

Rezoning Transportation Analysis

Petition Number: 2025-130

General Location Identifier: 05535119

From: Jake Carpenter, PE

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Reviewer:

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Revision Log:

Date	Description
12-22-25	First Review (IW)

General Review Information

The petition is located adjacent to Business Center Drive, a State-maintained local street, west of Little Rock Road, a State-maintained major arterial. The petition is located in a west corridor outside of Route 4, within the Westside Strategy Plan Study Area.

Active Projects Near the Site:

- There are no known active projects in the vicinity.

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located adjacent to Business Center Drive, a State-maintained local street, west of Little Rock Road, a State-maintained major arterial. A Comprehensive Transportation Review (CTR) is required for this site due to the site falling within the low intensity development. Based on the 1,311 daily trips, this will trigger a Tier 1 multimodal assessment. Site plan and/or conditional note revisions are needed to commit to showing future back of curb at 17.5' from centerline, showing more detail as it relates to site access, and adding conditional notes.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation			Source
			Daily Trips	AM Peak Hour Trips	PM Peak Hour Trips	
Existing Use	Vacant	-	-	-	-	<i>Tax Record</i>
Entitlement with Current Zoning	Industrial (ML-1, 4.09 acres)	40,900 SF	103	29	31	<i>General Guidance from Planning</i>
Proposed Zoning	Hotel (CG, 4.09 acres)	160 Rooms	1,311	73	91	<i>Site Plan: 11-17-25</i>

Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

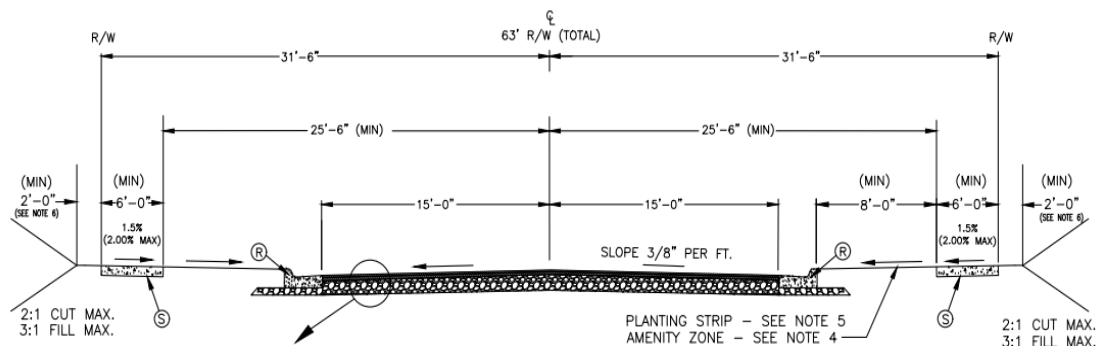
Outstanding Issues

Strikethrough = Resolved

1. Curblines:

- Business Center Drive:** Location of back of future curb and gutter to be shown 17.5' from centerline per Table 33-2 in the UDO.

CLDSM U-06



Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. Comprehensive Transportation Review (CTR):

A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating site trips under ordinance thresholds found in the Charlotte Streets manual Table 3.1 and/or not triggering any other City TIS requirements.

The petition's zoning falls within the medium to high intensity development which based on the 1,311 daily trips will trigger Tier 2 (6 mitigation points) for multimodal assessment. The petitioner shall review and assess the publicly accessible pedestrian network within ¼ mile walking distance of the site to identify multimodal infrastructure to meet the Tier 2 (6 mitigation points) for multimodal assessment.

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3. Revise site plan and conditional note(s) to commit to dedicate 63 feet of right-of-way from the road centerline. The site plan shall label and dimension the right-of-way from the Business Center Drive road centerline.
4. Add a conditional note specifying “A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.”
5. Revise site plan and conditional note(s) to commit to showing more detail in plans for CDOT and NCDOT to be able to determine how access is going to be accommodated.
6. Upon revision of site plan CDOT will coordinate with Subdivision to determine if it will be required for the petitioner to revise site plan and conditional note(s) to commit to extending Kanimbla Drive per Article 32.7 in the UDO.



- The petitioner may phase transportation improvements if said improvements and phasing are approved by CDOT and explicitly described in site plan notes.

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9. Add conditional note specifying “All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad north Western Mecklenburg area, by way of a private/public partnership effort or other public sector project support.”

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte Unified Development Ordinance.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and to comply with City of Charlotte Unified Development Ordinance regulations.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>