

Rezoning Transportation Analysis

Petition Number: 2026-012

General Location Identifier: 08106507

From: Jake Carpenter, PE

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Reviewer:

Brent Hyatt, PE

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Revision Log:

Date	Description
03-26-26	First Review (BH)

General Review Information

The petition is at the intersection of N Davidson St, a City-maintained arterial street, Parkwood Ave, a City-maintained arterial street, and Belmont Avenue a City-maintained collector street. The petition is located adjacent to Parkwood Ave, a City-maintained arterial street, and N Caldwell St, a City-maintained local street. The petition is located in the east wedge inside of Route 4 and is in the Community Activity Center place type of the 2040 Policy Map. The petition is also located in the adopted Northeast Inner Community Area plan.

Active Projects Near the Site:

- CIP - Parkwood Ave Streetscape
 - External Project Description: Construct separated bicycle lanes, SUP, lighting, street trees, and pedestrian refuge islands.
 - Limits From: Davidson St
 - Limits To: Belmont Ave
 - Project ID: PM51215021
 - Project Phase: Completed
 - Estimated Completion Date: Q4 2025
 - CDOT PM: Laura Matney
 - GS PM: Stephen Tosco

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

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Transportation Summary

The petition is at the intersection of N Davidson St, a City-maintained arterial street, Parkwood Ave, a City-maintained arterial street, and Belmont Avenue a City-maintained collector street. The petition is located adjacent to Parkwood Ave, a City-maintained arterial street, and N Caldwell St, a City-maintained local street. The petition is located in the east wedge inside of Route 4 and is in the Community Activity Center place type of the 2040 Policy Map. A Traffic Impact Study (TIS) is not required for the site due to the development generating site trips under ordinance thresholds and not meeting additional TIS triggers. Site plan and/or conditional note revisions are needed to commit to relocating curb and gutter, providing bicycle and pedestrian facilities, dedicating required right of way, and addressing access concerns in accordance with the Unified Development Ordinance (UDO) Article 32.7, the UDO Article 33.3, and the Charlotte Streets Map. Further details are listed below.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation			Source
			Daily Trips	AM Peak Hour Trips	PM Peak Hour Trips	
Existing Use	Automobile Parts and Service	1,736 SF	29	5	5	<i>Tax Record</i>
Entitlement with Current Zoning	TOD-UC*, 3.08 Acres	-	-	-	-	<i>General Guidance from Planning</i>
Proposed Zoning	Free Standing Emergency Room (CAC-1, 3.08 Acres)	16,000 SF	399	18	24	<i>Site Plan: 02-16-26</i>

*Trip generation not provided for this zoning district for conditional rezonings.

Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

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Outstanding Issues

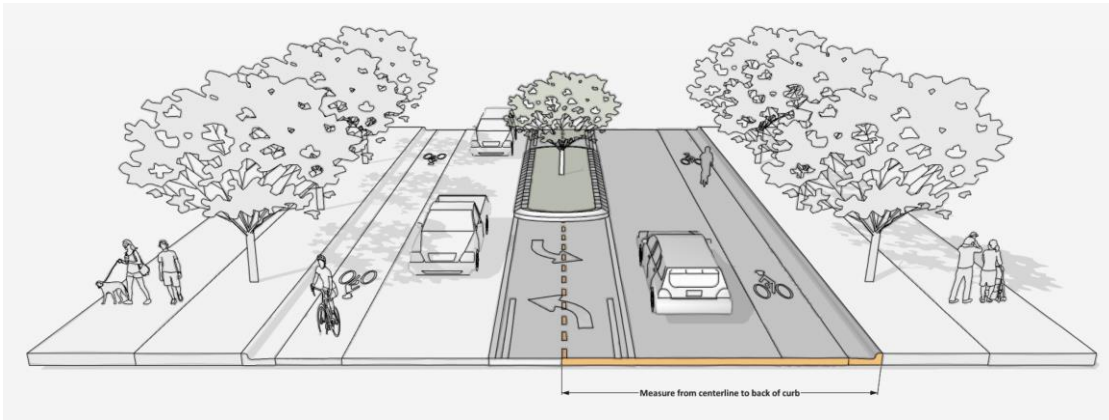
Strikethrough = Resolved

1. Curblines:

- a. **N Davidson St:** Location of future back of curb and gutter is to be shown at 24-feet from road centerline and must be shown in the future location according to UDO Article 32.7.

2+ Avenue with Bike Lanes (Charlotte Streets Map)

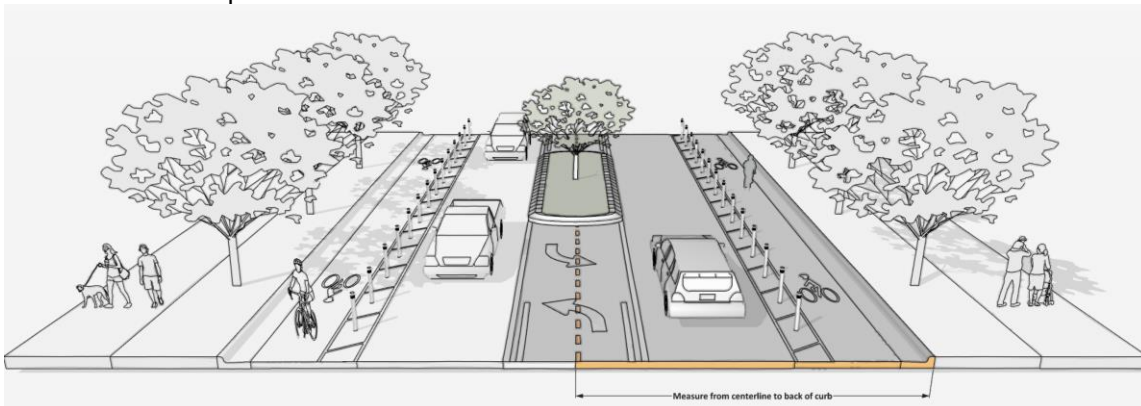
Label and dimension the existing and future curb and gutter form the centerline for each road on the site plan.



- b. **Parkwood Ave:** Location of future back of curb and gutter is to be shown at 27-feet from road centerline and must be shown in the future location according to UDO Article 32.7.

2+ Avenue with Buffered/Separated Bike Lanes (Charlotte Streets Map)

Label and dimension the existing and future curb and gutter form the centerline for each road on the site plan.



Rezoning Transportation Analysis

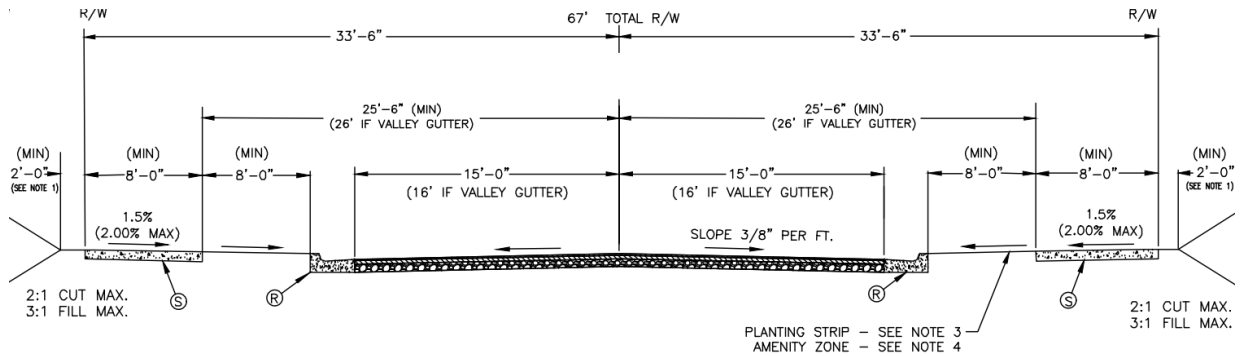
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- c. **Belmont Ave:** Location of future back of curb and gutter is to be shown at 17.5-feet from road centerline and must be shown in the future location according to Charlotte Land Development Standards Manual (CLDSM).

Collector Street (Charlotte Streets Map)

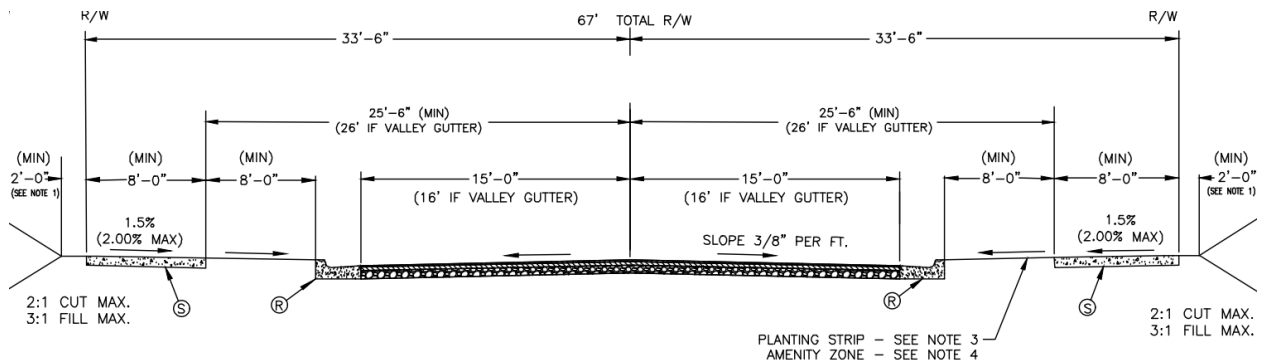
Label and dimension the existing and future curb and gutter form the centerline for each road on the site plan.



- d. **N Caldwell St:** Location of future back of curb and gutter is to be shown at 17.5-feet from road centerline and must be shown in the future location according to Charlotte Land Development Standards Manual (CLDSM).

Local Street (Charlotte Streets Map)

Label and dimension the existing and future curb and gutter form the centerline for each road on the site plan.



2. Comprehensive Transportation Review (CTR):

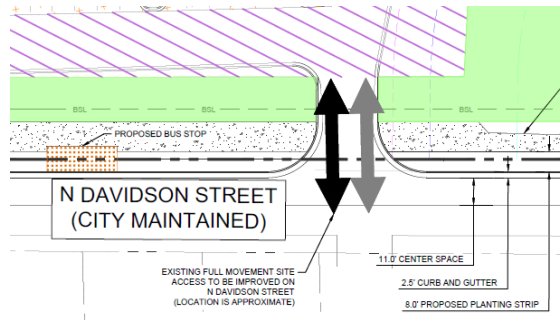
A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating site trips under ordinance thresholds found in the Charlotte Streets manual Table 3.1 and/or not triggering any other City TIS requirements.

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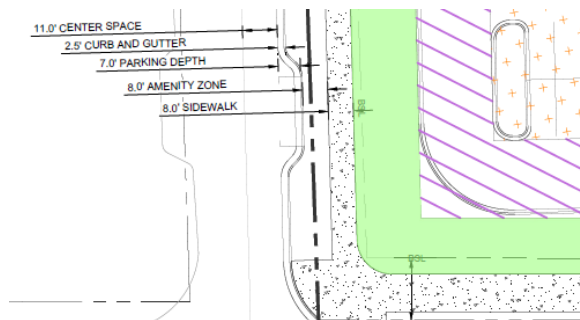
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3. Revise site plan and conditional note(s) to commit to dedicate 42 feet of right-of-way from the road centerline of N Davidson St. Label and dimension the right-of-way from the road centerline. An additional 2 feet of sidewalk and utility easement (SUE) will be needed from the back of sidewalk.
4. Revise site plan and conditional note(s) to commit to construct bicycle facilities along N Davidson St by locating the curb and gutter 24' from the road centerline.
5. Revise site plan and conditional note(s) to commit to dedicate 45 feet of right-of-way from the road centerline of Parkwood Ave. Label and dimension the right-of-way from the road centerline. An additional 2 feet of sidewalk and utility easement (SUE) will be needed from the back of sidewalk.
6. Revise site plan and conditional note(s) to commit to construct buffered/separated bicycle facilities along Parkwood Ave by locating the curb and gutter 27' from the road centerline.
7. Revise site plan and conditional note(s) to commit to dedicate 35.5 feet of right-of-way from the road centerline of Belmont Ave. Label and dimension the right-of-way from the road centerline. An additional 2 feet of sidewalk and utility easement (SUE) will be needed from the back of sidewalk.
8. Revise site plan and conditional note(s) to commit to dedicate 35.5 feet of right-of-way from the road centerline of N Caldwell St. Label and dimension the right-of-way from the road centerline. An additional 2 feet of sidewalk and utility easement (SUE) will be needed from the back of sidewalk.
9. Revise site plan and conditional note(s) to remove the full access from N Davidson St.



10. Revise site plan and conditional note(s) to remove the proposed on-street parking space west of the proposed site access.



11. Add a conditional note specifying "A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."

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12. Add a conditional note specifying “the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site’s first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2’ behind back of sidewalk where feasible.”
13. Add a conditional note specifying “All transportation improvements will be approved and constructed before the site’s first building certificate of occupancy is issued.”

The petitioner may phase transportation improvements if said improvements and phasing are approved by CDOT and explicitly described in site plan notes.

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte Unified Development Ordinance.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50’ x 50’ sight triangles (and two 10’ x 70’ sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and to comply with City of Charlotte Unified Development Ordinance regulations.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>