

Rezoning Transportation Analysis

Petition Number: 2025-126

General Location Identifier: 10306110

From: Jake Carpenter, PE

Jacob.Carpenter@charlottenc.gov

980-221-5675

Reviewer:

Brent Hyatt, PE

Brent.Hyatt@charlottenc.gov

704-942-7460

Revision Log:

Date	Description
12-22-25	First Review (BH)
01-14-26	Trip Generation Update (DR)
01-28-26	Second Review (BH)

General Review Information

The petition is located at the intersection of Wilora Lake Road, a City-maintained local street, and Mayberry Lane, a City-maintained local street, and N Sharon Amity Road, a City-maintained arterial street. The petition is located in an east wedge outside of Route 4, within the Eastland Area Plan.

Active Projects Near the Site:

- No projects near the site.

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located at the intersection of Wilora Lake Road, a City-maintained local street, Mayberry Lane, a City-maintained local street, and N Sharon Amity Road, a City-maintained arterial street. A Comprehensive Transportation Review (CTR) is required for this site due to the site falling within the low intensity development. Based on the 662 daily trips, CTR is not triggered. Site plan and/or conditional note revisions are needed to commit to addressing access concerns and removing on street parking from Wilora Lake Road. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation			Source
			Daily Trips	AM Peak Hour Trips	PM Peak Hour Trips	
Existing Use	Church	7,104 SF	89	1	7	<i>Tax Record</i>
Entitlement with Current Zoning	Single Family (N1-A, 6.6 acres)	19 DUs	219	16	21	<i>General Guidance from Planning</i>
Proposed Zoning	Affordable Housing Stacked (N2-B, 6.6 acres)	164 DUs	754	78	75	<i>Site Plan: 11-14-25</i>
Proposed Zoning	Affordable Housing Stacked Townhomes (N2-B, 6.6 acres)	144 DUs 20 DUs	778	75	76	<i>Site Plan: 01-14-26</i>
Proposed Zoning	Affordable Housing Stacked Townhomes (N2-B, 6.6 acres)	125 DUs 14 DUs	662	64	65	<i>Site Plan: 01-22-26</i>

Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

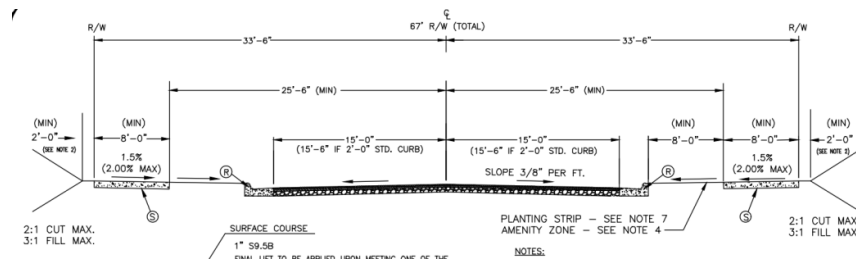
Strikethrough = Resolved

1. Curblines:

- a. **Wilora Lake Road:** Location of future back of curb and gutter is to be located 17.5 feet from road centerline and must be shown in the future location according to UDO Article 32.7 and 2040 Policy Map.

Local Residential Wide typical section with 8' sidewalk (CLDSM Standard No U-03A1).

Label and dimension the curb and gutter form the centerline for each road on the site plan.



Comment Rescinded 01-28-26: Please remove on-street parking from Wilora Lake Road and existing curb is to remain in place.

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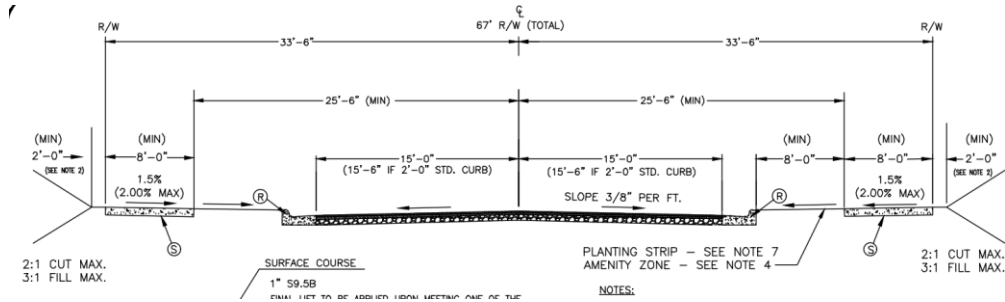
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- b. ~~Mayberry Lane~~: Location of future back of curb and gutter is to be located 17.5 feet from road centerline and must be shown in the future location according to UDO Article 32.7 and 2040 Policy Map.

~~Local Residential Wide typical section with 8' sidewalk (CLDSM Standard No U-03A1).~~

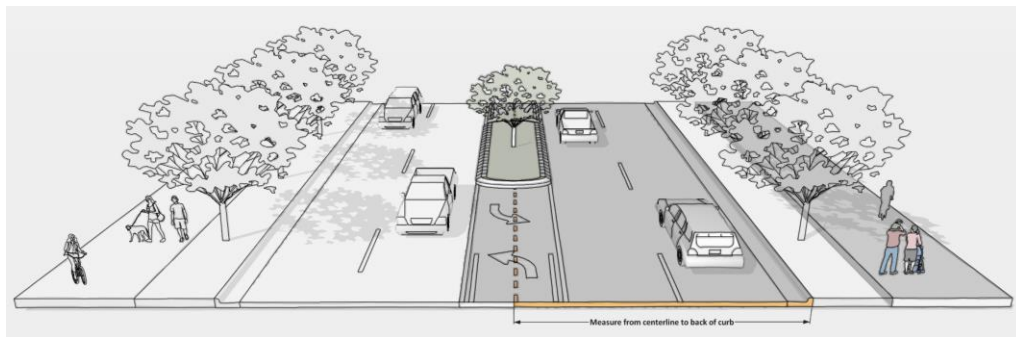
~~Label and dimension the curb and gutter form the centerline for each road on the site plan.~~



- c. ~~N Sharon Amity Road~~: Location of future back of curb and gutter is to be located 30 feet from road centerline, and must be shown in the future location according to UDO Article 32.7 and 2040 Policy Map. Existing curb may remain in its current location.

~~Arterial 4+ Avenue with Shared Use Path (Charlotte Streets Map).~~

~~Label and dimension the existing and future curb and gutter form the centerline for each road on the site plan.~~



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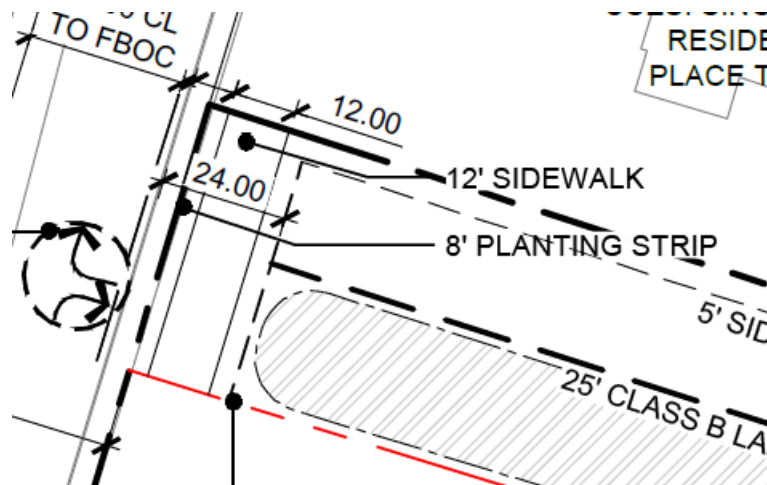
2. Comprehensive Transportation Review (CTR):

A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating site trips under ordinance thresholds found in the Charlotte Streets manual Table 3.1 and/or not triggering any other City TIS requirements.

The petition's zoning falls within the medium to high intensity development which based on the 778 daily trips will trigger Tier 1 (3 mitigation points) for multimodal assessment. The petitioner shall review and assess the publicly accessible pedestrian network within ¼ mile walking distance of the site to identify multimodal infrastructure to meet the Tier 1 (3 mitigation points) for multimodal assessment.

Complete CTR summary table and include on the revised plan. CTR table link (use Site Data Table - CTR Summary tab): <https://www.charlottenc.gov/files/sharedassets/city/v2/growth-and-development/documents/dev-center-fees/resources/ude-site-data-tables-curb-ramps-and-ctr.xlsx>

3. ~~Revise site plan and conditional note(s) to commit to dedicate 35.5 feet right of way from the road centerline of Wilora Lake Road. The site plan shall label and dimension the right of way from the road centerline. An additional 2 feet of SUE will be needed from the back of sidewalk.~~
4. ~~Revise site plan and conditional note(s) to commit to dedicate 35.5 feet right of way from the road centerline of Mayberry Lane. The site plan shall label and dimension the right of way from the road centerline. An additional 2 feet of SUE will be needed from the back of sidewalk.~~
5. ~~Revise site plan and conditional note(s) to commit to dedicate 52 feet right of way from the road centerline of Sharon Amity Road. The site plan shall label and dimension the right of way from the road centerline. An additional 2 feet of SUE will be needed from the back of sidewalk.~~
6. ~~Add a conditional note specifying "A Right of Way Encroachment Agreement is required for the installation of any non standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right of way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."~~
7. ~~Revise site plan to commit to tie into existing sidewalk.~~

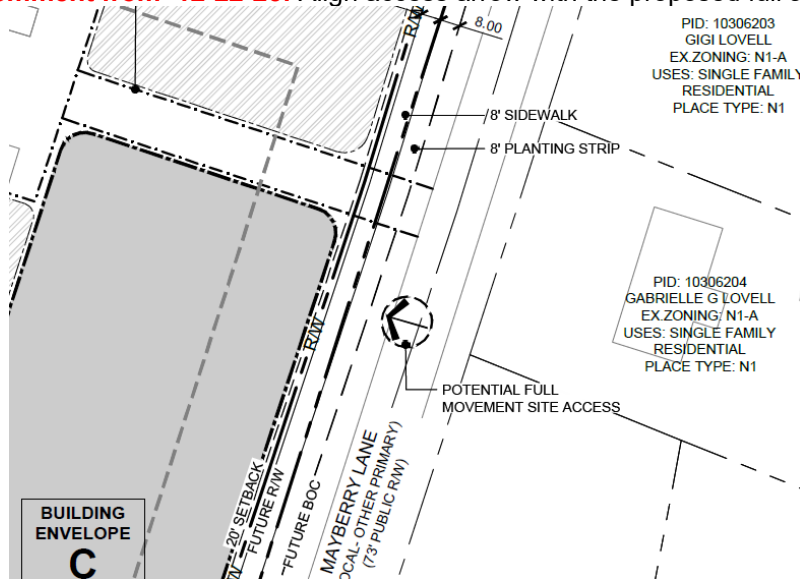


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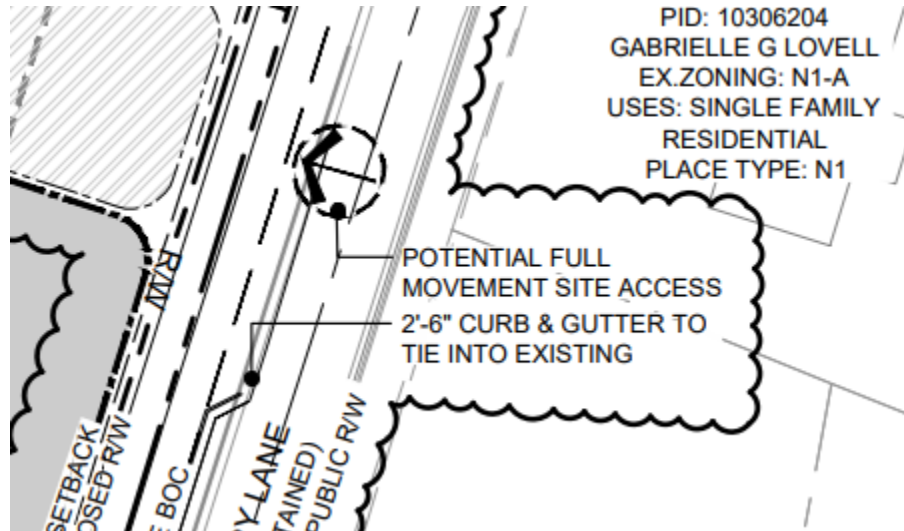
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8. **Outstanding Comment from 12-22-25:** Align access arrow with the proposed full access location.



Clarifying comment 01-27-26: Clarify location or remove "potential full movement site access" from Mayberry Lane.

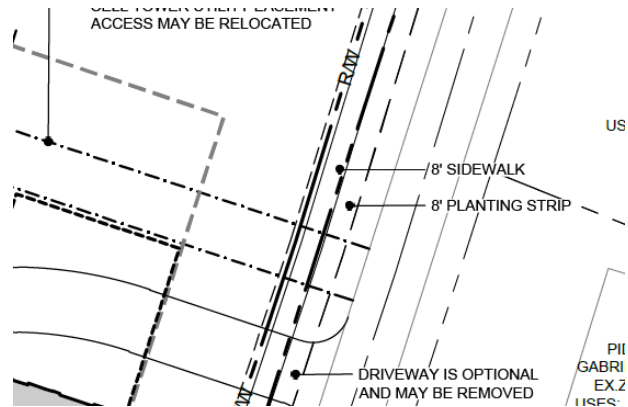


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9. **Outstanding comment from 12-22-25:** Clarify how the proposed cell tower utility easement is being accessed from Mayberry Lane.



10. Clarify what this area is that has the access shown going through it.



11. Add a conditional note specifying ~~“the Petitioner shall dedicate and convey in fee simple all rights-of-way to the City before the site’s first building certificate of occupancy is issued. CDOT requests rights-of-way set at 2’ behind back of sidewalk where feasible.”~~

12. Add a conditional note specifying ~~“All transportation improvements will be approved and constructed before the site’s first building certificate of occupancy is issued.”~~

13. ~~Revise note to show that N Sharon Amity Road is a City maintained street.~~

3. Access and Transportation Improvements:

- a. Access to the Site will be from N. Sharon Amity Road, Wilora Lake Road, and Mayberry Lane as generally depicted on the Rezoning Plan.
- b. N. Sharon Amity Road is designated a 4+ Avenue with a shared use path on the adopted Charlotte Streets Map. The required distance from the centerline to the future back of curb is a minimum of thirty (30) feet for this street designation. The Petitioner shall provide a minimum eight (8) wide planting strip and twelve (12) foot wide shared use path along the Site's N. Sharon Road Amity Road frontage. An additional right-of-way or permanent sidewalk easement will be located at a minimum of two (2) feet behind the sidewalk where feasible. It is noted that N Sharon Amity Road is a NCDOT maintained road and the final design and requirements are subject to their review and approval.

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Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte Unified Development Ordinance.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and to comply with City of Charlotte Unified Development Ordinance regulations.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>