotte and the new North Charlotte textile village being developed by the Highland Park Textile Company.

What is now known as the Elizabeth neighborhood had its birth in this phase of Charlotte's growth. Elizabeth came out of the blending of five separate developments that began over a period of roughly twenty years. The oldest of these areas, Highland Park, was platted in 1891 by the Highland Park Land Company, although a national economic downturn stunted the development until the end of the decade. In 1897, the Lutheran Church began construction of a women's college at the end of Elizabeth Avenue, on the site of the present-day Presbyterian Hospital. This college, the first major institution to be built in this new section of Charlotte, would come to be named Elizabeth College, and would give its name to the newly forming neighborhood.

In 1900, two new areas were laid out. Piedmont Park and Oakhurst were adjacent to Highland Park, near the Central Avenue corridor. Although these areas are part of what is historically considered the Elizabeth Neighborhood, they lie largely on the north side of the Independence Freeway.

In 1904, as these three earlier neighborhoods began to take shape, Elizabeth Heights was begun by Charlotte real estate investor W. S. Alexander. This new neighborhood was conceived to fill in the area between Highland Park and the newer



## Elizabeth Area Plan



This photo, "looking up 7th Avenue", was featured in a land auction for lots in 1907. It touted the handsome homes on this paved street with water and sewerage already in place.

Elizabeth College was also featured in the land auction advertisement, the caption claiming the Elizabeth Heights subdivision is "handy for Church, School and Business".



Piedmont Park and Oakhurst developments. The Elizabeth Heights layout included such streets as Kingston Avenue (now Hawthorne Lane), Fifth and Seventh Streets, and Lamar, Clement, Clarice, Ridgeway, and Laurel Avenues.

The next of the original smaller developments that grew together to create Elizabeth was Independence Park. Originally designed by the prominent landscape architect John Nolan, who was responsible for the plan for Myers Park, Independence Park was built on land given to the county by the Springs family. It was centered around an artificial lake that was conceived as the main water reservoir for Charlotte. As the city grew and it was apparent that Charlotte would need a much larger water supply, the lake plan was abandoned and lots were sold around the perimeter, most notably on the newly created Park Drive.

The final piece of Elizabeth was created in the mid 1910's with the platting of Rosemont, on land that was up until then the Dotger family farm. Rosemont encompassed the streets to the east of Caswell Avenue, including Greenway and Dotger Avenues.

As these various new neighborhoods developed, they all contained housing stock of similar styles and sizes, including a number of duplexes and

quadruplexes scattered throughout the area. As the area matured into the 1930's the original distinctions between these areas vanished, and the entire area came to be known as the Elizabeth neighborhood. This was reinforced by things like the construction of the present-day Elizabeth Elementary School in 1925, and the building of several large churches throughout the neighborhood that attracted congregants from the entire area.

As the neighborhood began to mature, a healthy commercial area blossomed along East Seventh Street, and some of Charlotte's most prominent citizens called the area home. Its proximity to the center city has kept Elizabeth healthy and vibrant throughout its history. The one event that had a major impact on Elizabeth that continues to today is the construction in the late 1940's of Independence Boulevard. The new road was named after Independence Park, a part of which was lost to the new roadway. Independence Boulevard also permanently divided the neighborhood into two unequal parts, and caused the loss of a great deal of the original housing stock of the area. It is a testament to the vision of the original developers of the areas that blended to become Elizabeth, as well as to the residents of the neighborhood past and present, that the neighborhood remains one of Charlotte's best known communities.

## Elizabeth Today



Today, Elizabeth remains a community rich in diversity and urban vitality where one can discover the joys of living in the city. Elizabeth is a place where residents can walk to Uptown, rent a foreign film or buy a comic book, visit a friend in the hospital, meet neighbors for coffee, take college courses, or stop to smell the roses in Independence Park. Maintaining this diverse urban environment is a continuing challenge.

Continued urban vitality depends upon using Elizabeth's location, diversity, walkability, history, and natural and built environment as guides for inevitable changes. Understanding Elizabeth requires understanding how single actions or places are connected to others, and how preserving Elizabeth's unique quality of life is directly linked to recognizing and building upon these relationships.

### Neighborhood Organizations' Priorities



In keeping with the tradition of neighborhood involvement in the area plan process, the **Historic Elizabeth Neighborhood Foundation (HENF)** and the **Elizabeth Community Association (ECA)** have identified the following five priorities for this area plan:

- **Protect, Save, and Enhance Independence Park**

Explore and initiate legal protections for Independence Park in perpetuity, build on its presence and significance to the neighborhood and the entire city, and articulate a precise means to more fully integrate Independence Park as a focal point for the neighborhood with an increased emphasis on access to the Park.

*continued*

## Neighborhood Organizations' Priorities

*continuation*



- **Integrate the Proposed Redevelopment of Elizabeth Avenue**

Promote the development of Elizabeth Avenue between Center City and the Elizabeth neighborhood, including adequate transition for the extension of the streetcar line along Hawthorne Lane and resulting development/design alterations. Development of Elizabeth Avenue is a critical step for the future of the neighborhood and the community at large, as it connects the areas of Elizabeth, Midtown, Center City, and Myers Park.

- **Preserve Residential Areas**

Protect the edges of the single family residential areas that are most at risk from development due to their proximity to Seventh Street, Hawthorne Lane, Pecan Avenue, and Independence Boulevard. Closely examine how proposed uses along these corridors could negatively impact adjacent residential areas.

- **Demand Pedestrian-Oriented Development along East Seventh Street**

Promote uses along the full length of East Seventh Street that achieve a cohesive land use vision and serve as an urban linkage and neighborhood seam that connects residential, commercial, historic, and service uses within the entire neighborhood and surrounding areas. There needs to be better “communication” between Seventh Street and Independence Park. Planning for Seventh Street should look beyond what is or could be built on the street and instead on to how it is integrated into the existing neighborhood.

- **Create Transit Options**

Provide pedestrian friendly intersections and enhanced accessibility to Independence Park to integrate the proposed streetcar into the Elizabeth Community.



## Organization of this Document

This document is organized into three parts as shown below. Only the first part, the Concept Plan, will become adopted City policy. Part II, the Implementation Guide, contains action steps identified to carry out plan policies, and will be used primarily to guide staff work. Part III, the Appendix, provides supporting information such as the existing conditions report and other supporting data used to develop the plan.

### Area Plan Document

#### Part 1: Concept Plan

- Purpose
- Vision Statement
- Goals
- Policies

*Concept Plan is adopted by City Council and will act as a policy guide for future decision making.*

#### Part 2: Implementation Guide

- Action Steps identified to Carry Out Plan Policies

*Provides direction to staff in implementing plan policies*

#### Part 3: Appendix

- Existing Conditions Report
- Trends & Forecasts
- Other Supporting Information

*Provides additional, supporting information*

## Planning Process

- This plan was initiated by the Charlotte-Mecklenburg Planning Department in response to a request made by the Elizabeth Community Association and the Historic Elizabeth Neighborhood Foundation. Development of the *Elizabeth Area Plan* occurred over a period of 18 months and included the following key public involvement opportunities:
- An *Elizabeth Area Plan (EAP)* web site was established to keep the public informed of plan progress and as a publicly-accessible repository of draft plan materials and maps, meeting notices, agendas, presentations, and minutes.



*Kickoff meeting attendees participate in an issue identification exercise while a planner records their comments.*



*Land use was the main topic at CAG Meeting 5, discussed in an informal café setting, much like friends and neighbors might talk about local issues over a cup of coffee.*



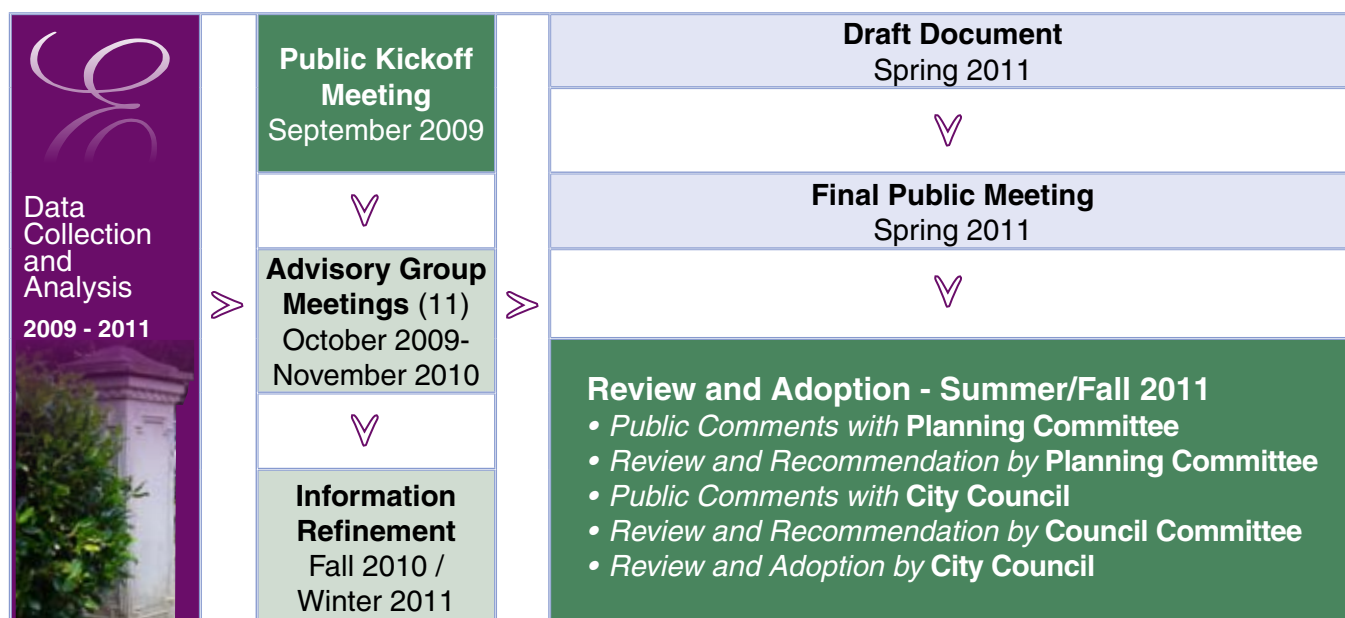
*CAG members discuss building heights, design, massing, and setback while using wooden blocks to construct scale models of hypothetical development.*

## *Elizabeth Area Plan*

- An online survey to identify key issues was publicized to area stakeholders, yielding more than 200 responses. The results of the survey were posted on the EAP web site.
- Two public kickoff meetings were held on September 22, 2009 at St. John's Baptist Church on Hawthorne Lane, with approximately 140 people in attendance. Attendees participated in a hands-on issue identification exercise.
- A 40-member volunteer Citizen Advisory Group (CAG) was formed to assist staff in the development of the plan. CAG membership included residents and property owners plus representatives of area neighborhood organizations and institutions (see Acknowledgements page). The group met eleven times over the course of thirteen months to assist staff in developing plan recommendations. Each CAG meeting focused on a particular element of the plan or a key topic.
- Two optional lunchtime meetings were held for interested CAG members. One focused on the Market Analysis performed by Noell Associates (see Appendix) and the other on transportation issues, particularly relating to East Seventh Street.
- A mid-point public meeting with about 40 attendees was held on June 10, 2010 at St. John's Baptist Church to provide an update on plan progress.
- A final public meeting was held on May 12, 2011 with approximately 30 people in attendance.

The Planning Committee of the Charlotte-Mecklenburg Planning Commission hears citizen comments and makes a recommendation to the Charlotte City Council. The City Council also hears citizen comments and makes the final decision concerning adoption of the plan (see *Steps in the Area Plan Process*, below).

### Steps in the Area Plan Process



## Policy Framework

The following key documents provided guidance and direction for this plan:

The **Centers, Corridors and Wedges Growth Framework** (adopted August 2010) is the overarching growth policy for Charlotte. This policy document revises the original Centers and Corridors concept by establishing a vision for Charlotte's future growth and development. It does this by: 1) identifying three geographic types used to categorize land in Charlotte's "sphere of influence" – Activity Centers, Growth Corridors, and Wedges; and, 2) outlining the desired characteristics of each of these geographies.

This policy intends to help guide growth into areas that can best support it, and away from areas that cannot. Thus, much of Charlotte's future moderate to higher intensity development is targeted within five major Growth Corridors and in designated Activity Centers, consistent with area plans. This will help maximize existing infrastructure and services. Low to moderate density residential and services supporting neighborhoods is targeted for areas between corridors, referred to as Wedges.

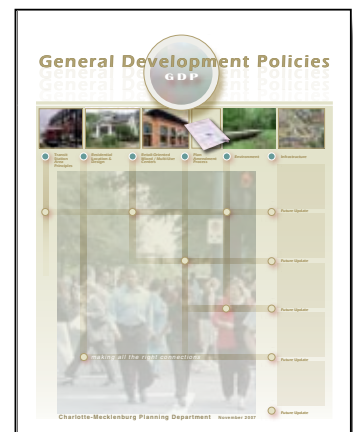
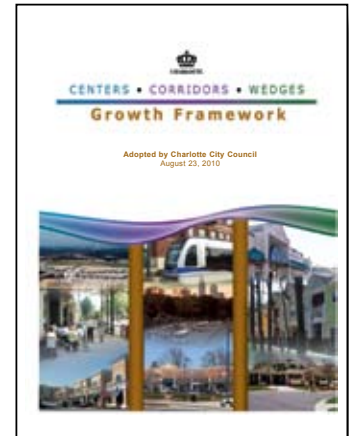
Activity Centers are focal points of economic activity, typically with concentrations of compact development. Many existing Activity Centers have the capacity for significant new growth in conjunction with enhancements to the supporting infrastructure. There are no Activity Centers within the plan area, although it is immediately adjacent to the Center City Activity Center.

Growth Corridors are characterized by the diversity of places they encompass – from historic neighborhoods to vibrant mixed use areas to significant employment and shopping districts – and by the accessibility and connectivity that they provide for these places. Some areas within the Growth Corridors, particularly the Transit Station subareas, are appropriate locations for greater intensity development with appropriate planning and neighborhood support and involvement.

Wedges are the large areas between Growth Corridors, where residential neighborhoods have developed and continue to grow. The Wedges comprise mainly low density housing, as well as a limited amount of moderate density housing and supporting facilities and services.

While most of the plan area is within a Growth Corridor, much of it falls within an Established Neighborhood subarea. Typically, a Growth Corridor is an appropriate location for higher intensity uses and development; *however, that is not the case for much of the plan area.* The **Centers, Corridors and Wedges Growth Framework** provided a starting point for maintaining and preserving the lower density, single family residential character of the Established Neighborhood Subarea, and ensuring that surrounding development in the General Corridor area and the preliminary future Transit Station Subarea respects the existing historic residential character and scale.

The **General Development Policies (GDP)** are adopted policies relevant to development and redevelopment in Charlotte-Mecklenburg. In particular, they address residential location and design, retail-oriented mixed/multi-use centers and transit supportive development. Additionally, the *General Development Policies* provide guidance to minimize





## Elizabeth Area Plan

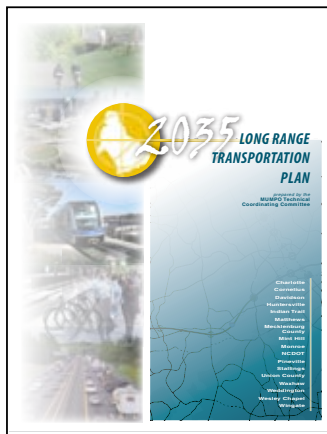
the negative environmental impacts of land development and to more closely link land use and development decisions to the availability of infrastructure to support it.

The **General Development Policies** were used in this plan, in conjunction with neighborhood and stakeholder feedback, to determine where higher intensity residential development could be supported and in providing the basis for the community design policies. This plan builds upon the 2007 GDP guidance for residential densities, community design, and environmental policies and provides more specificity in those particular areas.

**Adopted Land Use Plans** include six geographic districts that make up the City of Charlotte's planning jurisdiction: the Northeast, East, South, Southwest, Northwest and Central districts. Each of these areas has a district plan that addresses a wide range of physical development issues and provides parcel-specific land use recommendations for all properties within that district plan. This plan will update the *1993 Central District Plan*.

The **2035 Long Range Transportation Plan** (March 2010) defines the policies, programs and projects to be implemented over the next 20 years, providing transportation choices in Mecklenburg and western Union County. The widening of East Seventh Street is not a funded project in the *2035 LRTP* and this was an important factor in determining the future streetscape and development pattern along this corridor.

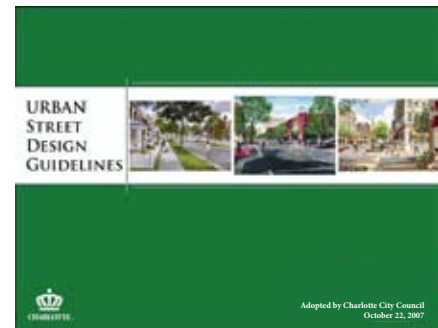
The **Transportation Action Plan** (2011) defines short and long term policies for accommodating motor vehicles, transit riders, bicyclists and pedestrians together with an implementation "blueprint" for improvements. The *Transportation*



*Action Plan's* comprehensive "toolbox" of transportation programs will help implement the policies in this plan.

The **2030 Transit Corridor System Plan** is a long-range multi-modal transportation plan that identifies five transportation corridors, a series of Center City improvements, and enhanced bus service and facility improvements throughout the region. The Southeast Transit Corridor supports a potential future rapid transit station within the plan area. This information was used in the area planning process to determine the preliminary future Transit Station Subarea and land use and development pattern within that area.

The **Urban Street Design Guidelines** (2007) offer a comprehensive approach to designing new and modified streets within the City. They are also a key component of the *Transportation Action Plan* and were used in developing this plan to assist with street classifications and cross-sections that will guide the design and redesign of streets. The *Urban Street Design Guidelines* also address integrating land use and transportation to create synergy between the streets and land uses adjacent to them.



The **Greenway Master Plan Update** (2008) identifies an expanded greenway network and greenway trail system throughout Mecklenburg County. As land is acquired and set aside over time, the greenway system should also improve water quality by reclaiming natural floodplains, protecting wildlife habitats and open space and providing recreational and educational opportunities. Little Sugar Creek Greenway and the Briar Creek Greenway traverse the plan area and are covered by the *Greenway Master Plan Update*.

## Key Opportunities and Challenges

An understanding of existing conditions, including feedback received from the public during the planning process, reveals opportunities to build upon and challenges to address. Key opportunities and challenges, highlighted below, were discussed at the first public meeting and formed the basis for the vision statement and ultimately, plan policies.

### ■ Opportunities ■

**Eclectic Character and Mix of Uses** The plan area includes a diverse and organic mix of residential, retail, institutional, and offices uses that contribute to the area's unique character. This plan provides an opportunity to preserve this mix and guide future development in such a way as to complement the existing neighborhood character, scale, and diversity.

**Parks, Recreation, and Open Spaces** The plan area includes Independence Park, American Legion Memorial Stadium, and Colonial Park, all offering passive or active recreational opportunities for area residents. The area surrounding Independence Park provides an opportunity for new and additional housing opportunities, oriented to take advantage of the park and its amenities. The developing Little Sugar Creek Greenway and the planned Briar Creek Greenway present additional open space, and the opportunity for future connections from the neighborhood to them, and from one to the other through the neighborhood.

**Distinctive Architectural Design** Historic homes and structures in the plan area blend their distinct architectural styles with the new to form a cohesive community identity. Future development has the opportunity to build on this relationship and strengthen that identity even further.

**Accessibility** Being one of Charlotte's oldest inner ring neighborhoods, the plan area in general has good pedestrian, bicycle, and vehicular connectivity via a well-established street grid. Opportunities exist to complete missing pieces of the grid to further enhance accessibility and to preserve the desirable elements of the existing grid. The area is also well-served by public transportation.

**Natural Features** The plan area benefits from an established tree canopy and green spaces. Opportunities exist to preserve and add to these neighborhood assets.

### ■ Challenges ■

**Development Pressure** Future development will jeopardize the character of the plan area if it is not sensitive to the scale, massing, and context of adjacent neighborhoods.

**Memorial Stadium Site** The future of this site, including preservation of historic Memorial Stadium, the Grady Cole Center, and the remaining surface parking areas, is an issue of focused concern for area residents.

**Institutional Expansion** The acknowledged need for plan area institutions such as hospitals and schools to expand and grow has to be carefully balanced with the desire to preserve and protect neighborhood character and scale.

**Traffic** Traffic volumes and speeds, along with limited crosswalks, present a particular challenge for pedestrians on some plan area streets.

**Aging Infrastructure** Upgrading and maintaining the area's aging infrastructure will be costly but should be a high priority.



*Colonial Park in Crescent Heights offers residents opportunities for both active and passive recreation.*

The desired future for the plan area was developed with input and participation from the community and is best expressed in a comprehensive Vision Statement. This statement describes the kind of place the community envisions for itself in the future. It generates common goals, offers the possibility for change, and gives the community something to move toward. When developing the Vision Statement, staff and the advisory group considered all of the opportunities and challenges identified during the public meetings.



## *Vision Statement*

Elizabeth is a community that embraces its urban vitality, preserving its historic heritage and diversity of residents, businesses, and institutions. The area is a finely-woven tapestry that delicately balances the buildings, open spaces, landscapes, and pedestrian scale. Elizabeth will strive to accommodate area-contextual housing types for various income levels. Well-conceived infill development, important to the ongoing vitality of the area, will be encouraged and balanced with neighborhood preservation. There should be broad community participation in planning and development decisions that affect the fabric of the community.





## Elizabeth Area Plan Goals

To achieve the future envisioned for the plan area, the following goals have been identified. The goals build upon adopted plans and policies.

### Land Use

Maintain and preserve the area's predominant historic residential character and parks and open spaces, balanced with well-crafted and appropriately scaled businesses, health care facilities, and educational institutions.

### Community Design

Plan new development that respects the scale, design, and proximity of adjacent buildings and land uses. Good architectural and site design and quality construction materials should be utilized to ensure the long-term sustainability and economic vitality of all development.

### Transportation

Support and develop a connected, affordable, balanced transportation system that encourages walking. It should safely and sensibly accommodate pedestrians, bicyclists, and transit users while allowing for neighborhood-friendly automobile speeds for motorists and business patrons traveling to and through Elizabeth.

### Infrastructure and Public Facilities

Provide and maintain public facilities such as parks, greenways, recreational facilities, and infrastructure in excellent condition and emphasize environmental and economic sustainability in the design and planning of new or updated public facilities and infrastructure.

### Natural Environment

Preserve and protect the natural environment, parks, tree canopy, open spaces, and natural resources. Encourage measures that will help ensure the long-term sustainability of the tree canopy in Independence Park and other public spaces. Support expansion of the greenway network, enhancement and improvement of existing greenways, and connections between greenways and to neighborhood centers. Promote measures to minimize the adverse affects of new development on air and water quality, and encourage the use of eco-friendly building technologies and renewable resources.

## Overall Plan Concept

**Map 3, Concept Map**, page 17, illustrates the recommended development pattern for the plan area. As illustrated, the plan area is expected to remain predominantly low density residential, with opportunities for more intense, mixed-use development near the future streetcar line and a proposed rapid transit station. It is anticipated that existing institutional uses (hospitals, colleges, and churches) remain an important component of the plan area. Office uses, many of which complement the nearby hospitals, are supported at key locations. This development pattern will be sustained by improvements to enhance accessibility and safety for pedestrians, vehicles and cyclists. Key to this accessibility is support for the greenway system, its future improvements and connections to other uses outside the plan area, and the preservation of existing parks and open space.

- **Maintain and build upon the character of the established, historic neighborhood.** The foundation of the concept is maintaining and building upon the existing, established character of the Elizabeth neighborhood. As such, most of the plan area remains predominantly low density, single family residential. Design guidelines will help to ensure that when higher intensity uses are located nearby, they respect the low density, historic character of the Established Neighborhood Subarea.
- **Preserve and enhance parks and open space.** Protecting the existing parks and open spaces, linking them with greenways and overland connectors, making sure they are available for public use, and ensuring they are well-maintained, are important to the area's continued livability.
- **Create a more inviting pedestrian environment.** Another key component of the overall concept is the creation of a more vibrant, pedestrian-friendly environment. This is accomplished by:
  - Making streets like East Seventh Street safer for walking and adding some new street connections.

- Concentrating potential future higher density development at selected nodes.
- Developing a continuous greenway connector throughout the plan area that links parks and open spaces both within and outside the Plan Area.

- **Recognize the importance of institutional and office uses.** Existing institutional uses (hospitals, colleges, and churches) will remain important components of the plan area.
- **Continue the transition to more intense, mixed use development near future transit stations and selected streetcar stops.** The appropriate location for more intense, mixed use development is adjacent to the potential future rapid transit station and streetcar stops on Elizabeth Avenue.

### Public Sector

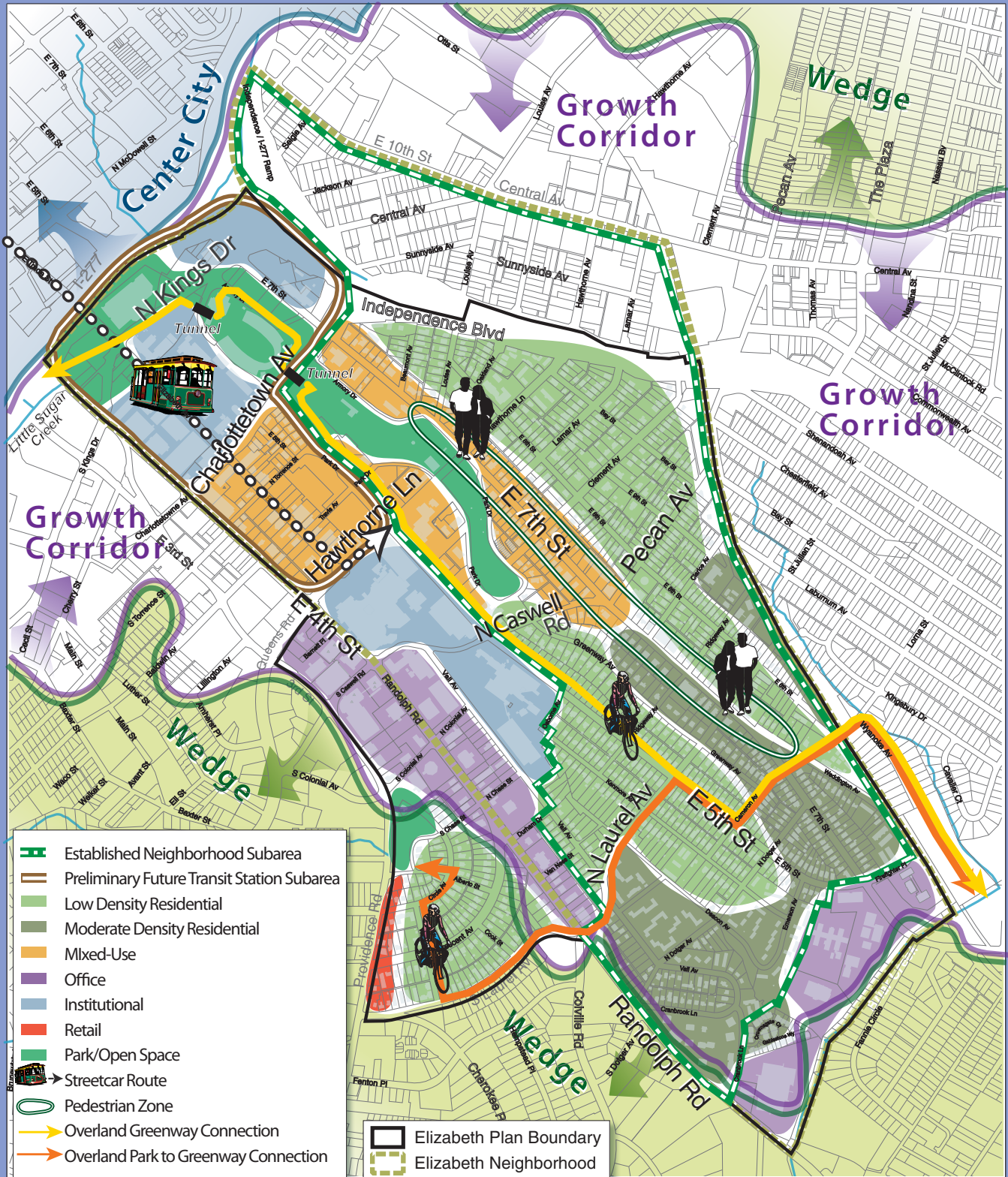
With input from the community, the public sector will provide the policy framework for land development and will be responsible for making a number of infrastructure improvements, including some investment in streetscape amenities that can be accommodated within the existing right-of-way. In addition, the Charlotte-Mecklenburg Planning Department, in consultation with other City and County departments, is responsible for initiating and guiding the corrective rezoning process and monitoring and reviewing rezonings proposed for the plan area to ensure developers meet the required development standards and carry out the intent of the adopted policy plan for the area.

### Private Sector

The private sector will be responsible for developing and redeveloping properties within the plan area in alignment with the vision, policies, and development and design standards included in the Concept Plan. Providing the required infrastructure improvements will be part of these development responsibilities.

# Elizabeth Area Plan Concept Map

Map 3



Produced by the Charlotte-Mecklenburg Planning Department.



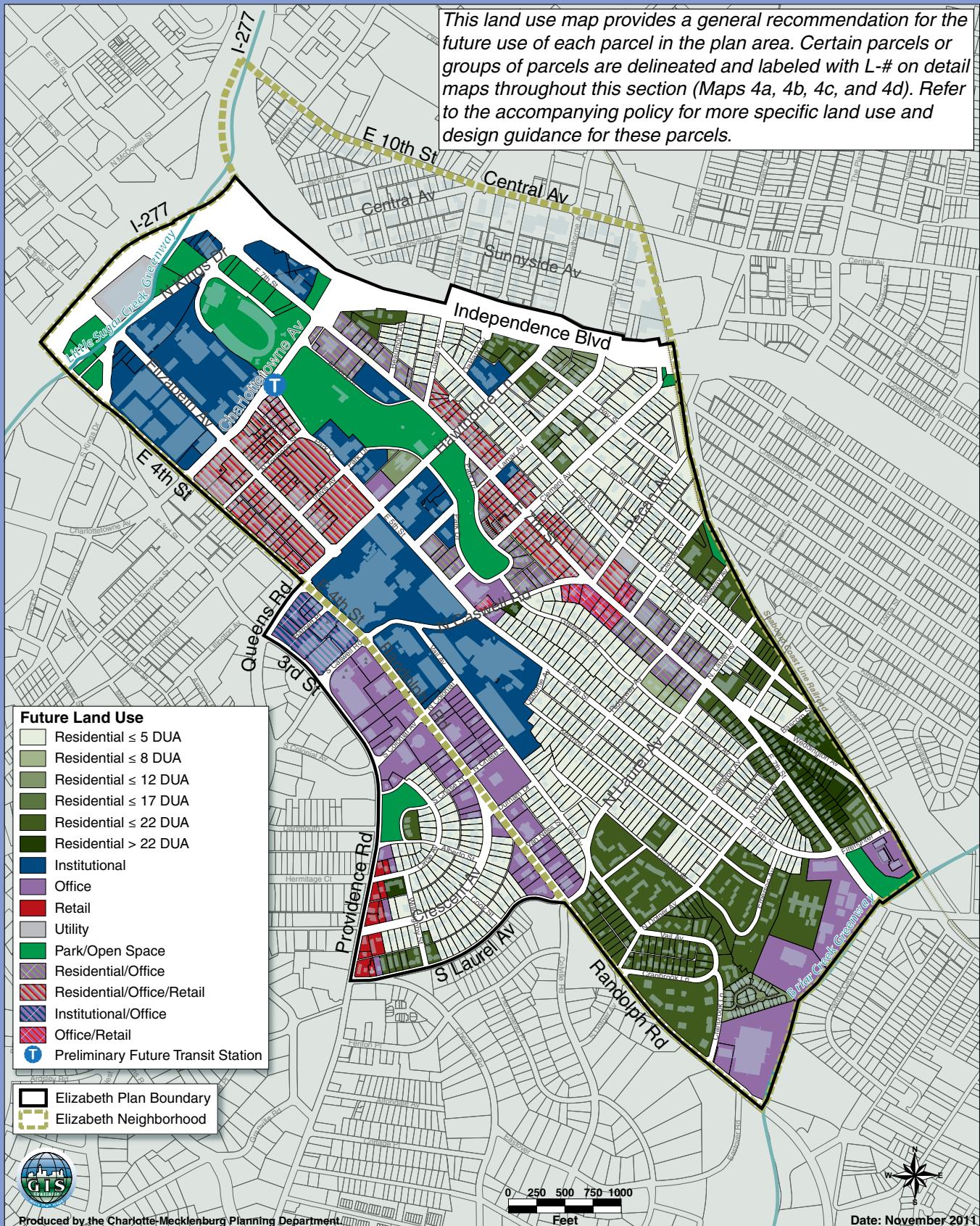
Date: November 2011



# Elizabeth Area Plan

## Future Land Use

This land use map provides a general recommendation for the future use of each parcel in the plan area. Certain parcels or groups of parcels are delineated and labeled with L-# on detail maps throughout this section (Maps 4a, 4b, 4c, and 4d). Refer to the accompanying policy for more specific land use and design guidance for these parcels.



## Plan Policies

### Overview

The following sections set forth plan policies for Land Use, Community Design, Transportation, Infrastructure and Public Facilities, and the Natural Environment in order to realize the vision and goals for the plan area. Goals for each section are briefly summarized, followed by the recommended plan policies. Plan policies will guide future land use, zoning, and other growth and development/redevelopment decisions.

### Land Use

Elizabeth is one of Charlotte's most appealing neighborhoods, benefiting from lush tree-lined streets, a charming historic residential character, central location, proximity to employment and education opportunities, architectural and economic diversity, and neighborhood stability. Elizabeth is well-positioned to continue to thrive, especially as demographic and economic trends increasingly favor its walkable, interactive, convenient, efficient, and authentic character. Key to Elizabeth's success will be the ability to take advantage of opportunities while maintaining the character and qualities that make the area desirable.



***Goal** Maintain and preserve the area's predominant historic residential character and parks and open spaces, balanced with well-crafted and appropriately scaled businesses, health care facilities, and educational institutions.*

This section establishes future land use policies and recommends appropriate land uses to realize the plan vision and the land use goal. In developing these policies, transportation and community design issues were evaluated, considered, and incorporated as appropriate. The separate sections for Transportation and Community Design policies that follow build upon the policies discussed in this land use section.

In accordance with the *Centers, Corridors, and Wedges Growth Framework (2010)*, the land use policies for the plan area are organized into two major geographic areas, Growth Corridors and Wedges, illustrated on **Map 2**. The *Centers, Corridors, and Wedges Growth Framework* provides an overall vision for Charlotte's future growth and development. It does this by identifying three geographic types used to categorize land in Charlotte's "sphere of influence": Activity Centers, Growth Corridors, and Wedges. Each of these areas has general, desired characteristics *that are further refined for more specific geographies through the area planning process*.

The land use policies for the entire plan area are illustrated on **Map 4, Future Land Use** on page 18. Additional information is provided for several of the plan's land use policies, keyed to the numbered areas on the maps in this section.

Certain properties in the plan area may have deed restrictions that restrict their development and permitted use. However, no research was done in the development of this area plan to determine which properties may be subject to deed restrictions. Deed restrictions are not maintained or enforced by the Charlotte-Mecklenburg Planning Department, but are instead tied to the property and administered through the legal system.

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### Land Use Policies

#### 1. Growth Corridors

Virtually the entire plan area, with the exception of the Crescent Heights neighborhood, is within the Southeast Growth Corridor, as delineated by the *Centers, Corridors, and Wedges Growth Framework*. Growth Corridors may include one or more of the following subareas: Transit Station Areas, Interchange Areas, Established Neighborhood Areas, and/or General Corridor Areas. The *Centers, Corridors, and Wedges Growth Framework* provides guidance for these subareas within Growth Corridors to recognize their unique characteristics and vision for the future. The Elizabeth plan area includes an Established Neighborhood Subarea, General Corridor Subareas, and a preliminary future Transit Station Subarea.



## Elizabeth Area Plan

Today, much of the plan area includes established residential neighborhoods, with a mix of uses along East Seventh Street, institutional uses at the edges, and predominantly office uses along Randolph Road. In the future, the residential neighborhoods are expected to retain their existing character and density. The designation of a specific geography within the plan area as an Established Neighborhood Area Subarea will help preserve this character and density.

East Seventh Street, as it traverses diagonally through the plan area from Little Sugar Creek to Briar Creek, is expected to maintain a mixture of residential, neighborhood-oriented retail, and office uses, with some increase in intensity and density, although still in keeping with the scale of the surrounding area. Greater intensity and a broader mix of land uses are anticipated in the area adjacent to a potential future rapid transit station near Memorial Stadium/CPCC, including Charlottetowne and Elizabeth Avenues. Memorial Stadium is recognized as a valued community resource and designated as Parks/Open Space. Randolph Road is expected to maintain predominantly Office uses.

### A. Preliminary Future Transit Station Subarea

The *Centers, Corridors and Wedges Growth Framework* describes Transit Station Areas, which are subareas within Growth Corridors, as areas surrounding existing or planned rapid transit stations. They generally do not include established single family neighborhoods. They are envisioned as pedestrian-oriented transit villages, designed to include a mixture of complimentary moderate to high-intensity residential, office, retail/entertainment, and/or civic uses located within easy walking distance of a transit station, typically one-half mile.

The Southeast Corridor Rapid Transit Project (Silver Line) is one of five rapid transit corridors identified in the 2030 Transit Corridor System Plan, adopted by the Metropolitan Transit Commission in November 2006. While the proposed alignment for the Silver Line passes through the plan area, it is

unlikely to be completed within the next 20 years. The location of a proposed transit station within the plan area has not been precisely identified, and there is continuing discussion about what the mode of transit will ultimately be.

Because the Southeast Rapid Transit Corridor is included in an adopted transit plan, this area plan should attempt to recognize and account for its future impact on the plan area. However, due to the uncertainties surrounding this transit line, **the establishment of a Transit Station Subarea at this time is premature.** Instead, this plan delineates proposed boundaries of a preliminary future Transit Station Subarea. Since the exact location of the station had not been determined as of the date of this plan, an approximate station location has been identified. The proposed boundaries of the preliminary future Transit Station Subarea and the potential station location are indicated on **Map 4a** on page 22. These proposed boundaries generally reflect a half-mile walking distance from this proposed rapid transit station.

Any development or redevelopment that occurs within the preliminary future Transit Station Subarea should adhere to the land use, community design, and mobility guidelines outlined in the Transit Station Area Principles summary on page 24. These guidelines are appropriate for this location, even absent an operational rapid transit line. More specifically, the area bounded by Hawthorne Lane, East Fifth Street/Park Drive, Charlottetowne Avenue, and East Fourth Street is



*This area of residential quadrplexes on Park Drive is a short walking distance from a potential rapid transit station near Memorial Stadium and CPCC.*



## Elizabeth Area Plan

envisioned to include a mixture of complimentary moderate to high-intensity residential, office, retail/entertainment, and/or civic uses. Equally as important as land use are how the uses are arranged and designed and the interaction between parcels. The design of a project should encourage walking, providing connections to the various available modes of transit (such as bus and future streetcar) and the surrounding neighborhood.

The initial phase of the Charlotte Streetcar will connect Presbyterian Hospital with the Charlotte Transportation Center utilizing Hawthorne Lane, Elizabeth Avenue, and East Trade Street. Three streetcar stops are planned within the preliminary future Transit Station Subarea: Hawthorne at East Fifth, Elizabeth Avenue at Travis Avenue, and Central Piedmont Community College on Elizabeth Avenue between Charlotte and Kings. The streetcar will eventually connect the Eastland Mall area with Rosa Parks Community Transit Center on Beatties Ford Road.

The Charlotte Streetcar project will bring with it urban design principles that are compatible with the goals and policies of this plan, such as connecting stops with adjacent neighborhoods and promoting a mix of uses and public spaces that will draw people together and highlight the area's diversity. With streetcars once again rolling through

the neighborhood's streets, Elizabeth will have taken a step toward reclaiming a past identity as one of Charlotte's original "streetcar suburbs".

**The adoption of this area plan will not result in a Council adopted transit station area plan. Thus, Transit Oriented Development (TOD) zoning will not be possible.** Instead, other urban zoning categories should be considered if parcels in this area are rezoned. These include MUDD (Mixed-Use Development District), NS (Neighborhood Services), UR-1 (Urban Residential – Single Family), UR-2 (Urban Residential – Moderate Density), UR-3 (Urban Residential – High Density), and UR-C (Urban Residential – High Density Mixed Use). Other urban districts may be developed in the future and may also be appropriate to consider.

The boundaries, plan policies, and development guidelines for the preliminary future Transit Station Subarea should be reviewed and amended as appropriate when plans for rapid transit become more solidified (i.e., mode determined, station locations refined, funding identified). This may include the adoption of a Transit Station Area Plan.

The following section provides additional information on several land use policies for the preliminary future Transit Station Subarea.

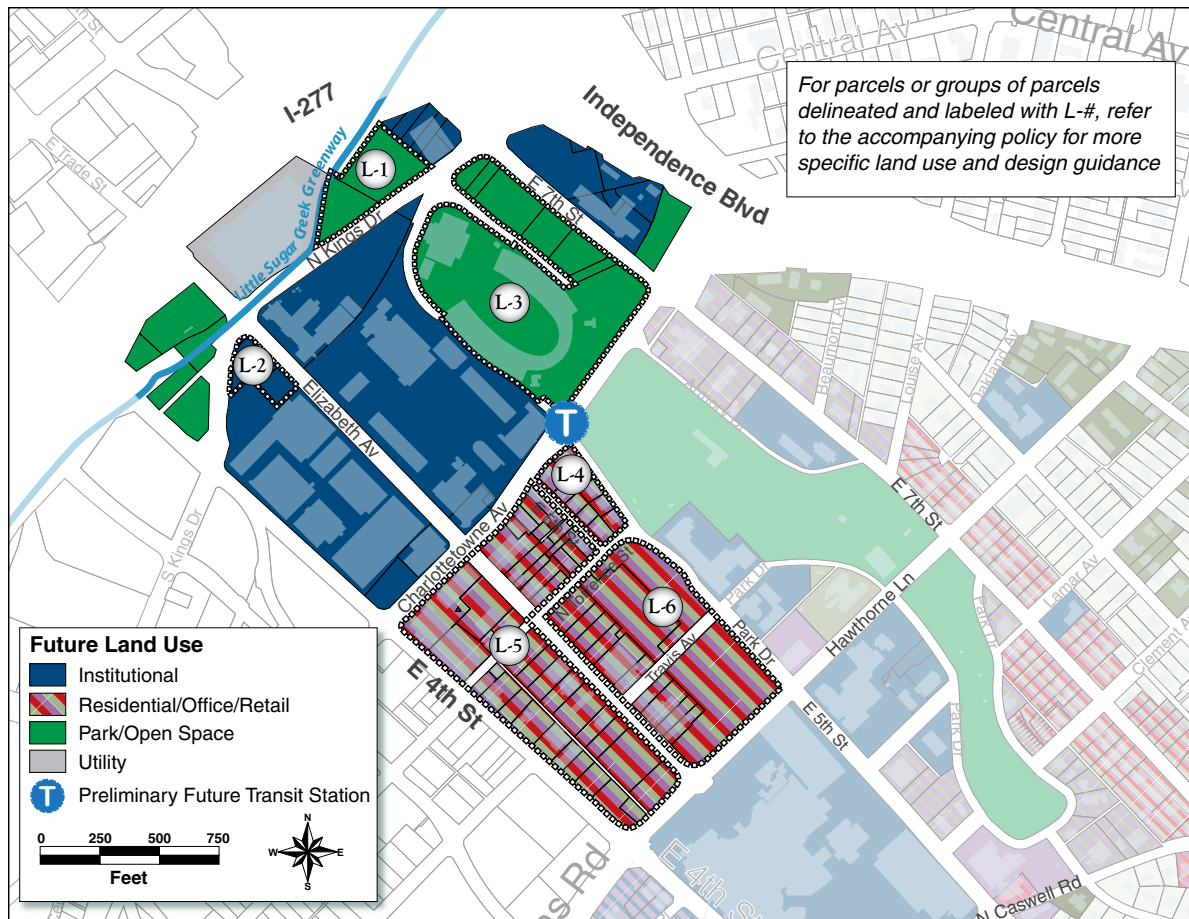


*Memorial Stadium and Grady Cole Center, with the Independence Park playing fields beyond. A pedestrian/bicycle connection alongside the stadium will link the Little Sugar Creek Greenway to the park.*



*A mix of residential, office, and retail uses are recommended for this section of Elizabeth Avenue, as well as for much of the surrounding area. Tracks are in place for the Charlotte streetcar project.*

Map 4a Preliminary Future Transit Station Subarea Land Use Policy Recommendations L-1 through L-6



## Preliminary Future Transit Station Subarea Land Use Policy Recommendations L-1 through L-6

### **L-1 Support the continued use of these CPCC-owned parcels as Park/Open Space**

for use by students and the public. This area includes a portion of the Little Sugar Creek Greenway and also frames the entrance to the CPCC Culinary Arts Building with a formal plaza.

### **L-2 Encourage this area to remain as Park/Open Space.**

Although shown as appropriate for Institutional use, the area should remain as open space for the enjoyment of students and the public. It is part of an expanse of open lawn in front of CPCC's Overcash Building; CPCC does not have plans to build in this area in the next several years, if ever. Removal of the existing fence around the perimeter would open up this lawn to the street and enhance its usability.

### **L-3 Support the continued use of this multiple-parcel site for Park/Open Space uses.**

Comprising six parcels owned by Mecklenburg County and one owned by CPCC, the site is currently home to American Legion Memorial Stadium, the Grady Cole Center, and a multi-story parking deck serving the stadium and CPCC. The stadium, which is a designated Historic Landmark, should be maintained and preserved. If it is no longer feasible or practical to maintain the stadium as a usable

## *Elizabeth Area Plan*

facility, it should be replaced with a similar facility or other public recreational use. If the Grady Cole Center is replaced or redeveloped, the site should include a recreational facility available for public use. Appropriate uses for the parcels fronting East Seventh Street are park and open space, surface or structured parking, or recreational facilities. These parcels should be activated at street level with landscape and design elements.

This site is presently zoned MUDD-O (Optional). However, reverter clauses exist for a large portion of this site (the Memorial Stadium and Grady Cole Center portions) stipulating that if the land is used for other than park purposes, it reverts to the previous owners, or their heirs. Some of the smaller parcels fronting East Seventh Street, as well as the Memorial Stadium and Grady Cole parcel, are also subject to deed restrictions imposed by the City of Charlotte requiring park and recreation uses.

The stadium, as well as Tax Parcel #08017101 on which it is located, enjoys Historic Landmark status. The Grady Cole Center is specifically excluded from this designation. The Charlotte-Mecklenburg Historic Landmarks Commission has jurisdiction over designated historic landmarks in terms of any proposed material alterations or modifications. In the event of such proposed changes, nearby property owners as well as other interested parties (such as neighborhood associations) would be notified and provided an opportunity to comment.

Any proposed development activities on this site are likely to receive focused attention from neighborhood residents and other stakeholders who have a desire to see the site maintained as public parkland.

### ***L-4 Indicate Residential/Office/Retail as appropriate for this location.***

Residential > 22 DUA is appropriate in this area that now comprises mainly two-story quadriplexes. Also appropriate is ground floor neighborhood-serving retail or office uses, with retail preferably fronting Charlottetowne Avenue and the park.

### ***L-5 Indicate Residential/Office/Retail as appropriate for this location.***

The commercial heart of the northern section of the plan area, it is bisected by Elizabeth Avenue which links Uptown Charlotte with Hawthorne Lane and Elizabeth's hospitals and other health care facilities. Development in this area should feature active ground floor non-residential uses such as retail, office, and civic. High-density residential (>22 DUA) and/or office uses are appropriate for upper floors. Buildings should be oriented toward the primary streets (East Fourth Street, Elizabeth and Charlottetowne Avenues, North Torrence Street, and Hawthorne Lane), and should provide for ground floor pedestrian interest with amenities such as outdoor seating/dining, plazas and urban open space, street trees, pedestrian scale lighting, and public art. Parking should be structured and/or behind buildings, with parking structures wholly or partially wrapped with ground-level uses that activate the street and provide pedestrian interest.

### ***L-6 Indicate Residential/Office/Retail as appropriate for this location.***

Most of this area, now a combination of small commercial uses and vacant land, is zoned MUDD – Optional, with approved conditional development plans. If these plans are not implemented, development in this area should be as described for L-5 above.



## Transit Station Area Principles



### Land Use

- Preserve existing stable neighborhoods.
- Encourage highest density uses (15 - 20 DUA/ 0.5 - 0.75 FAR) closest to the transit station and transition to lower densities adjacent to existing single family neighborhoods.
- Encourage a mixture of residential, office, service-oriented retail and civic uses, either through mixed or multi-use development.
- Disallow automobile-dependent uses, such as automobile sales lots, car washes and drive-thru windows.
- Consider special traffic generators - such as cultural, educational, entertainment or recreational uses - to locate in station areas.
- Encourage a mixture of housing types, including workforce/affordable housing.

This figure summarizes the policies of the *Transit Station Area Principles* (2001). The original wording for the policies can be found in the first chapter of the *General Development Policies*.



### Community Design

- Orient buildings to front onto public streets or open spaces.
- Minimized setbacks and locate parking to the rear.
- Provide windows and doors at street level and minimize walking distance to entrances.
- Screen unsightly elements, such as dumpsters, loading docks, service entrances and outdoor storage from the transitway.
- Include active uses on the ground floor of parking structures.
- Include elements such as street trees, pedestrian scale lighting and benches in streetscape design to encourage pedestrian activity.
- Place utilities under ground, wherever possible.
- Establish public open spaces that act as development catalysts and serve as focal points around transit stations.
- Design open spaces to be centers of activity that include items such as benches, fountains, and public art.



### Mobility

- Create a multi-modal environment that emphasized pedestrians, bicyclists and vehicles.
- Provide an extensive pedestrian system throughout the station area to minimize walking distances, connect to neighborhoods, accommodate large groups of people, and eliminate sidewalk gaps.
- Design the pedestrian system to be accessible, safe and attractive, by using planting strips, street trees, onstreet parking and bicycle lanes.
- Develop an interconnected street network with maximum block lengths of 400'; provide mid-block crossings if blocks are larger.
- Establish parking maximums, rather than minimums.
- Minimize surface parking and encourage shared parking facilities.

## B. Established Neighborhood Subarea

The *Centers, Corridors and Wedges Growth Framework* includes a provision for Established Neighborhood subareas within Growth Corridors. The Established Neighborhood Subarea category was created in response to concerns that neighborhoods located within Growth Corridors should not only be preserved, but also should be provided with an additional level of protection against the type and intensity of development typically thought to be associated with a Growth Corridor. The designation of an area within a Growth Corridor as an Established Neighborhood subarea is intended to help ensure the preservation and protection of the existing neighborhood scale and character, and to provide additional guidance on future growth and development.

The Elizabeth Established Neighborhood Subarea boundary is illustrated on **Map 2, Centers, Corridors and Wedges**, page 4. This subarea

encompasses the residential core of Elizabeth, and also includes the area between Independence Boulevard and East Tenth Street/Central Avenue, a geography considered to be part of the Elizabeth neighborhood but not encompassed by this area plan. Land use policies for this area are included in the *Belmont Area Revitalization Plan (2003)*, the *Plaza Central Pedscape Plan (2003)*, and the *Sunnyside Pedscape and Land Use Plan (2004)*. Because the area between Independence Boulevard and East Tenth Street/Central Avenue was not examined in detail during the planning process, these boundaries may be refined through future planning. For example, some of this area may fall within the boundary of a preliminary future Transit

Station Subarea surrounding a potential rapid transit station in the vicinity of Pecan Avenue and Independence Boulevard.

Established Neighborhood subareas are existing, well-established, primarily low-density residential areas where the predominate land use is, and should continue to be, single family residential. The Elizabeth Established Neighborhood Subarea also

*The designation of an area as an Established Neighborhood Subarea is intended to help ensure*  
**THE PRESERVATION AND PROTECTION**  
*of the existing neighborhood scale and character.*



(Left) Today the intersection of North Caswell Road and East Seventh Street is the heart of Elizabeth's commercial center, just as it was in 1969 (right).

## Elizabeth Area Plan

contains low and moderate density multi-family residential uses, civic uses including parks and schools, small-scale offices, and neighborhood-serving retail uses, all of which are appropriate for an Established Neighborhood.

East Seventh Street is the Established Neighborhood's commercial center. It should continue to function as an important vehicular corridor for residents, area employees, and commuters, but with a more pedestrian-friendly character and streetscape, especially between Charlottetowne and Laurel Avenues. A mixture of residential and office uses are recommended for the length of the street, with ground floor retail use added to this mix between Louise and Clarice Avenues, East Seventh Street's central commercial district. Land uses along East Seventh Street should be either a vertical mix of uses within the same parcel or site, or single use parcels which complement other nearby parcels to form a mixed use area that is organic in character.

Future non-residential development in this Established Neighborhood Subarea should be of low or moderate intensity, located primarily along East Seventh Street, East Fifth Street (north of North Caswell Road), North Caswell Road, and Park Drive. Incorporation of existing buildings into new development or redevelopment should

be encouraged where possible, particularly if the existing structures are of particular historical, architectural, or aesthetic value. Development should provide for the addition of sidewalks, planting strips, lighting, and other amenities so the area will gradually become a better, safer environment for pedestrians.

Also in the Established Neighborhood Subarea, a future streetcar route traverses Hawthorne Lane from Elizabeth Avenue across Independence Boulevard into Sunnyside and beyond. Despite the presence of the streetcar, the existing residential land uses along Hawthorne Lane should be retained with little or no increase in intensity, and the parks and open spaces on the street should be protected and preserved.

To achieve the goals of this plan for the Established Neighborhood Subarea, the land use policies must work hand-in-hand with the design policies contained

in the Community Design section on page 36. Well-designed development can help ensure the Elizabeth Established Neighborhood will continue to thrive while at the same time maintaining and preserving the qualities so valued by area residents.

The following section provides additional information on several Land Use policies for the Established Neighborhood Subarea.

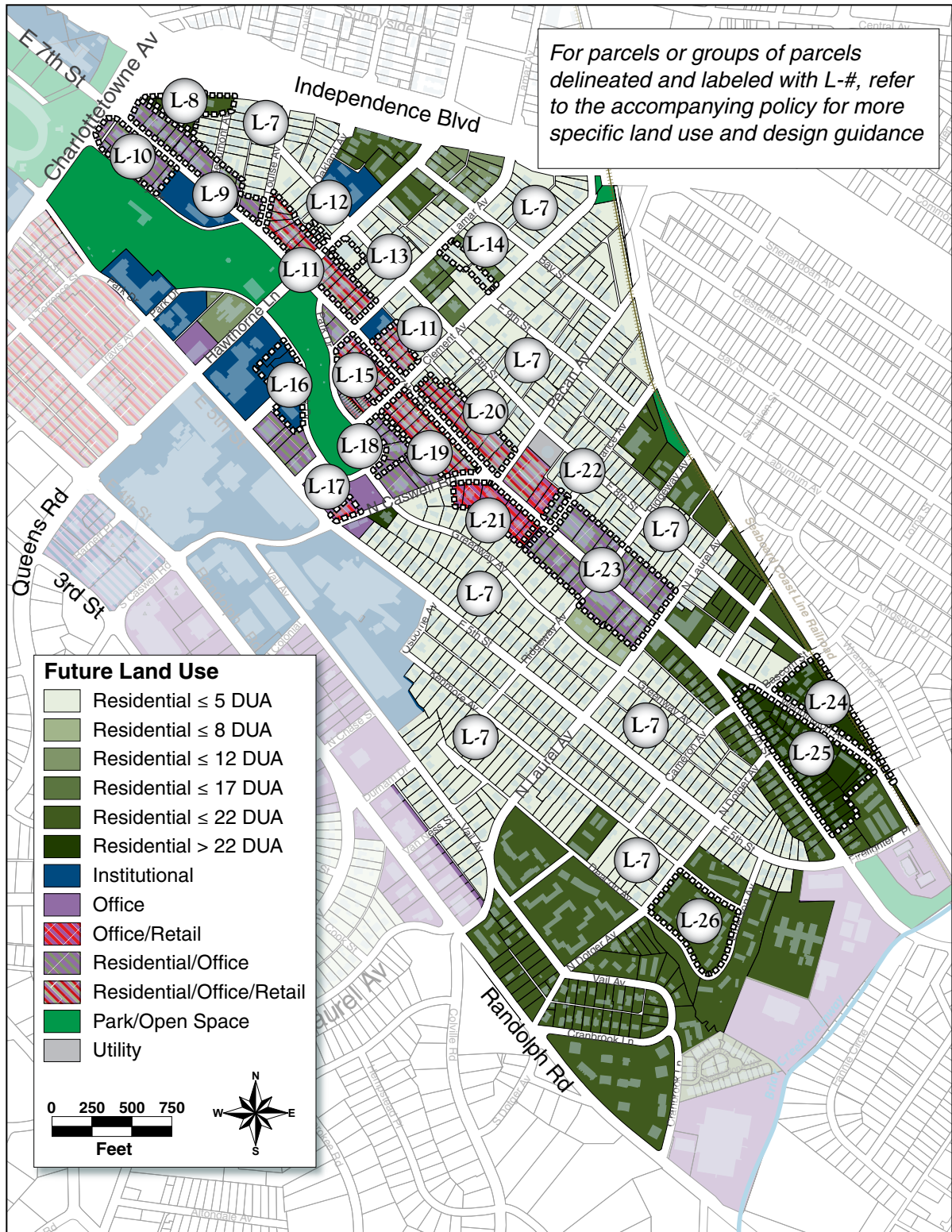


*Once a single family home, this East Seventh Street building has more recently housed non-residential uses. Historic structures like this that contribute to the fabric of the established neighborhood should be reused or incorporated into new development if possible.*



# Elizabeth Area Plan

Map 4b Established Neighborhood Subarea Land Use Policy  
Recommendations L-7 through L-26





*Duplexes, triplexes, and quads contribute to the diverse character of Elizabeth's predominately single family neighborhoods.*



*The south side of East Seventh Street from Lamar to Clement Avenues (L-15) is appropriate for mixed-use development in a pedestrian-oriented setting.*

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### ***Established Neighborhood Subarea Land Use Policy Recommendations L-7 through L-26***

#### **L-7 Maintain the lower density residential areas as Single Family Residential $\leq 5$ DUA.**

These areas, shown in light green on the accompanying map, should be preserved and protected, retaining their existing scale and character, with duplexes, triplexes, and quadriplexes organically interspersed amongst the neighborhood's predominantly single family structures.

#### **L-8 Indicate this site as appropriate for Multi-Family Residential $\leq 12$ DUA.**

However, the current institutional use (Charlotte Fire Department's Fire Prevention Bureau) is also appropriate for this location, as are other low-impact institutional uses. If a secondary access to East Seventh Street is established, a density higher than 12 DUA may be appropriate. For any proposed re-use or redevelopment, a study to determine the impact on neighborhood streets, particularly Beaumont Avenue, should be completed.

#### **L-9 Show these parcels fronting East Seventh Street as appropriate for Residential/Office.**

Characterized by two-story residential buildings, most of which house non-residential uses, this area is suitable for residential or office. Single-use residential development should have densities  $\leq 22$  DUA. Building heights should not exceed 40 feet. Development in this area should be sensitive to the adjacent single family neighborhood to the rear.

#### **L-10 Indicate this area as appropriate for Residential/Office.**

This area, currently comprising both office uses and a residential quadriplex, is suitable for residential or office. Single-use residential development may have densities  $> 22$  DUA. Building heights of up to 60 feet may be appropriate.

#### **L-11 Delineate this area along East Seventh Street as Residential/Office/Retail.**

These three blocks should host a mix of small scale neighborhood-serving retail, office, and/or residential uses. Small-scale neighborhood-serving retail or office uses on the ground floor with office and/or residential above is appropriate, as are single-use residential, office, or retail uses. Single-use residential development should have densities  $\leq 22$  DUA. Buildings should not exceed 40 feet in height. Development in this area should be sensitive to adjacent single family neighborhoods to the rear.



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### **L-12 Show this parcel as Single Family Residential ≤ 5 DUA.**

This property is currently used as a single family residence. However, the present zoning is for office use. A rezoning should be considered to better align the zoning with the existing land use.

### **L-13 Indicate this site as Residential/Office.**

Currently an office use, residential ≤ 12 DUA, office, or a mix of office and residential is appropriate in this location.

### **L-14 Show these parcels as appropriate for Single Family Residential ≤ 5 DUA**

The three subject parcels are currently used as single family but are zoned for multi-family. A rezoning should be considered to better align the zoning with the existing land use.

### **L-15 Indicate this East Seventh Street block as appropriate for Residential/Office/Retail.**

The parcels on this block are suitable for mixed use. Small-scale neighborhood-serving retail or office uses on the ground floor with office and/or residential above is appropriate, as are single-use residential, office, or retail uses. Single-use residential development may have densities > 22 DUA. Buildings should not exceed 40 feet in height. Incorporation of existing buildings into new development or redevelopment should be encouraged where possible, particularly if the existing structures are of particular historical, architectural, or aesthetic value. Alternatively, such buildings should be preserved through their relocation to a suitable site. Demolition of such buildings should be strongly discouraged. Primary or secondary parking and vehicle access should be from alleys, if possible.

### **L-16 Indicate Institutional as the appropriate land use for this area.**

These parcels currently comprise the King's College campus. If this institutional use should relocate, redevelopment of this site to a multi-family residential use (> 22 DUA) would be appropriate. Building heights should be limited to 60 feet.

### **L-17 Maintain Office/Retail as the appropriate land use for this parcel.**

This corner site, currently occupied by a single-story commercial building, is appropriate for small-scale neighborhood serving retail and/or office use.



*An 80' wide area of paved parking separates retail shops (located to the right) from the sidewalk, a condition that is unfavorable for pedestrians. If this site redevelops, the buildings should be brought closer to the sidewalk and the parking should be at the rear. (L-20)*



*This 1930's era single-story retail building at East Seventh Street and Pecan Avenue is a valued contributor to the neighborhood character and should be preserved, even if the adjacent single-story retail plaza redevelops (L-20).*



**L-18 Show this area as appropriate for Residential/Office.**

These parcels should be residential and/or office. Single-use residential development may have densities  $\leq 22$  DUA. Redevelopment should preserve the existing buildings if possible, and new construction should reflect the existing scale and size of the adjacent structures.

**L-19 Show this block as appropriate for Residential/Office/Retail.**

Small scale retail buildings, several of which are former residential structures, typify the existing conditions in this area. The parcels on this block are suitable for a mix of uses. Small-scale neighborhood-serving retail or office uses on the ground floor with office and/or residential above is appropriate, as are single-use residential, office, or retail uses. Single-use residential development may have densities  $> 22$  DUA. Incorporation of existing buildings into new development or redevelopment should be encouraged where possible, particularly if the existing structures are of particular historical, architectural, or aesthetic value. Alternatively, such buildings should be preserved through their relocation to a suitable site. Demolition of such buildings should be strongly discouraged. Buildings in this area should not exceed 40 feet in height, except that a taller iconic structure is appropriate for the corner of East Seventh Street and North Caswell Road. This intersection has been identified as the signature intersection of the neighborhood, and can support increased height of up to 60 feet with careful planning and sensitivity to adjacent parcels.

**L-20 Indicate this block as appropriate for Residential/Office/Retail.**

These parcels should be a mix of small scale neighborhood-serving retail, office, and/or residential. Small-scale neighborhood-serving retail or office uses on the ground floor with office and/or residential above is appropriate, as are single-use residential, office, or retail uses. Single-use residential development may have densities  $> 22$  DUA. Buildings should not exceed 40 feet in height and should step down to adjacent single family parcels. Primary or secondary parking and vehicle access should be from alleys, if possible.

The existing single-story retail use that comprises most of this block is a suburban-style automobile-oriented shopping plaza, with a wide expanse of surface parking between the street and the buildings. If this site is redeveloped, the new form of development should be multi-story mixed use, with retail or office uses on the ground floor and office or residential uses on the upper floor(s). Buildings should be sited closer to the street with surface and/or structured parking to the rear, accessed from the public alley if possible. There should be an 8' planting strip and 13' sidewalk behind the curb. The single-story retail building at the corner of East Seventh Street and Pecan Avenue is an important part of the neighborhood fabric and should be retained and preserved.

The *Development Concept for East Seventh Street* on page 32 illustrates one example of how these recommendations could be incorporated into the design for redevelopment of this site. It is included for illustrative purposes only.

**L-21 Show Office/Retail as the appropriate land use for this area.**

These parcels are suitable for small scale neighborhood-serving retail and/or office, consistent with their current use. The mixture of uses may be by parcel, and not necessarily a vertical mix. Buildings should not exceed 40 feet in height and should step down to adjacent single family parcels.

**L-22 Indicate the appropriate land use for these parcels as Residential/ Office.**

These parcels, currently used for single family residential, are suitable for office and/or residential. Parcels in this area may be single-use office or residential, or a mixed use. Single-use residential development should have densities  $\leq 22$  DUA. Building heights should not exceed 40 feet. Redevelopment or new development in this area should be sensitive to the adjacent single family neighborhood. If this site redevelops, the two parcels should be combined, with any non-residential uses fronting East Seventh Street.

**L-23 Show these blocks along East Seventh Street as appropriate for Residential/Office.**

Consistent with current uses on these blocks, this area is suitable for office and/or residential. Parcels in this area may be single-use office or residential, or a mixed-use. Single-use residential development should have densities  $\leq 22$  DUA. Building heights should not exceed 40 feet. Development in this area should be sensitive to the adjacent single family neighborhood.

**L-24 Indicate the appropriate land use for this site as Multi-Family Residential  $> 22$  DUA.**

The current industrial land use is acceptable. However, if this site redevelops, multi-family with densities of from 22 to 43 DUA would be an appropriate land use.

**L-25 Show this site as Multi-Family Residential  $> 22$  DUA.**

This area is conditionally zoned MUDD-O for a residential project with a density of approximately 65 DUA. If this project does not materialize or the site is redeveloped under a different scenario, a density  $\leq 43$  DUA should be considered.

**L-26 Maintain this site as Multi-Family Residential  $\leq 22$  DUA.**

The southernmost section of the Established Neighborhood area is appropriate for multi-family residential use. The Martha Washington Apartments site is currently developed at a density of approximately 8 DUA. Any redevelopment of this site should maintain a similar amount of open space, as well as preserving the mature tree canopy.

## C. General Corridor Subarea

General Corridor subareas are those within the Growth Corridor boundary that are not part of one of the other three types of subareas (Transit Station Areas, Established Neighborhood Areas, and Interchange Areas). Typically these General Corridor subareas are appropriate for a variety of moderate intensity uses, usually with a wide choice of transportation modes and facilities which will vary according to location. Development should be typically low- and in some cases mid-rise, with low scale development next to residential neighborhoods.

High quality streetscapes should be provided to unify the corridor and soften the urban environment. There should be a strong emphasis on pedestrian-oriented design features, such as buildings oriented toward the street with good pedestrian access, sidewalks, planting strips, and street trees, and parking located in the rear.

Elizabeth's General Corridor Subareas include medical facilities and office uses on Randolph Road and Vail Avenue, as well as similar uses adjacent to Briar Creek in the southernmost part of the plan area. These types of institutional and offices uses are generally appropriate in General Corridor subareas.

The two major hospitals in this area, Presbyterian and CMC-Mercy, are both high intensity uses with buildings of up to eight stories in height. This



*The Martha Washington Apartments site enjoys a generous amount of open space and large trees, giving this multi-family development its charm and character.*



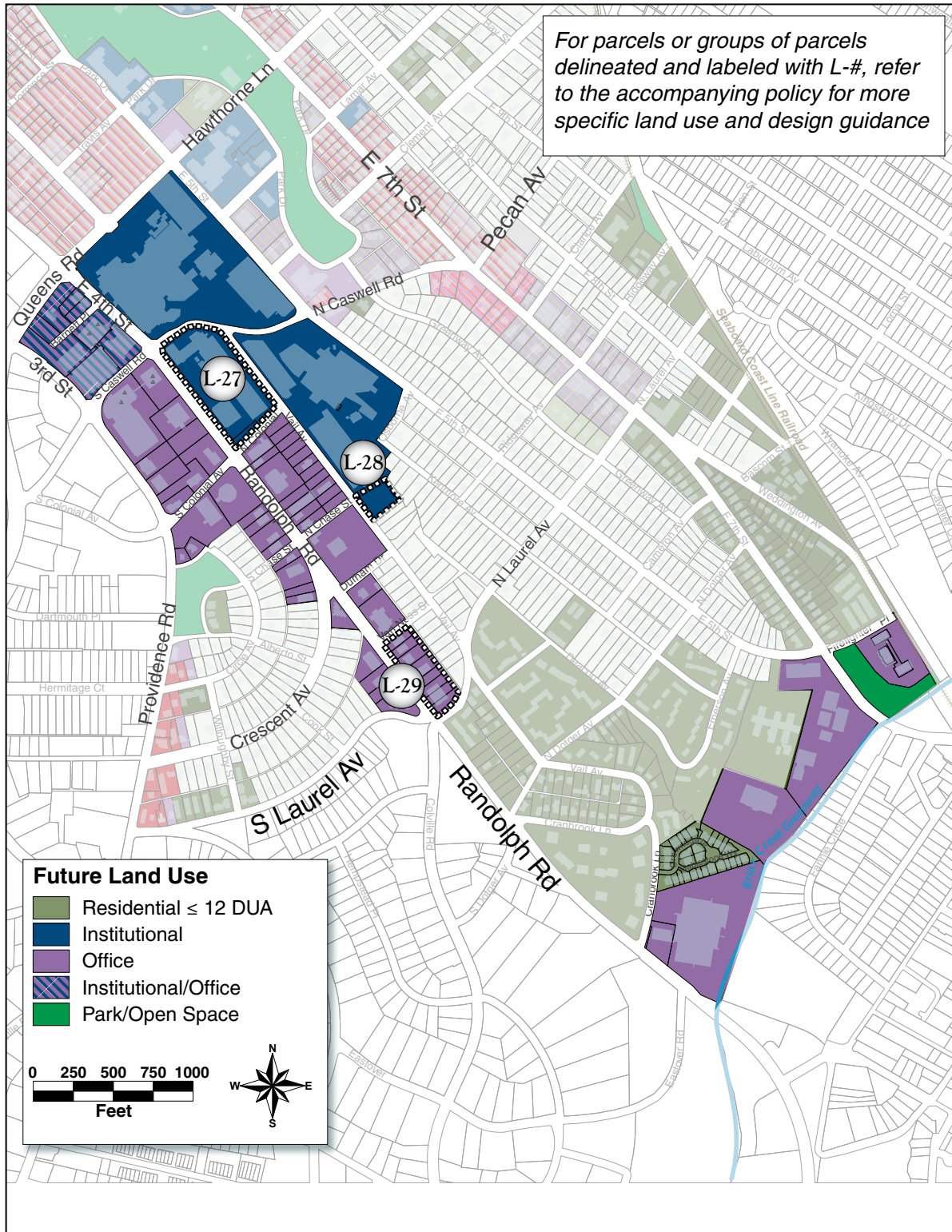
## Development Concept for East Seventh Street



Shown above is a conceptual redevelopment plan for an existing commercial shopping center at East Seventh Street and Pecan Avenue. The recommended future land use for this area is a mix of residential, office, and retail. This concept depicts a mixed-use development with structured parking accessible from a rear alley. The historic structure on the corner is retained. This development concept is included as an example of how the land use policies discussed in L-20 and the community design policies might be applied to this site.



Map 4c General Corridor Subarea Land Use Policy Recommendations  
L-27 through L-29



intensity and scale, not found anywhere else within the plan area, should be limited to the General Corridor and the preliminary future Transit Station Subareas.

The following section provides additional information on several Land Use policies for the General Corridor Subareas.

### **General Corridor Subarea Land Use Policy Recommendations L-27 through L-29**

#### **L-27 Indicate Institutional as the appropriate land use for this area.**

Existing uses include hospital, medical office/office, and structured parking. The site is appropriate for these and similar uses.

#### **L-28 Designate this area as Institutional.**

The southernmost portion of the Mercy Hospital site is zoned for single family residential use (R-5). The current use is a parking deck, a use that is not apt to change in the long term. The land use for the entire parcel should be Institutional. A rezoning should be considered for the R-5 zoned portion of the site to better align the zoning with the existing land use.

#### **L-29 Show Office as the appropriate land use for these parcels.**

The parcels on this Randolph Road block, between Van Ness Street and North Laurel Avenue, are primarily office and medical office uses, housed in two-story buildings with surface parking to the sides and rear of the building. New development or redevelopment should be similar in height, scale, and setback to the existing buildings, maintaining parking to the rear and sides.

## **2. Wedge Area**

Wedges are the large areas between the Growth Corridors, primarily made up of low-density residential neighborhoods. Wedge areas provide a range of housing choices, as well as supporting facilities and services.

The Crescent Heights section of the plan area is within a Wedge (see **Map 2**, page 4). It is a

primarily residential neighborhood sandwiched between office uses on Randolph Road to the east and small retail and office uses along Providence Road to the west. Crescent Heights is characterized by streets shaded with mature trees, lined with single family homes with an occasional duplex or triplex organically interspersed. Multi-family development is found in three separate areas and serves as a buffer between the single family areas and the non-residential uses along the Providence Road edge of the area. Colonial Park provides the neighborhood with both passive and active recreational opportunities. The current land use pattern of low density residential, with limited moderate density multi-family, and non-residential uses along the edges, is envisioned to continue in the future.

The following section provides additional information on several Land Use policies for the Wedge area.



*The office and institutional uses near Briar Creek in the southernmost part of the plan area are appropriate General Corridor land uses.*

## General Corridor Subarea Land Use Policy Recommendations L-30 through L-29

### **L-30 Maintain the lower density residential areas as Single Family Residential $\leq 5$ DUA.**

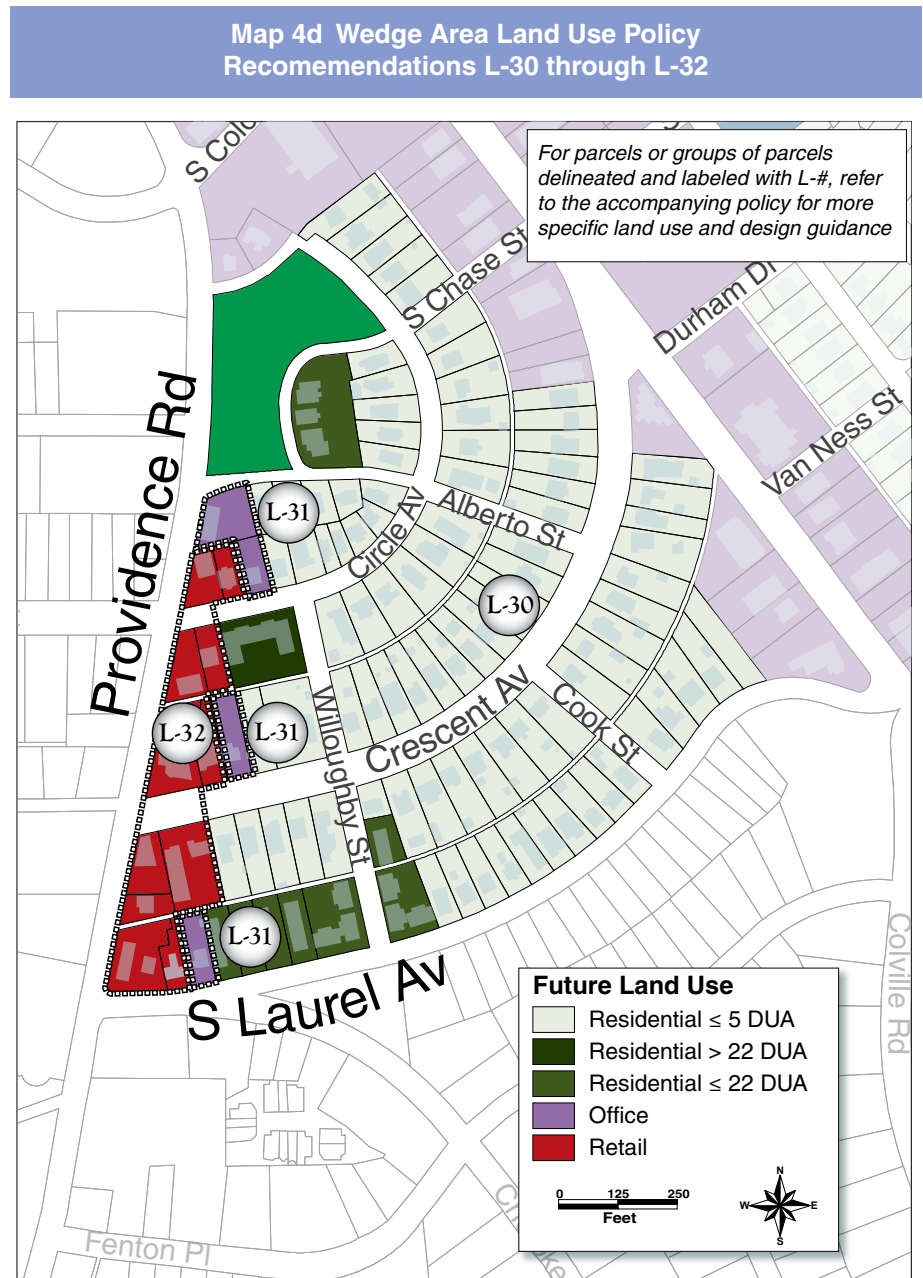
Maintain, preserve, and protect the existing single family character, density, and organic development pattern of the Crescent Heights residential neighborhood.

### **L-31 Indicate these parcels as appropriate for Office.**

These three locations are small-scale office uses, often in former residential buildings. The existing building scale and size of these office uses should be maintained, and these non-residential uses should not encroach further into the existing residential areas adjacent to these parcels.

### **L-32 Show these parcels as appropriate for Retail.**

The existing retail uses on the Providence Road edge of the Crescent Heights area should not encroach into the existing residential neighborhood beyond these parcels.





## Community Design

Community design policies help ensure that new development complements the existing or desired character of the Elizabeth community. While design policies alone do not dictate land use or zoning, they are used to strengthen how various land uses fit together. These policies consider not only the built environment, but also consider how people move through and amongst those spaces as pedestrians, bicyclists, or automobile drivers.

In the course of the area plan development, several Citizen Advisory Group members submitted photos depicting examples of good community design. These helped the CAG in its discussions of the various design qualities they wanted for the Plan Area, and ultimately in the advocacy of certain of the plan-specific design policies found herein.



**Goal** *New development should be contextual, respecting the scale, design, historic character, and proximity of adjacent buildings and land uses. Superior architectural design and the highest quality construction materials should be utilized to ensure the long-term sustainability and economic vitality of all development.*

### Residential Design Policies

Residential design in Elizabeth should recognize the size, scale, materials, rhythm, and massing of the surrounding neighborhood. This does not mean that new residential buildings should copy or mimic historic structures, but they may reflect certain design elements commonly found in the area's historic residential buildings. Good site design that recognizes adjacent development patterns, and the use of traditional building materials, can help ensure that new residential development blends as seamlessly as possible into the nearby neighborhood.



*Residential structures of differing sizes, scale, massing, and materials blend to give the plan area's residential neighborhoods an organic quality that residents value.*

RESIDENTIAL DESIGN POLICIES <sup>1</sup>			
D-1. Building Architecture and Site Design			
Policy Number	Single Family Detached	Single Family Attached and Multifamily	<p><i>Close attention to building design serves to enhance the community. Design policies do not result in conformity of design, but provide direction for both new and infill development.</i></p>
1A	+	+	Preserve and/or reuse historically or architecturally significant structures.
1B	+	+	Avoid blank walls along pedestrian circulation areas.
1C	+	+	Orient building towards street (unless a thoroughfare) or common open space and provide pedestrian access to the street.
1D	+	+	<b>Design infill residential buildings to blend harmoniously with adjacent residential structures, respecting their surroundings in size, scale, massing, setback, and materials.</b>
1E	+	+	<b>Encourage architectural design that complements the style, character, and materials of surrounding structures that contribute to the neighborhood character.</b>
1F	+	+	<b>Housing should be integrated into the areas surrounding Independence Park. Residential buildings adjacent to, or across the street from, Independence Park should be oriented to take advantage of the park and its amenities.</b>
1G	+	+	<b>Buildings greater than 40' in height immediately adjacent to single-family residential areas should "step down" to 40' or less along shared boundaries.</b>
1H	+	+	<b>Encourage the use of publicly-owned alleys for vehicular access to garages and interior parking areas, and/or to accommodate overhead utilities.</b>
<p><sup>1</sup> Adopted by City Council, the <i>General Development Policies</i> (GDP) provide guidance for the location, intensity and form of future development and redevelopment throughout the community. Many of the residential and mixed/multi-use center design policies for the plan area are based upon these adopted GDP policies. <b>Additional, plan-specific policies are shown in bold font.</b> Plan-specific policies augment the GDP design guidelines.</p>			



**1C** These live-work residential units on Park Drive are oriented toward Independence Park, with good pedestrian access to the street.



**1D** A recent duplex residence on Vail Avenue is a good example of how infill development can blend in with the surrounding neighborhood.



**1E** The newer house on the right is exemplary of residential design that complements the character of surrounding structures.



## *Elizabeth Area Plan*

### RESIDENTIAL DESIGN POLICIES <sup>1</sup>

#### D-2. Natural Environment

Policy Number	Single Family Detached	Single Family Attached and Multifamily	<i>These policies provide direction to accommodate growth and change while protecting the natural environment on which we depend, including the quality of our air, water and land.</i>
2A	+	+	Provide a meaningful amount of usable and accessible open space. In single family development, this should be common open space.
2B	+	+	Incorporate functional, unique, natural, and/or historical elements into the open space.
2C	+	+	Preserve steep slopes along streams or adjacent to significant natural landscape features.
2D	+	+	Preserve at least 15% of the site as "tree save area" consistent with residential tree ordinance.
2E	+	+	<b>Design new development and redevelopment to minimize adverse impacts to the tree canopy.</b>
2F	+	+	<b>Encourage 'green' site design and building practices especially to reduce storm water runoff and minimize adverse environmental impacts.</b>

<sup>1</sup> Adopted by City Council, the *General Development Policies* (GDP) provide guidance for the location, intensity and form of future development and redevelopment throughout the community. Many of the residential and mixed/multi-use center design policies for the plan area are based upon these adopted GDP policies. **Additional, plan-specific policies are shown in bold font.** Plan-specific policies augment the GDP design guidelines.



**2A** A generous amount of common open space has been provided for residents of this North Dotger Avenue development.



**2E** Several mature trees were saved and incorporated into the site plan when these residential condominium units were constructed in 2004.



**3D** Mid-block trolley walk connects residential areas with public transit stops on East Seventh Street.



## *Elizabeth Area Plan*

RESIDENTIAL DESIGN POLICIES <sup>1</sup>			
D-3. Pedestrian and Vehicular Networks			
Policy Number	Single Family Detached	Single Family attached and Multifamily	<i>Providing a safe, accessible and comfortable network of streets, sidewalks and pathways for all modes of transportation is an important step in establishing a livable community. The policies below provide guidance for achieving mobility and connectivity for pedestrians, bicyclists, and automobile drivers. Refer also to the City's <b>Urban Street Design Guidelines</b> (2007) and the Transportation section of this area plan.</i>
3A		+	Provide pedestrian amenities, such as street furniture and pedestrian scale lighting.
3B	+	+	Provide bicycle parking in appropriate common areas (for example, near playgrounds, parks, and swimming pools).
3C	+	+	Provide pedestrian/bicycle connections to parks, greenways, bikeways and trails.
3D	+	+	Provide direct pedestrian and bicycle connections between abutting or adjacent developments and transit stops.
3E	+	+	Design streets with pedestrian safety and comfort in mind.
3F	+	+	Encourage shared alleys and other forms of access.

3G	+	+	Design street system to calm traffic and enhance pedestrian and bicycle activity.
3H	+	+	Align collectors at thoroughfare intersections to promote safe crossings for pedestrians, cyclists, and automobile drivers.
3I		+	Provide on-street parking to reduce the size of surface parking lots.
3J	+	+	Provide controlled vehicular entry points through the use of medians and fewer and more strategically located curb cuts.
3K		+	Design an internal street system with spine road, including: <ul style="list-style-type: none"> <li>• Parallel parking, street trees, and sidewalks.</li> <li>• Driveways, secondary streets, and/or pedestrian paths to connect parking lots and primary street.</li> <li>• Sidewalks on secondary streets.</li> </ul>

<sup>1</sup> Adopted by City Council, the *General Development Policies* (GDP) provide guidance for the location, intensity and form of future development and redevelopment throughout the community. Many of the residential and mixed/multi-use center design policies for the plan area are based upon these adopted GDP policies. **Additional, plan-specific policies are shown in bold font.** Plan-specific policies augment the GDP design guidelines.



**3F** Public alleys like this one between Louise Avenue and Hawthorne Lane provide vehicle access and off-street parking for multiple properties.



**3I** On-street parking can reduce the amount of surface parking required for single family attached and multifamily development.

## Non-Residential Design Policies

Non-residential buildings should be contextual to their surroundings yet exhibit their own unique design qualities and features. What is appropriate for a mixed-use building on Elizabeth Avenue, for example, may not be desirable for East Seventh Street, in terms of height, massing, architectural style, building materials, and site plan. It is important to recognize the history, character, and development pattern of the neighborhood when making design decisions for non-residential buildings.



**4A** The large multiple panes, arched windows, and colorful awnings on this East Eighth Street commercial building provide pedestrian-scale interest to this facade.

NON-RESIDENTIAL DESIGN POLICIES <sup>1</sup>						
D-4. Building Architecture and Site Design						
Policy Number	Retail-Oriented Mixed/Multi Use	Retail	Office	Mixed Use	Institutional	<i>Architecture and landscape design define streets and public spaces as areas of shared use. Streets lined by buildings, well-lit open spaces and common areas, and trees rather than vast expanses of parking lots provide a visually rich and physically safe environment.</i>
4A	+	+	+	+	+	Design buildings with transparent openings, ornamentation, and architectural character.
4B	+	+	+	+	+	Establish entrances with pedestrian interest.
4C	+	+	+	+	+	Orient buildings towards street and provide pedestrian access.
4D	+	+	+	+		Arrange buildings in an orderly block pattern.
4E	+	+	+	+	+	<i>Discourage tearing down historic or architecturally significant structures and, instead, <b>reusing them.</b></i>
4F	+	+	+	+	+	Design buildings with human scale and visual interest in mind.
4G	+	+	+	+		Minimize impacts of drive-thru development.
4H	+	+	+	+	+	Design for pedestrian safety.



**4B** A life-sized 3-dimensional figure adds a bit of whimsy and interest to this hair salon's entrance.



**4E** The historic Palmer Fire School enjoys new life as a banquet and meeting facility.



## Elizabeth Area Plan

4I	+	+	+	+	+	Locate dumpsters and service areas away from residential areas.
4J	+	+	+	+		<b>Ensure the scale and setback of buildings are compatible with adjacent structures.</b>
4K	+	+	+	+	+	<b>Encourage architectural design that complements the style, character, and materials of surrounding structures that contribute to the neighborhood character.</b>
4L	+	+	+	+	+	<b>Integrate significant functional open space into the design of new development.</b>
4M	+	+	+	+	+	<b>Locate parking to the rear of buildings whenever feasible, with alley access if available.</b>
4N	+	+	+	+	+	<b>Buildings greater than 40' in height immediately adjacent to single-family residential areas should "step down" to 40' or less along shared boundaries.</b>
4O	+	+	+	+	+	<b>Place utilities underground wherever possible. Screen above-ground utilities with landscaping or architectural features or walls.</b>
4P	+	+	+	+	+	<b>Encourage the use of publicly-owned alleys for vehicular access to garages and interior parking areas, and/or to accommodate overhead utilities.</b>

<sup>1</sup> Adopted by City Council, the *General Development Policies* (GDP) provide guidance for the location, intensity and form of future development and redevelopment throughout the community. Many of the residential and mixed/multi-use center design policies for the plan area are based upon these adopted GDP policies. **Additional, plan-specific policies are shown in bold font.** Plan-specific policies augment the GDP design guidelines.



**4K** Built in 2005, these office buildings on Firefighter Place complement the scale and character of the adjacent historic Palmer Fire School and training tower.



**4L** Functional open space provides a quiet location for reading, taking a break from work, or eating lunch at this Elizabeth medical office complex.



**4O** A masonry wall incorporated into the building architecture screens these above ground utility structures from view.



## NON-RESIDENTIAL DESIGN POLICIES<sup>1</sup>

### D-5. Natural Environment

Policy Number	Retail-Oriented Mixed/Multi Use	Retail	Office	Mixed Use	Institutional	Site development should respect the natural environment and historical context and landscape of the area.
5A	+	+	+	+	+	Use a bridge rather than a culvert at existing creeks, where possible. Avoid piping creeks and minimize channelization.
5B	+	+	+	+	+	Preserve steep slopes along stream beds or adjacent to significant natural landscape features.
5C	+	+	+	+	+	Consider pervious pavement for overflow parking.
5D	+	+	+	+	+	Retain existing landscaping, including the tree canopy, where possible.
5E	+	+	+	+	+	Design new development and redevelopment to minimize adverse impacts to the tree canopy.
<b>5F</b>	+	+	+	+	+	<b>Encourage 'green' site design and building practices, especially to reduce storm water runoff and provide other environmental benefits.</b>

<sup>1</sup> Adopted by City Council, the *General Development Policies* (GDP) provide guidance for the location, intensity and form of future development and redevelopment throughout the community. Many of the residential and mixed/multi-use center design policies for the plan area are based upon these adopted GDP policies. **Additional, plan-specific policies are shown in bold font.** Plan-specific policies augment the GDP design guidelines.



**5A** Crossing a creek or stream with a bridge, such as this one over Little Sugar Creek, is a better option than a culvert.



**5C** Six inches of pervious concrete pavement allows surface water to drain through this Wilmore parking lot.

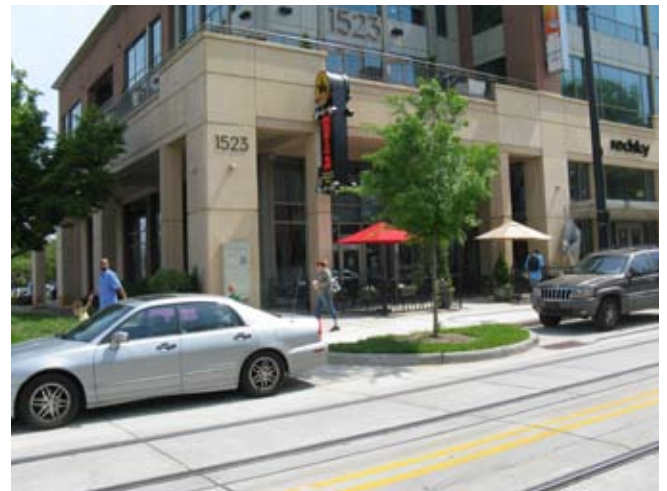


**5F** Uptown Charlotte's Federal Reserve Bank has a retrofitted green roof that reduces storm water run-off and saves energy.

NON-RESIDENTIAL DESIGN POLICIES <sup>1</sup>						Circulation in and through the site should be comfortable, safe, secure and efficient for pedestrians, bicyclists and automobile drivers.
Policy Number	Retail-Oriented Mixed/Multi Use	Retail	Office	Mixed Use	Institutional	
6A	★	★	★	★	★	Create an interconnected sidewalk system.
6B	★	★	★	★	★	Design and preserve short blocks with an organized street pattern.
6C	★	★	★	★	★	Provide pedestrian/ bicycle connections to nearby parks, greenways, bikeways and trails.
6D	★	★	★	★	★	Design streets with pedestrian safety and comfort in mind.
6E	★	★	★	★	★	Provide ample sidewalk width to accommodate pedestrian circulation, congregation, outdoor dining, and amenities such as street furnishings, trash receptacles, art work, and trees.
6F	★	★	★	★	★	Establish clear “way-finding” signage for pedestrians and automobile drivers.
6G	★	★	★	★	★	Provide safe pedestrian circulation throughout the development, including through parking lots and decks, by incorporating traffic calming measures such as stop signs, speed humps, and speed limit signs.
6H	★	★	★	★	★	Provide bicycle parking and storage areas.
6I	★	★	★	★	★	Provide safe and secure transit waiting facilities.
6J	★	★	★	★	★	Establish a central vehicular access into the site from the more auto-oriented street and provide secondary access options from the minor streets.
6K	★	★	★	★	★	Design parking lots on a street/block pattern to minimize large surface lots.



**6C** A cyclist enjoys this pedestrian/bicycle path along North Kings Drive, part of the Little Sugar Creek Greenway system.



**6D** Pedestrian safety and comfort are enhanced on Elizabeth Avenue by the ample planting strip, street trees, and recessed on-street parking.



**6E** Two lively Parisian streetscapes provide for enhanced pedestrian amenities, including outdoor dining.



## NON-RESIDENTIAL DESIGN POLICIES<sup>1</sup>

### D-6. Pedestrian and Vehicular Networks

Policy Number	Retail-Oriented Mixed/Multi Use	Retail	Office	Mixed Use	Institutional	
						<i>Please see previous page for beginning of Pedestrian and Vehicular Networks.</i>
6L	★	★	★	★	★	Minimize large surface parking lots with smaller pods and extensive landscaping.
6M	★	★	★	★		Encourage commercial or residential uses in parking decks fronting pedestrian circulation areas.
6N	★	★	★	★		Design access from surrounding neighborhood so that the appearance is residential in character.
6O	★	★	★	★	★	Provide structured parking where feasible to conserve land and minimize surface parking.
6P	★	★	★	★	★	Integrate landscaping with seating along facades when possible and, when practical, work to integrate the existing tree canopy into the site design.
6Q	★	★	★	★		Encourage shared driveways and alleys within the development.
<b>6R</b>	★	★	★	★		<b>Encourage shared parking among different uses where feasible to minimize the amount of parking space needed.</b>

<sup>1</sup> Adopted by City Council, the *General Development Policies* (GDP) provide guidance for the location, intensity and form of future development and redevelopment throughout the community. Many of the residential and mixed/multi-use center design policies for the plan area are based upon these adopted GDP policies. **Additional, plan-specific policies are shown in bold font.** Plan-specific policies augment the GDP design guidelines.



**6H** One on-street parking space has been replaced with bicycle parking on this retail block in Ann Arbor.



**6L** Trees and landscaped islands break up this surface parking lot into smaller areas while providing shaded parking on hot sunny days.



**6P** Landscaping, trees, and low walls provide a pleasant place to sit along the façade of this Elizabeth institutional building.



## NON-RESIDENTIAL DESIGN POLICIES<sup>1</sup>

### D-7. Freestanding Interconnected Buildings

Policy Number	Retail-Oriented Mixed/Multi Use	Retail	Office	Mixed Use	Institutional	Freestanding single tenant buildings can be classified as either independent or interconnected. Independent buildings function separate of the surrounding complex while interconnected buildings share parking and site amenities with other buildings in the same complex. Typically interconnected buildings are preferred. The design of any freestanding buildings should be sensitive to the surrounding neighborhood, both built and natural environments.
7A	+	+	+	+	+	Design buildings so that they relate to the overall scale, height and configuration of the center. If drive-through windows and services are included they must not compromise pedestrian circulation.
7B	+	+	+	+	+	Design to encourage (and facilitate) pedestrians to walk to the freestanding building from other buildings within the center. The connections should be directly accessible without creating conflicts with automobiles by providing safe pedestrian pathways and crossings.
7C	+	+	+	+	+	Design the site so that dumpsters, service areas or auxiliary storage do not interfere with, and are not visible from, the pedestrian circulation area and do not negatively impact surrounding residential areas.
7D	+	+	+	+	+	Cluster site layout in a village arrangement around shared amenities.
7E	+	+	+	+	+	<b>Design buildings to address the primary street with minimized setback wherever possible.</b>
7F	+	+	+	+	+	<b>Encourage 'green' site design and building practices, especially to reduce storm water runoff and provide other environmental benefits.</b>

<sup>1</sup> Adopted by City Council, the *General Development Policies* (GDP) provide guidance for the location, intensity and form of future development and redevelopment throughout the community. Many of the residential and mixed/multi-use center design policies for the plan area are based upon these adopted GDP policies. **Additional, plan-specific policies are shown in bold font.** Plan-specific policies augment the GDP design guidelines.

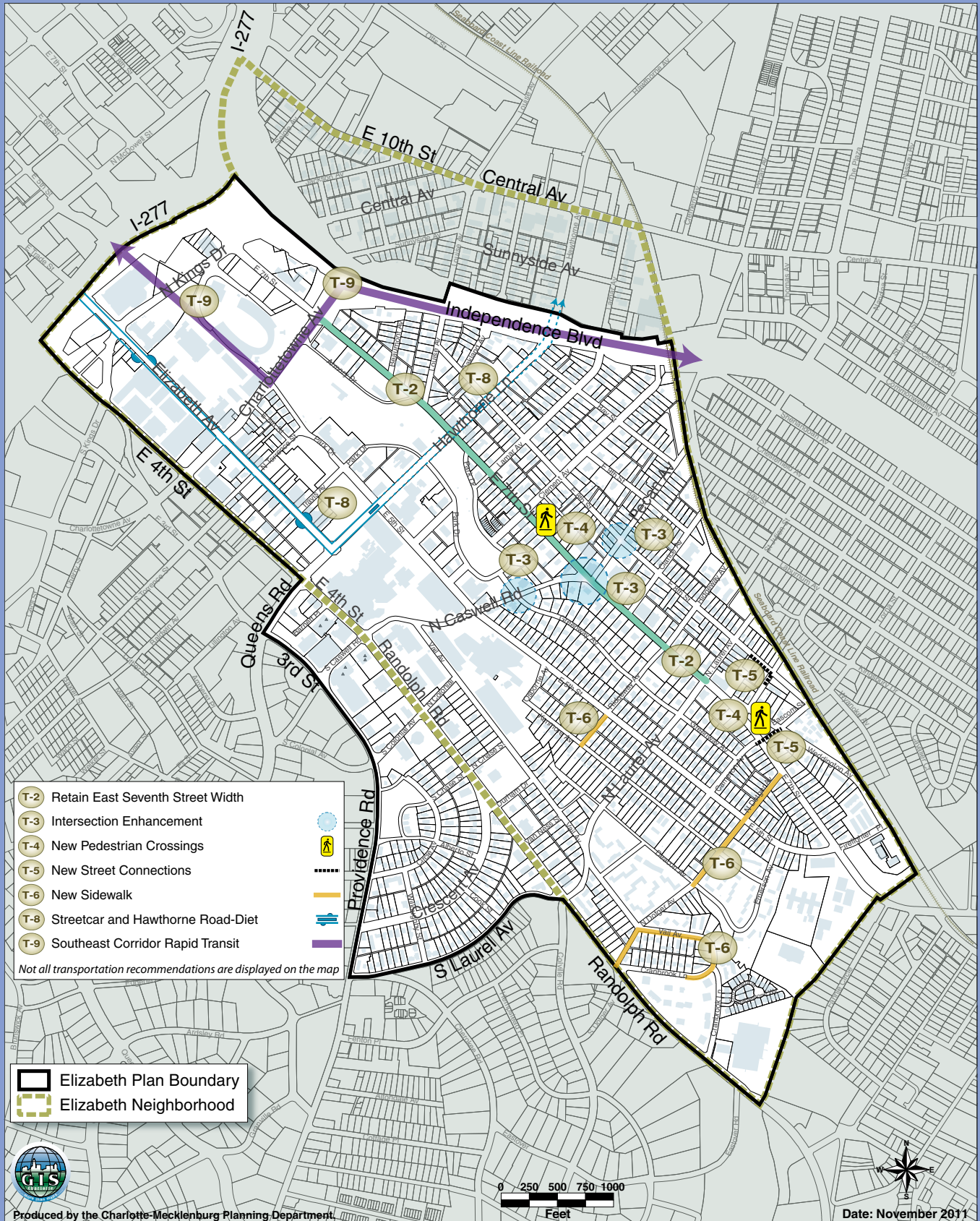


**7B** The freestanding buildings in this office complex are interconnected with a series of shaded walkways, allowing pedestrian travel between them without having to cross roads or parking areas.



**7D** These interconnected buildings are clustered in a village arrangement around shared amenities.

# Elizabeth Area Plan Future Transportation Improvements





## Transportation

Streets are more than just pathways through an area. They connect people to places. As the public face of a neighborhood, streets can set the tone for the surrounding environment and create a sense of community. Elizabeth is characterized by many great streets. They should continue to reflect and support a pedestrian-scaled environment that encourages walking while also supporting other modes of transportation and adjacent land uses.

***Goal** Support and develop a connected, affordable, balanced transportation system that encourages walking. It should safely and sensibly accommodate pedestrians, bicyclists, and transit users while allowing for neighborhood-friendly automobile speeds for motorists and business patrons traveling to and through Elizabeth.*

The purpose of this section is to provide policy guidance that will both protect the Established Neighborhood Subarea and enhance commercial districts within Elizabeth. The Elizabeth plan area has good land use accessibility and connectivity. This means area residents of all ages are able to live, work, play, socialize, and shop within walking distance of their homes. In addition, a well-connected street network means Elizabeth residents experience minimal levels of congestion and shorter travel distances.



*Sidewalk dining contributes to a lively neighborhood "downtown" environment.*

East Seventh Street is Elizabeth's neighborhood-scaled commercial area and community meeting place. It is envisioned as a tree-lined avenue with wide sidewalks, where people can comfortably walk, relax at a sidewalk café, or stroll between neighborhood businesses. To help achieve this vision, several transportation and land use policies are focused on East Seventh Street in order to improve its pedestrian environment.

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### Transportation Policies

This section outlines transportation policies, addressing both proposed new streets and enhancements to existing streets, to make them more pedestrian and bicycle friendly. These improvements will likely be accomplished as new development occurs. The general locations of the improvements are noted on **Map 5, Future Transportation Improvements** on page 46.

#### **T-1 Promote the use of Independence Boulevard as a preferred alternative route to East Seventh Street for motor vehicles.**

Independence Boulevard should carry most of the motor vehicle trips originating and ending outside the plan area. East Seventh Street should carry mostly local traffic and be more accommodating to pedestrians. As Independence Boulevard is fully converted to an expressway, motorists will be able to take advantage of the greater available capacity and reliable travel times of Independence Boulevard, helping maintain East Seventh Street for local travel.

#### **T-2 Retain the existing street width for travel lanes on East Seventh Street.**

This plan does not recommend any widening of East Seventh Street's travel lanes. It supports a future cross-section that holds the existing curb lines for travel lanes. During the area plan process, there was extensive community discussion about a "road-diet" concept that would convert the center reversible lane to a formal two-way left-turn lane with pedestrian refuge islands. Several scenarios were studied, trying to find a variation that included pedestrian islands, yet balanced corridor travel needs. All of these scenarios increased traffic congestion during peak periods and could not be supported. (see Appendix, page 110, for summary of East Seventh Street Conversion Analysis)

**T-3 Enhance the intersection of East Seventh Street at Pecan Avenue and North Caswell Road for pedestrians.**

Intersection improvements may include a combination of crosswalks on all legs of the intersection, geometric design changes, pedestrian countdown signals, pedestrian leading clearance interval signals, and curb ramps.

**T-4 Provide additional pedestrian crossings of East Seventh Street and intersection calming on Pecan Avenue and North Caswell Road.**

Plan for additional street crossings and potential traffic calming measures on three key streets. Mid-block crossings would help pedestrians safely cross East Seventh Street without having to walk to street corner intersections. Traffic calming at non-signalized intersections at Pecan Avenue and East Eighth Street and North Caswell Road and Greenway Avenue should be considered to make street crossing easier and safer.

**T-5 Provide street connections to support new development.**

Street connections should be provided as development occurs to support vehicular and pedestrian connectivity, as shown on **Map 5, Future Transportation Improvements**, page 46:

- Extension of Bascom Street from Weddington Avenue to East Seventh Street opposite Cameron Avenue.
- Extension of East Eighth Street to Ranier Avenue and/or Bascom Street (the second block between Ranier and Bascom Avenues is dependent upon CSX Railroad approval).

**T-6 Eliminate gaps in the sidewalk system within existing residential areas.**

A few streets in Elizabeth do not have sidewalks because they were built prior to adopted standards that required them. The City's Sidewalk Program can help to address some of these gaps by prioritizing and implementing continuous and direct routes that link residential areas to parks, schools, and shopping. Identified sidewalks on **Map 5** may require support through a petition-based process prior to implementation.

**T-7 Provide alternative bicycle accommodations along or parallel to all thoroughfares.**

Existing street widths make it difficult to retrofit bicycle lanes along most of the thoroughfares within the plan area. An exception is Hawthorne Lane, which now has bicycle lanes northeast of East Eighth Street.

Additional street conversions, or "road-diets", may be implemented southwest of East Eighth Street as the Center City Streetcar project is built. Elsewhere, strategies should include signing parallel designated bicycle routes (such as East Fifth Street parallel to East Seventh Street), or possibly adding shared lane markings, sometimes called "sharrows", to outside travel lanes of thoroughfares.



*The intersection of East Seventh Street, North Caswell Road, and Pecan Avenue should be enhanced for pedestrians (Policy T-3).*



**T-8 Recognize the future Center City Streetcar and coordinate planning around stops in the plan area.**

Future street cross-sections, development standards, and pedestrian crossings of Hawthorne Lane should be coordinated with the planned alignment of the streetcar. The initial phase of the streetcar line will be constructed between the Charlotte Transportation Center and Presbyterian Hospital on Hawthorne Lane. A later phase of the streetcar is planned to continue on Hawthorne Lane past East Fifth Street to cross Independence Boulevard and connect with Central Avenue. As part of that extension, a median stop is planned on Hawthorne Lane at East Eighth Street.

**T-9 Coordinate planning of Southeast Rapid Transit Corridor.**

Plan future development and street improvements with the extension of East Fifth Street from McDowell Street to Charlottetowne Avenue along the southwestern edge of Memorial Stadium to facilitate the Locally Preferred Alternative of the Southeast Rapid Transit Corridor. Options to retain future space for this Rapid Transit Corridor within the existing right-of-way along the northwestern edge of Charlottetowne Avenue between Independence Boulevard and Central Piedmont Community College should also be pursued.

## Street Cross-Sections

The streetscape cross-sections and development standards on the following pages will help shape the character of the future street network. Future cross-sections have been determined for plan area streets except for streets where few changes to existing conditions are anticipated, primarily in residential neighborhoods. These cross-sections have been developed in accordance with the *Urban Street Design Guidelines (USDG)*, adopted by City Council in 2007. **Map 6, Transportation Cross-Sections Locations**, page 50, indicates where each of the cross-section types are located, referenced by number.

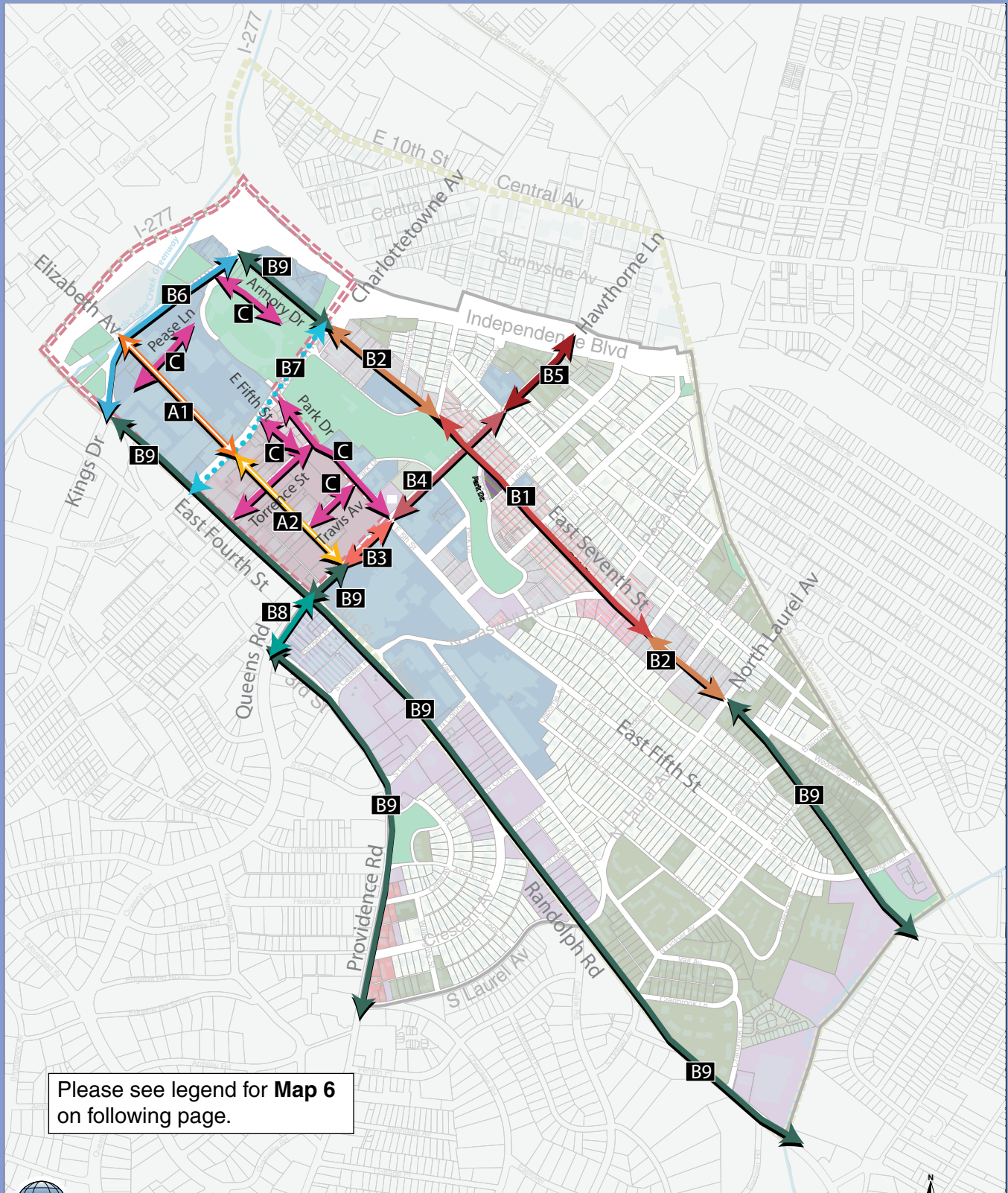
The streetscape cross-sections specifically define the character and width of the area behind the curbs, including accommodations for sidewalks, landscaping, and pedestrian amenities as well as building setback guidelines. The cross-sections also illustrate the future character of the street inside the curbs, establishing the location and width of travel lanes, bicycle lanes, transit, and provisions for on-street parking.

The dimensions on the streetscape cross-section diagrams reflect typical street sections at mid-block locations. They will be used by the Charlotte Department of Transportation (CDOT), in consultation with the Charlotte-Mecklenburg Planning Department, to determine the location of the ultimate curb lines. If variations from the typical street sections are needed to address physical conditions, enhance operations, or better meet the intent of this area plan, such variations may be identified during future design and analysis phases.

When this plan is adopted, the streetscape standards specified herein will become the official Streetscape Plan for the plan area, as referenced in the *Charlotte Zoning Ordinance*. As such, future development zoned MUDD, NS, UR, TOD, TS, PED, UMUD, or other urban zoning districts that may be established, must be designed in accordance with these standards. Future development not zoned to one of these urban districts is not required to construct the new street cross-section elements.

There are three street types (Main Street, Avenue, and Local Street) recommended for the plan area, corresponding to the street types identified in the *USDG*.

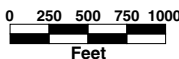
# Elizabeth Area Plan Transportation Cross-Sections Locations



Please see legend for **Map 6**  
on following page.



Produced by the Charlotte-Mecklenburg Planning Department.



Date: November 2011



## Legend for Transportation Cross-Sections Locations

## Map 6

Please see **Map 6** on preceding page.

===== Preliminary Future Transit Station Subarea

### Main Streets



Elizabeth Avenue (Kings Drive to Charlottetowne Avenue)



Elizabeth Avenue (Charlottetowne Avenue to Hawthorne Lane)

### Avenues



East Seventh Street (Louise Avenue to Clarice Avenue)



East Seventh Street (Charlottetowne Avenue to Louise Avenue and Clarice Avenue to Laurel Avenue)



Hawthorne Lane (Elizabeth Avenue to East Fifth Street)



Hawthorne Lane (East Fifth Street to East Eighth Street)



Hawthorne Lane (East Eighth Street to Independence Boulevard)



Greenway Avenue - Kings Drive



Crosstown Avenue - Charlottetowne Avenue



Median Avenue - Queens Road



Radial Avenues - East Third Street, East Fourth Street, Randolph Road, Providence Road, East Seventh Street (Kings Drive to Charlottetowne Avenue and North Laurel Avenue to Briar Creek) and Hawthorne Lane (East Fourth Street to Elizabeth Avenue)

### Local Streets



Preliminary Future Transit Station Subarea Local Streets



*Whether a quiet Local Street or a vibrant Main Street or Avenue, all streets include street trees and pedestrian amenities.*

## Main Streets

Main Streets are destination locations that provide access to and function as centers of civic, social, and commercial activity. Main Streets are designed to be pedestrian-oriented to complement the development next to the street. Although they also serve transit, bicyclists, and automobiles, Main Streets are designed to provide the highest level of comfort, access, and security to pedestrians. The Main Street in the plan area, with two separate recommended cross-sections according to location, is Elizabeth Avenue.

↔ **A1** ↔ **Elizabeth Avenue** (Kings Drive to Charlottetowne Avenue)

↔ **A2** ↔ **Elizabeth Avenue** (Charlottetowne Avenue to Hawthorne Lane)

### Elizabeth Avenue

Elizabeth Avenue was reconstructed in 2007-2009 as part of a major City capital improvement project. The cross-sections shown in this plan largely reflect that project's resulting streetscape. Two sets of rails were installed in the street during the road reconstruction, from Kings Drive to Hawthorne Lane, in anticipation of a planned streetcar line.

Cross-Section **A1** corresponds to the stretch from Kings Drive to Charlottetowne Avenue, where bicycle lanes are incorporated. The Institutional land use prevalent on this block (CPCC) is supported with a wide sidewalk to accommodate intense pedestrian activity. Future development along this section will afford an opportunity to request enhanced back-of-curb streetscape improvements consistent with the **A1** cross-section.

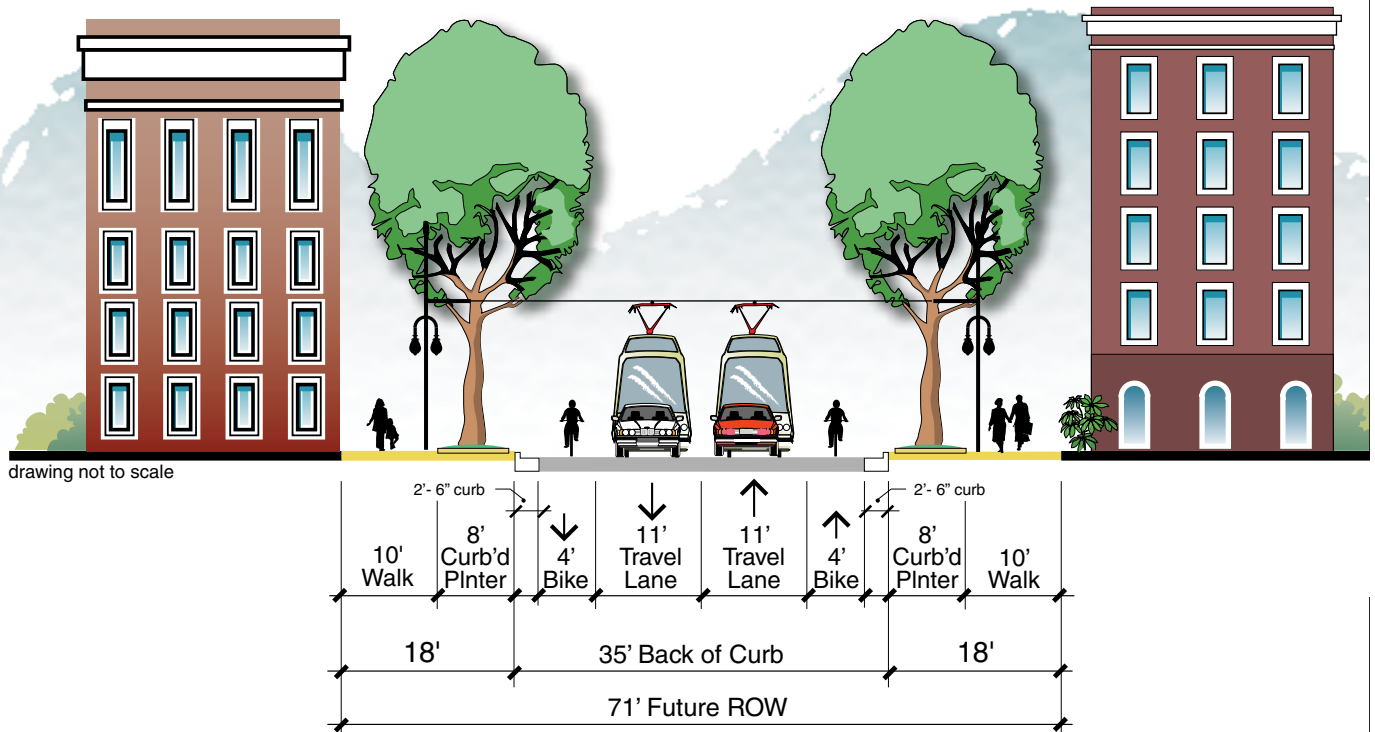
Cross-Section **A2** corresponds to the stretch from Charlottetowne Avenue to Hawthorne Lane. The wide sidewalk and recessed on-street parking on this block support a mix of land uses, with street-level retail or office likely to be predominant. The back-of-curb streetscape was not fully constructed as part of the recent City project in places where the adjoining property was vacant or underutilized. The **A2** cross-section will guide back-of-curb streetscape improvements abutting future development, either through rezoning or through the urban plan review process.



*Elizabeth Avenue with streetcar tracks.*



**A1 Elizabeth Avenue**  
(Kings Drive to Charlottetowne Avenue)



**A1 Future Development Standards**

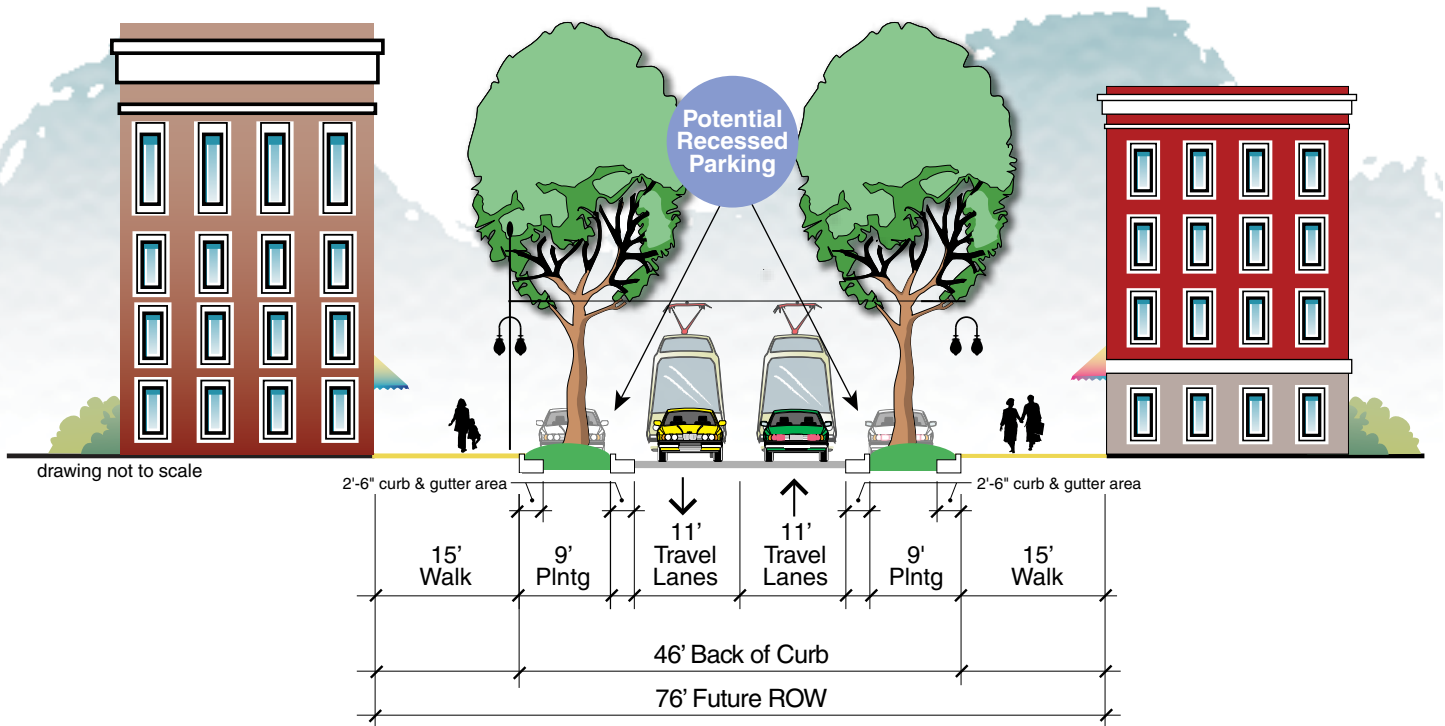
<b>Roadway and On-Street Parking</b>	<ul style="list-style-type: none"> <li>Two 11' travel lanes shared by streetcar and other vehicles</li> <li>2'6" curb and gutter</li> <li>4' bicycle lane on each side between the curb and the travel lane</li> <li>No on-street parking</li> <li>Curb lines will remain at existing locations.</li> </ul>
<b>Planting Strip</b>	<ul style="list-style-type: none"> <li>No continuous planting strip</li> <li>Street trees required per <i>Charlotte Tree Ordinance</i> in curbed planters</li> </ul>
<b>Sidewalk</b>	<ul style="list-style-type: none"> <li>18' sidewalk comprising 8' amenity zone behind curb for street trees, benches, utility structures, public art, and other amenities, and 10' clear walk area</li> </ul>
<b>Recommended Building Setbacks</b>	<ul style="list-style-type: none"> <li>Buildings at back edge of sidewalk, or as near to back of sidewalk as zoning permits</li> </ul>



Street tree in a curbed planter. Ornamental grasses or seasonal flowers can be used as underplantings for added visual interest.

Source: Charlotte Department of Transportation

## A2 Elizabeth Avenue (Charlottetowne Avenue to Hawthorne Lane)



### A2 Future Development Standards

<b>Roadway and On-Street Parking</b>	<ul style="list-style-type: none"> <li>Two 11' travel lanes shared by streetcar and other vehicles</li> <li>2'-6" curb and gutter</li> <li>No bicycle lanes</li> <li>Recessed parking may be installed in place of the planting strip</li> <li>Must be broken up with vegetated bump-outs with street trees at minimum 100' intervals</li> <li>Curb lines will remain at existing locations.</li> </ul>
<b>Planting Strip</b>	<ul style="list-style-type: none"> <li>9' planting strip</li> <li>Street trees required per <i>Charlotte Tree Ordinance</i></li> </ul>
<b>Sidewalk</b>	<ul style="list-style-type: none"> <li>15' sidewalk</li> <li>Sidewalk may accommodate outdoor dining, pedestrian amenities, decorative lighting, public art</li> <li>Optional street trees in curbed planters near street-side edge of sidewalk</li> </ul>
<b>Recommended Building Setbacks</b>	<ul style="list-style-type: none"> <li>Buildings at back edge of sidewalk, or as near to back of sidewalk as zoning permits</li> </ul>



Elizabeth Avenue between Hawthorne Lane and Charlottetowne Avenue.

Source: Charlotte Department of Transportation



## Avenues

Avenues can serve a diverse set of functions in a wide variety of land use contexts. They are the most common non-local street type in the plan area, as well as in Charlotte. Avenues provide access from neighborhoods to commercial areas, between areas of the city, and in some cases, through neighborhoods. They are designed to provide a balance of service for all users, but with special emphasis on pedestrians and localized transit services. Avenues may also have options for on-street parking and dedicated bicycle lanes. Avenues in the plan area, some with differing recommended cross-sections according to location, include:

- ← B1 → **East Seventh Street** (Louise Avenue to Clarice Avenue)
- ← B2 → **East Seventh Street** (Charlottetowne to Louise and Clarice to Laurel)
- ← B3 → **Hawthorne Lane** (Elizabeth Avenue to East Fifth Street)
- ← B4 → **Hawthorne Lane** (East Fifth Street to East Eighth Street)
- ← B5 → **Hawthorne Lane** (East Eighth Street to Independence Boulevard)
- ← B6 → **Charlottetowne Avenue**
- ← B7 → **Kings Drive**
- ← B8 → **Queens Road**
- ← B9 → **East Third Street; East Fourth Street; Hawthorne Lane** (southwest of Elizabeth Avenue); **Providence Road; Randolph Road; East Seventh Street** (northwest of Charlottetowne Avenue); and, **East Seventh Street** (southeast of Laurel Avenue)

### East Seventh Street

The two recommended streetscape cross-sections for East Seventh Street are designed to help realize the plan vision for this street, as a “tree-lined avenue with wide sidewalks, where people can comfortably walk, relax at a sidewalk café, or stroll between neighborhood businesses”.

Both cross-sections begin with an important premise, namely that *the existing street width will not increase to accommodate additional travel lanes*. The recommended cross-sections are intended to help transform East Seventh Street into a better environment for walking by providing for wide planting strips



*Existing condition: Surface parking between the sidewalk and the commercial building impedes pedestrian access to shops and restaurants.*



*Future Condition: A wide sidewalk enhances retail shopping and allows room for sidewalk cafes and pedestrian amenities. Shoppers and strollers are buffered from the street traffic by a planting strip or recessed on-street parking.*

## Elizabeth Area Plan

with street trees and/or recessed on-street parking, a buffer between the roadway and the sidewalk for a greater sense of safety and comfort, and wide sidewalks to allow pedestrians ample room to walk. The recommended streetscape also has room for sidewalk cafes and dining, retail activities, and amenities such as benches, decorative lighting, street trees, planters, and public art. The cross-sections will also allow buildings to be placed closer to the sidewalk, rather than set back away from the street where they are often separated from pedestrians on the sidewalk by parking lots. This is especially important in areas where street-level retail or restaurant uses are preferred.

The existing right-of-way width on East Seventh Street inside the plan area is generally 60', but this width varies along the corridor due to various factors. Both recommended cross-sections for this street will require a right-of-way width of 80' to accommodate the various elements of the wider back-of-curb streetscape. Bringing the East Seventh Street streetscape into conformity with the recommended cross-sections will happen gradually, as parcels are developed or significantly altered. Existing buildings, most of which are farther back from the curb than these cross-sections recommend, need not be modified or relocated. However, as development occurs, the elements of the proscribed streetscape should be constructed. Over time, the streetscape will begin to take on the envisioned character and scale. The PED overlay zoning district is a tool that could be considered for portions of East Seventh Street in the future to implement several land use recommendations and to promote a pedestrian-oriented setting with high quality design, which complements the adjacent neighborhood. The City will not initiate the process of creating a PED overlay district unless requested to do so by a recognized neighborhood organization.

Along East Seventh Street from Louise Avenue to Clarice Avenue (**B1**), the land use recommendation is generally for mixed use residential/office/retail, with retail or office uses preferred on the ground level. This is the Elizabeth's neighborhood commercial core, where residents can gather to shop, dine, stroll, or get a haircut. The recommended street cross-sections, therefore, provides for ample sidewalk width here to allow for these activities.

On the outer sections of East Seventh Street, from Charlottetowne to Louise Avenue and from Clarice to North Laurel Avenue (**B2**), the land use recommendation is generally for a mix of office and residential uses, with few or no ground level retail uses. The recommended streetscape cross-section for these areas, therefore, is different than in the commercial core between Louise and Clarice Avenues.



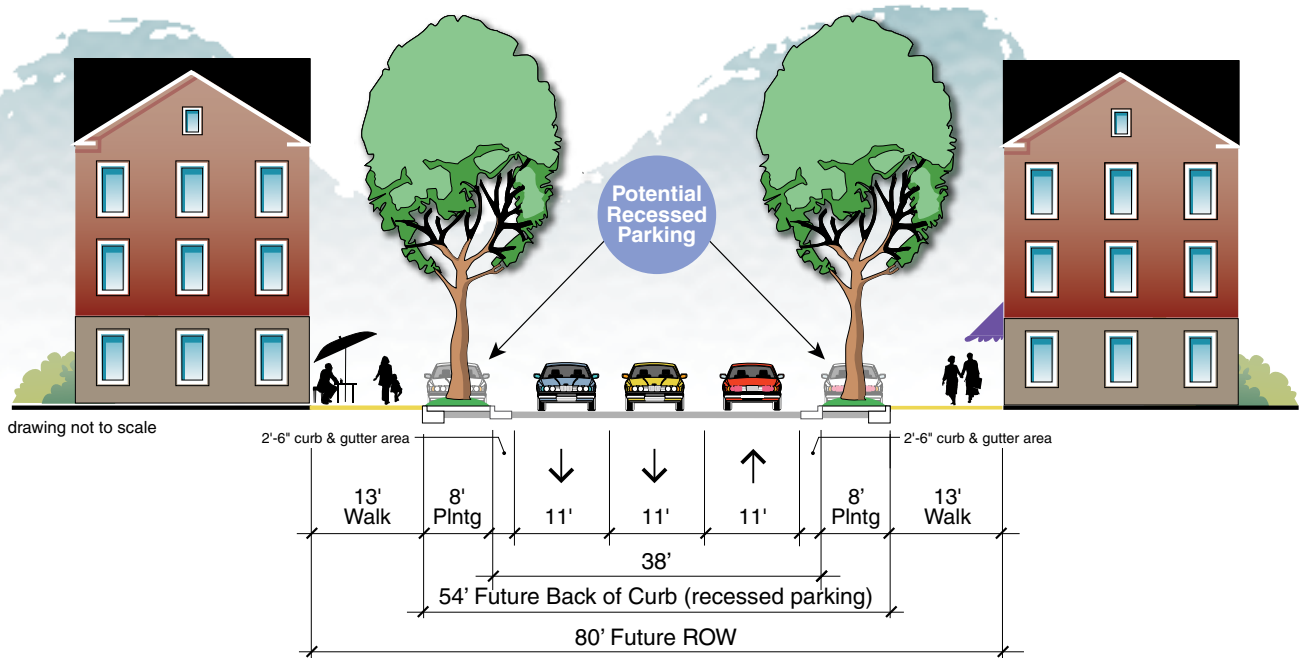
*In areas where ground floor residential uses are prevalent, a 13' wide planting strip and 8' sidewalk are recommended. A secondary row of street trees can supplement the street's existing mature trees. Landscaped front yards may separate the buildings from the back of the sidewalk.*



*This residential development on Weddington Avenue, a good example of cross-section **B2**, features a wide planting strip with trees. This area also has recessed on-street parking. Landscaped front yards separate the dwelling units from the public sidewalk, connected with individual walkways.*

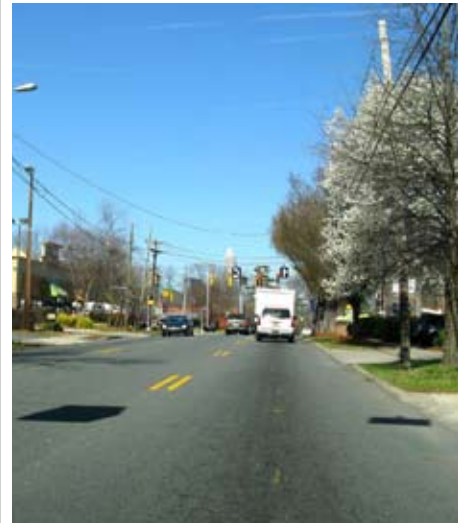


**B1 East Seventh Street**  
(Louise Avenue to Clarice Avenue)



**B1 Future Development Standards**

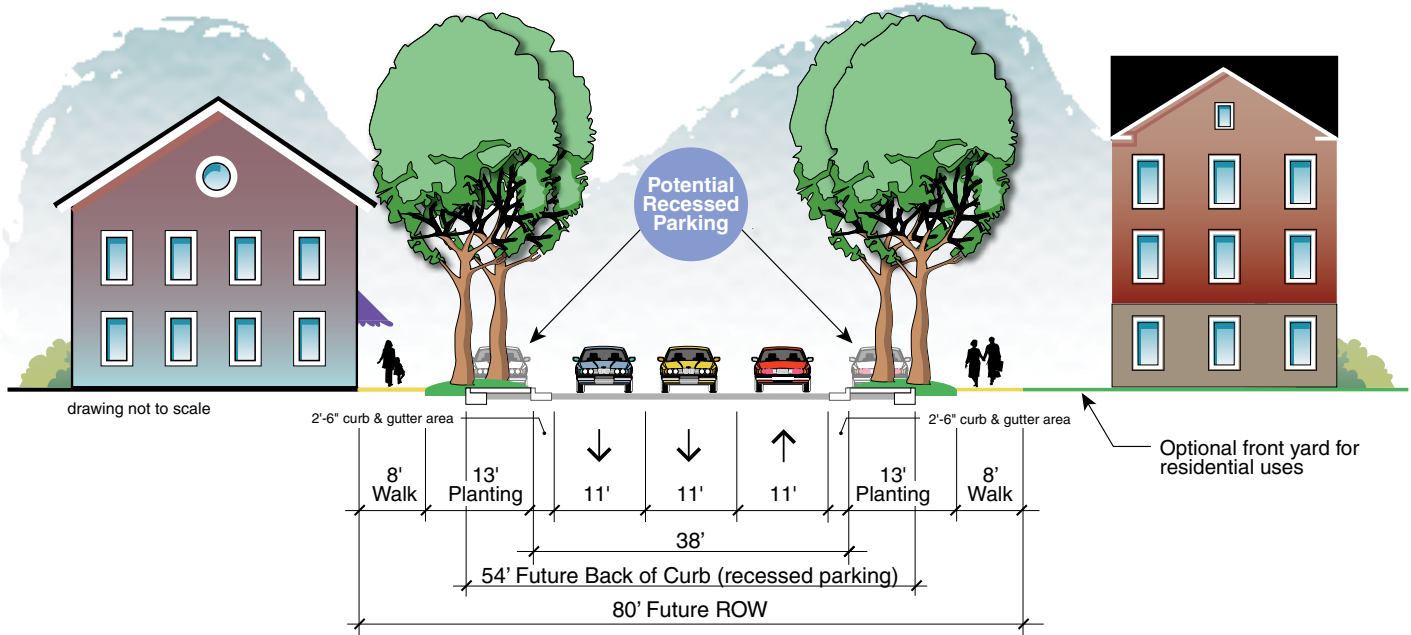
<b>Roadway and On-Street Parking</b>	<ul style="list-style-type: none"> <li>• Three 11' travel lanes</li> <li>• Center lane is either reversible travel lane (peak hours) or turn lane (off-peak)</li> <li>• 2'-6" curb and gutter</li> <li>• No bicycle lanes or additional travel lanes are included, based on the plan policy (T-2) of not widening the street</li> <li>• Recessed parking may be installed in place of the planting strip where it can be accommodated without damage to existing mature trees</li> <li>• Recessed parking must be broken up with vegetated bump-outs with street trees at minimum 100' intervals</li> <li>• Except for recessed parking spaces, curb lines will remain at existing locations</li> </ul>
<b>Planting Strip</b>	<ul style="list-style-type: none"> <li>• 8' planting strip with street trees per <i>Charlotte Tree Ordinance</i></li> </ul>
<b>Sidewalk</b>	<ul style="list-style-type: none"> <li>• 13' sidewalk</li> <li>• Sidewalk may accommodate outdoor dining, pedestrian amenities, decorative lighting, public art</li> <li>• Optional street trees in curbed planters near street-side edge of sidewalk</li> </ul>
<b>Recommended Building Setbacks</b>	<ul style="list-style-type: none"> <li>• At back edge of sidewalk (21' from back of curb), or as near to back of sidewalk as zoning permits</li> </ul>



East Seventh Street near the Pecan/Caswell intersection.

Source: Charlotte Department of Transportation

**B2 East Seventh Street**  
(Charlottetowne Avenue to Louise Avenue  
and Clarice Avenue to North Laurel Avenue)



**B2 Future Development Standards**

**Roadway and On-Street Parking**

- Three 11' travel lanes
- Center lane is either reversible travel lane (peak hours) or turn lane (off-peak)
- 2'-6" curb and gutter
- No bicycle lanes or additional travel lanes are included, based on the plan policy (T-2) of not widening the street
- Recessed parking may be installed into the planting strip where it can be accommodated without damage to existing mature trees
- Recessed parking must be broken up with vegetated bump-outs with street trees at minimum 100' intervals
- Except for recessed parking spaces, curb lines will remain at existing locations

**Planting Strip**

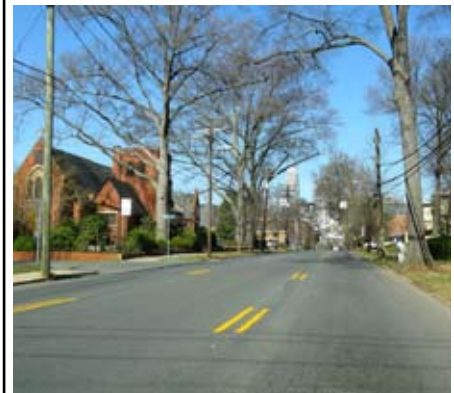
- 13' planting strip with street trees per *Charlotte Tree Ordinance*
- New street trees should be set back farther from the curb to establish a new row of trees to supplement existing mature trees

**Sidewalk**

- 8' sidewalk

**Recommended Building Setbacks**

- For buildings with ground floor retail or office: At back edge of sidewalk (21' from back of curb), or as near to back of sidewalk as zoning permits
- For buildings with ground floor residential: At back edge of sidewalk, or with a front yard of up to 20' in width



*East Seventh Street between Louise and Beaumont Avenues.*

Source: Charlotte Department of Transportation



## Hawthorne Lane

Hawthorne Lane from Elizabeth Avenue to East Eighth Street is a four-lane avenue, which reduces to two lanes with on-street parking from East Eighth Street to Independence Boulevard. One of the primary routes across the plan area, it passes both Presbyterian Hospital and Independence Park. Once beyond East Seventh Street proceeding toward Independence, land uses along the street turn from Institutional to predominantly Residential, both single family and multi-family.

The three future cross-sections shown for Hawthorne Lane are largely driven by the Center City Streetcar project. With the streetcar comes an opportunity to convert Hawthorne Lane to fewer travel lanes and add bicycle lanes, on-street parking, and/or additional pedestrian crossings, depending on the segment of the street.

Cross-section **B3**, from Elizabeth Avenue to East Fifth Street, will be partially built as part of the initial phase of the streetcar project. However, the northwest side of **B3** may not be fully constructed until streetcar is extended by a subsequent phase beyond East Fifth Street, since the inside travel lane may serve as an interim exclusive lane for the streetcar's temporary terminus.

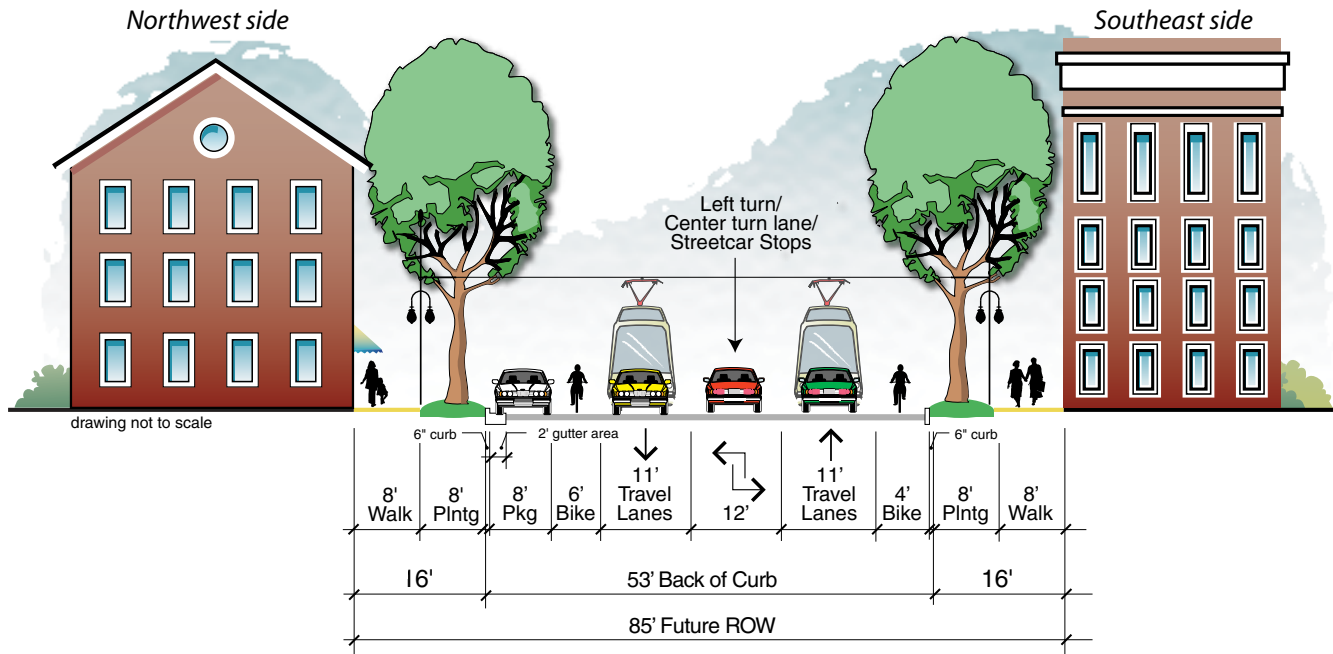
Cross-section **B4**, from East Fifth to East Eighth Streets, transitions from the **B3** cross-section established by the initial Streetcar project and the **B5** cross-section resulting from the street-conversion completed in 2010. There may be an opportunity to convert this length of Hawthorne Lane between the curbs to the desired **B4** cross-section prior to the extension of the streetcar beyond the initial phase. The cross-section improvements behind the curb would be requested when significant development occurs. The existing back-of-curb streetscape along Independence Park should remain as-is without any further encroachment onto park land.

The **B5** cross-section has largely been implemented inside the curbs through a recent street-resurfacing project. The back-of-curb streetscape for **B5** would be requested when significant development occurs.



*Hawthorne Lane near East Eighth Street*

**B3 Hawthorne Lane**  
(Elizabeth Avenue to East Fifth Street)



**B3 Future Development Standards**

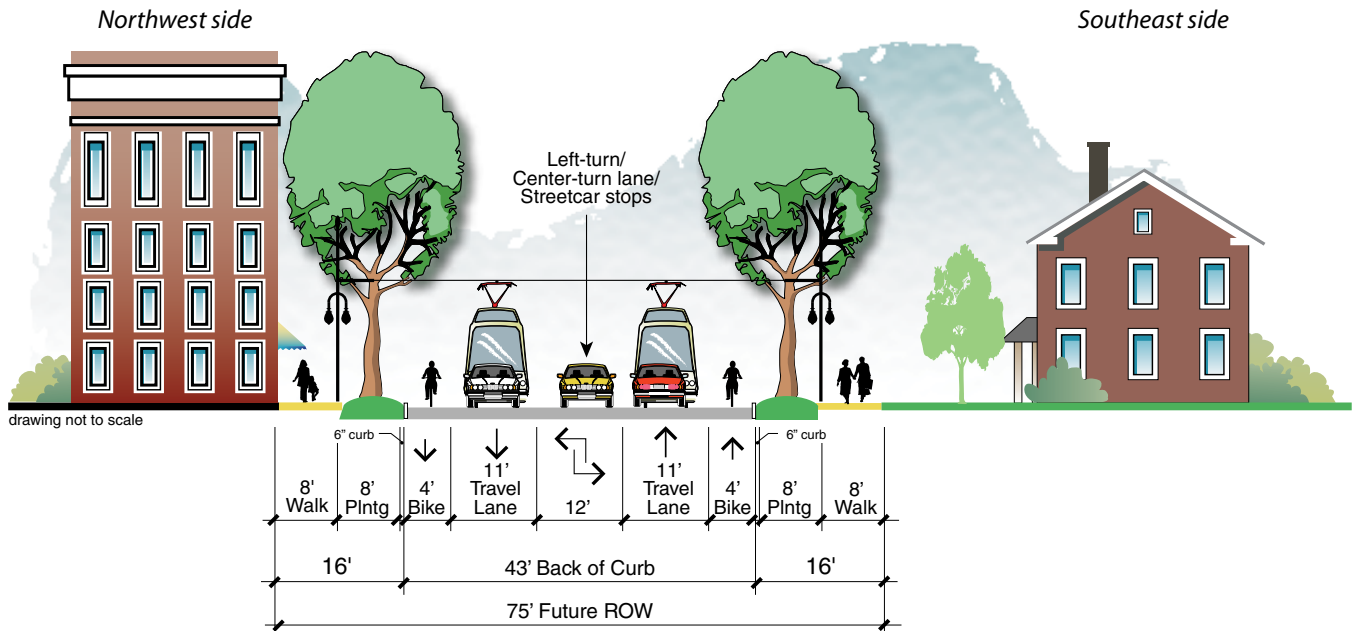
<b>Roadway and On-Street Parking</b>	<ul style="list-style-type: none"> <li>Two outside 11' travel lanes shared by streetcar and other vehicles</li> <li>One 12' center lane used as left turn lane or streetcar stops</li> <li>2'6" curb and gutter on north side, 6" vertical curb on south side</li> <li>6' bicycle lane on north sides, 4' bicycle lane on south side</li> <li>8' on-street parking on north side of street</li> <li>Parking with adjacent bike lane on north side may be used as additional travel lane during peak periods</li> </ul>
<b>Planting Strip</b>	<ul style="list-style-type: none"> <li>8' planting strip</li> <li>Street trees required per <i>Charlotte Tree Ordinance</i></li> </ul>
<b>Sidewalk</b>	<ul style="list-style-type: none"> <li>8' sidewalk</li> </ul>
<b>Recommended Building Setbacks</b>	<ul style="list-style-type: none"> <li>For buildings with ground floor retail or office: At back edge of sidewalk, or as near to back of sidewalk as zoning permits</li> <li>For other buildings: Either at back edge of sidewalk, or in accordance with zoning setback requirements</li> </ul>



Construction of the initial phase of the streetcar project will alter this section of Hawthorne Lane between Elizabeth Avenue and East Fifth Street.

Source: Charlotte Department of Transportation

**B4 Hawthorne Lane**  
(East Fifth Street to East Eighth Street)



**B4 Future Development Standards**

<b>Roadway and On-Street Parking</b>	<ul style="list-style-type: none"> <li>Two 11' travel lanes shared by streetcar and other vehicles</li> <li>One 12' center lane used as left turn lane or streetcar stops</li> <li>6" vertical curb (existing)</li> <li>4' bicycle lane on either side</li> <li>No on-street parking</li> <li>Curb lines will remain at existing locations.</li> </ul>
<b>Planting Strip</b>	<ul style="list-style-type: none"> <li>8' planting strip with street trees per <i>Charlotte Tree Ordinance</i></li> </ul>
<b>Sidewalk</b>	<ul style="list-style-type: none"> <li>8' sidewalk</li> </ul>
<b>Recommended Building Setbacks</b>	<ul style="list-style-type: none"> <li>Per zoning setback requirements</li> </ul>

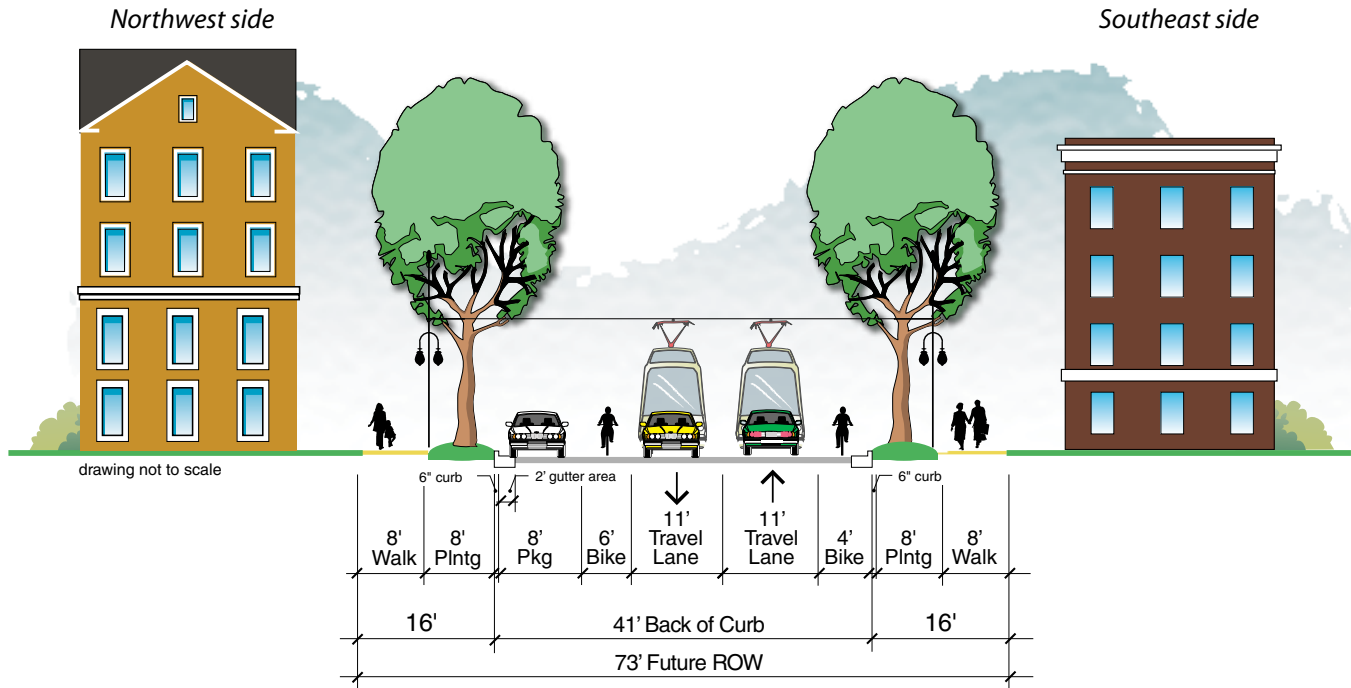


*Independence Park, on the left side of the street in this photo, would not be subject to the B4 development standards.*

Source: Charlotte Department of Transportation



**B5 Hawthorne Lane**  
(East Eighth Street to Independence Boulevard)



**B5 Future Development Standards**

<b>Roadway and On-Street Parking</b>	<ul style="list-style-type: none"> <li>Two 11' travel lanes shared by streetcar and other vehicles</li> <li>2'6" curb and gutter</li> <li>6' bicycle lane on north side between travel lane and on-street parking, 4' bicycle lane on south side</li> <li>8' on-street parking lane on north side</li> <li>Curb lines will remain at existing locations.</li> </ul>
<b>Planting Strip</b>	<ul style="list-style-type: none"> <li>8' planting strip with street trees per <i>Charlotte Tree Ordinance</i></li> </ul>
<b>Sidewalk</b>	<ul style="list-style-type: none"> <li>8' sidewalk</li> </ul>
<b>Recommended Building Setbacks</b>	<ul style="list-style-type: none"> <li>Per zoning setback requirements</li> </ul>



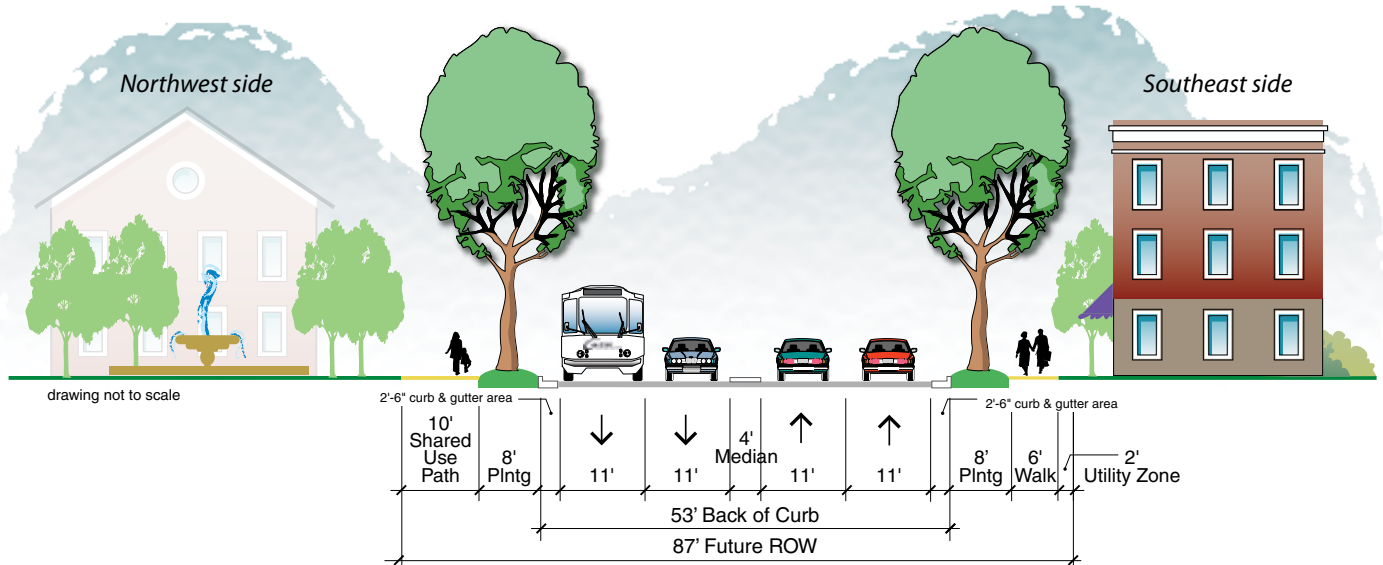
Hawthorne Lane near the Independence Boulevard overpass exhibits many elements of the recommended future street cross-section for this area. This includes on-street parking on one side and bicycle lanes in both directions.

Source: Charlotte Department of Transportation

## Kings Drive

Kings Drive has two travel lanes in each direction separated with a 4' concrete median, with no on-street parking or bicycle lanes. Little Sugar Creek Greenway provides a parallel shared-use path along the northwest edge of Kings Drive. Currently the solid median divider eliminates the possibility of left turns to or from the street into adjacent properties. The elimination of the median should be examined in the future should land use patterns along the street change such that left turn access would become more desirable. The back-of-curb streetscape shown on the southeast side would be requested through the plan review process when significant development occurs.

### B6 Kings Drive



### B6 Future Development Standards

**Roadway and On-Street Parking**

- Four 11' travel lanes, two in each direction separated by a 4' median divider
- 2'-6" curb and gutter
- No bicycle lanes. Bicycles would use the shared use path on the north side
- No on-street parking
- Curb lines will remain at existing locations.

**Planting Strip**

- 8' planting strip with street trees per *Charlotte Tree Ordinance*

**Sidewalk**

- 8' sidewalk on south side of street
- 2' utility zone behind sidewalk on south side of roadway
- 10' shared use path on north side of street for pedestrians and bicycles as part of the Little Sugar Creek Greenway system
- West of Elizabeth Avenue, the shared use path diverts further from the curb

**Recommended Building Setbacks**

- Per zoning setback requirements

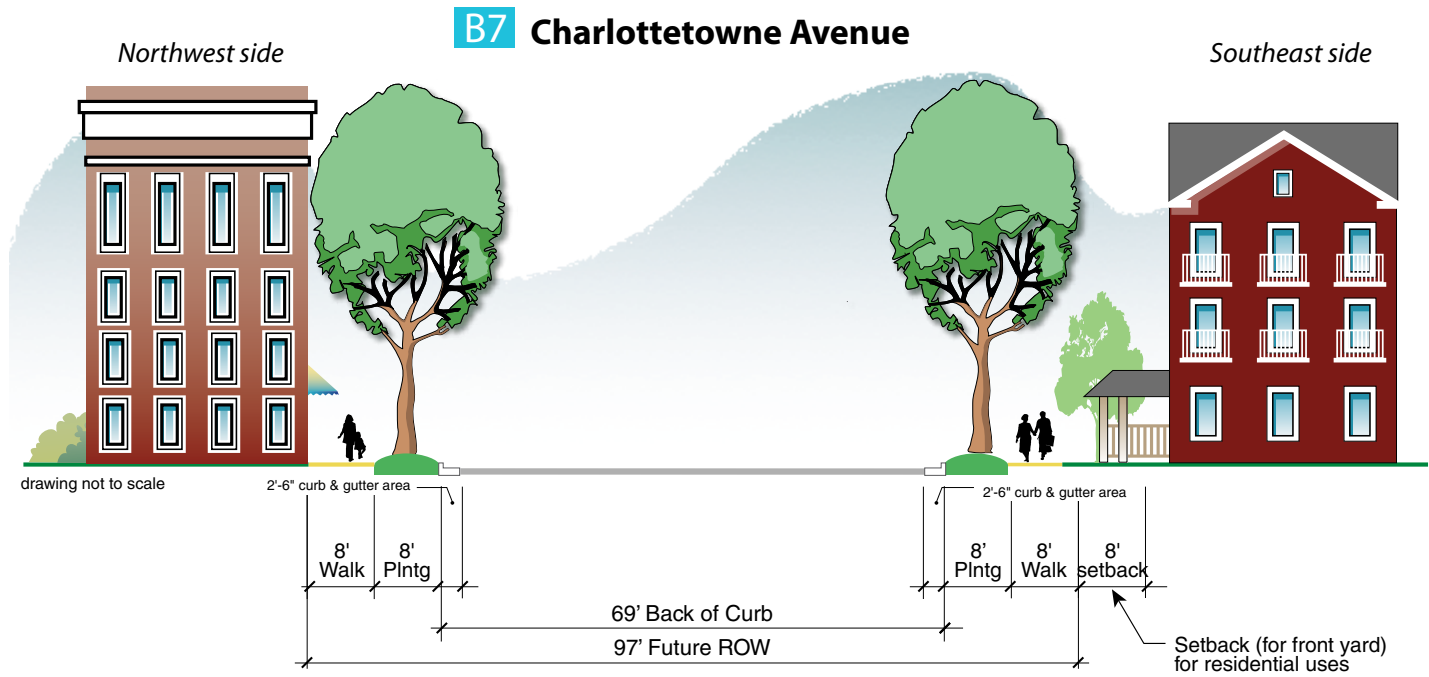


The 10' shared used path, part of the Little Sugar Creek Greenway, is on the right side of Kings Drive in this photo.


Source: Charlotte Department of Transportation

## Charlottetowne Avenue

Formerly Independence Boulevard, Charlottetowne Avenue is currently a concrete street with two travel lanes in each direction with left turn lanes and a narrow concrete median. The multi-modal vision for Charlottetowne Avenue accommodates rapid transit, motorists, cyclists, and pedestrians. The future Southeast Corridor Rapid Transit traverses part of this street, but because the mode of transit (either LRT or BRT) had not been determined at the time of this plan's adoption, this B7 cross-section is not specific about the area between the existing curbs. The future cross-section may eliminate the median and include space for vehicle travel lanes and rapid transit, with possible left turn lanes, pedestrian refuge islands, bicycle lanes, and/or on-street parking, all entirely within the existing curb-to-curb roadway width. The future back-of-curb streetscape configuration would be requested through the rezoning and urban zoning plan review process and when significant development occurs. The existing back-of-curb streetscape along Independence Park should remain as-is without any further encroachment onto park land.



B7 Future Development Standards	
<b>Roadway and On-Street Parking</b>	<ul style="list-style-type: none"> <li>To be fully developed when future rapid transit mode has been decided upon</li> <li>Curb lines will remain at existing locations.</li> </ul>
<b>Planting Strip</b>	<ul style="list-style-type: none"> <li>8' planting strip with street trees per <i>Charlotte Tree Ordinance</i></li> </ul>
<b>Sidewalk</b>	<ul style="list-style-type: none"> <li>8' sidewalk</li> </ul>
<b>Recommended Building Setbacks</b>	<ul style="list-style-type: none"> <li>Buildings at back edge of sidewalk, <i>or as near to back of sidewalk as zoning permits</i></li> <li>Buildings with street level residential uses should have an 8' front yard area behind the sidewalk which may accommodate porches or stoops</li> </ul>



*The existing curb-to-curb width of Charlottetowne Avenue should be sufficient to accommodate vehicle travel lanes as well as future rapid transit.*

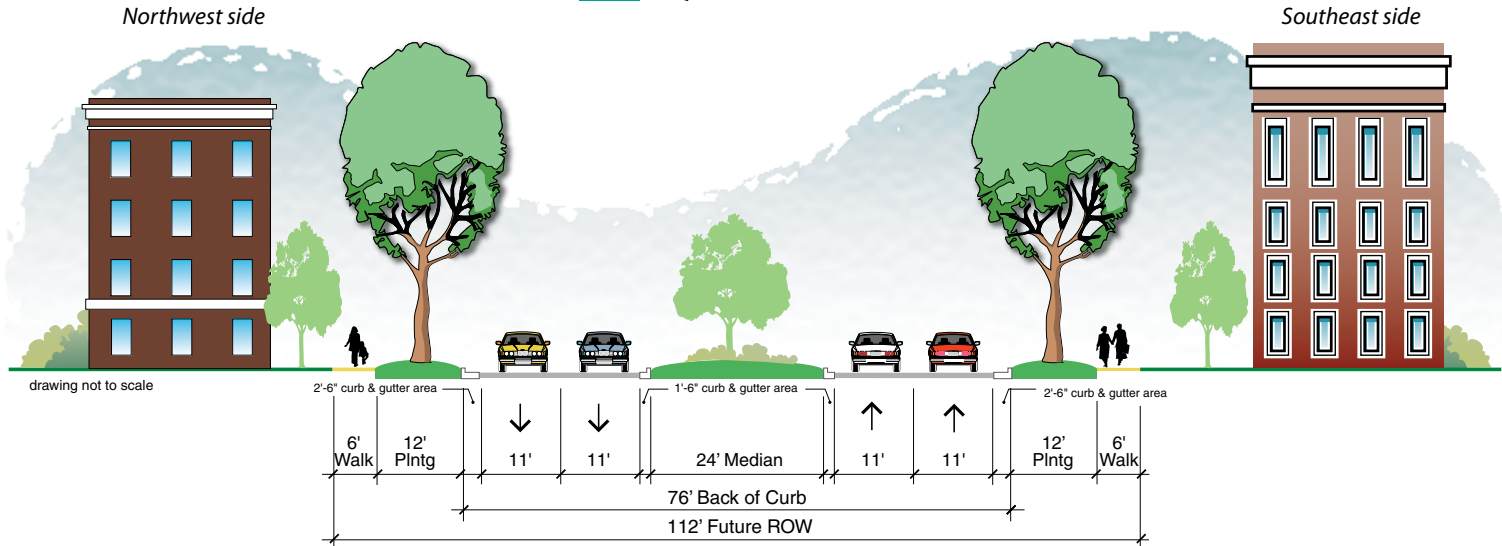
Source: Charlotte Department of Transportation



## Queens Road

One short section of Queens Road is within the plan area, from East Fourth Street, where it becomes a continuation of Hawthorne Lane, to East Third Street. As designed by John Nolen in 1905, Queens Road has a very wide median, which was once a streetcar route, and extra wide planting strips of sesquicentennial Willow Oak trees. No changes are proposed to its classic design. Any new streetscape resulting from land development should respect the established cross-section of this heritage street.

### B8 Queens Road



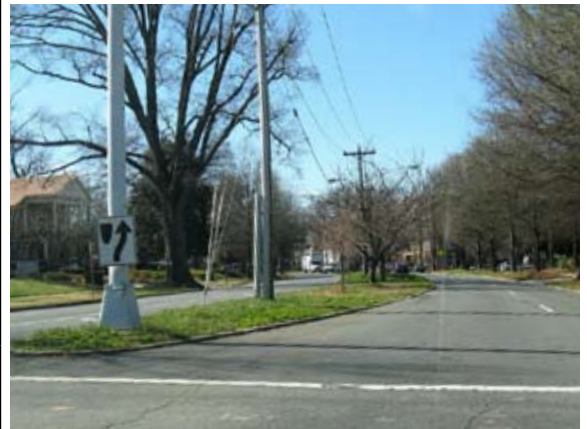
### B8 Future Development Standards

<b>Roadway and On-Street Parking</b>	<ul style="list-style-type: none"> <li>Four 11' travel lanes, two in each direction separated by a 24' landscaped median divider</li> <li>2'-6" curb and gutter on outer edges of roadway</li> <li>1'-6" curb and gutter on edges of median divider</li> <li>No bicycle lanes.</li> <li>No on-street parking</li> <li>Curb lines will remain at existing locations.</li> </ul>
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<b>Planting Strip</b>	<ul style="list-style-type: none"> <li>12' planting strip with street trees per <i>Charlotte Tree Ordinance</i></li> </ul>
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<b>Sidewalk</b>	<ul style="list-style-type: none"> <li>6' sidewalk</li> </ul>
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<b>Recommended Building Setbacks</b>	<ul style="list-style-type: none"> <li>Per zoning setback requirements</li> </ul>
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No changes are proposed to Queens Road.

Source: Charlotte Department of Transportation

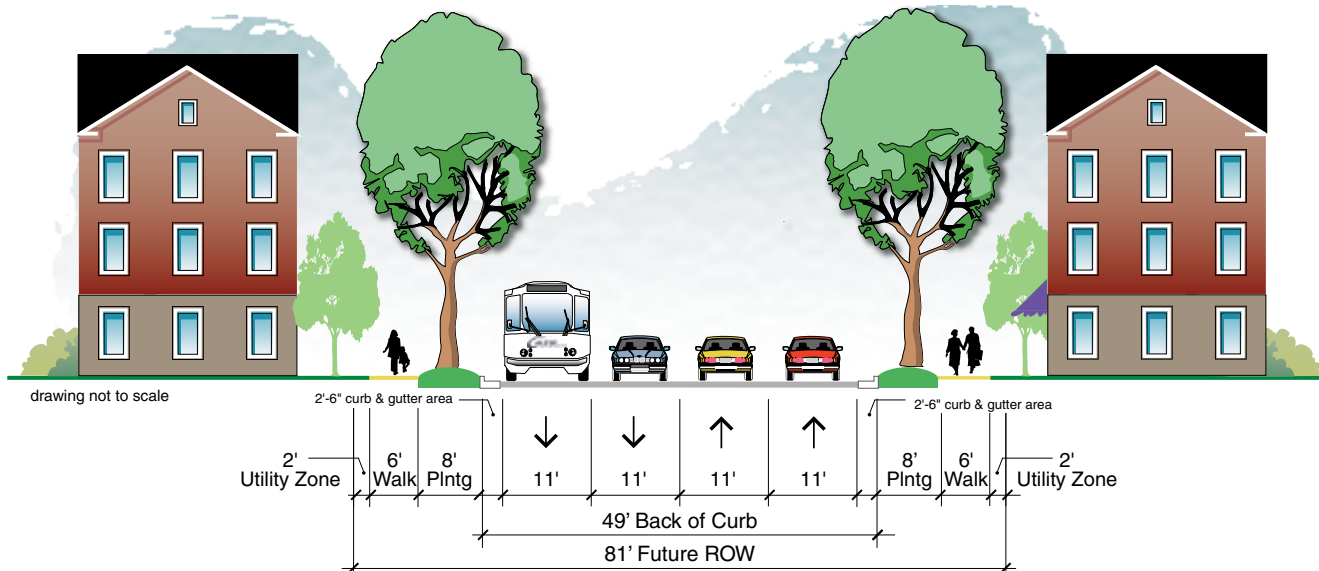
## Elizabeth Area Plan

**East Third Street; East Fourth Street; East Seventh Street** (Kings Drive to Charlottetowne Avenue); **East Seventh Street** (North Laurel Avenue to Briar Creek); **Hawthorne Lane** (East Fourth Street to Elizabeth Avenue); **Providence Road**; and, **Randolph Road**.

These Avenues carry significant commuter traffic, necessitating multiple vehicle lanes. Combined with existing physical constraints, motorist demand on these streets poses a challenge to providing space for bicyclists or additional pedestrian crossings. Still, back-of-curb streetscape improvements can be enhanced for pedestrians through the rezoning and urban zoning plan review process and when significant development occurs.

**B9**

**East Third Street; East Fourth Street;  
East Seventh Street** (Kings Drive to Charlottetowne Avenue);  
**East Seventh Street** (North Laurel Avenue to Briar Creek);  
**Hawthorne Lane** (East Fourth Street to Elizabeth Avenue);  
**Providence Road**; and, **Randolph Road**



### B9 Future Development Standards

<b>Roadway and On-Street Parking</b>	<ul style="list-style-type: none"> <li>• Four 11' travel lanes, two in each direction</li> <li>• 2'-6" curb and gutter</li> <li>• No bicycle lanes. Bicycles would share the travel lanes with vehicles.</li> <li>• No on-street parking</li> <li>• Curb lines will remain at existing locations.</li> </ul>
<b>Planting Strip</b>	<ul style="list-style-type: none"> <li>• 8' planting strip with street trees per <i>Charlotte Tree Ordinance</i></li> </ul>
<b>Sidewalk</b>	<ul style="list-style-type: none"> <li>• 6' sidewalk with 2' utility zone</li> </ul>
<b>Recommended Building Setbacks</b>	<ul style="list-style-type: none"> <li>• Per zoning setback requirements</li> </ul>



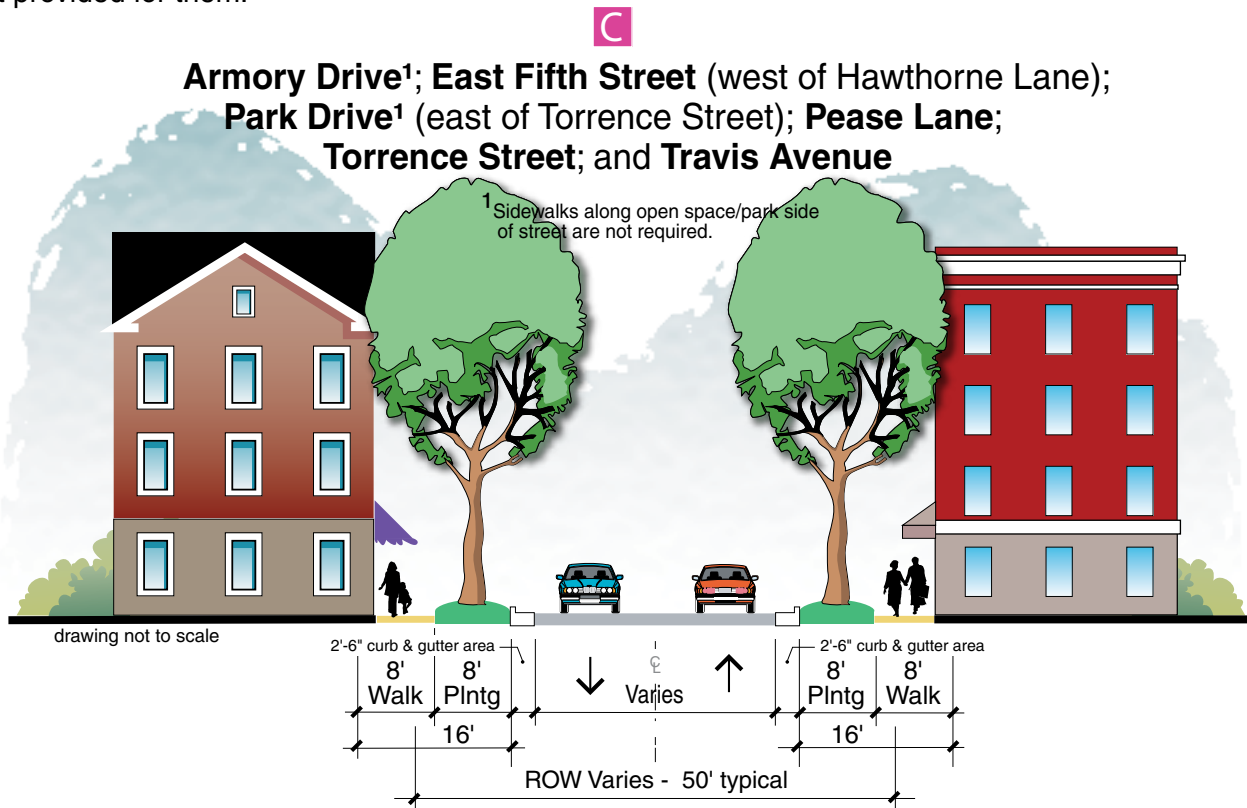
*Significant commuter traffic and constraints on its widening make Randolph Road a difficult route for bicycle travel.*

Source: Charlotte Department of Transportation

## Local Streets

**Armory Drive; East Fifth Street** (west of Hawthorne Lane); **Park Drive** (east of Torrence Street); **Pease Lane; Torrence Street;** and, **Travis Avenue**

Local Streets provide access to residential, commercial, or mixed use districts. The majority of Charlotte's streets are classified as local streets and are typically built as development occurs. This cross-section is for Local Streets within the preliminary future Transit Station Subarea. Local Streets within the Established Neighborhood Subarea are likely to remain unchanged and therefore a specific cross section is not provided for them.



### C Future Development Standards

<b>Roadway and On-Street Parking</b>	<ul style="list-style-type: none"> <li>Travel area varies, typically one travel lane in each direction</li> <li>2'-6" curb and gutter</li> <li>No bicycle lanes. Bicycles would share the travel lanes with vehicles.</li> <li>Recessed parking should be incorporated as development occurs</li> </ul>
<b>Planting Strip</b>	<ul style="list-style-type: none"> <li>8' planting strip with street trees per <i>Charlotte Tree Ordinance</i></li> </ul>
<b>Sidewalk</b>	<ul style="list-style-type: none"> <li>8' sidewalk</li> </ul>
<b>Recommended Building Setbacks</b>	<ul style="list-style-type: none"> <li>For buildings with ground floor retail or office: At back edge of sidewalk, or as near to back of sidewalk as zoning permits</li> <li>For other buildings: Either at back edge of sidewalk, or in accordance with zoning setback requirements</li> </ul>



*This plan serves as the adopted streetscape plan for local streets in the preliminary future Transit Station Subarea, such as Torrence Street (above).*

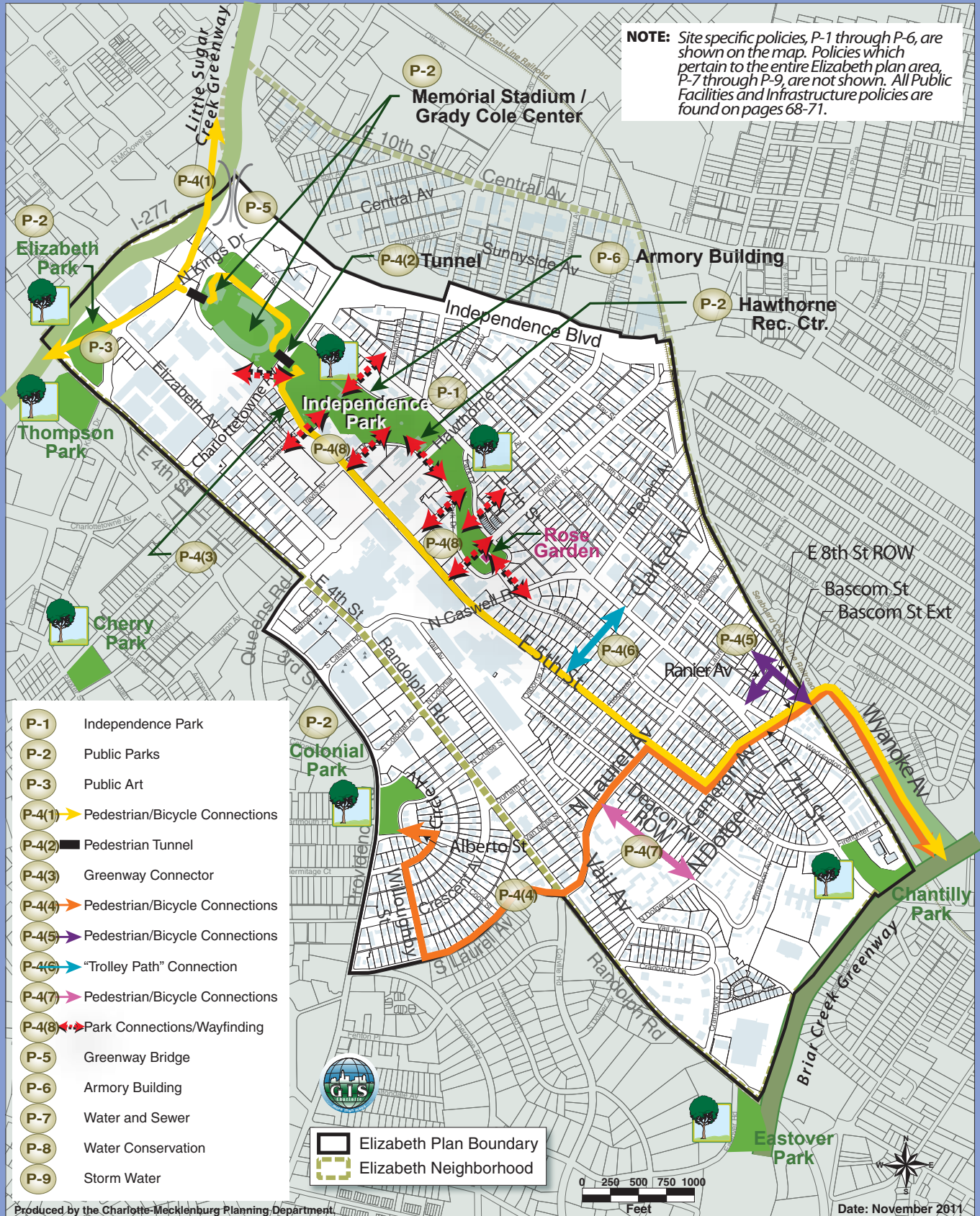
Source: Charlotte Department of Transportation



# Elizabeth Area Plan

## Future Public Facilities and Infrastructure Improvements

**NOTE:** Site specific policies, P-1 through P-6, are shown on the map. Policies which pertain to the entire Elizabeth plan area, P-7 through P-9, are not shown. All Public Facilities and Infrastructure policies are found on pages 68-71.



## Infrastructure and Public Facilities

Public facilities and services addressed in the plan include public water and sewer, storm water, police, fire, parks, recreation, and schools. As Charlotte-Mecklenburg continues to grow and develop, timely planning for, and coordination of, these services is essential to maintaining the high quality of life residents have come to expect. Other public facilities, such as libraries, medical, and social services, are not addressed in this area plan.

***Goal** Provide and maintain public facilities such as parks, greenways, recreational facilities, and infrastructure in excellent condition and emphasize environmental and economic sustainability in the design and planning of new or updated public facilities and infrastructure.*

There are several public parks, indoor and outdoor recreational facilities, public and private open spaces, and greenway facilities in or adjacent to the plan area. The area also boasts a number of schools, both neighborhood-serving (Elizabeth Traditional Elementary School) and those that draw students from a wider base, such as Central

Piedmont Community College (CPCC), Kings College, and Queens College/ Presbyterian Hospital School of Nursing. Refer to **Map 7, Future Public Facilities and Infrastructure Improvements**, page 68, and the Appendix for location and a description of these institutions.

Public infrastructure components such as water, sewer, and storm water are currently of adequate scope and capacity to serve both the area's present needs and those of anticipated development. The overall condition of this infrastructure is good.

### **Infrastructure and Public Facilities Policies**

The following policies are intended to enhance, preserve, and protect the area's existing public facilities, and to encourage infrastructure that serves today's requirements while incorporating innovative practices to meet future needs.

### **Parks, Greenways, and Recreational Facilities**

The Mecklenburg County Park & Recreation Department's draft *Center City Parks and Recreation Plan (January 2010)* provides recommendations for several of the plan area's parks, greenways, recreational facilities, and open spaces. None of the following policies are in conflict with these recommendations.



*King's College's three campus buildings overlook Independence Park at Park Drive and Lamar Avenue.*



*The Charlotte-Mecklenburg Utilities water tower at Pecan Avenue and East Eight Street is an Elizabeth landmark.*



**P-1. Preserve the character of Independence Park.**

Charlotte's first public park, Independence Park, is the symbolic and geographical heart of the Elizabeth neighborhood. It continues to be a refuge amidst the bustle of nearby schools, hospitals, offices, and businesses. Its history, natural beauty, diverse landscapes, and range of passive and active recreational opportunities enhance the neighborhood's livability and provide an immeasurable value to the community. The character Independence Park should be preserved. The portion of the park from Charlottetowne Avenue eastward (excluding all buildings) is a designated Historic Landmark and enjoys certain protections under North Carolina state law. The Charlotte-Mecklenburg Historic Landmarks Commission must authorize any proposed material alterations or modifications to Independence Park. In the event of such proposed changes, nearby property owners as well as other interested parties (such as neighborhood associations) would be notified and provided an opportunity to comment. There are also reverter clauses associated with this property that stipulate it must be used for park or recreation purposes or it will revert to the previous owners of the land.

The following improvements in support of this policy should be considered:

- Removing the asphalt surface parking area adjacent to the rose garden and landscaping the area to blend with surrounding parkland, and possibly expanding the rose garden into this area.
- Limiting future improvements in this area to those that do not generate significant additional vehicle traffic.
- Creating recessed on-street parking along the edge of the park in places where it will not disturb large mature trees or steep slopes.
- Expanding and enhancing the pedestrian-friendly environment and establishing pedestrian connections from this area to the surrounding neighborhood.

- Providing safe pedestrian connections across Hawthorne Lane.
- Identifying and protecting/preserving existing scenic views and vistas.
- Coordinating these and any additional improvements with the update of the *Independence Park Master Plan*.

**P-2. Maintain public parks, Hawthorne Recreation Center, Grady Cole Center, and American Legion Memorial Stadium in good condition and make them available for use by the community to the greatest extent feasible.**

The plan area is home to a 21,000-seat regionally-serving municipal stadium, two indoor civic/recreational facilities, neighborhood playgrounds, basketball and tennis courts, and ball fields. The useful life and functionality of these amenities should be extended through diligent scheduled maintenance and upgrade or replacement of facilities as they become outmoded or obsolete. The following actions are intended to support this policy:

- Ensuring playgrounds, courts, and ball fields in Independence Park are available to the public when not in use by organized recreational leagues or schools.
- Maintaining the Hawthorne Recreation Center for public use once current contractual obligations to outside groups are met, and keeping its restrooms available to the public during normal hours of operation.
- Preserving and maintaining American Legion Memorial Stadium for use as a sports stadium, or in the event the condition of this facility deteriorates to a point where it is no longer viable, replacing it with a similar facility or other public recreational use.

**P-3. Encourage the incorporation of public art into existing parks, greenways, and open spaces, and include public art as an integral component in the design of new park facilities.**

Public art endows a sense of place, a distinct



## Elizabeth Area Plan

identity, to a park or outdoor space. It can serve as the visual focal point of a place, or as a meeting spot. Over time public art may evolve into a community landmark, such as with the four bronze figures at the intersection of Trade and Tryon Streets in Uptown Charlotte. Public art can enhance and beautify an area, spark a conversation, peak one's curiosity, or educate about an important person or event. Public art can even encourage or discourage certain behaviors or movements within a public space.

**P-4. Provide additional pedestrian and bicycle connections within the neighborhood and between neighborhood employment and commercial centers, streetcar and transit stops, and greenways and neighborhood parks and open spaces.**

The orderly street grid, pedestrian scale, and topography of Elizabeth lends itself to walking or bicycling as a means of getting from one place to another within the plan area. The following actions should be explored to improve the level of connectivity and enhance non-vehicular travel. See **Map 7, Future Public Facilities and Infrastructure Improvements**, page 68.

1. Providing a pedestrian/bicycle connection from Little Sugar Creek Greenway through the plan area to Chantilly Park/Briar Creek Greenway,
2. Renovating the pedestrian tunnel under Charlottetowne Avenue from Memorial Stadium to Independence Park to provide adequate headroom for bicyclists, if possible, and improving lighting to enhance personal safety and security.
3. Extending the greenway connector beside Memorial Stadium, under Charlottetowne Avenue via the aforementioned tunnel, and across Independence Park to Park Drive. The new pathway across Independence Park should be designed to take as direct a route as possible, and should use existing park pathways where available to reduce the impact on existing trees and recreational fields.
4. Providing a pedestrian/bicycle connection from Colonial Park to Chantilly Park/ Briar Creek Greenway using Laurel Avenue as the primary crossing street.



The "Spirit of Mecklenburg" commemorates Capt. Jack's delivery of the Mecklenburg Declaration of Independence to the Continental Congress in Philadelphia in 1775.



This pedestrian tunnel crosses under Charlottetowne Avenue, connecting American Legion Memorial Stadium with Independence Park.

## Elizabeth Area Plan

5. Establishing a pedestrian/bicycle path from Laurel Avenue to Bascom Street along the unimproved portion of the East Eighth Street right-of-way, which would connect to Chantilly Park via Bascom Street and Wyanoke Avenue, and connecting this path to Ranier Avenue along the unimproved public right-of-way. (If the proposed East Eighth Street/Ranier Avenue street connections discussed in the Transportation section of this plan are implemented first, this ped/bike path should be modified accordingly.)
6. Maintaining the historic “trolley path” pedestrian connection between East Seventh and East Fifth Streets as a publicly-accessible walkway. This may involve securing and/or renewing necessary easements or rights-of-way from some adjacent property owners.
7. Providing a pedestrian connection from North Dotger Avenue to North Laurel Avenue adjacent to the stream bed along the extension of the Deacon Avenue right-of-way, and stabilizing the stream banks.
8. Install wayfinding signs along overland connector routes between primary greenways, and to guide pedestrians from neighborhood employment and commercial centers to Independence Park.

### **P-5. Plan for a signature greenway bridge.**

A dedicated bicycle and pedestrian bridge should be planned long-term through the I-277/Independence Boulevard interchange that would provide for a more continuous Little Sugar Creek Greenway.

### **P-6. Enhance the way in which the Armory Building and associated outdoor storage and parking areas on Armory Drive interact with Independence Park and adjacent areas.**

The Armory Building is used by Mecklenburg County Park and Recreation Department as a training and maintenance facility, with office space on the upper level fronting Armory Drive, and equipment storage and garage space on the lower level. There is also a vehicle storage area on the upper level off Armory Drive. It is possible that the uses within this facility could be relocated to other sites. This should be fully investigated in the Independence Park master plan process. In the meantime, landscaping and/or architectural screening should be used to obscure stored vehicles and materials from view from both the park and from Armory Drive.



*Brick columns and a metal archway mark the East Fifth Street entrance to the trolley walk, a mid-block pedestrian crossing to East Seventh Street.*



*Mecklenburg County Park and Recreation's Armory Building is adjacent to the Independence Park baseball field.*

## Water and Sewer

***P-7. Maintain a level of service equal or superior to the capacity and condition of current services to support the future development anticipated.***

System components should be repaired, updated, or replaced as they become outmoded or outdated. As development occurs, periodic reviews of water and sewer service may be needed to confirm that such service meets demands. This includes adequate water pressure for fire protection.

***P-8. Encourage the use of water conservation techniques to minimize demand on the water supply.***

Reduced water usage can be accomplished through the use of native landscaping and sustainable building materials, infrastructure, and site design.

## Storm Water

***P-9. Upgrade storm water management facilities as needed when improvements are made to the street network.***

Upgrading storm water management facilities at the same time that other road improvements are made will minimize unnecessary disruption of traffic and will be more cost-efficient.

## Schools

The schools and colleges in and near the plan area are meeting the educational and career training needs of area residents. Therefore, no specific policies pertaining to schools are proposed.

## Post Offices, Police and Fire Stations

Because the needs of area residents for these services are being met through existing facilities outside the plan area, no policies pertaining to post offices, police or fire stations are included in this area plan. (Refer to **Map 18, Existing Public Facilities**, page 114 and Appendix for location and descriptions.)

## Natural Environment

A good land use plan balances preservation of the area's natural resources with the demand for new development. This is possible with careful planning and adherence to policies and regulations. The specific environmental policies contained in this plan are based on the more general principles and policies taken from the *General Development Policies*, 2007.



***Goal*** *Preserve and protect the natural environment, minimize the adverse affects of new development on air and water quality, and encourage the use of eco-friendly building technologies and renewable resources.*

In addition to the policies in this section, the land use, transportation, and infrastructure/public facilities policy recommendations of this area plan recognize the importance of the natural environment. For example, if the Martha Washington Apartments were redeveloped, the land use and design recommendations support clustering development that would allow for both residential density and the preservation of open space and tree canopy.

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## ***Natural Environment Policies***

The following policies are intended to protect the area's natural resources while permitting well-planned development that can preserve and enhance the natural environment. While they are organized as either land, water or air policies, they are interrelated and often, what is beneficial for one aspect of the environment is also beneficial for the others. For example, tree preservation is not only good for land quality, but also for air and water quality.



## Land Quality

The plan area includes environmentally-sensitive features such as floodplain, steep topography along streams and creeks, mature tree canopy, and brownfield sites.

Floodplain areas are primarily adjacent to the two major creeks, Little Sugar Creek and Briar Creek. While new buildings are allowed in the floodplain under certain conditions, most of the undeveloped floodplain in the plan area should be maintained as open space or undeveloped land. Steep topography along streams and creeks should be preserved and protected.

Although Elizabeth is rightly known for its mature tree canopy, as a percentage of land area covered it is less than the city of Charlotte's as a whole (40 percent vs. 46 percent). This is probably not unusual compared to other established neighborhoods close to Center City. However, the loss of tree cover should be monitored since between 1985 and 2008 Charlotte lost 49 percent of its tree canopy, according to a 2010 study by American Forests. The environmental benefits provided by trees include improved water quality, a reduction in ground level ozone and other forms of air pollution, and carbon storage and sequestration. These environmental benefits also translate into significant economic benefits, in the form of reduced societal health care costs, decreased need for storm water management, and increased property values.

There are also a few brownfield sites in the plan area as identified by the North Carolina Department of Environment and Natural Resources (DENR). The environmental hazards on these sites are typically associated with former uses such as dry cleaners or gas stations. There may be other sites with contaminated land in the plan area as well. With proper remediation this land can be reused, with exceptions for certain types of uses such as child care facilities, parks, or playgrounds. However, proper clean-up is often difficult and costly. The land use recommendations of this area plan are not in conflict with the reuse of brownfield sites in the plan area.

### ***E-1. Encourage measures that will help ensure the long-term sustainability of the tree canopy.***

One measure to help reverse the loss of tree canopy is the development of a strategy to plant additional trees in parks, public and private open spaces, and in planting strips along public streets, where practicable. This strategy should also address replacement of trees that are lost or damaged through disease, development, or other causes.

### ***E-2. Minimize impacts to existing tree canopy when developing, maintaining, or constructing streets, sidewalks, pedestrian/bicycle paths, public facilities, and utilities.***

A strategy is needed to prevent practices that damage or destroy mature trees. Such a strategy should be developed in consultation with utility companies and the construction industry to increase its acceptance and enhance compliance efforts.

## Air Quality

As an urban area with major hospitals, institutions, commercial areas, and heavily-travelled thoroughfares, the Elizabeth area's air quality is impacted by vehicle emissions. One way of reducing this type of pollution is by facilitating other "clean" modes of transportation through and within the plan area, such as bicycling and walking. Another way is by encouraging the use of existing public transportation and supporting efforts to expand transit in the future.



*Air quality can be improved by reducing the number of automobiles travelling in and through the area.*

## Elizabeth Area Plan

The level of connectivity in an area is a major determinant as to whether residents, workers, and visitors will be willing to forsake their automobiles in favor of alternate means of transportation. The more complete the street and sidewalk network is, the easier and more convenient it becomes for people to walk or bicycle from place to place. Hence, automobile use can be reduced by enhancing the street network and adding connections for walking and cycling.

### ***E-3. Maintain a high level of street connectivity and encourage new connections for pedestrian and bicycle travel to reduce vehicle miles travelled.***

Being an established “streetcar suburb”, Elizabeth’s street grid network is fairly complete, with just a few missing pieces. This network should be preserved and, where possible, expanded as development occurs in areas where new streets can be added to help complete the grid.

Specific policies regarding new street connections, sidewalks, pedestrian and bicycle paths, the preservation of existing walking connections, and enhancement of greenways are in the Infrastructure and Public Facilities section.

### **Water Quality**

The Elizabeth plan area lies between Little Sugar Creek on the northwest and Briar Creek on the southeast, with 42 acres of associated floodplain.



*The section of Briar Creek from Randolph Road to the tracks of the Seaboard Coast Line RR forms the southeast boundary of the Elizabeth plan area.*

There are 4.2 miles of streams in the plan area protected by Surface Water Improvement and Management (SWIM) and Post-Construction Controls Ordinance (PCCO) buffers.

To protect the area’s water resources, diligent water quality measures should be implemented as development or redevelopment occurs.

### ***E-4. Encourage the use of low impact design to facilitate better water quality protection.***

Low impact design measures such as minimizing the clearing of natural areas and working with the existing natural topography to minimize grading can improve the storm water quality and reduce the amount of storm water run-off.

### ***E-5. Utilize innovative best management practices (BMP) for urban development and redevelopment projects.***

The use of innovative and eco-friendly storm water management practices should be integrated into the design of new development to the greatest extent practicable. This may include rain gardens, green roofs, rain barrels, dedicated use of gray water, porous concrete parking areas, and other approved measures that may come into accepted use. The testing of proprietary storm water management measures for possible approval should be considered as well.

### ***E-6. Support environmental education as a means of reducing the amount of pollutants entering area streams and creeks.***

Encourage local community involvement in efforts to educate area residents about topics such as the proper use of lawn fertilizers and pesticides, disposal of leaves, grass clippings, and yard debris, storm drain marking, and Adopt-A-Stream programs. Vigorous enforcement of existing water quality regulations by the appropriate local and State agencies is also vitally important.

## Conclusion

As Charlotte continues to attract new residents and businesses, pressures for development will likely intensify. The ability to successfully accommodate growth, while maintaining a high quality of life, is not likely to occur without good private and public sector planning and active neighborhood involvement. To that end, this document sets the stage for managing future growth and development consistent with the *Centers, Corridors, and Wedges Growth Framework* and the community's vision for its future. With adoption of Part I - Concept Plan, by City Council, this document updates previously adopted plans for the community, including the *Central District Plan* (1993) and the *Centers, Corridors, and Wedges Growth Framework*. It also identifies the boundaries of a preliminary future Transit Station Subarea and an Established Neighborhood Subarea within the Southeast Growth Corridor.

The policy recommendations in this plan knit together land use, transportation, community design, and infrastructure and public facilities policies to ensure the desired vision for the plan area is achieved. That vision includes encouraging well-conceived infill development, important to the ongoing vitality of the area, balanced with

neighborhood preservation. It also encourages broad community participation in planning and development decisions that affect the fabric of the community.

With input from the community, the public sector will provide the policy framework for land development and will be responsible for making a number of infrastructure improvements, including some investment in streetscape amenities that can be accommodated within the existing right-of-way. In addition, the Charlotte-Mecklenburg Planning Department, in consultation with other City and County departments, is responsible for initiating and guiding the corrective rezoning process and monitoring and reviewing rezonings proposed for the plan area to ensure developers meet the required development standards and carry out the intent of the adopted policy plan for the area.

The private sector will be responsible for developing and redeveloping properties within the plan area in alignment with the vision, policies, and development and design standards included in the Concept Plan. Providing the required infrastructure improvements will be part of these development responsibilities.







## Part II: IMPLEMENTATION GUIDE

## ***Part II: Implementation Guide***

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### ***Introduction***

Achieving the vision articulated in this plan will occur incrementally over time through the combined efforts of local government, private property owners, residents, developers, and many others. This Implementation Guide is primarily a staff document that outlines specific steps that can be taken by various public and private bodies so that the desired future envisioned in this plan may be realized. These strategies are listed in the accompanying table. The lead responsible agency and tentative time frame is also included. The strategies are numbered sequentially and correspond to the policies discussed in *Part 1 - Concept Plan*. This allows staff to track progress of the plan's policies over time.

Some strategies are physical such as the addition of pedestrian, bicycle, and greenway connections, or park enhancements. Other recommended strategies are organizational, such as maintaining public recreation facilities for public use or encouraging low impact design. Some may even involve changes to existing City policies, regulations, and/or practices.

The implementation strategies do not imply a public or private sector commitment. The Charlotte City Council will not be asked to adopt this *Implementation Guide*. However, many of the strategies in this section will require future action by elected officials and will be presented to them for approval as needed on a case-by-case basis.

### **Public Sector Responsibilities**

The public sector will provide the policy basis, the oversight, and some infrastructure improvements for the implementation of the Elizabeth Area Plan. However, many changes to the area will become evident only as private investment begins to fill in the framework that the community and the City of Charlotte have laid out in this Plan. Local government will likely provide some investment in streetscape amenities that can be accommodated within the existing right-of-way. Major public investment, such as constructing new roadway cross sections, may not until redevelopment happens.

### **Private Sector Responsibilities**

The private sector will be responsible for delivering pedestrian-friendly development that is consistent with the general development guidance provided in this plan. In addition, this plan serves as a tool for the Plan Area in guiding land use and development decisions. As such, the Elizabeth Community Association and Historic Elizabeth Neighborhood Foundation and the private sector will be responsible for monitoring development to ensure that the plan is implemented.

Finally, since conditions change over time, staff will update this *Implementation Guide* periodically to reflect changes and progress.

**Recommended Implementation Strategies** The number of each action corresponds to the policies beginning on page 19 of the Concept Plan.

**Recommended Implementation Strategies**

No.	Policy	Action Item	Project Type	Lead Agency	Priority
<b>Land Use</b>					
1.	L-1 L-32	Use land use policies to guide and evaluate development proposals.	Land Use	Planning	On-going
2.	L-2	Encourage CPCC to remove the fence enclosing the lawn in front of CPCC Overcash Building to open up the area for public use.	Park	Park & Rec/ CPCC	Short (1-5 years)
3.		Pursue corrective rezonings (see <b>Recommended Corrective Rezonings table, page 82.</b> )	Zoning	Planning	Short (0-5 years)
<b>Community Design</b>					
4.	D-1 D-7	Use community design policies to guide and evaluate development proposals.	Design	Planning	On-going
5.	D-1G D-3F D-4M D-4P	Initiate a survey of public alleys to determine those that can practically be activated for vehicular access and identifying the obstacles preventing those that cannot be practically used.	Design	Planning/ CDOT	Short (0-5 years)
<b>Transportation and Streetscape</b>					
6.	T-1	Support improvements to increase the capacity of Independence Boulevard and encourage, through signage and education, the use of Independence instead of East Seventh/Monroe for vehicle trips originating and ending outside the Elizabeth neighborhood.	Transportation	CDOT	Short (0-5 years)
7.	T-2	Initiate removal of East Seventh Street widening from future Long Range Transportation Project lists.	Transportation	CDOT	Short (0-5 years)
8.	T-3 T-4	Evaluate intersection of East Seventh/Caswell/Pecan and implement improvements needed to enhance pedestrian safety and comfort. Evaluate East Seventh Street to identify locations for additional pedestrian crossings and implement necessary improvements. Identify and implement traffic calming measures at the Pecan/East Eighth and North Caswell/Greenway intersections.	Transportation	CDOT	Short (1-5 years)
9.	T-5	Extend Bascom Street from Weddington Avenue to 7th Street opposite Cameron Avenue. Extend East Eighth Street to Ranier Avenue and/or Bascom Avenue.	Transportation/ Planning	CDOT	As development occurs
10.	T-6	Construct sidewalks to eliminate gaps in the sidewalk system.	Transportation	CDOT/ Planning	As requested through a petition process
11.	T-7	Provide alternative bicycle accommodations along or parallel thoroughfares by designating preferred routes with new signage or striping for shared lanes (sharrows).	Transportation	CDOT	Short (1-5 years)
12.	T-8	Coordinate planning for Center City Streetcar.	Transportation	E&PM/ CDOT	Medium (5-10 years)



# *Elizabeth Area Plan*

No.	Policy	Action Item	Project Type	Lead Agency	Priority
13.	T-9	Coordinate planning for the Southeast Corridor Rapid Transit	Transportation	CATS/ Planning/ CDOT	Long (10+ years)
14.		Use street cross-sections to guide and evaluate development proposals.	Planning/ Transportation	Planning/ CDOT	As development occurs
<b>Infrastructure and Public Facilities</b>					
15.	P-1	Pursue a park conservation easement to provide added protection for Independence Park.	Public Facilities	Neigh Org	Short (0-5 years)
16.	P-1	Provide a gazebo or shaded sitting area to accommodate small gatherings.	Park	Park & Rec	Medium (5-10 years)
17.	P-1	Expand and enhance pedestrian connections to the park. Access and connectivity for daily use and special events, such as sidewalks, ramps, steps, overland connectors, grade separated crossings and crosswalks should be identified through the development of a master plan for Independence Park.	Park	Park & Rec	Short (1-5 years)
18.	P-1	Provide safe pedestrian connections across Hawthorne Lane to the park.	Park/ Transportation	Park & Rec/ CDOT	Medium (5-10 years)
19.	P-2	Maintain Hawthorne Recreation Center for public use once contractual obligations with outside groups are met.	Park	Park & Rec	Short (1-5 years)
20.	P-3	Evaluate parks, greenways, and public open spaces to determine potential sites for public art. Explore integration of new public art into these sites. The Arts and Science Council will lead this effort through the Public Arts Ordinance.	Park	Arts and Science Council/ Park & Rec/ Planning	Short (1-5 years)
21.	P-4 (1)	Provide a pedestrian/bicycle connection from Little Sugar Creek Greenway through the Plan Area to Briar Creek Greenway.	Park/ Transportation	Park & Rec/ CDOT	Medium (5-10 years)
22.	P-4 (2)	Renovate the pedestrian tunnel under Charlottetowne Avenue from Memorial Stadium to provide adequate headroom for bicyclists and improve lighting for safety and security.	Park	Park & Rec	Medium (5-10 years)
23.	P-4 (3)	Complete greenway connector at western edge of Independence Park by constructing a ped/bike pathway between the pedestrian tunnel under Charlottetowne Avenue and Park Drive.	Transportation	CDOT/ Park & Rec	Short (1-5 years)
24.	P-4 (4)	Provide a pedestrian/bicycle connection from Colonial Park to Briar Creek Greenway along Chase Street and Vail Avenue, incorporating the open space area on Vail Avenue adjacent to the Mercy Hospital parking deck.	Park/ Transportation	Park & Rec/ CDOT	Medium (5-10 years)
25.	P-4 (5)	Establish a pedestrian/bicycle connection from Laurel Avenue to Bascom Street along the unimproved portion of the East Eighth Street ROW, which would connect to Chantilly Park via Bascom Street and Wyanoke Avenue and connecting this path to Ranier Avenue long the unimproved public ROW.	Park/ Transportation	Park & Rec/ CDOT	Medium (5-10 years)
26.	P-4 (7)	Provide pedestrian connection from North Dotger Avenue to North Laurel Avenue adjacent to the stream bed along the extension of the Deacon Avenue ROW and stabilizing the stream banks.	Park/ Transportation/ Storm Water	Park & Rec/ CDOT/ E&PM	Medium (5-10 years)

## *Elizabeth Area Plan*

No.	Policy	Action Item	Project Type	Lead Agency	Priority
27.	P-4 (8)	Install wayfinding signs along overland connector routes between primary greenways. Install wayfinding signs on connections from neighborhood employment and commercial centers to Independence Park.	Transportation	CDOT/ Park & Rec	Short (1-5 years)
28.	P-5	Plan for a signature greenway bridge across I-277/ Independence Boulevard.	Transportation	CDOT/ Park & Rec	Long (10+ years)
29.	P-6	Screen equipment and vehicle storage areas at the Armory Building from view from Armory Drive and Independence Park.	Park	Park & Rec	Short (1-5 years)
30.	P-7	Repair, update, or replace water and sewer system components as they become outmoded or outdated to maintain a superior level of service.	Utilities/Storm Water	CMUD/ E&PM	As needed
31.	P-8	Encourage sustainability in site design and infrastructure and the use of sustainable building materials in the evaluation of development proposals.	Planning	Planning/ E&PM/ CDOT	As development occurs
32.	P-9	Upgrade storm water management facilities as needed when improvements are made to the street network.	Storm Water	E&PM	As needed
33.		After plan adoption, convene key neighborhood organizations and community leaders, such as the ECA, HENF, CPCC, area hospitals, Park and Recreation, and Planning Department, to discuss roles and strategies in implementing many of the plan recommendations. The discussion should include the potential of developing a wayfinding system.	Public Facilities	TBD	Short (0-5 years)
34.		Establish an annual meeting to include key neighborhood organizations, institutions, and community leaders, to keep channels of communication open and facilitate discussions about issues affecting the neighborhood.	Public Facilities	TBD	Short (0-5 years)
<b>Natural Environment</b>					
35.	E-1	Develop a strategy to help reverse the loss of tree canopy, including the planting of additional trees in parks, public and private open spaces, and in planting strips along public streets.	Environment	E&PM	Medium (5-10 years)
36.	E-2	Develop a strategy to minimize impacts to existing tree canopy when developing or constructing streets, public facilities, etc.	Environment	E&PM/ CDOT	Medium (5-10 years)
37.	E-4	Encourage the use of low impact design to facilitate better water quality protection in the evaluation of development proposals.	Environment	E&PM	Medium (5-10 years)
38.	E-5	Encourage the use of innovative storm water management practices such as rain gardens, green roofs, rain barrels, dedicated use of gray water, porous concrete parking areas in the evaluation of urban development proposals.	Environment	E&PM	Medium (5-10 years)
39.	E-6	Support and encourage community involvement in environmental education as a means of reducing the amount of pollutants entering area streams and creeks.	Environment	Elizabeth Community Association/ E&PM	Short (1-5 years)

## Recommended Corrective Rezoning

The Planning Department will initiate corrective rezonings to implement the Concept Plan's land use vision and recommendations. Corrective rezonings should meet one of the following criteria:

1. **To align zoning with existing land use** to protect the surrounding residential area. This is particularly appropriate when the land is currently being used for lower density residential development, but zoned for higher intensity uses (residential or non-residential). When recommending a "down zoning" of this nature, consideration should be given to the surrounding land uses and vision for the area. The difference between the intensity of the existing zoning and the recommended zoning should also be considered. In some cases, the recommended corrective rezoning may still be more intense than the existing use.
2. **To align zoning with adopted future land use.** This should be pursued, particularly, when the zoning is more intense than the adopted future land use. In addition, consideration should be given to the timing of new development in determining the schedule for implementing the corrective rezoning. A rezoning to a more intense use is sometimes appropriate, particularly in implementing transit station area plans and Pedscape plans. Efforts should be made to minimize the creation of new non-conforming uses when recommending corrective rezonings. This may mean that the recommended rezoning will not align exactly with the adopted land use.
3. **To protect environmentally sensitive areas.** A specific land use or development intensity may be identified as inconsistent with the environmentally sensitive nature of a specific area, necessitating a zoning change.

The rezoning process will occur only after the completion of the plan and will require additional public input. Proposed rezonings are organized by Center, Corridor and Wedge designation and are listed in the table below and shown on the accompanying map.

### Recommended Corrective Rezoning

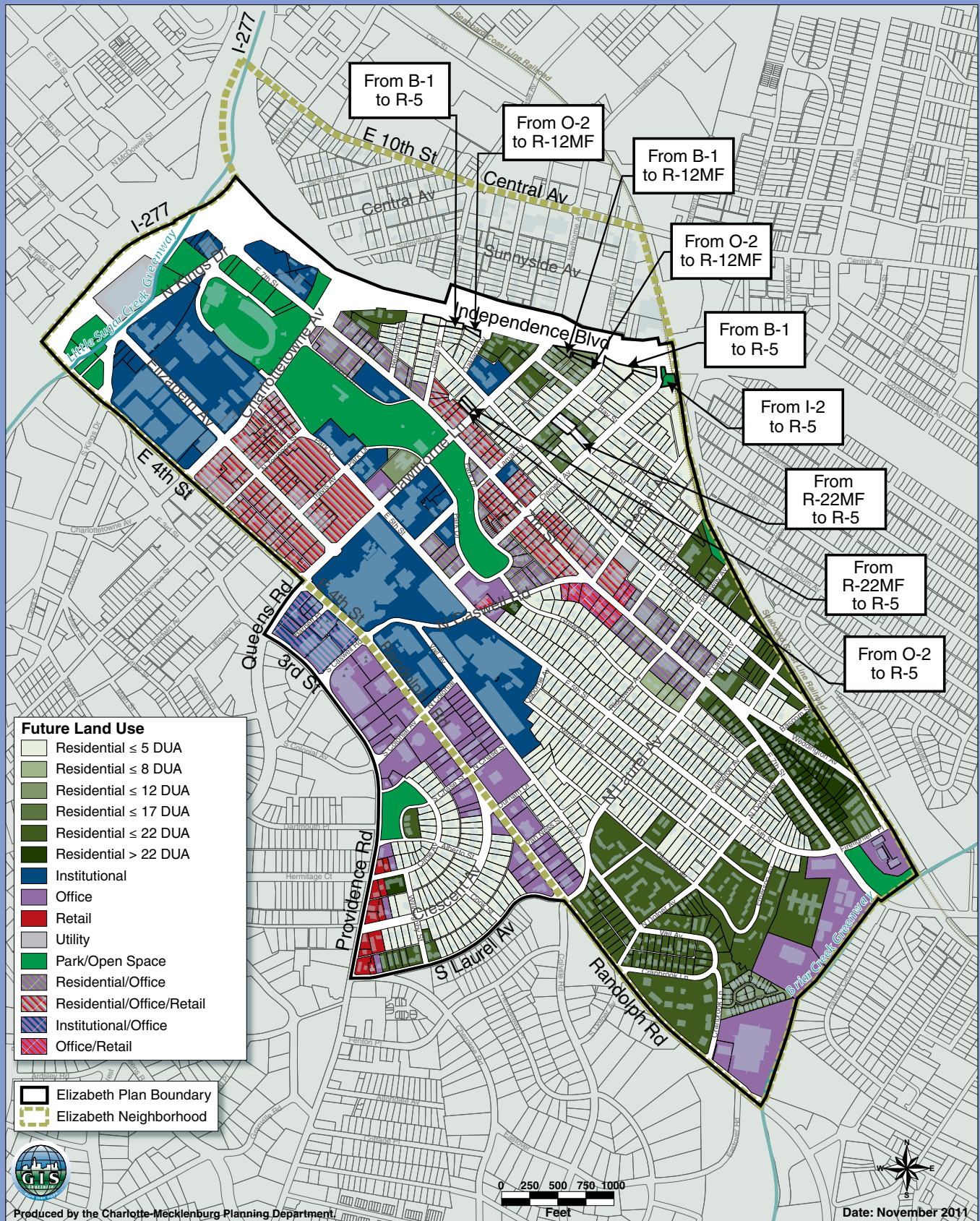
#	PID	Recommended Zoning Change	Existing Land Use	Previously Adopted Future Land Use	Recommended Future Land Use
<b>Growth Corridor - Established Neighborhood</b>					
L-7	08020212 08020304	B-1 to R-5	Single Family Residential	Park/Open Space	Residential ≤5 DUA
L-7	08020315 08020316	O-2 to R-12MF	Multi-Family Residential	Park/Open Space	Residential ≤12 DUA
L-7	12701609 12701620	I-2 to R-5	Vacant	Park/Open Space	Residential ≤5 DUA
L-7	12701711 12701712 12701720 (partial)	B-1 to R-5	Single Family Residential and Vacant	Park/Open Space	Residential ≤5 DUA
N/A	02701109 02701113	B-1 to R-12MF	Vacant	Park/Open Space	Residential ≤12 DUA
N/A	02701108	O-2 to R-12MF	Single Family Residential	Park/Open Space	Residential ≤12 DUA
L-14	12701316 12701315 12701306	R-22MF to R-5	Single Family Residential	Multi-family	Residential ≤5 DUA
L-12	08020505	O-2 to R-5	Single Family Residential	Single Family	Residential ≤5 DUA
L-7	08020506	R-22MF to R-5	Single Family Residential	Single Family	Residential ≤5 DUA



# Elizabeth Area Plan

## Recommended Corrective Rezoning

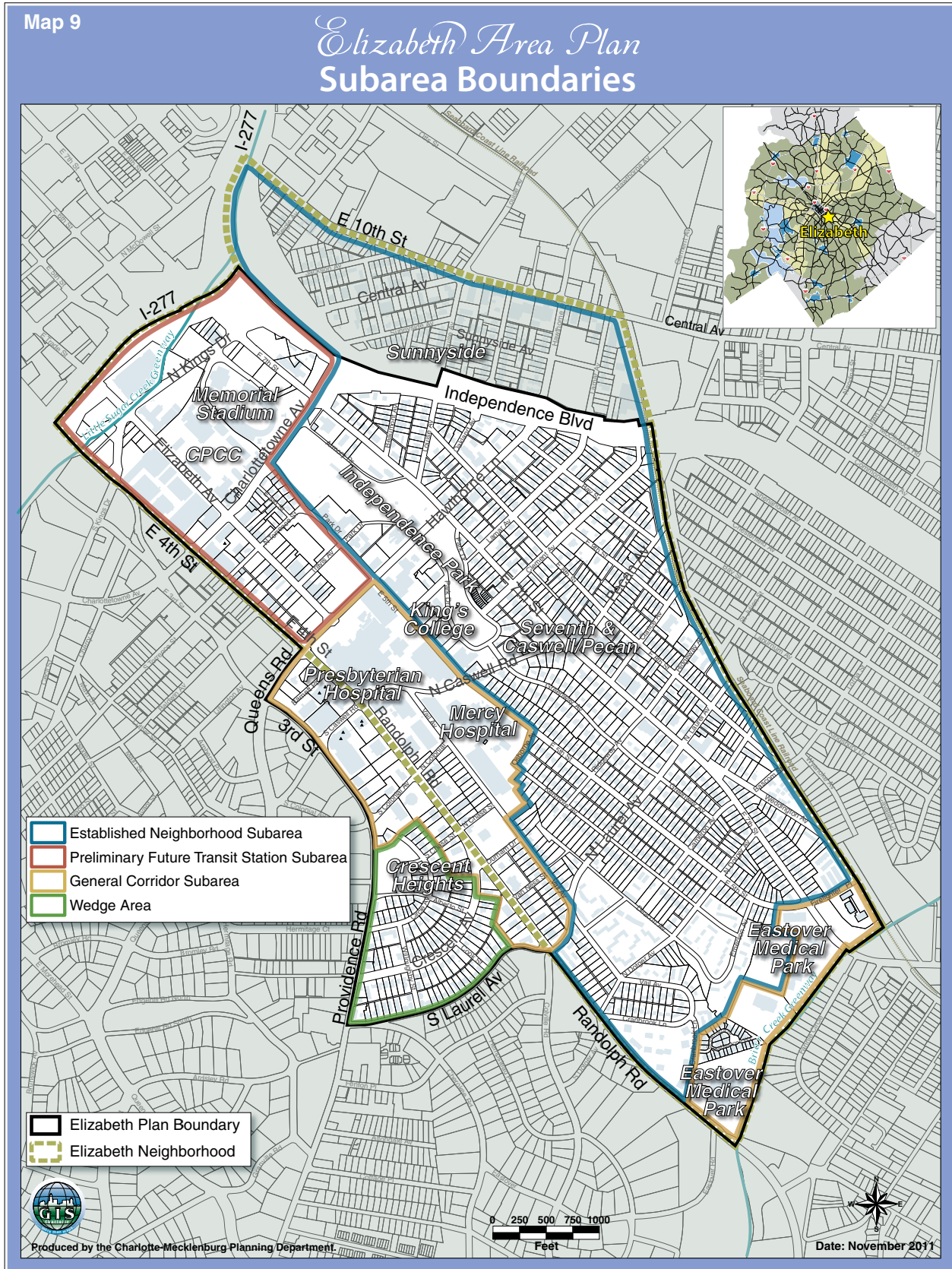
Map 8





## Other Implementation Strategies

Other implementation strategies will likely include transportation improvements and other capital projects.







# *Elizabeth Area Plan*

Part III: APPENDIX



## Part III: Appendix

The information contained in this Appendix was gathered throughout the planning process and was used to produce the *Elizabeth Area Plan*.

### Existing Conditions, Trends, and Forecasts

A thorough review and understanding of existing conditions – physical, economic, and social – was needed to identify the opportunities and constraints facing the plan area, and to develop the plan vision, goals, and policies. The following discussion highlights the existing conditions, trends, and forecasts most significant to the plan area.

#### Demographics

The 2010 U.S. Census indicated that 3,401 people reside in the Elizabeth plan area; of those, the majority (88%) are Caucasian, with about 8% being African-American. Hispanic residents comprise just two percent of the area's population. As of the 2000 Census, there are more females (55%) than males. Twelve percent of the population is under age 18, and senior citizens (65+) comprise 10% of the total.

The Elizabeth plan area has seen a slight decrease in population since the 2000 Census. The 2010 population for the area is 3,401, which is an average annual decrease of less than one quarter of one percent.

The 2000 Census also provides the following information:

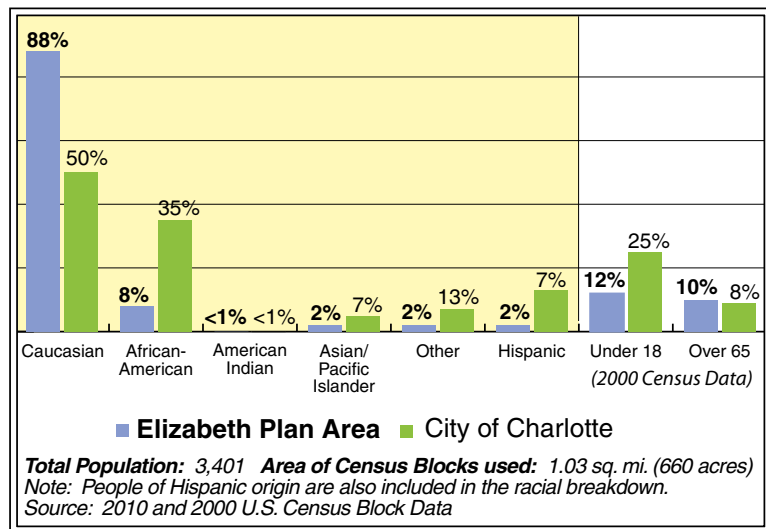
- The median household income of the plan area was \$46,281 (1999 income data). This was slightly less than the median household income for the City of Charlotte as a whole, \$46,975.
- A majority (53%) of the households in the area were single-person households.
- The average household size in the plan area was 1.71 persons.
- About 66% of area homes were valued at more than \$200,000 dollars. The median home value was \$230,100; this was

#### Population Trends

Geographic Area	1980 Census data	1990 Census data	2000 Census data	2010 Census data
Elizabeth Plan Area	3,594	3,789 +5.4%	3,484 -8.0%	3,401 -2.38%
City of Charlotte	314,447	395,934 +25.9%	540,828 +36.6%	731,424 +35.24%
Mecklenburg County	404,270	511,433 +26.5%	695,454 +36.0%	919,628 +32.23%

Source: U.S. Census Data

#### Population



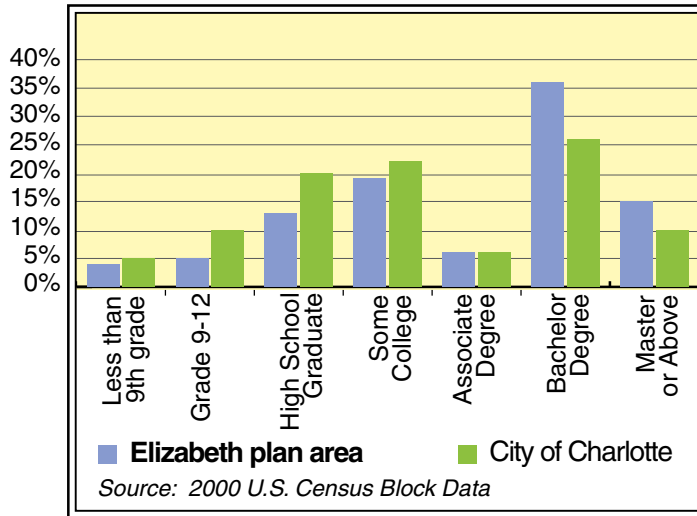
significantly greater than the median home value for the City which stood at \$134,300.

- Residents of the plan area were generally more well-educated than residents city-wide, as illustrated in the following table.

## Elizabeth Area Plan

- More area residents worked in the management/professional category (49%) than in any other. This was followed by residents employed in sales/office work (30%) and the service sector (11%). The total number of jobs held by study area residents was 2,601.

**Highest Attained Educational Level**



**Employment**

Occupational Group	Elizabeth Plan Area		City of Charlotte
	Pop.	%	%
Management/Professional	1,286	49%	38%
Service	275	11%	13%
Sales/Office	771	30%	29%
Construction/Maintenance	158	6%	8%
Production	111	4%	12%
<b>TOTAL</b>	<b>2,601</b>	<b>100%</b>	<b>100%</b>

Source: 2000 U.S. Census

## Residential Building Permits

Nineteen new residential permits were issued in the plan area for single family detached dwellings from 2000 to 2010, whereas 332 multi-family units were permitted in the same period. Most of the new multifamily residential units are condominiums, the permits for which were issued in years 2006 and 2007. No residential apartment units have been constructed in the plan area in the past two decades.

There were 1,808 households in the plan area as of the 2000 U.S. Census. Of these, 65% were renters and 35% homeowners. Of the total households in the plan area, 69% were in multi-family housing and 31% in single family.

- As of 2008, there were 2,095 housing units in the plan area (see table on the following page).
- Of the 604 single family detached homes in the plan area, 569 (94%) were built prior to 1980.
- Of the 719 condominium units in the plan area, all but 251 (65%) were built after 1980.

**Residential Building Permits 2000 - 2010**

Year	Single Family Detached	All Attached Units	TOTAL
2000	0	2	2
2001	0	30	30
2002	2	0	2
2003	1	0	1
2004	2	33	35
2005	3	8	11
2006	2	183	185
2007	1	76	77
2008	5	0	5
2009	0	0	0
2010	3	0	3
<b>TOTAL</b>	<b>19</b>	<b>332</b>	<b>351</b>

Source: Charlotte-Mecklenburg Planning Department

## *Elizabeth Area Plan*



*Single Family Detached*



*Single Family Attached*



*Condominiums*



*Apartments*

### Existing Residential Units in Plan Area Through 2009

Year Built	Single Family Detached	Single Family Attached	Condo	Apartment	Grand Total
Before 1980	569	281	251	407	1,508
1980 - 1989	1	3	208	32	244
1990 - 1999	8	2	63	0	73
2000	30	16	18	0	37
2001	1	0	2	0	3
2002	0	208	8	0	28
2003	1	7	0	0	8
2004	8	3	33	0	44
2005	6	0	0	0	6
2006	0	0	8	0	8
2007	3	0	98	0	101
2008	4	1	30	0	35
2009	0	0	0	0	0
<b>TOTAL</b>	<b>604</b>	<b>333</b>	<b>719</b>	<b>439</b>	<b>2,095</b>
<i>Source: Charlotte-Mecklenburg Planning Department</i>					

## Existing Zoning

There are a total of 629.55 zoned acres in the study area. This land area is larger than the land use inventory because zoning calculations include all roads, railroads, and utility rights-of-way. Land zoned for single family residential uses dominates the plan area with over 180 acres, or 28.72% (see **Map 10**). Multi-family and office are the next most frequent zoning categories as show in the table below.

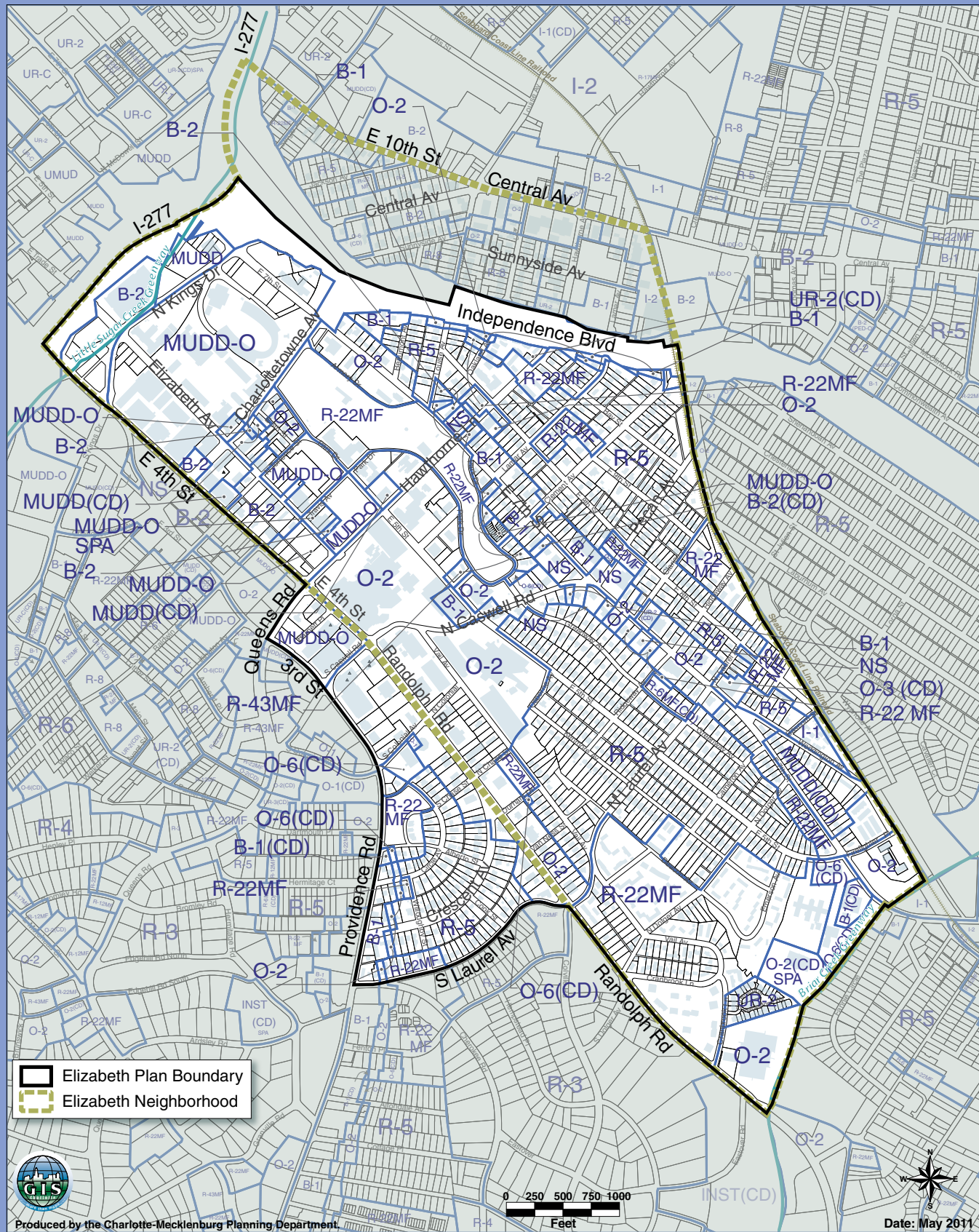
### Existing Zoning for the Plan Area

Zoning Category	Acres	% of Total
Single Family	180.79	28.72%
Multi-family	134.80	21.41%
Urban Residential	6.19	0.98%
Retail	54.80	8.71%
Office	149.83	23.80%
Mixed Use	98.93	15.71%
Industrial	4.21	.67%
<b>TOTAL</b>	<b>629.55</b>	<b>100%</b>
<i>Source: Charlotte-Mecklenburg Planning Department</i>		



# Elizabeth Area Plan Existing Zoning

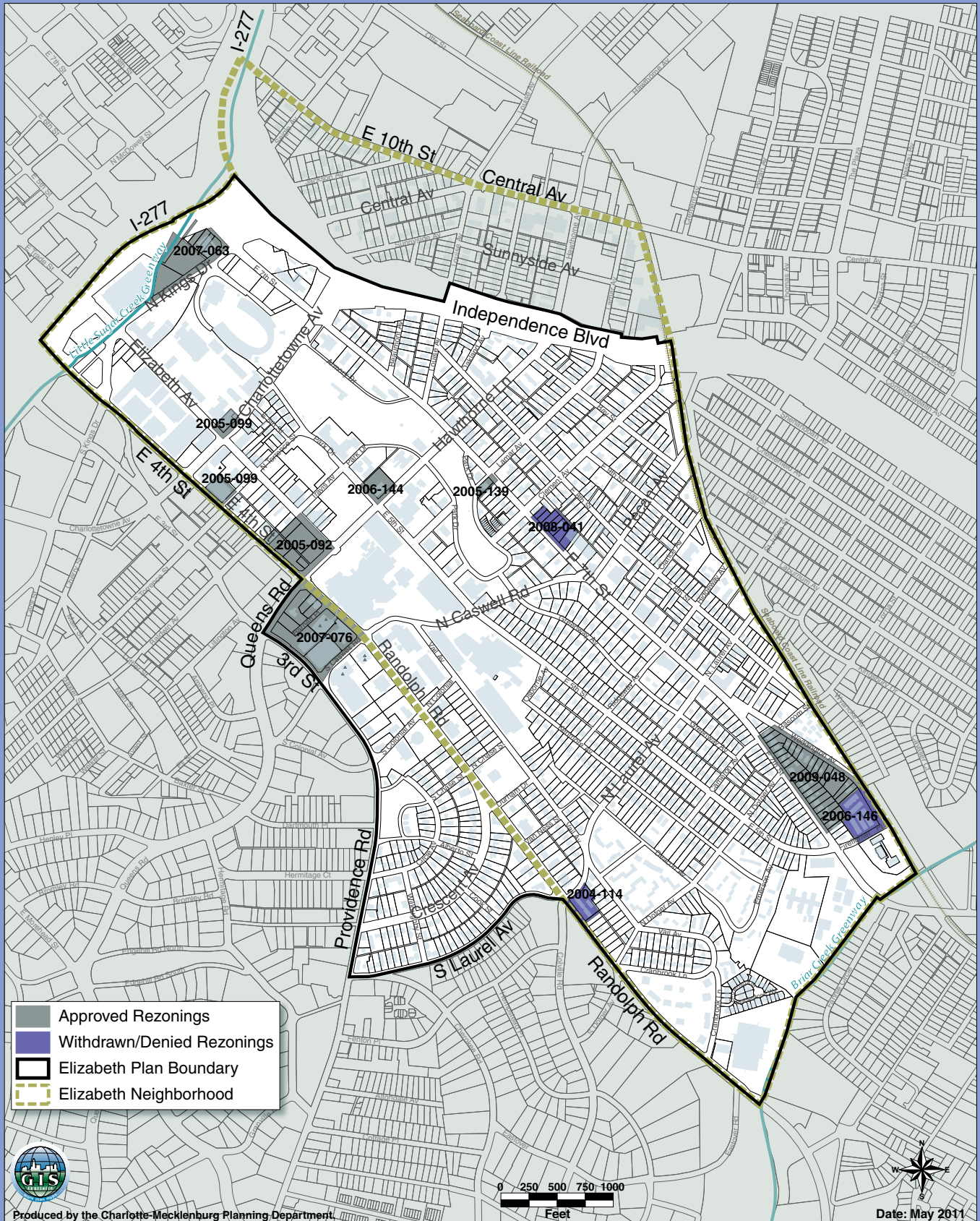
Map 10





# Elizabeth Area Plan

## Rezoning Petitions: 2004 - 2010



## Rezoning

Several rezoning petitions were filed from 2004 through 2010. (See **Map 11**)

### Approved (2004-2010)

Petition Number/ Name	Acres	Former Zoning	Requested Zoning	Action	Development Proposed
<b>2005-92</b> Providence Road Land Partners, LLC	3.07	B-2	MUDD-O	Approved 2-19-07	188,000 SF of commercial floor space plus 300 residential units
<b>2005-99</b> Central Piedmont Community College	1.86	B-2	MUDD-O	Approved 7-18-05	194,545 SF of expansion comprising two buildings on separate parcels
<b>2005-139</b> Rob Gottfried	0.25	B-1	MUDD-O	Approved 12-19-05	863 SF expansion of restaurant and reduction in parking ratio
<b>2006-144</b> B & E Properties	0.90	O-2	MUDD-(CD)	Approved 12-18-06	New 20,000 SF medical office building
<b>2007-63</b> Central Piedmont Community College	4.00	B-2 & O-2	MUDD	Approved 6-18-07	To accommodate future expansion and maintain consistency in zoning for campus
<b>2007-76</b> Novant Health, Inc.	6.40	O-2	MUDD-O	Approved 4-21-08	Up to 775,000 SF of additional hospital expansion plus parking deck
<b>2009-48</b> Winter Elizabeth, LLC	6.87	R-22MF	MUDD-(CD)	Approved 1-19-10	Up to 366 units of new residential development in two structures

### Denied or Withdrawn (2004-2010)

<b>2004-114</b> The Boulevard 2000, LLC	0.97	R-22MF	UR-3 (CD)	Denied 3-19-05	48 new residential units
<b>2006-146</b> Gateway Homes, LLC	2.10	R-22MF	UR-2 (CD)	Withdrawn	45 new residential units
<b>2008-041</b> The Boulevard Company	1.48	B-1	MUDD	Withdrawn	Mixed-use development

### Zoning Classifications in the Table

**B-1 & B-2** Business

**O-2** Office

**R-22MF** Multi-Family Residential zoning with a maximum of 22 dwelling units per acre

**MUDD** Mixed Use Development District - can have one or more land use types on the same site (retail, office, residential)

**UR** Urban Residential

**O** Optional (petitioner requests to opt out of one or more zoning requirements). Site plan submission required.

**CD** Conditional Zoning District (allows particular uses to be established only in accordance with specific standards and conditions pertaining to each individual development project). Site plan submission required.



## Land Development and Community Design

Elizabeth has a diversity of land uses including established residential areas, large institutions including Presbyterian and CMC-Mercy hospitals and Central Piedmont Community College (CPCC), and concentrations of commercial and office uses.

### Residential

Elizabeth is primarily an older residential neighborhood. There are several established single and multi-family residential units in the plan area. Much of the housing stock dates to the early 1900s, and there is a preponderance of historic homes throughout. Many of the single family homes in the neighborhood are in the bungalow style, single story and of a modest size and scale. The area also has some Victorian, Colonial Revival, and Tudor Revival style houses, especially in the southeast (Crescent Heights) part of the plan area.

There are almost as many multi-family units in the neighborhood as single family, and these are interspersed with the single family homes resulting in a vibrant yet cohesive residential fabric. The multi-family units typically comprise two and three story townhouse-style brick and/or stucco dwellings, and generally blend well with their surroundings. Infill and redevelopment has occurred throughout the neighborhood over the years, contributing to the variety and mix of housing types and architecture. Most of the new development has respected the scale and style of the existing residences.

The area's residential neighborhoods are characterized by a mature tree canopy. Typically, streets and public paths are tree-lined and well-landscaped. Most of the area's single family housing stock is within walking distance of public transit, shops, restaurants, and parks.

### Non-Residential

#### Retail

Retail development within the plan area is concentrated along East Seventh Street



*This newer multi-family development is compatible in scale and massing with surrounding buildings on Hawthorne Lane.*



*This East Fifth Street home is typical of many residential areas in Elizabeth. The streetscape features a planting strip with mature trees and a sidewalk.*



*An example of a commercial use, in this case a restaurant on East Seventh Street, being housed in a former residential building.*

## Elizabeth Area Plan



*Suburban style retail in a single-story building with surface parking between the street and the front of the store.*



*Elizabeth Traditional Elementary School.*



*One of the many houses of worship in the Elizabeth area.*

and Elizabeth Avenue. East Seventh Street, between Hawthorne Lane and Laurel Avenue, is characterized by a mix of offices, restaurants, and neighborhood-serving retail uses. Many of these are housed in residential structures that have been adapted for retail and office use, which creates a unique “Main Street” urban feel and helps preserve Elizabeth’s historic character. Most other retail uses along Seventh are located in suburban-style single-story buildings and shopping plazas. Elizabeth Avenue between Charlottetowne Avenue and Hawthorne Lane links Central Piedmont Community College (CPCC) and Presbyterian Hospital with a mixture of retail and office uses in one- to three-story buildings. Medical offices and other office uses dominate the Randolph Road area. A smaller concentration of retail uses is found along Providence Road.

### Office

Medical offices are the predominant office type in the plan area, many of which are associated with nearby hospitals. These medical office uses are primarily found along Third Street/Providence Road and Fourth Street/Randolph Road. There are also other professional office uses on Elizabeth Avenue and East Seventh Street.

### Industrial

The sole industrial/warehouse use in the Elizabeth area is the Queen City Lumber yard, at the intersection of Bascom Street and Weddington Avenue, adjacent to the railroad tracks.

### Civic

The Elizabeth plan area consists of several large regional-scale civic and institutional uses such as schools, hospitals, and churches. Presbyterian Hospital and Carolinas Medical Center-Mercy occupy significant tracts of land within the plan area between East Third and East Fifth Street, south of Hawthorne Lane. There are also several churches in the Independence Park and Hawthorne Lane areas. Central Piedmont Community College (CPCC), Kings College, and the Queens University/Presbyterian School of Nursing are the primary post-secondary educational institutions located within the plan area’s boundary. Elizabeth Traditional Elementary School is located at 1601 Park Drive.



## Public Facilities

### Public Services

There are no fire stations, police stations, or post offices located within the plan boundary. Elizabeth is served by the following:

#### **Charlotte Fire Department -**

- Fire Station 1, in Uptown Charlotte at 221 North Myers Street, houses Engine 1 and Ladder 1.
- Fire Station 6 is located at 249 South Laurel Avenue. Housing Engine Company 6, it opened in 1929 and is listed in the National Register of Historical Places.
- Fire Station 8, opened in 1948, is located at Commonwealth Avenue and The Plaza and houses Engines 8 and 64.

**Charlotte-Mecklenburg Police Department** - The Elizabeth plan area is protected by two separate patrol divisions of the Charlotte-Mecklenburg Police Department. The northern third of the area is covered by the Central Division, with offices at East Seventh Street and North College Street. The southern two-thirds of the plan area is within the CMPD's Providence patrol division, headquartered off North Wendover Road near Monroe Road.

**Post Offices** - Elizabeth is in close proximity to the Downtown Charlotte Post Office on North McDowell Street, the Midwood Post Office on The Plaza, and the Central Avenue Post Office.

### Institutions

**Presbyterian Hospital** is a private, non-profit regional medical center, one of the largest health care institutions in the Carolinas. It is the flagship hospital of Presbyterian Healthcare.

Located at 2001 Vail Avenue, **CMC-Mercy Hospital** is an adult acute care facility that provides a full-range of services, including emergency room care, surgery, and rehabilitation.

The **Central Piedmont Community College (CPCC)** Central Campus is located at 1201 Elizabeth Avenue. It is the oldest and largest campus in the CPCC system. CPCC's enrollment at the central campus is approximately 13,000, all



*Fire Station 6*



*Fire Station 8*



*Presbyterian Hospital*



## Elizabeth Area Plan



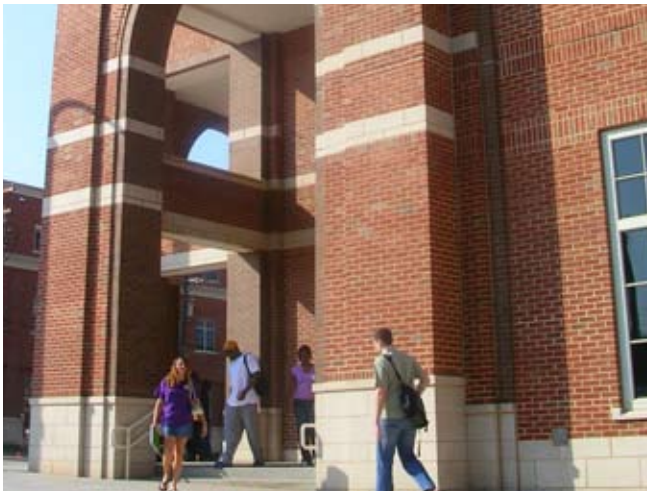
*Carolinas Medical Center - Mercy Hospital*

non-resident, and the school is in the process of adding new buildings to its expanding campus to meet projected needs.

**Kings College** is a private career college located at 322 Lamar Avenue. It offers diplomas and associate degrees in ten major areas of study. About 135 of its 500 enrolled students live in the college's on-campus housing.

**Queens University/Presbyterian School of Nursing** is the largest private producer of new RNs in North Carolina. Operated in partnership with Presbyterian Hospital, the school has 22 full-time faculty and 500 students.

### Parks and Greenways



*Central Piedmont Community College*

**Independence Park** was developed in 1907 and was the first public park built in Charlotte. It is a 24-acre facility that features a baseball field, two basketball courts, two tennis courts, a volleyball court, walking trails, a playground, two picnic shelters and a stone shelter that includes a reflecting pond and rose garden wedding site. Pedestrian connectivity and usability on the eastern end of the park has been lacking for some time and will be addressed as part of a 10 year vision for future projects, which Mecklenburg County Park and Recreation is developing. The resulting plan will evaluate historical context, current and future needs of the surrounding neighborhood, and ways to improve the overall appearance of the park.

**Colonial Park** is a 3-acre park located on Providence Road in the Crescent Heights section of the plan area. It is a neighborhood-serving park that features a multi-purpose field, basketball court, two tennis courts, a playground, and a walking trail.



*Independence Park's amenities include walking paths and picnic shelters.*

The Elizabeth section of the **Little Sugar Creek Greenway** is currently under construction from East Seventh Street to Morehead Street. At a length of 1.1 miles, the greenway features a linear park and stream restoration. The design will provide space for events, passive recreation and educational opportunities, public art, fountains and water features. The full length of the Little Sugar Creek Greenway is expected to open by the end of 2011.

## Existing Land Use

There are a total of 500.12 acres (excluding streets and rights-of-way) within the plan area. Single family residential land uses comprise over 26% of the total acreage. Another 19% of the plan area is used for multi-family residential land uses and over 20% for institutional land uses. Existing land uses are shown on **Map 12**.



Single Family



Multi-Family



Institutional



Vacant

### Existing Land Use for the Plan Area

Land Use	Acres	% of Total
Single Family	129.40	26%
Institutional <sup>1</sup>	98.81	20%
Multi-family	92.53	19%
Office <sup>2</sup>	71.15	14%
Park/ Open Space	34.77	7%
Parking	29.82	6%
Retail	18.33	4%
Vacant	16.32	3%
Warehouse/ Distribution	4.51	1%
Utility	4.47	1%
<b>TOTAL</b>	<b>500.12</b>	<b>100%</b>
<sup>1</sup> Institutional includes Hospital <sup>2</sup> Office includes Medical Office Source: Charlotte-Mecklenburg Planning Department		



Office



Park/Open Space



Medical Office



Warehouse/Distribution



Hospital



Parking



Utility

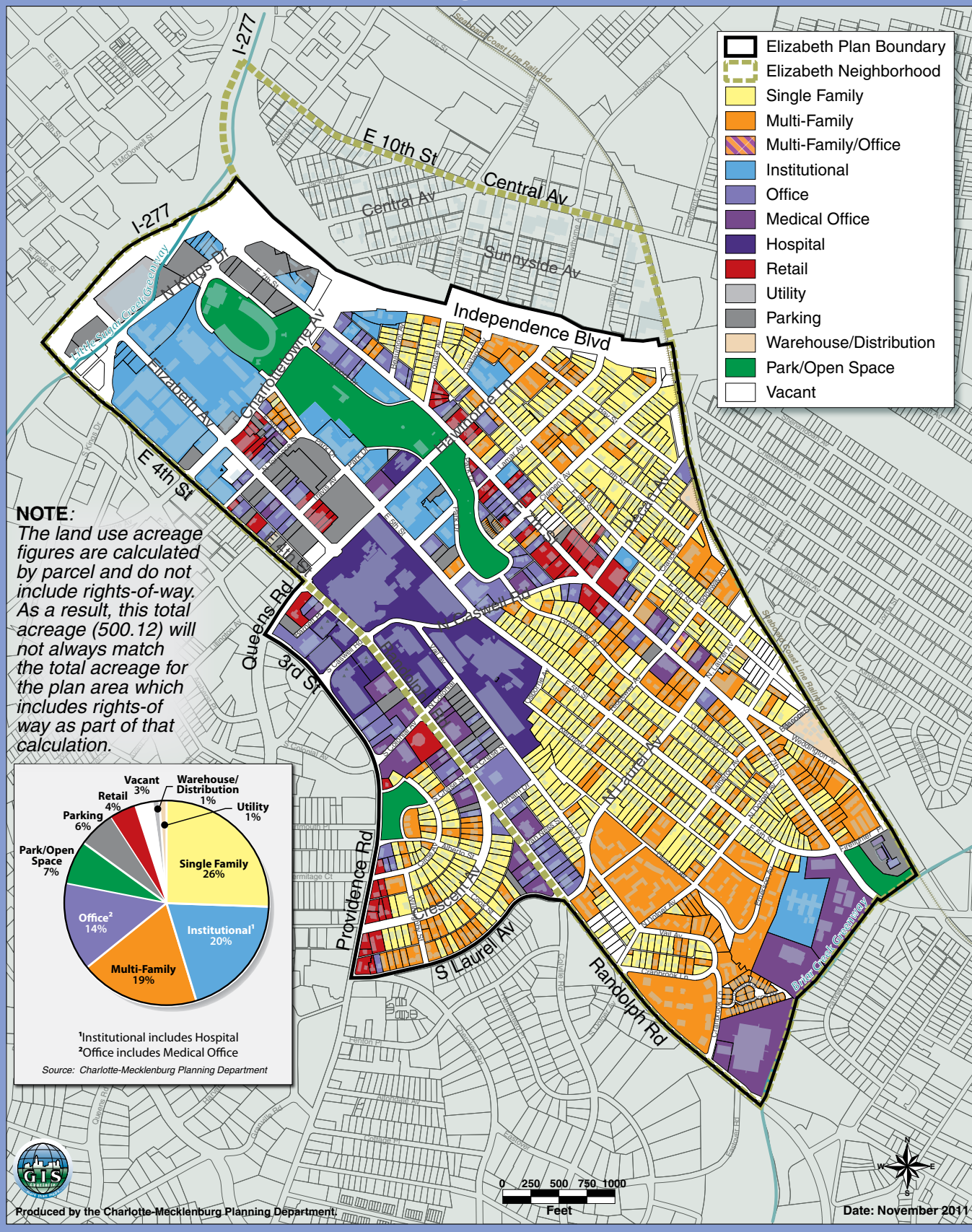


Retail



# Elizabeth Area Plan Existing Land Use

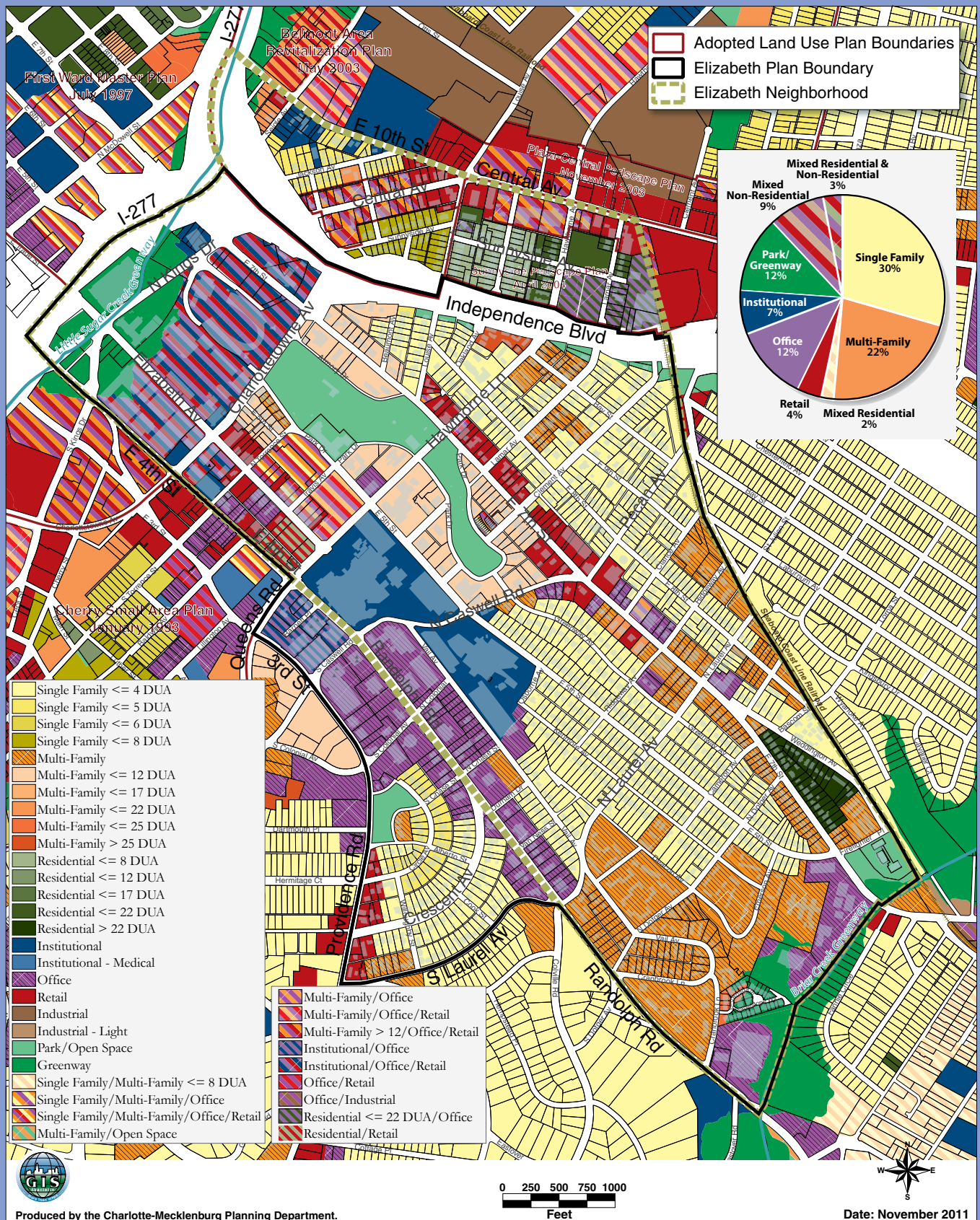
Map 12





# Elizabeth Area Plan

## Central District Plan Adopted Future Land Use



## Adopted Future Land Use

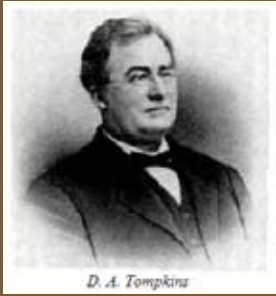
The Central District Plan, adopted in 1993 and updated by subsequent approved rezonings and area plans, provides the land use guidance for the study area (**Map 13**). In that plan, a majority of the area is recommended to remain single family residential, with retail and office uses concentrated along Randolph Road and Seventh Street. Multi-family residential uses are also recommended along Seventh Street. Presbyterian and Mercy hospitals are institutional uses that predominate in the Hawthorne Lane/Caswell Road area. Central Piedmont Community College is the predominant institutional use in the area around Elizabeth Avenue between North Kings Drive and Charlottetowne Avenue. Independence Park is recognized as a park use between Charlottetowne Avenue, East Seventh Street, North Caswell Road, and Park Drive.

**Adopted Land Use for the Plan Area**

Land Use	Acres	% of Total
Single Family	148.80	29.74%
Multi-family	111.68	22.32%
Mixed Residential	9.43	1.89%
Retail	19.84	3.97%
Office	59.27	11.85%
Institutional	33.53	6.70%
Park/Greenway	59.58	11.91%
Mixed Non-Residential	44.25	8.84%
Mixed Residential and Non-Residential	13.93	2.78%
<b>TOTAL</b>	<b>500.32</b>	<b>100%</b>
<i>Source: Charlotte-Mecklenburg Planning Department (2010)</i>		

**Non-Residential Square Footage in the Plan Area Through 2009**

Non-Residential Category	Heated Sq Ft	% of Total
Industrial	16,151	0.20%
Institutional	3,354,759	42.51%
Office	1,633,696	20.70%
Retail	209,741	2.66%
Parking Garages/Decks	2,575,998	32.64%
Utility	2,044	0.03%
Warehouse	99,706	1.26%
<b>TOTAL</b>	<b>7,892,095</b>	<b>100%</b>
<i>Source: Mecklenburg County Tax Parcel Data</i>		



## Independence Park

*Daniel Augustus Tompkins (1851-1914), noted industrialist, founder of the Charlotte Observer and spokesman for the principles and policies of the New South movement, was responsible for the establishment of Independence Park, the first public park in Charlotte.*



## Historic Landmarks

The Elizabeth neighborhood, like Charlotte's other streetcar suburbs, has a number of designated historic landmarks. Most date from the early twentieth century and include residences, churches, public facilities, a park, and a stadium. These historic landmarks provide a valued sense of place, as well as being an important reference and point of context for subsequent development within the Elizabeth neighborhood.

### Legal Protections for Independence Park and Memorial Stadium

Independence Park and American Legion Memorial Stadium each enjoy legal protections that may effectively prevent certain types of development or land use changes that are inconsistent with the policies and recommendations of this area plan. These protections include reverter clauses associated with land donated to the City of Charlotte for present-day Independence Park, and the designation of the park and stadium as local historic landmarks:

#### 1) Reverter Clauses

The following language is from the reverter clause from one of the deeds for land that is now a part of Independence Park. It is typical of reverter clauses associated with other parcels that form part of the park. This language is from a deed dated August 1, 1904 by and between the Board of Water Commissioners of the City of Charlotte and the City of Charlotte – filed January 20, 1905 and registered

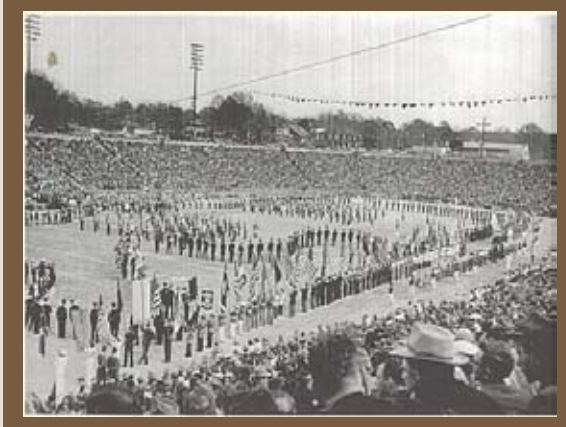
January 25, 1905 - Book 198 Page 204.

*"...This land is granted and conveyed to the grantee herein as and for a public park ...and it is to be held, used, and maintained by the grantee and its successors for that purpose and none other and whenever the grantee herein or its successors cease to keep and maintain said property for the purposes aforesaid same is at once to revert to the grantor herein its successors and assigns. "*



## American Legion Memorial Stadium

Ground was broken on the stadium in 1934 and the gates were officially opened two years later in 1936. Named in honor of local soldiers who fell in World War I, the stadium was a project of the Works Progress Administration.



### 2) Historic Landmark Designations Clauses

#### **City of Charlotte Ordinance No. 1044-X (1981)**

Ordinance Book 31 – Page 34

An ordinance designating Independence Park as a historic property.

<http://landmarkscommission.org/IndepParkp1.htm>

Paragraph 2 of the ordinance stipulates:

*“That said designated property may be materially altered, restored, moved or demolished only following the issuance of a certificate of appropriateness by the Charlotte-Mecklenburg Historic Commission.”*

#### **City of Charlotte Ordinance No. 2623-X (2004)**

Ordinance Book 52 – Page 870

An ordinance designating as a Historic Landmark a property known as the “American Legion Memorial Stadium” (listed under Tax Parcel number 08017101 as of January 1, 2004, and including the interior and exterior of the buildings, the structures, the landscaping and the parcel of land...but not including the building known as the “Grady Cole Center”).

<http://landmarkscommission.org/AmericanLegionp1.htm>

Paragraph 3 of the ordinance stipulates:

*“That said designated historic landmark may be materially altered, restored, moved or demolished only following issuance of a Certificate of Appropriateness by the Charlotte-Mecklenburg Historic Landmarks Commission.”*

## *Elizabeth Area Plan*



**5** Hawthorne Lane United Methodist Church



**7** Jane J. (Jennie) Alexander Duplex



**9** John Baxter & Mary Mott Alexander House



**12** Thaddeus Awasaw & Emma Ford Adams House



**17** Myers Park Streetcar Waiting Station

★ Designated Historic Landmarks Key, Original Owner's Name & Location of Property		Year Built
1	Rev. George H. & Mattie Griffin Detwiler House 801 Sunnyside Avenue	1911
2	Jake F. & Frances Black Newell House 819 Sunnyside Avenue	1911
3	Independence Park Armory and Park Drives	1906
4	John Paul and Alice Craft Lucas House 1601 East Seventh Street	1913
5	Hawthorne Lane United Methodist Church 501 Hawthorne Lane	1916
6	Dr. Baxter & Caroline Brevard-Golden Moore House 1701 East Eight Street	1910
7	Jane J. (Jennie) Alexander Duplex 1801-1803 East Eight Street	1922
8	Harry Arthur & Nancy Dilling Ziem House 1812 East Eighth Street	1910
9	John Baxter & Mary Mott Alexander House 509 Clement Avenue	1913
10	Walter Lamar & Ernestine Bridges Alexander House 523 Clement Ave. (one of 2 homes on property)	1915
11	Walter Lamar & Ernestine Bridges Alexander House 521 Clement Ave. (one of 2 homes on property)	1915
12	Thaddeus Awasaw & Emma Ford Adams House 604 Clement Avenue	1908
13	S. Bryce McLaughlin House 2027 Greenway Avenue	1911
14	Trolley Walk corner of Clarice Avenue and East Seventh Street	1913
15	American Legion Memorial Stadium Sam Ryburn Walk & East Seventh Street	1936
16	Richard C. & Laura Eisfeld Biberstein House 1600 Elizabeth Avenue	1906
17	Myers Park Streetcar Waiting Station corner of East Fourth Street & Queens Road	1912
18	William Henry & Mary Irwin Belk House 200 Hawthorne Lane	1924
19	Ratcliffe-Otterbourg House 2100 Randolph Road	1925
20	Palmer Fire School 2601 East Seventh Street	1940
21	Charles Walter & Louise Anthony Parker House 901 Central Avenue	1903
Source: Historic Landmarks Commission		



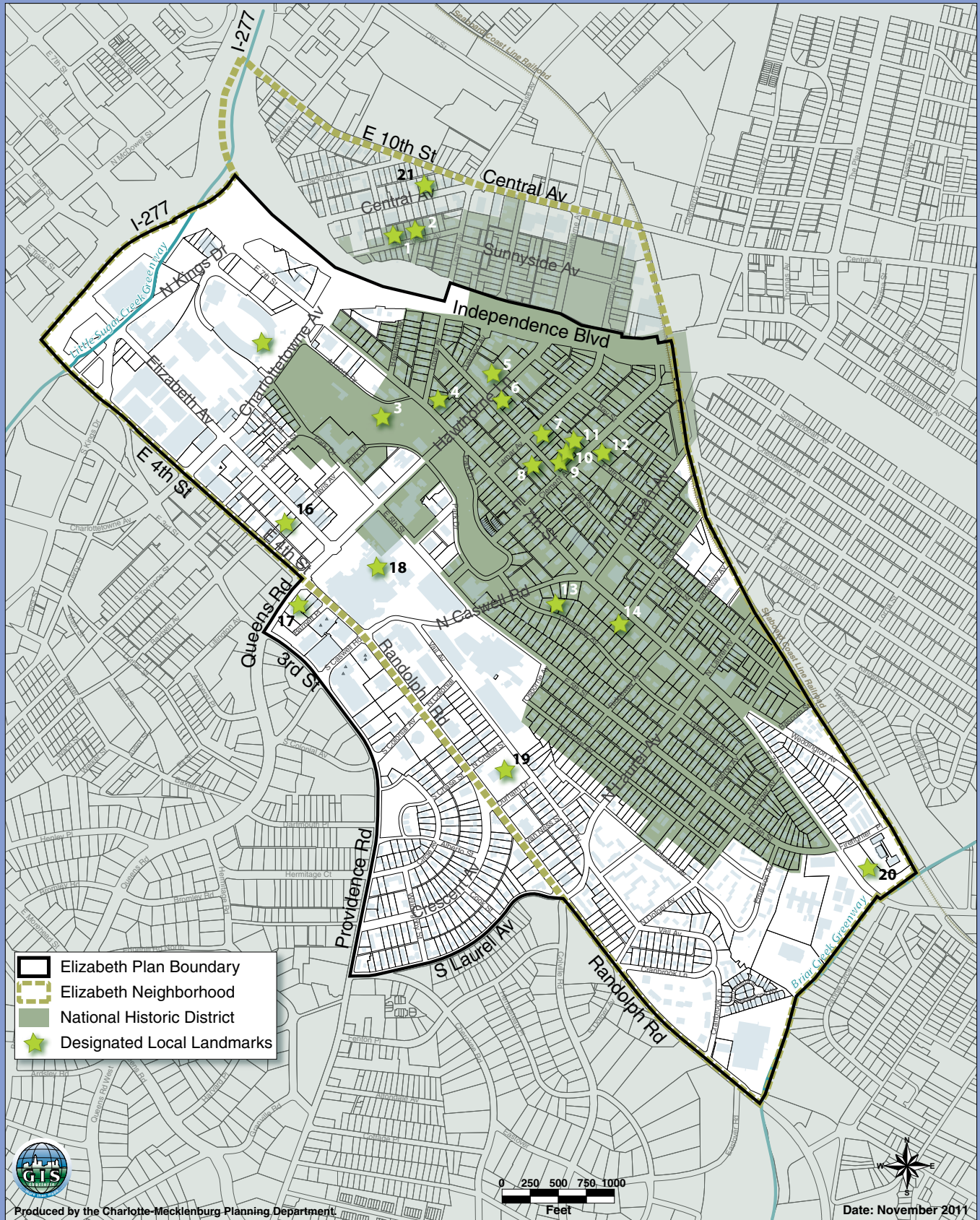
**18** William Henry & Mary Irwin Belk House



**20** Palmer Fire School



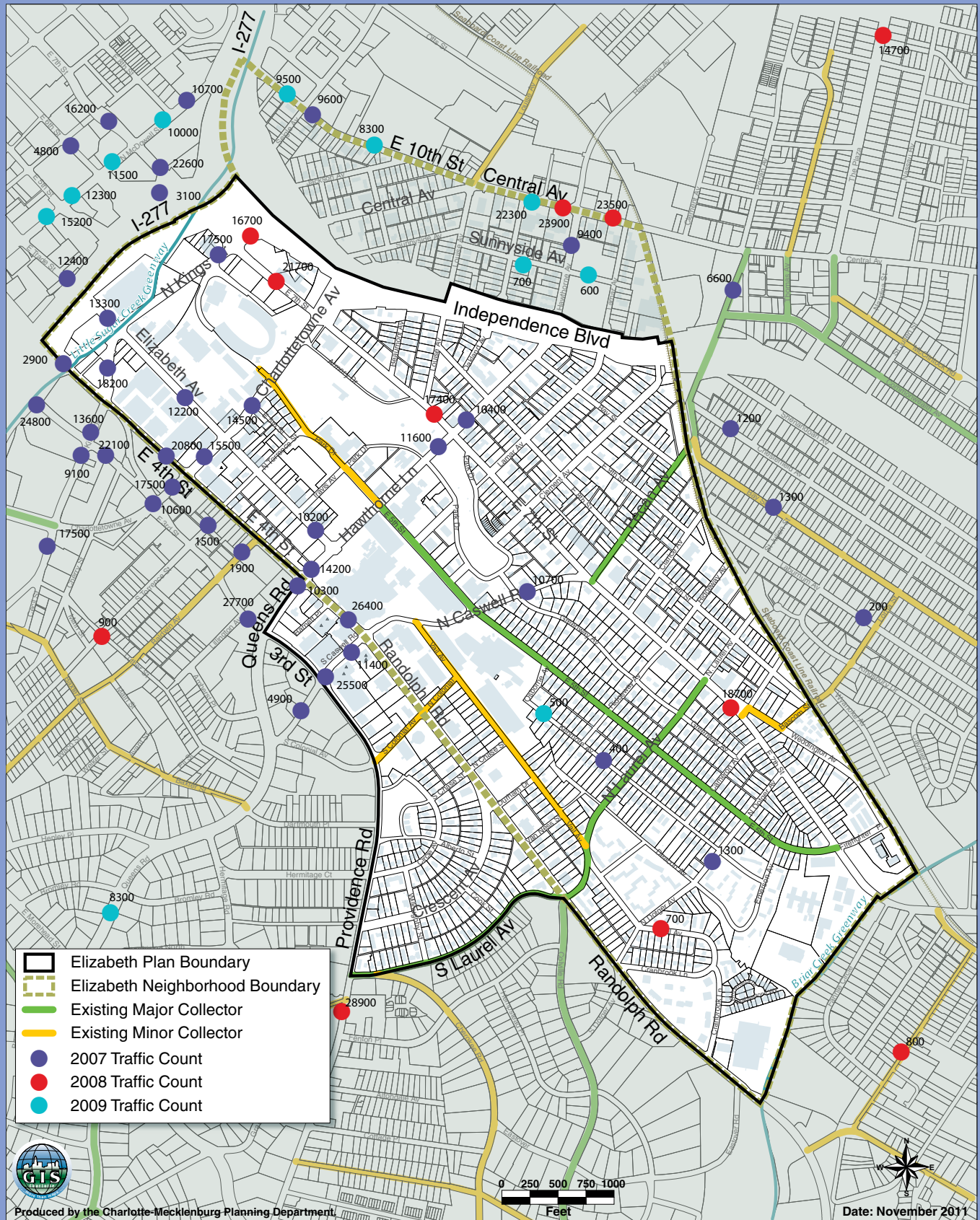
# National Historic District & Designated Local Landmarks





# Elizabeth Area Plan

## Existing Traffic Counts and Street Classifications



## Transportation

### Street Network

The Elizabeth Area has strong land use accessibility and a relatively high degree of connectivity. Elizabeth area residents benefit from being in close proximity to a mixture of land uses that enable them to live, work, play, socialize, and shop within a short distance of home. In addition, the relatively well-connected street network shortens travel distances for all transportation users and improves overall transportation system capacity by providing area residents and visitors with more route choices.

Route choices are measured by the number of lane-miles of streets, number of intersections (nodes), number of blocks (links), and the connectivity index. A lane-mile is one mile of a single roadway lane. The more lane-miles of streets there are, the greater the overall traffic carrying capacity. The connectivity index quantifies how well a roadway network connects destinations and is calculated by dividing the number of roadway links by the number of roadway nodes. A connectivity index of 1.45 or more is desirable for transit station areas and in-town neighborhoods, such as Elizabeth.

Key measures of the Elizabeth plan area include:

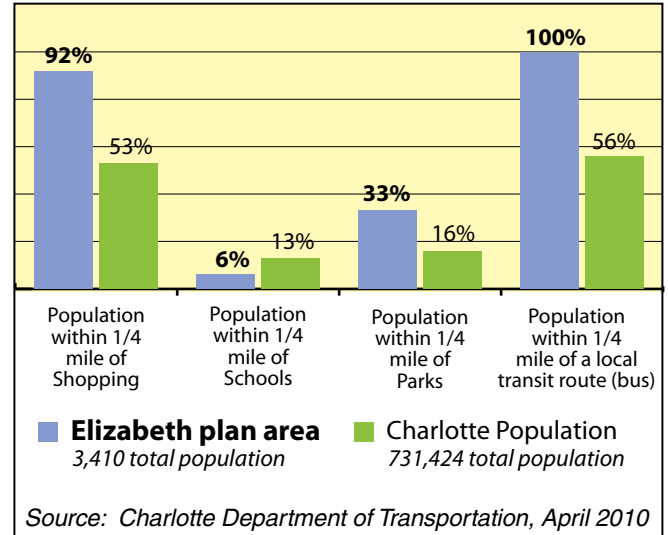
Miles of streets:	23
Lane-mile of streets:	62
Connectivity Index:	1.46

As shown in the table at right, the Elizabeth Area currently receives high marks relating to land use accessibility relative to the city average, with the major exception being accessibility to schools.

### Thoroughfare Plan

The *Mecklenburg-Union Thoroughfare Plan* is the adopted major roadway plan for Mecklenburg and Union counties and is used to assure that the most appropriate street system is developed to meet existing and future travel needs. Streets are classified and designed according to their intended functions so that land use and traffic conflicts are

### Land Use Accessibility



minimized. The street classifications applicable to the Elizabeth Area Plan are as follows:

**Thoroughfares:** Providence Road, Randolph Road, East Third Street, East Fourth Street, Kings Drive, Hawthorne Lane, Elizabeth Avenue, and East Seventh Street are major thoroughfares serving the Elizabeth area. As major thoroughfares, these streets are designed to accommodate large volumes of traffic at moderate speeds and provide access to major commercial, employment, and residential land uses. Charlottetowne Avenue and Caswell Road are minor thoroughfares that collect traffic from the local collector streets and carry it to the major thoroughfares. Minor thoroughfares are designed to carry moderate traffic volumes at moderate speeds and provide access to residential, commercial, and institutional land uses.

**Collectors and Locals:** Pecan Avenue, East Fifth Street, Laurel Avenue, Colonial Avenue, Vail Avenue, and Park Drive are major collectors. Bascom Street, Colonial Avenue, and Vail Avenue are minor collectors. Collectors are designed to carry traffic between the thoroughfares and local streets at moderate volumes and speeds and provide access to adjacent land uses. The remaining roadways are local streets that carry low traffic volumes, have slow operating speeds, and provide access to individual properties.

## Level of Service

Level of Service (LOS) measures the quality of service provided by a transportation facility to its users. The City of Charlotte evaluates LOS for pedestrians, bicyclists, and motorists and uses the results to help balance their competing needs when planning and designing streets. LOS ranges from A through F, with desirable levels of service based on the street typologies of the *Urban Street Design Guidelines*.

Pedestrian and bicycle LOS is rated according to the level of comfort and safety offered by the design features at signalized intersections, while motor vehicle LOS is based on motorist delays. Motor vehicle quality of service is also measured by the Volume to Capacity ratio (V/C), which describes an intersection's ability to process traffic within peak time periods. Values greater than .95 suggest an intersection is near its capacity during this time period and that motorists may experience substantial congestion.

**Pedestrian System:** Of the 23 miles of streets within the Elizabeth Area, 66% have sidewalks on both sides of the street, 8% have sidewalk on one side, and 14% have no sidewalk at all. There are 20 signalized intersections in the area, six of which have a desirable LOS for pedestrians (A or B).

**Bicycle System:** Bicycle lanes exist on Elizabeth Avenue and Hawthorne Lane. Of the 20 signalized intersections within the plan area, none have desirable LOS for cyclists.

**Motor Vehicle Network:** The Elizabeth area is currently experiencing moderate levels of congestion as indicated by the number of signalized intersections in the plan area with an undesirable LOS (E or F), as shown in the table. Congestion levels are expected to remain the same or slightly increase as the area and its greater surroundings grow in travel demand.

**Existing Level of Service (LOS)**

No.	Signalized Intersection	Pedestrian	Bicyclist	Motorist			
		LOS <sup>1</sup>	LOS <sup>1</sup>	V/C <sup>2</sup> (AM)	LOS <sup>1</sup> (AM)	V/C <sup>2</sup> (PM)	LOS <sup>1</sup> (PM)
1	Fourth Street & Kings Drive	C+	D+	1.30	F	0.92	F
2	Elizabeth Avenue & Kings Drive	D+	E	0.69	C	0.66	C
3	Seventh Street & Kings Drive/Central Avenue	D	D	0.93	D	0.97	D
4	Fourth Street & Charlottetowne Avenue	C	D	0.82	B	0.55	B
5	Elizabeth Avenue & Charlottetowne Avenue	C+	D	0.47	B	0.50	B
6	Seventh Street & Charlottetowne Avenue	D	E	0.81	C	0.83	C
7	Third Street & Queens Road	D+	E	0.82	B	0.80	D
8	Fourth Street & Queens Road/Hawthorne Lane	C	D-	0.97	D	0.96	D
9	Elizabeth Avenue & Hawthorne Lane	C	D-	0.77	B	0.67	B
10	Fifth Street & Hawthorne Lane	B	D	0.45	B	0.35	B
11	Seventh Street & Hawthorne Lane	B	E	0.60	B	0.76	C
12	Third Street/Providence Road & Caswell Road	B-	E	0.67	B	1.00	C
13	Fourth Street/Randolph Road & Caswell Road	C+	E	0.73	B	0.64	B
14	Fifth Street & Caswell Road	B	E	0.81	D	0.44	B
15	Seventh Street & Pecan Avenue/Caswell Road	B	D	0.95	D	1.06	D
16	Providence Road & Cherokee Road/South Laurel	C+	D	0.85	C	0.95	C
17	Randolph Road & Laurel Avenue/Colville Road	B	E	1.05	F	0.98	E
18	Seventh Street & Fifth Street/Firefighter Place	C	D+	0.60	B	0.87	D

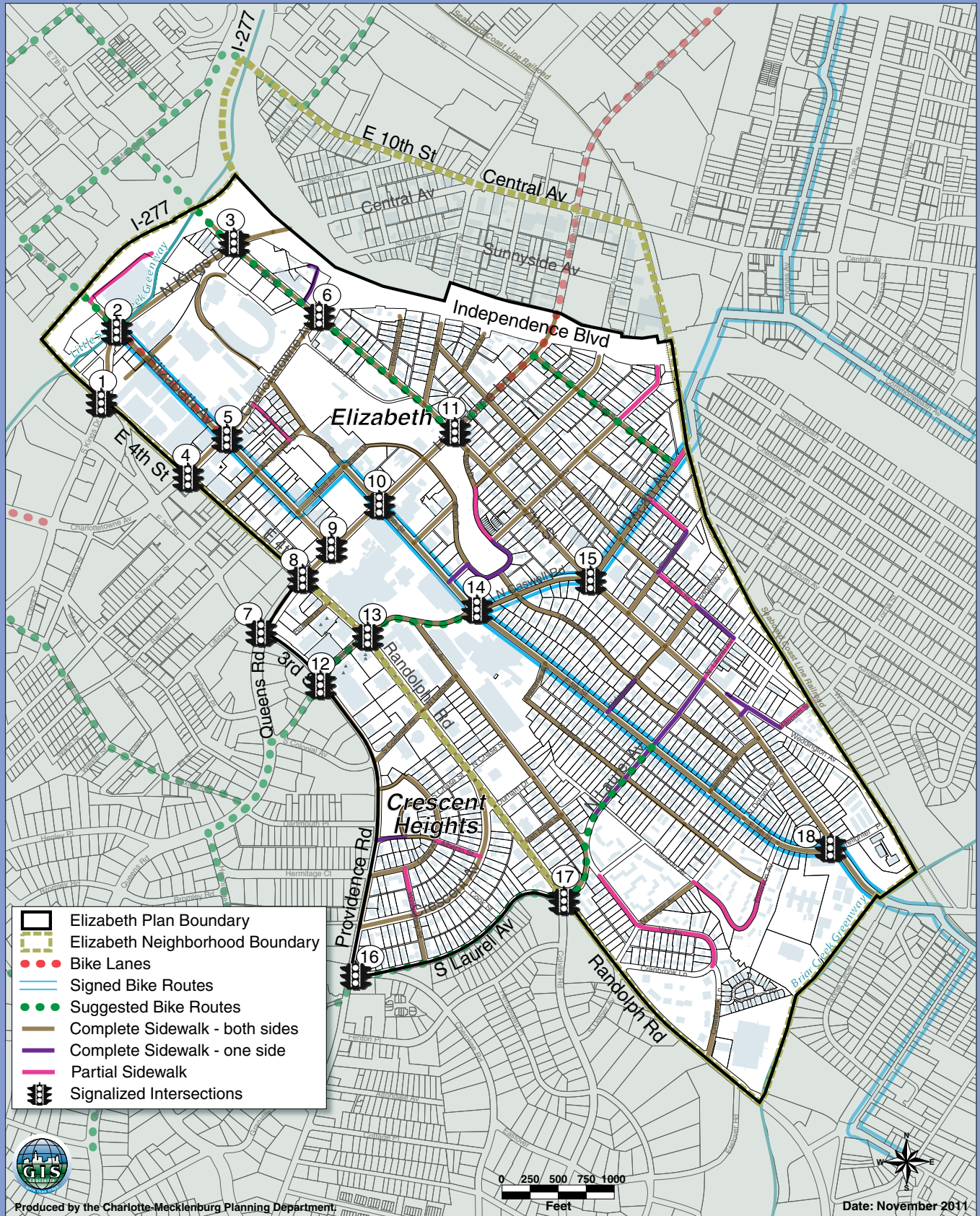
<sup>1</sup>LOS - Level of Service    <sup>2</sup>V/C - Volume/Capacity    Source: Charlotte Department of Transportation, 2010



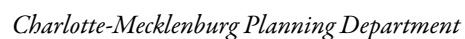
# Elizabeth Area Plan

Map 16

## Existing Pedestrian & Bicycle Facilities & Signalized Intersection LOS







## Existing Transit Services

### Fixed-Route Bus Services

The Charlotte Area Transit System (CATS) currently provides the following fixed-route bus service in the Elizabeth Area Community. All routes have at least one active bus stop within the plan area. See **Map 17, Existing Transit**, page 108.

- **Route 9:** Central Avenue operates to/from Uptown primarily along Central Avenue.
- **Route 14:** Providence Road operates to/from Uptown primarily along Providence Road. This route serves CPCC and Presbyterian Hospital.
- **Route 15:** Randolph Road operates to/from Uptown primarily along Randolph Road. The route serves CPCC, Presbyterian Hospital, and Mercy Hospital.
- **Route 17:** Commonwealth operates to/from Uptown along Trade Street, Kings Drive, Central Avenue, Commonwealth Avenue, and Independence Boulevard.
- **Route 20:** Sharon Road operates to/from Uptown along Trade Street, Queens Road, Sharon Road and Park Road.
- **Route 25:** Clanton Road/Midtown operates bi-directionally along West Boulevard, Clanton Road, Manchester Drive, Barringer Drive, East Boulevard, Scott and Kenilworth Avenue, Charlottetowne Avenue, Third and Fourth Streets, and Hawthorne Lane.
- **Route 27:** Monroe Road operates to/from Uptown along Trade Street, Seventh Street and Monroe Road.
- **Route 39:** Eastway Drive operates to/from Uptown along Trade Street, Elizabeth Avenue, Hawthorne Lane, Central Avenue, Eastway Drive, and North Tryon Street.
- **Route 45x:** Carmel Road Express operates to/from Uptown primarily along Randolph and Carmel Roads.
- **Route 61x:** Arboretum Express operates to/from Uptown primarily along Providence Road.
- **Route 62x:** Rea Road Express operates to/from Uptown primarily along Providence Road, Old Providence Road, and Rea Road.

### Fixed Route Ridership:

In FY 2009, the routes serving the Elizabeth Area community carried an average of 404,703 passengers per month.

### **CATS Fiscal Year 2010 Average Monthly Ridership within the Elizabeth Plan Area**

Route	Route Name	Monthly Ridership <sup>1</sup>
9	Central Avenue	127,551
14	Providence Road	27,911
15	Randolph Road	47,774
17	Randolph Road	45,119
20	Sharon Road	14,158
25	Clanton Road	5,287
27	Monroe Road	66,963
39	UNCC / Uptown	35,492
45x	Carmel Road Express	2,448
61x	Arboretum Express	6,274
62x	Rea Road Express	3,596
<b>TOTALS</b>		<b>382,573</b>
<sup>1</sup> 12-Month Average Calculated with CATS Monthly Ridership Data Reports for FY2010. Source: Charlotte Area Transit System, 2010		



## East Seventh Street Conversion Analysis

### Overview

During development of the *Elizabeth Area Plan*, there was extensive discussion with the Citizens Advisory Group (CAG) about a “road-diet” concept (similar to East Boulevard) that would convert the center reversible lane to a formal two-way left-turn lane with pedestrian refuge islands. In response, CDOT analyzed various scenarios, trying to find a road-diet option that remained true to the community-supported concept of pedestrian islands, yet balanced corridor travel needs.

### Traffic Analysis

#### East Seventh Street

Alternate scenarios were developed that could convert the center reversible lane on East Seventh Street to a two-way left turn lane with intermittent pedestrian refuge islands. The graphic on page 111 illustrates the basic concept.

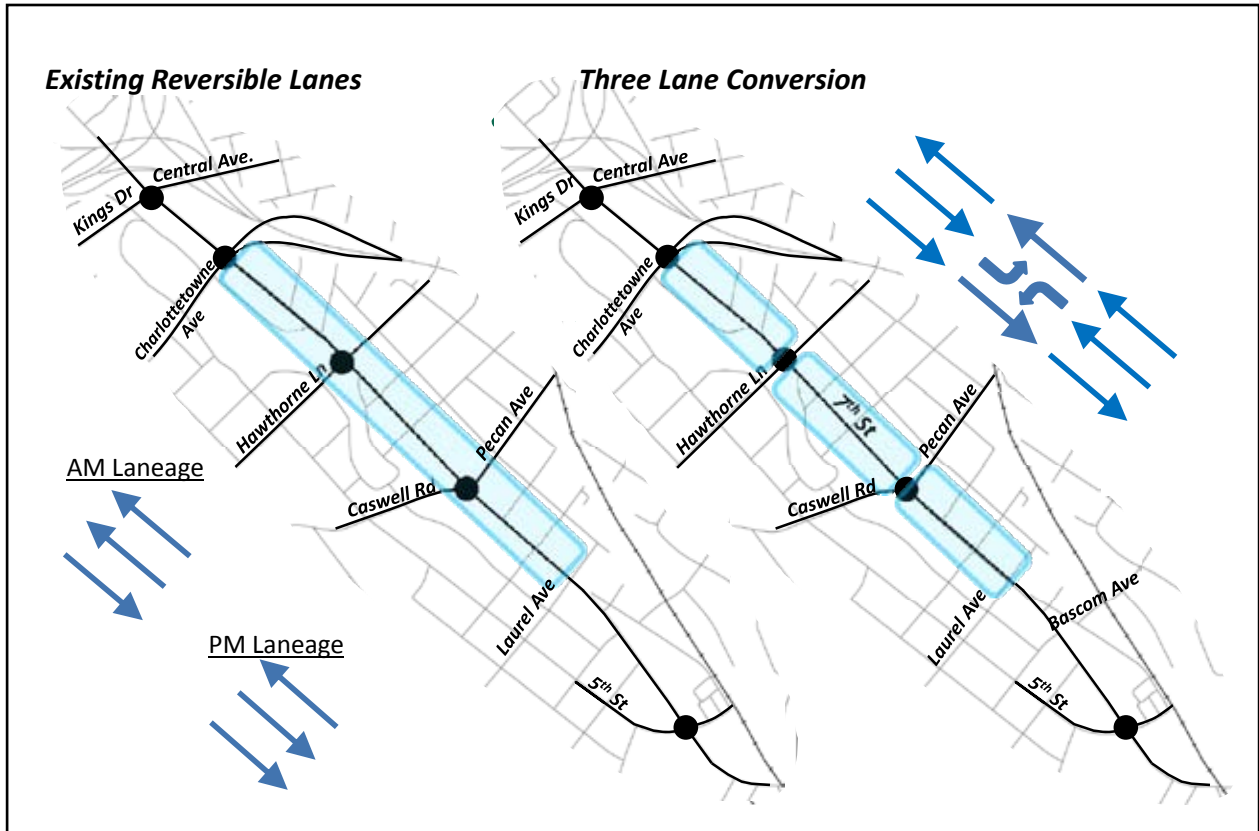


*As pedestrians stroll nearby, a car crosses East Seventh Street from North Caswell Road to Pecan Avenue. Split phase signal timing results in less green light time for vehicular traffic.*

Based on CDOT’s traffic analysis, an abrupt transition from two lanes to one lane in each direction at Charlottetowne Avenue or East Fifth Street appeared to cause traffic problems. At Charlottetowne Avenue, two lanes appear to be needed to process traffic through the signal, similar to how there are two lanes in each direction on East Boulevard across South Boulevard. At the other end of the corridor, a left-

lane drop to North Caswell Road appeared to be the more logical transition point than East Fifth Street, given existing traffic volumes and operations. Today, there is a heavy left-turn movement (one in three cars) from East Seventh Street to North Casewell Road.

### East Seventh Street: Alternate Scenarios



### Pecan/Caswell Intersection

Upon addressing transitions into and out of the “road-diet” segment, further traffic simulation showed the Pecan/Caswell intersection as the key chokepoint on the corridor. The unusual geometry of the intersection requires split-phase signal timing, where Pecan Avenue and Caswell Road each require exclusive green lights, thereby reducing the potential green time for East Seventh Street.

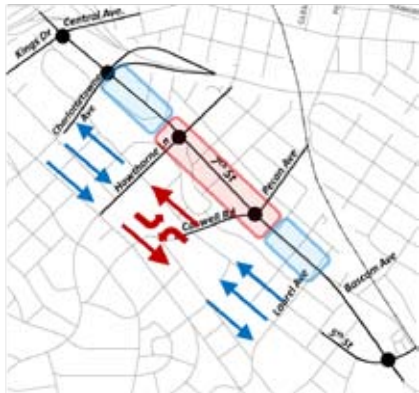
To address this challenging intersection, CDOT developed three new alternatives.

- **Alternative A** Eliminate left turns from Pecan and Caswell in order to eliminate split-phasing and thereby re-allocate savings in “green time” to East Seventh Street and process a single lane of outbound Seventh Street.
- **Alternative B** Partially widen East Seventh Street between Pecan and Laurel Avenues in order to process two outbound lanes of East Seventh Street (Note: This was shown to be possible asymmetrically on the northeast side of the street, where there are fewer existing mature trees, and outer lanes could be used outside of peak periods for parking).
- **Alternative C** Retain reversible lanes but focus on adding more crosswalks within the corridor.

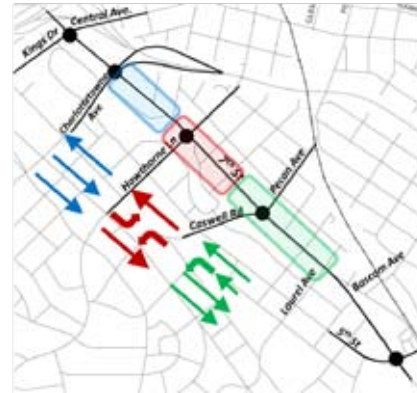


# Elizabeth Area Plan

**Alternative A**  
Eliminate Lefts on Pecan/Caswell



**Alternative B**  
Partial Widening of 7th Street



**Alternative C**  
Add Signalized Crosswalk



Consensus among the CAG was to move forward with Alternative A, but as two variations (“A-1” and “A-2”).

- **A-1** Change left-turn signal phase for inbound East Seventh Street to North Caswell Road from protected to permissive during PM peak period (eliminate green left arrow from East Seventh to Caswell, with left turn yield on flashing yellow arrow from 4-6 pm).
- **A-2** Change signal phasing of Caswell and Pecan from separate split-phasing to a single phase by restricting left turns onto East Seventh from Caswell and Pecan (prohibit lefts from side streets onto East Seventh).



The unusual geometry of the intersection of East Seventh Street, North Caswell Road, and Pecan Avenue.



Alternative A-2 would have restricted left turns from Pecan onto East Seventh, a maneuver which the vehicles in the Pecan Avenue left lane are preparing to make.



### **Key PM-Peak Performance Measures**

Below are tables summarizing the key afternoon peak-period congestion measures observed in traffic simulations of refined alternatives. *Essentially, any scenario processing a single lane of outbound East Seventh Street in the PM peak appears to result in increased congestion.*

**East Seventh Street: Corridor Travel Time (min : sec)**

Corridor Direction	Baseline	3-Lane Alt. Road-Diet	Alt. A-1	Alt. A-2
Outbound Seventh Street	4:18	5:43 (+1:25)	4:45 (+0:27)	4:09 (-0:09)
Inbound Seventh Street	5:44	3:53 (-1:51)	3:55 (-1:49)	3:52 (-1:52)
<i>Source: Charlotte Area Transit System</i>				

**East Seventh at Pecan/Caswell: Average Queue (feet)**

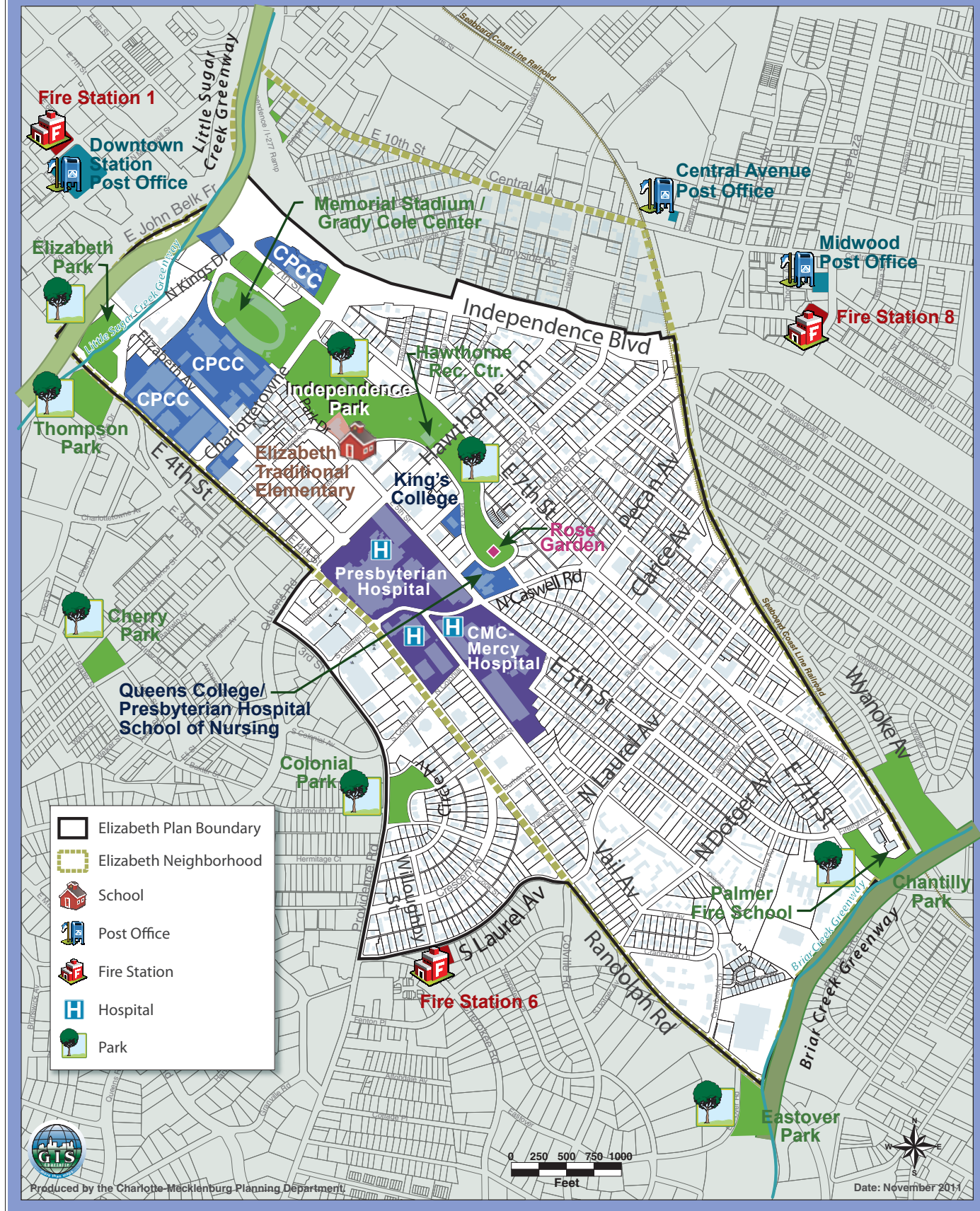
Intersection Approach	Baseline	Road-Diet	Alt. A-1	Alt. A-2
Outbound Seventh Street	101	892 (+791)	791 (+690)	671 (+570)
Inbound Seventh Street	559	47 (-512)	225 (-334)	125 (-434)
North Caswell Road	580	992 (+412)	1024 (+444)	996 (+416)
Pecan Avenue	245	295 (+50)	319 (+74)	45 (-200)
<i>Source: Charlotte Area Transit System</i>				

**East Seventh at Pecan/Caswell: Intersection Delay (seconds)**

Intersection Approach	Baseline	Road-Diet	Alt. A-1	Alt. A-2
Outbound Seventh Street	24.4	181.7	211.7	179.3
Inbound Seventh Street	60.7	67.9	66.3	53.9
North Caswell Road	81.3	206.0	222.2	35.6
Pecan Avenue	180.6	16.8	46.0	25.6
<b>Total</b>	66.2	86.3	97.3	62.8
<i>Source: Charlotte Area Transit System</i>				

Based on this analysis revealing significant degradation to peak period travel for motorists, a street conversion project cannot be supported at this time. However, the City remains open to re-evaluating whether East Seventh Street could undergo a street conversion to allow for crosswalks with pedestrian islands, should conditions change in the future that may make such a project more feasible.

# Elizabeth Area Plan Existing Public Facilities





## Infrastructure

### Public Utilities

#### Water/Sewer

The water distribution and sanitary sewer systems within the plan area consist of water and sewer mains of varying sizes. Charlotte-Mecklenburg Utilities (CMU) does not anticipate any water or sewer capacity issues. As changes occur and as densities increase, CMU will evaluate its system models to monitor any additional needs.

A sewer line replacement is proposed at Deacon Avenue, as are improvements to the trunk sewer along Briar Creek at the edge of the study area. Water main replacements are planned on Hawthorne Lane between Seventh and Eighth Streets, on Eighth Street between Clement Avenue and Hawthorne Lane, and at Seventh Street and Laurel Avenue.



*Tributary of Briar Creek as seen from North Dotger Avenue.*

## Environmental

### Topography

The topography of the Elizabeth plan area ranges from gently-sloping to relatively flat. The highest point in the area is near the intersection of Caswell Road and Vail Avenue, with an elevation of about 740' above sea level. From there, the northern and western sections of the plan area gradually slope toward Little Sugar Creek while the eastern and southern sections slope toward Briar Creek, following the general topography of the land from a "spine" running northeast-southwest at about the mid-point of the plan area. Little Sugar Creek and Briar Creek are the low points of the plan area at about 630' and 620' above sea level, respectively.

### Watershed and Creeks

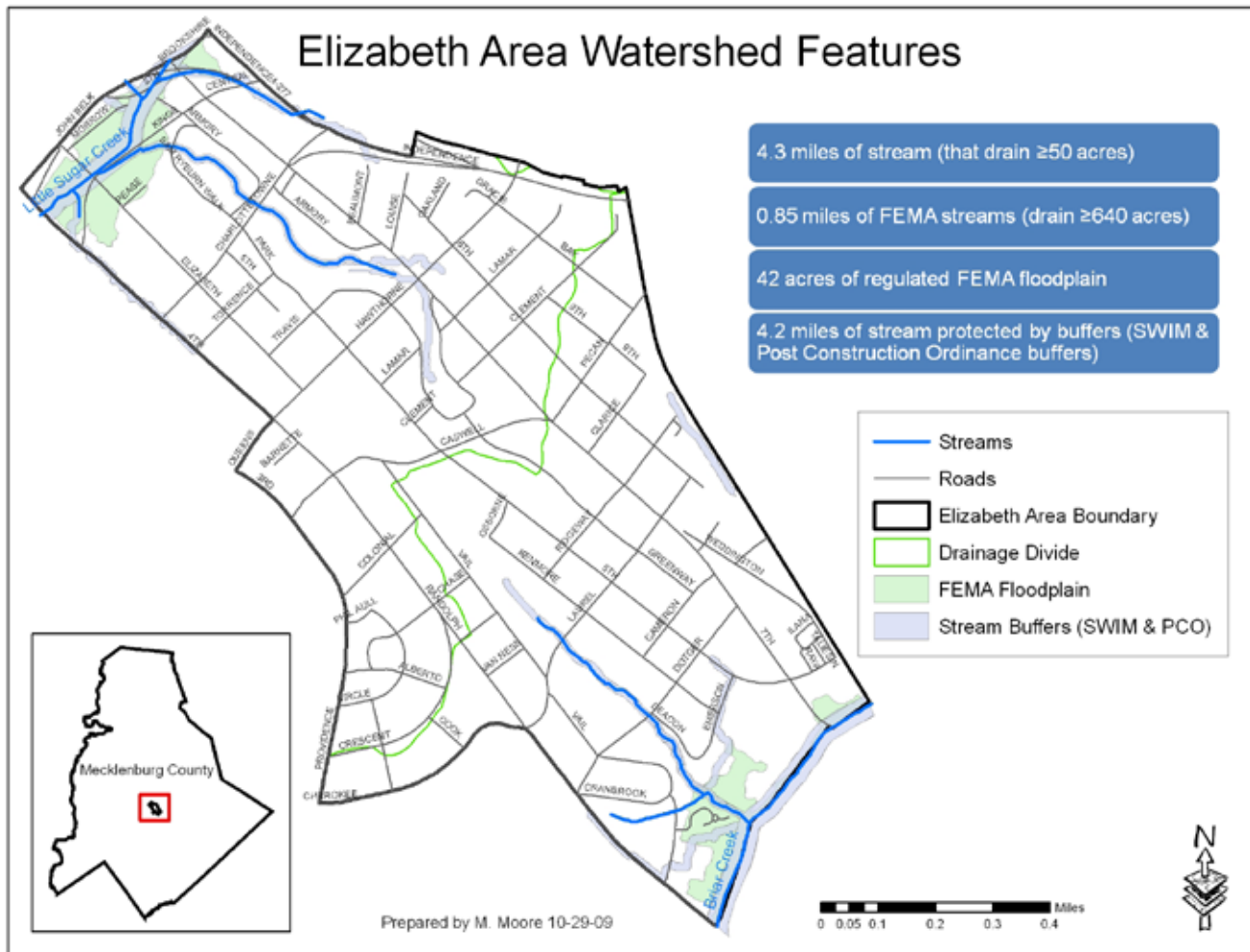
The Elizabeth plan area is located in the Catawba River watershed. Storm water from the Elizabeth area drains into Little Sugar Creek and Briar Creek, both listed as Class C waters. This classification denotes freshwaters protected for secondary recreation, fishing, wildlife, fish and aquatic life propagation and survival, and other uses, as defined by the State of North Carolina. Little Sugar Creek is listed as impaired by the State.

Important water features located in the Elizabeth area include: 4.3 miles of stream, 0.85 miles of FEMA streams, 42 acres of regulated FEMA floodplain, and 4.2 miles of protected stream buffers. These stream buffers include both Surface Water Improvement and Management (S.W.I.M.) and Post Construction Ordinance (PCO) buffers. Stream buffers are naturally vegetated areas of land along lakes and streams whose main function is to protect water quality.

### Groundwater

The plan area has had some reported groundwater contamination incidents, all of them associated with petroleum releases from either heating oil tanks or former gas stations. Most of them are centered around the East Seventh/Pecan/Caswell intersection and the Elizabeth Avenue corridor between Hawthorne Lane and Charlottetown Avenue.

## *Elizabeth Area Plan*



### Tree Canopy

Approximately 263 acres, or about 40% of the plan area, has trees as its most prominent land cover, as shown in **Map 19**. This is a lower percentage than the city of Charlotte as a whole (50%), and also lower than Mecklenburg County (46%). The greatest concentration of tree cover is in the predominantly residential portions in the eastern and southern areas of the plan area, and less in the institutional and commercial areas in the north and west.

In addition to their aesthetic value, trees provide other environmental benefits such as reducing air pollution, storing and sequestering carbon, reducing stormwater runoff, and improving water quality.

### Heritage Sites

There are no heritage sites within the plan boundary.

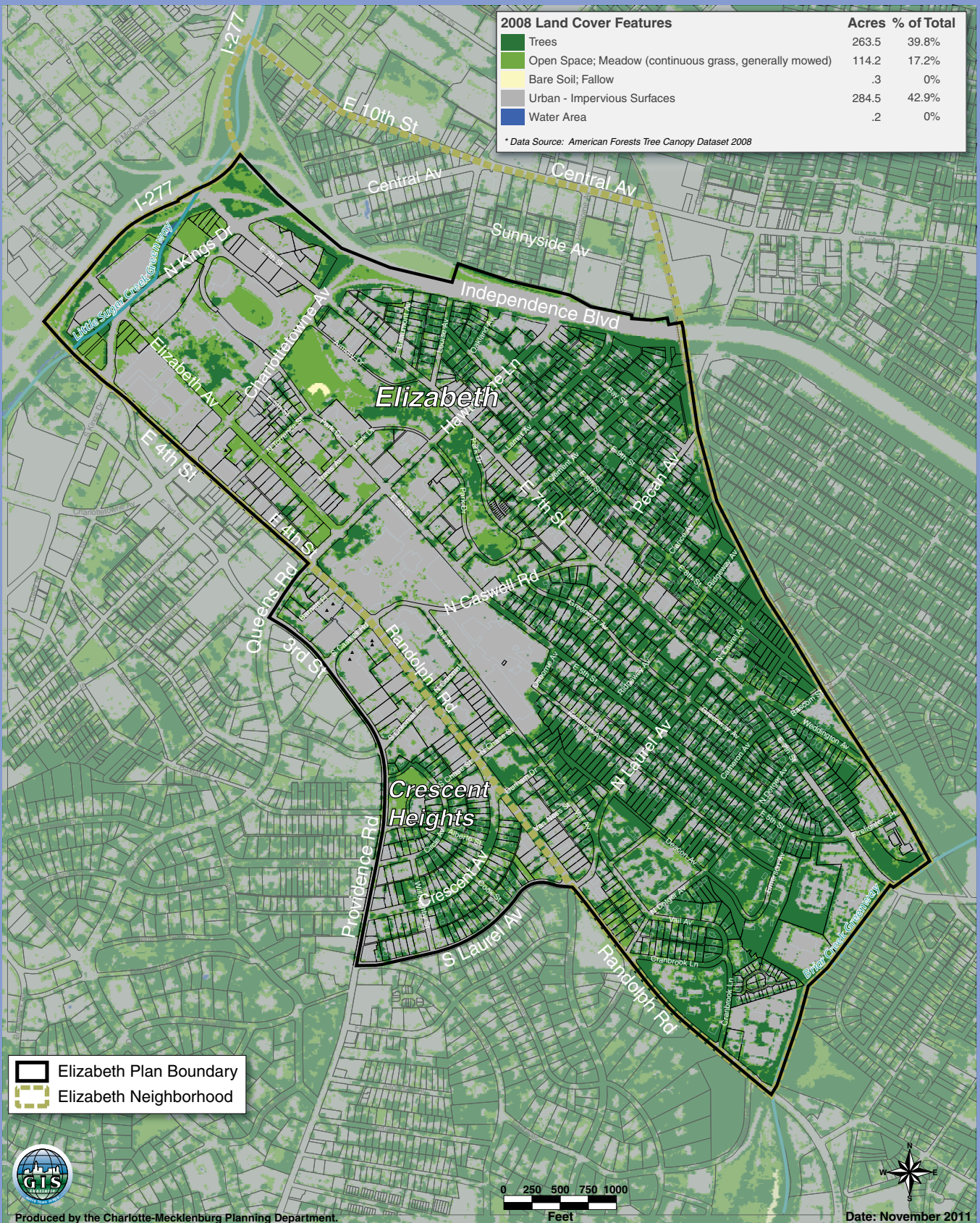


*Elizabeth's trees provide many benefits to the area.*



# Elizabeth Area Plan Land Cover

Map 19





[www.charlotteplanning.org](http://www.charlotteplanning.org)

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