

Rezoning Transportation Analysis

Petition Number: 2026-035

General Location Identifier: 04108110

From: Travis Miller, PE

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Reviewer:

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Revision Log:

Date	Description
6-25-26	First Review (IW)
7-23-26	Second Review (IW)
8-25-26	Third Review (IW) ZC

General Review Information

The petition is located adjacent to Beatties Ford Road, a City-maintained major arterial, south of Slater Road, a City-maintained major collector. The petition is located in a northwest wedge outside of Route 4, within the Westside Strategy Study Area.

Active Projects Near the Site:

- There are no known Transportation projects in the vicinity.

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located adjacent to Beatties Ford Road, a City-maintained major arterial, south of Slater Road, a City-maintained major collector. A Traffic Impact Study (TIS) is not required for the site due to the development generating site trips under ordinance thresholds and not meeting additional TIS triggers. Site plan and/or conditional note revisions are needed to label and dimension the back of curb in its final location per Charlotte Streets Map, labeling and dimensioning the 50-feet of right of way on Beatties Ford Road and updating conditional notes. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation			Source
			Daily Trips	AM Peak Hour Trips	PM Peak Hour Trips	
Existing Use	Vacant	-	-	-	-	<i>Tax Record</i>
Entitlement with Current Zoning	Single Family Detached Housing (N1-B, 5.96 acres)	23 DU	261	20	25	<i>General Guidance from Planning</i>
Proposed Zoning	Single Family Attached Housing (N2-A, 5.96 acres)	61 DU	444	26	33	Site Plan: 05-15-26
Proposed Zoning	Single Family Attached Housing (N2-A, 5.96 acres)	64 DU	437	28	34	<i>Site Plan: 07-13-26</i>

Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

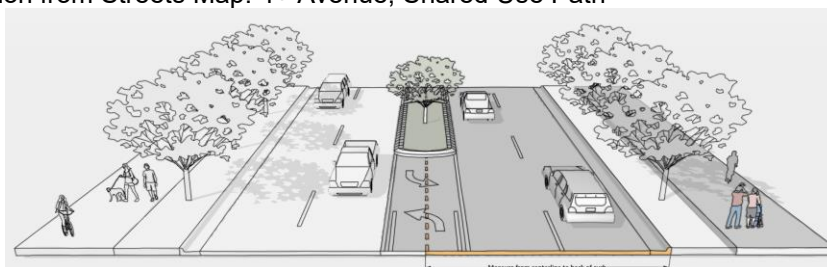
Outstanding Issues

Strikethrough = Resolved

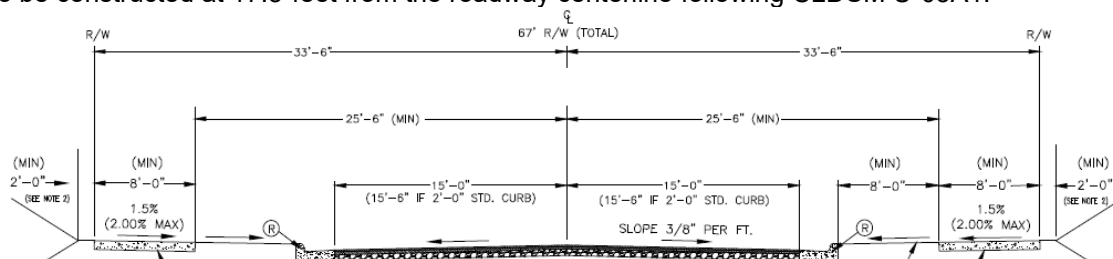
1. Curblines:

- Outstanding comment from 6/25/26: Beatties Ford Road:** Location of future back of curb and gutter is 30-feet from centerline.

Cross-section from Streets Map: 4+ Avenue, Shared Use Path



- Outstanding comment from 6/25/26: Public Street B:** Location of the future back of curb is to be constructed at 17.5-feet from the roadway centerline following CLDSM U-03A1.



Clarifying Comment 8/25/26: Label and dimension the curb and gutter from the centerline for each road on the site plan. Site plan shows right of way but doesn't have a call out.

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2. ~~Comprehensive Transportation Review (CTR):~~

~~A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating site trips under ordinance thresholds found in the Charlotte Streets manual Table 3.1 and/or not triggering any other City TIS requirements.~~

3. **Outstanding comment from 6/25/26:** Revise site plan and conditional note(s) to commit to dedicating 50-feet of right-of-way from the Beatties Ford Road centerline. **The site plan shall label and dimension the right-of-way from the road centerline.** Conditional Note “h” under Transportation should be updated to have specific dimension of right of way.

Clarifying Comment 8/25/26: The site plan needs to label and dimension right of way. Add dimensions from the roadway centerline to show the future back of curb location as well as the ROW. All infrastructure is to be placed in the final location according to Charlotte Streets Map.

- ~~4. Revise site plan and conditional note(s) to commit to construct bicycle facilities by installing a 12' multi-use path in accordance with the council-adopted Charlotte Streets Map. Multi-use paths maintained by the City shall meet ADA/PROWAG requirements.~~

- ~~5. Add a conditional note specifying “A Right of Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner’s/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.”~~

6. **Outstanding comment from 6/25/26:** Add a conditional note specifying “the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site’s first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2’ behind back of sidewalk where feasible.”

Outstanding Clarifying Comment 7/23/26: Remove “Public Street B” to alleviate any confusion about dedication of right of way. Right of Way dedication is required for all public streets.

f. THE PETITIONER WILL DEDICATE VIA FEE SIMPLE CONVEYANCE RIGHT-OF-WAY **FOR PUBLIC STREET B** AND ANY ADDITIONAL RIGHT-OF-WAY INDICATED ON THE REZONING PLAN AS RIGHT-OF-WAY TO BE DEDICATED. THE ADDITIONAL RIGHT-OF-WAY WILL BE DEDICATED AND CONVEYED PRIOR TO THE ISSUANCE OF THE FIRST CERTIFICATE OF OCCUPANCY. ADDITIONAL RIGHT-OF-WAY OR A PERMANENT SIDEWALK EASEMENT WILL BE LOCATED AT A MINIMUM OF TWO (2) FEET BEHIND THE SIDEWALK WHERE FEASIBLE.

- ~~7. Add a conditional note specifying “All transportation improvements will be approved and constructed before the site’s first building certificate of occupancy is issued.”~~

~~The petitioner may phase transportation improvements if said improvements and phasing are approved by CDOT and explicitly described in site plan notes.~~

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Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte Unified Development Ordinance.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and to comply with City of Charlotte Unified Development Ordinance regulations.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>