

SHEET INDEX

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NOTES:

1. REMOVE WIRE AND NYLON THINE FROM BALL AND CANOPY.
2. SOAK ROOT BALL AND PLANT PIT IMMEDIATELY AFTER INSTALLATION.
3. STAKING IS REQUIRED FOR ALL TREES IN R.O.W. OR UPON REQUEST OF ARBORIST.
4. REMOVE EXCESS SOIL FROM SITE AND DISPOSE OF IN A LEGAL MANNER.
5. RESEED UNMULCHED, DISTURBED AREAS.

ALL TREES SHALL MEET AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z60.1-2004) for example: CALIPER HEIGHT (RANGE) MAX HEIGHT MIN ROOT BALL DIA MIN ROOT BALL DIA

3"	12-14"	16"	24"	16"
3"	14-16"	18"	32"	21"

PLANTING DETAIL - SINGLE/MULTI-STEM TREE

NO SCALE

GLD STD # 40.01

LEGEND

- EXISTING PAVEMENT/CURB
- PROPOSED PAVEMENT/CURB
- ADJOINING PROPERTY LINES
- EXISTING BUILDINGS
- PROPOSED 5' SIDEWALK
- TREE PROTECTION FENCE
- PROPOSED TREE SAVE AREA
- PROPOSED NATURAL AREA

BMP INSET TABLE

PROJECT NAME:	BLAKENEY RETREAT
SEQUENCE ID:	1.00
DRAINAGE AREA (ACRES):	2.00
LAND USE/DEVELOPMENT TYPE:	1/4 - 1/2 AC RESIDENTIAL
PERCENT BUILT-UPON AREA:	31.00
SEDIMENT CHAMBER LENGTH (FT.):	41.00
SEDIMENT CHAMBER WIDTH (FT.):	34.00
SEDIMENT CHAMBER HEIGHT (FT.):	3.00
SAND FILTER TYPE:	SURFACE
MEDIA DEPTH (FT.):	2.50
FLOW DIVERTER PRESENT (Y/N):	NO
REGULATED BY:	PCO
TREATMENT EFFECTIVENESS:	OPTIMAL
NC STATE PLANE X (EASTING):	1456,760.00
NC STATE PLANE Y (NORTHING):	478,943.00

PCCO SUMMARY

ORIGINAL PARCEL #	224-083-34
DEVELOPMENT TYPE	RESIDENTIAL
SUBJECT TO PCGO (Y/N) ?	YES
IF NO, WHY?	
WATERSHED	SIX MILE CREEK
DISTURBED AREA (AC)	3 AC
SITE AREA (AC)	3.70 AC
TOTAL ON-SITE DA (AC)	2 AC
EXIST. BUA (SF)	0 SF
EXIST. BUA TO BE REMOVED (SF)	0 SF
EXIST. BUA TO REMAIN (SF)	0 SF
PROP. NEW BUA (SF)	3,170 SF
PROP. % BUA	36.5 %
DENSITY (HIGH/LOW)	HIGH
TOT. POST-PROJ. BUA FOR SITE	3,170 SF
DEVELOPMENT OR REDEVELOPMENT?	DEVELOPMENT
NATURAL AREA RES (AC)	0.66 AC
NATURAL AREA PROV (AC)	0.66 AC
INDIST. TREED NATURAL AREA PRESERVED	0.59 AC
TOT. STREAM BUFFER PROT (AC)	0
TRANSIT STA AREA (Y/N)	N
DISTRESSED BUS. DIST (Y/N)	N
MITIGATION TYPE (IF APPLICABLE)	
NATURAL AREA MITIGATION (Y/N)	N
BUFFER MITIGATION (Y/N)	N
TOT. PHOSPHORUS MITIGATION (Y/N)	N

CURVE TABLE

CURVE	LENGTH	RADIUS	CHORD	BEARINGS
C1	268.06	870.00	267.00	S50°30'39"E
C2	54.04	100.00	53.39	S48°34'48"W
C3	11.61	59.00	11.59	S20°06'25"E
C4	14.19	75.00	14.19	S50°15'02"E

TREE REQUIREMENTS

FRONTAGE	L6	S1	STREET NAME
DESCRIPTION	TREES	TREES	
650	15	0	BLAKENEY HEATH ROAD
SEE DEVELOPMENT DATA FOR BREAKDOWN			
ROAD FRONTAGE:	650 LF		
STREET TREES REQUIRED:	650 / 50 = 13 (LARGE MATURING)		
STREET TREES PROPOSED:	15 (LARGE), 0 (SMALL)		
* USE EXISTING TREES THAT QUALIFY, WHERE ALLOWED			
(L) LARGE MATURING TREES SHALL BE OVERCUP OAKS.			
(S) CALIPER (PER CM HISTORIC CORP)			
NO HERITAGE TREES EXIST ON THIS PROPERTY			

SPECIMEN TREES

TREE	TREE CANOPY	TREE CANOPY	TREE CREDIT
DESCRIPTION	TOTAL (SF)	WITHIN COS (SF)	ALLOWED (SF)
A 36" OAK	2821	9709	1053
B 42" OAK	4930	2481	126
C 36" OAK	2128	1818	404
D 30" OAK	2265	1441	721
E 30" OAK	3211	2881	1446
F 24" OAK	2463	2330	165
G 24" OAK	1824	1140	510
TOTAL	19028	15556	7520 (51.06)

LANDSCAPE PLAN NOTES:

MINIMUM TREE SIZE AT PLANTING IS 2" CALIPER, 8' TALL IF SINGLE STEM, MINIMUM 8' TALL AND 3 STEMS MAXIMUM IF MULTI-STEM. MULCH IS REQUIRED, STAKING/STOPS IS OPTIONAL.

FOR NEW PLANTING AREAS, REMOVE ALL PAVEMENT, GRAVEL, SUB-BASE AND CONSTRUCTION DEBRIS, REMOVE COMPACTED SOIL AND ADD 2" NEW TOPSOIL OR UNCOMPACT AND AMEND THE TOP 24" OF EXISTING SOIL TO MEET TOPSOIL PLANTING MIX STANDARDS FOR TREES (WITHIN ENTIRE MINIMUM AREA OF 274 SQUARE FEET PER TREE). CALL 104-336-4624 FOR AN INSPECTION OF SOIL BEFORE PLANTING TREES.

ALL STAKING AND 2" OF WIRE BASKET MUST BE CUT AWAY FROM ROOT BALL AND REMOVED FROM ROOT BALL PRIOR TO BACKFILLING PLANTING PIT. REMOVE TOP 1/3 OF THE BURLAP FROM ROOT BALL.

PLEASE CALL 104-336-4624 FOR FINAL INSPECTION OF TREES, 1 TO 10 DAYS BEFORE THE CERTIFICATE OF OCCUPANCY IS NEEDED.

PREFERRED TREE SPACING IS: 30' FOR SMALL MATURING TREES (20MINIMUM IS PLANTED IN GROUPS), 40-50' FOR LARGE MATURING TREES (20 MINIMUM IF PLANTED IN GROUPS).

ADJUST TREE PLANTING LOCATIONS TO AVOID UNDERGROUND UTILITIES. PLACE 15-20' OFF SEWER AND STORM DRAINAGE LINES, 10-15' OFF GAS, WATER, PHONE AND UNDERGROUND ELECTRICAL LINES.

EXISTING HEALTHY TREES WITHIN 25 FEET OF THE RIGHT-OF-WAY OR LINE MAY BE CREDITED TOWARD THE PERIMETER REQUIREMENT IF SHOWN LANDSCAPE PLAN MAY BE PRESERVED HEALTHY.

LARGE MATURING TREES MAY NOT BE PLANTED WHERE OVERHEAD DISTRIBUTION OR TRANSMISSION LINES EXIST. IF TREES OVERLAP WITH POWER LINES OR SIGNAL, CALL URBAN FORESTRY TO RESOLVE BEFORE PLANTING.

GENERAL NOTES

1. THIS PRELIMINARY SUBDIVISION PLAN IS SUBMITTED, AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS OF THE CITY OF CHARLOTTE ZONING AND SUBDIVISION ORDINANCES, AND THE CHARLOTTE LAND DEVELOPMENT STANDARDS MANUAL.
2. BOUNDARY SURVEY INFORMATION PER SURVEY PREPARED BY PHOENIX LAND SURVEYING.
3. TOPOGRAPHIC INFORMATION TAKEN FROM PHOTOGRAMMETRIC MAPPING BY CITY OF CHARLOTTE.
4. CHIEF SUBDIVISION INSPECTOR SHALL BE GIVEN 48 HOURS ADVANCE NOTICE PRIOR TO COMMENCEMENT OF CONSTRUCTION.
5. LOT DIMENSIONS ARE APPROXIMATE; REFERENCE RECORD PLAT FOR EXACT LOT DIMENSIONS.
6. ALL STORM DRAINAGE PIPE SHALL BE CLASS 3 REINFORCED CONCRETE PIPE (RCP) (UNLESS OTHERWISE NOTED). PIPE LENGTHS INDICATED ON THE PLANS ARE APPROXIMATE AND ARE SUBJECT TO CHANGE DUE TO FIELD CONDITIONS.
7. SUBSURFACE DRAINAGE FACILITIES MAY BE REQUIRED IN THE STREET RIGHT-OF-WAY AS DEEMED NECESSARY BY THE INSPECTOR.
8. REFERENCE SHEETS 3 & 4 FOR EROSION CONTROL PLAN AND NOTES. REFERENCE SHEET 5 FOR EROSION CONTROL DETAILS.
9. CONSTRUCTION OF ALL DEVELOPMENT IMPROVEMENTS SHALL BE IN ACCORDANCE WITH THE CHARLOTTE LAND DEVELOPMENT STANDARDS MANUAL. REFERENCE SHEET 5 FOR SITE DETAILS. THE FOLLOWING ADDITIONAL DETAILS ARE INCLUDED BY REFERENCE:
  - ITEM CULVERT AND GUTTER 1011
  - SIDEWALK 1022
  - HEADWALL 836.01/836.11
10. APPROXIMATE COMPLETION TIME IS FALL 2013.
11. WATER AND SANITARY SEWER SERVICE SHALL BE PROVIDED BY LATERALS TO THE EXISTING CHARLOTTE-MECKLENBURG UTILITY SYSTEM.
12. SIDEWALK NOTE: THE CONTRACTOR SHALL COORDINATE WITH THE SUBDIVISION INSPECTOR FOR LATEST REVISION OF HANDICAP RAMP DETAILS PRIOR TO PLACEMENT OF ANY HIC RAMPS AND/OR SIDEWALK.
13. THE DEVELOPER SHALL MAINTAIN EACH STREAM, CREEK, OR BACKWASH CHANNEL IN AN UNOBTURBATED STATE AND SHALL REMOVE FROM THE CHANNEL AND BANKS OF THE STREAM ALL DEBRIS, LOGS, TIMBER, JUNK AND OTHER ACCUMULATIONS.
14. DRAINAGE EASEMENTS INDICATED AS SDE (STORM DRAINAGE EASEMENT) ARE EASEMENTS WHICH ARE THE RESPONSIBILITY OF THE INDIVIDUAL PROPERTY OWNER.
15. DEVELOPER WILL PROVIDE STREET SIGNS PER GLDSM 50.05 (4" SIGNS ONLY).
16. COORDINATE ALL CURB AND STREET GRADES IN INTERSECTIONS WITH THE CITY INSPECTOR.

17. NON-STANDARD ITEMS (IE: PAVERS, IRRIGATION SYSTEMS, ETC.) IN THE RIGHT-OF-WAY REQUIRE A RIGHT-OF-WAY ENCROACHMENT AGREEMENT WITH THE (CHARLOTTE DEPARTMENT OF TRANSPORTATION NORTH CAROLINA DEPARTMENT OF TRANSPORTATION) BEFORE INSTALLATION.
18. "AS-BUILT" DRAWINGS AND PLANS OF THE STORM DRAINAGE SYSTEM, INCLUDING DESIGNED SLOPES, MUST BE SUBMITTED PRIOR TO SUBDIVISION FINAL INSPECTION TO THE CITY/COUNTY ENGINEERING DEPARTMENT IN ACCORDANCE WITH THE CITY/COUNTY SUBDIVISION ORDINANCE.
19. IN ORDER TO ENSURE PROPER DRAINAGE, KEEP A MINIMUM OF 0.5% SLOPE ON THE CURB.
20. IN ROLLING AND HILLY TERRAINS, SHEEPING OF THE STONE BASE AND/OR APPLICATION OF TACK COAT MAY BE REQUIRED NEAR INTERSECTIONS. THESE REQUIREMENTS WILL BE ESTABLISHED BY THE INSPECTOR AND BASED ON FIELD CONDITIONS.
21. APPROVAL OF THIS PLAN IS NOT AN AUTHORIZATION TO GRADE ADJACENT PROPERTIES. WHEN FIELD CONDITIONS WARRANT OFF-SITE GRADING, PERMISSION MUST BE OBTAINED FROM THE AFFECTED PROPERTY OWNERS.
22. SITE BENCHMARK FOR PROJECT IS THE RIM OF AN EXISTING SAN. SWR. MH IN BORGHETTA DRIVE, ELEV. = 648.11 (NAD 83), USED FOR 100H FLOOD STUDY.
23. CURB AND GUTTER SHOWN ON PLANS ALONG BLAKENEY HEATH ROAD MAY BE ADJUSTED BASED UPON FIELD STAKING BY CITY ENGINEERS. ASSOCIATED STORM DRAINAGE BASED UPON FIELD CONDITIONS.
24. COMMON OPEN SPACE TO BE CONVEYED TO THE HOMEOWNERS ASSOCIATION IN ACCORDANCE WITH THE CHARLOTTE ZONING ORD. 4.205.
25. ALL ROAD IMPROVEMENTS AT BLAKENEY HEATH ROAD ARE TO BE COORDINATED WITH THE CITY OF CHARLOTTE ENGINEERING DEPARTMENT PRIOR TO CONSTRUCTION.
26. TREE PROTECTION BARRICADES MUST MEET OR EXCEED TREE ORD. GUIDELINES STD.
27. TREE BARRICADES MUST BE INSTALLED BEFORE ANY DEMOLITION/GRADING/CONSTRUCTION AND NOT REMOVED UNTIL AFTER FINAL INSPECTION BY URBAN FORESTRY STAFF.
28. NO SOIL DISTURBANCE OR COMPACTION, CONSTRUCTION, MATLS, TRAFFIC, BURIAL, PITS, TRENCHING OR OTHER LAND DISTURBING ACTIVITY ALLOWED IN TREE SAVE AREA.
29. VIOLATIONS OF THE TREE PROTECTION REQUIREMENTS ARE SUBJECT TO FINES, AND/OR IMMEDIATE CORRECTIVE ACTION/INVESTIGATION.
30. NO DEMOLITION LANDFILLS WILL BE LOCATED ON THIS SITE.
31. THE DEVELOPER SHALL CONTACT THE CHARLOTTE DEPARTMENT OF TRANSPORTATION (GUS JORDI, 336-1086) TO IDENTIFY ANY CONFLICTS WITH TRAFFIC SIGNALIZATION EQUIPMENT. 60-90 DAYS WILL BE REQUIRED TO COORDINATE RELOCATION. DEVELOPER SHALL BE RESPONSIBLE FOR ALL RELATED COSTS AND/OR ANY REPAIR COSTS CAUSED BY THE CONTRACTOR/DEVELOPER.
32. COMMON OPEN SPACE/TREESAVE AREA TO REMAIN IN A NATURAL STATE. AREAS TO BE CLEARED OF TRASH AND DEBRIS.
33. THE PURPOSE OF THE STORM DRAINAGE ESMT. (SDE) IS TO PROVIDE STORM WATER CONVEYANCE. BUILDINGS ARE NOT PERMITTED IN THE EASEMENT AREA. ANY OTHER OBJECTS WHICH IMPEDE STORM WATER FLOW OR SYSTEM MAINTENANCE ARE ALSO PROHIBITED.
34. PE SEALED SHOP DRAWINGS FOR RETAINING WALLS MUST BE SUBMITTED TO CITY ENGINEER PRIOR TO CONSTRUCTION.
35. THE CITY OF CHARLOTTE WILL NEITHER BE RESPONSIBLE FOR THE GROUNDS WITHIN THE PERMANENT STORM DRAINAGE EASEMENT NOR REMOVAL OF ANY OBSTRUCTIONS IN THAT AREA.
36. SIGHT TRIANGLES SHOWN ARE MINIMUM REQUIRED.
37. HIGH-DENSITY POLYETHYLENE (HDPE) STORM DRAINAGE PIPE INSTALLED WITHIN EXISTING OR PROPOSED PUBLIC STREET RIGHT-OF-WAY MUST BE APPROVED BY THE CITY'S INSPECTOR PRIOR TO ANY BACKFILL BEING PLACED. BACKFILL MATERIAL MUST BE APPROVED BY THE CITY INSPECTOR PRIOR TO PLACEMENT OF THE MATERIAL WITHIN THE PUBLIC STREET RIGHT-OF-WAY.
38. RIGHT-OF-WAY FOR BLAKENEY HEATH RD FROM DB 2440 PS 240.
39. CERTIFICATION AND STREET CLOSURE PERMITS ARE REQUIRED FOR UTILITY CUTS ON CITY STREETS, ALLOW 1 DAY'S PROCESSING FOR PERMITS. FOR INFORMATION CONTACT CDOT (104-336-4025) OR VISIT <http://www.charmeck.org/departments/transportation/street-maintenance/home.htm>
40. PER SECTION 18-1(E) OF THE CITY CODE AND SECTION 10.0 OF THE CITY'S POST CONSTRUCTION CONTROLS ADMINISTRATIVE MANUAL, ALL REQ'D NATURAL AREAS AND/OR POST CONSTRUCTION CONTROLS EASEMENTS (PCGES) MUST BE RECORDED PRIOR TO THE ISSUANCE OF THE CERTIFICATE OF OCCUPANCY.
41. TREESAVE AREA MUST HAVE INVASIVE PLANTS INCLUDING ENGLISH IVY REMOVED PRIOR TO THE FIRST CERTIFICATE OF OCCUPANCY BEING ISSUED. ANY GAPS IN THE EXISTING TREESAVE AREA WILL BE PLANTED WITH 2" CALIPER LARGE MATURING TREES FROM THE APPROVED SPECIES LIST. COORDINATE WITH URBAN FORESTRY SPECIALIST.
42. CONTACT CDOT, BOB STALEY 104-432-1562 FOR RIGHT-OF-WAY USE PERMIT PRIOR TO STARTING ANY WORK WITHIN EXISTING ROAD RIGHTS-OF-WAY.
43. NO DIRECT ACCESS FROM ANY LOT TO BLAKENEY HEATH ROAD, THE EXISTING DIRT DRIVEWAY WILL BE (REMOVED) SEEDED AS PART OF THE DEVELOPMENT.

APPROVED FOR  
CONSTRUCTION  
CHARLOTTE-MECKLENBURG PLANNING DEPARTMENT  
[By: Joshua Weaver 12-18-2013]



<http://development.charmeck.org>

ENGINEERING  
PCO / DETENTION / DRAINAGE PLAN

EROSION CONTROL  
NOTE: SCHEDULE PRE-CONSTRUCTION MEETING AT LEAST 48 HRS. PRIOR TO ANY LAND DISTURBING ACTIVITY USING THE ONLINE FORM AT <http://development.charmeck.org>

URBAN FORESTRY  
TREE ORDINANCE

CDOT

APPROVED

By Brendan Smith (bsmith@charlottenc.gov) at 4:48 pm, Nov 26, 2013

APPROVED

By Brendan Smith (bsmith@charlottenc.gov) at 4:48 pm, Nov 26, 2013

APPROVED

By Gary Turner (704-336-4330) at 9:03 am, Dec 20, 2013

APPROVED

By Candice Leonard at 9:47 am, Dec 19, 2013

DEVELOPMENT DATA

TAX PARCEL NO.: 224-083-34  
ZONING CLASSIFICATION: R-3 (DEVELOPED AS CLUSTER\*)  
ZONING JURISDICTION: CITY OF CHARLOTTE  
SITE ACRES: 3.70 AC  
NO. OF UNITS ALLOWED BY ZONING: 3.70 x 3 = 11.3  
AREA IN EASEMENT: 0.07 AC  
NET TOTAL SITE AREA: 3.71 AC  
TREESAVE REQUIRED: 108 x 3.71 AC = 0.31AC  
TREESAVE PROVIDED: 0.30 AC + 0.18 AC = 0.48 AC\*  
NO. OF UNITS PROPOSED: 1  
THE MAXIMUM BUA IS 3,500 SF PER LOT

R-3 CLUSTER LOT DATA:  
MIN. LOT SIZE = 8,000 SF  
MIN. LOT WIDTH = 60'  
MIN. SETBACK = 32' BOC\*  
MIN. SETBACK = 6' EXTERNAL, 5' INTERNAL\*  
MIN. REAR YARD = 45' EXTERNAL, 30' INTERNAL\*  
\* TREESAVE INCENTIVE W/ SPECIMEN TREES  
0.30 AC IN WOODED AREA, 0.18 AC IN SPEC. TREES  
SEE SPECIMEN TREE TABLE (THIS SHEET)

SINGLE FAMILY	MAX. BLDG
LOT SIZE (SF)	COVERAGE
UP TO 4,000	50%
4,001 - 6,500	45%
6,501 - 10,000	40%
10,001 - 15,000	35%
15,001 OR GREATER	30%

Preliminary Subdivision Site/Landscape Plan

Blakeney Retreat

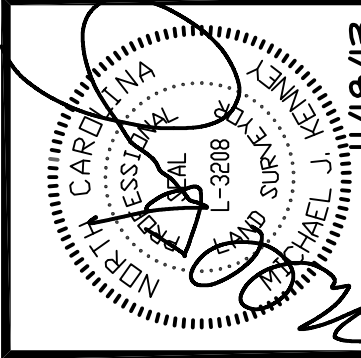
City of Charlotte, Mecklenburg County, North Carolina  
Classica Homes, 1101 Wood Ridge Center Dr, Suite #155, Charlotte, NC 28217

Sheet No.

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Revisions:  
1. DRAWS REVISED PER CDOT, CDD & CDOT REVIEW COMMENTS.  
2. DRAWS REVISED PER CDOT, CDD & CDOT REVIEW COMMENTS.  
3. DRAWS REVISED PER CDOT, CDD & CDOT REVIEW COMMENTS.

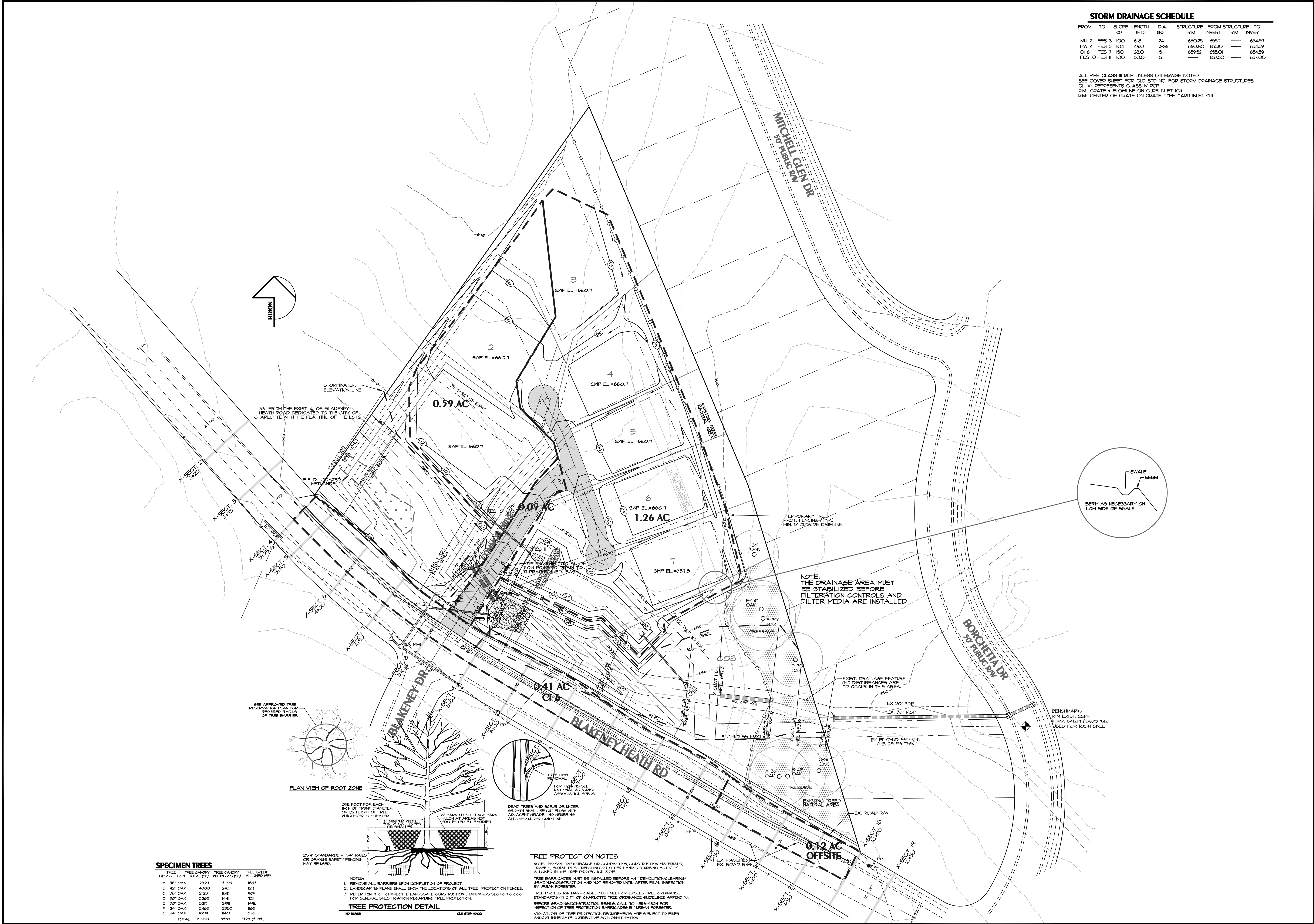


Scale: 1"=40'  
Date: 6/28/13  
Drawn By: MK  
Designed By: MK  
Job No.: 0712

KENNEY DESIGN  
GROUP, PA

C-1986  
1316 GREENWOOD CLIFF  
CHARLOTTE, NORTH CAROLINA 28204  
PH: 704/377-6099 FAX: 704/377-6097  
EMAIL: KENNEY@KENNEYDESIGN.COM





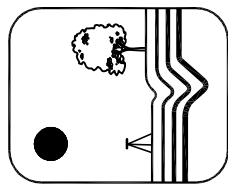
STORM DRAINAGE SCHEDULE

FROM	TO	SLOPE	LENGTH	DIA.	STRUCTURE	FROM STRUCTURE	TO
		(%)	(FT)	(IN)		RM	INVERT
MH 2	FES 3	1.00	61.8	24	660.25	655.2	654.59
MH 4	FES 5	1.04	49.0	2-36	660.20	655.0	654.59
CI 6	FES 7	1.50	28.0	15	659.52	655.01	654.59
FES 10	FES 11	1.00	50.0	15	657.50	657.00	657.00

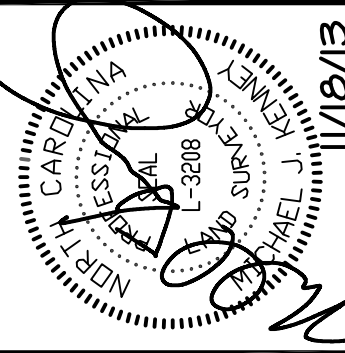
ALL PIPE CLASS III RCP UNLESS OTHERWISE NOTED  
SEE COVER SHEET FOR CUL STD NO. FOR STORM DRAINAGE STRUCTURES  
CL IV REPRESENTS CLASS IV RCP  
RM: GRATE • FLOWLINE ON CURB INLET (CI)  
RM: CENTER OF GRATE ON GRATE TYPE YARD INLET (YI)

KENNEY DESIGN  
GROUP, PA

C-1986  
1316 GREENWOOD CLIFF  
CHARLOTTE, NORTH CAROLINA 28204  
PH: 704/377-6099 FAX: 704/377-6097  
EMAIL: KENNEY@KENNEYDESIGN.COM



REVISIONS:  
1. DRAINAGE REVISED PER CADD, CDD & CDD REVIEW COMMENTS.  
2. 11/18/13 REVISED PER CDD & CDD REVIEW COMMENTS.  
3. 11/18/13 REVISED PER CDD & CDD REVIEW COMMENTS.



Scale: 1"=40'  
Date: 6/28/13  
Drawn By: MK  
Designed By: MK  
Job No.: 0712

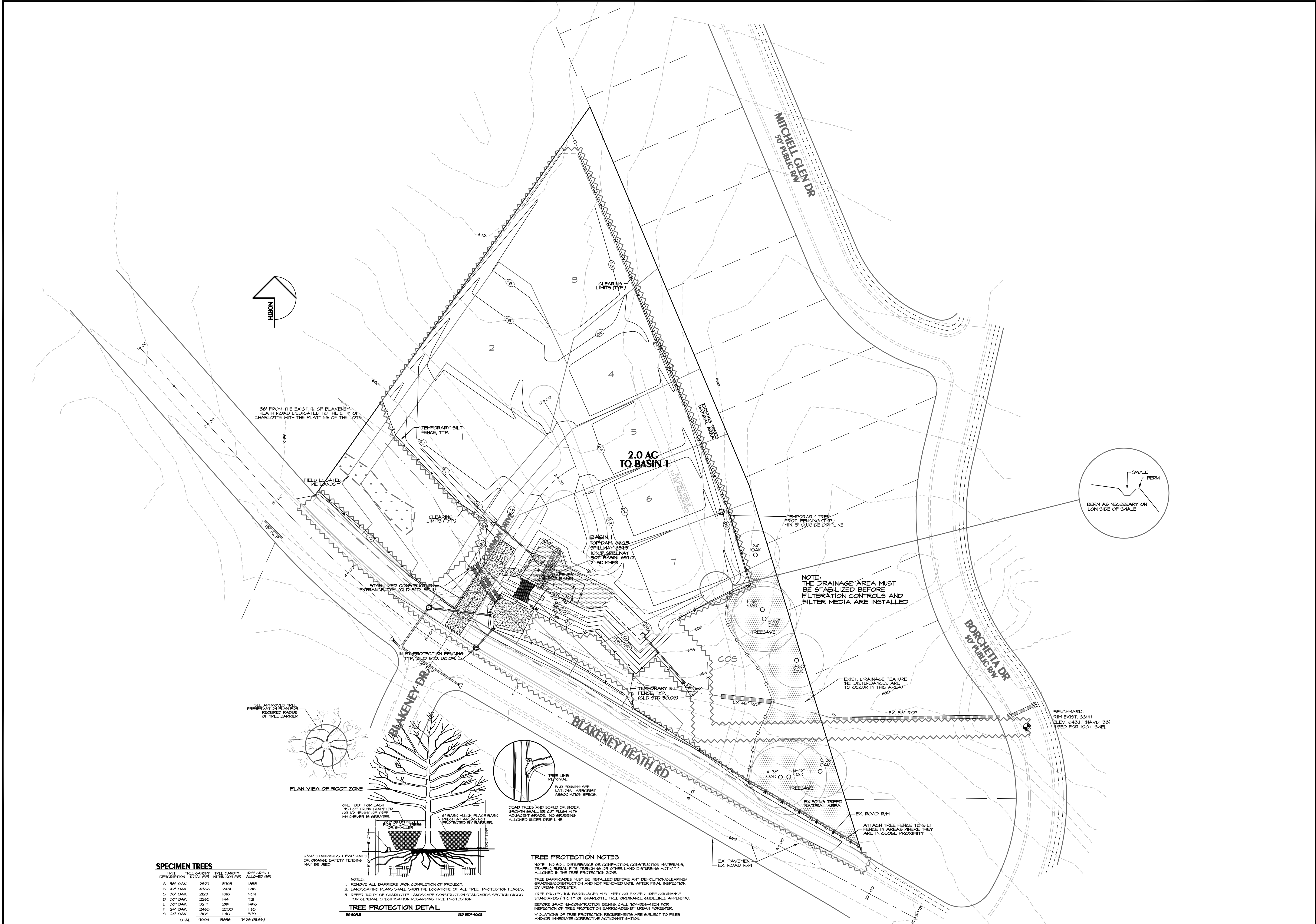
STORM DRAINAGE AND GRADING PLAN  
**BLAKENEY RETREAT**  
City of Charlotte, Mecklenburg County, North Carolina  
Classica Homes, 1101 Wood Ridge Center Dr, Suite #155, Charlotte, NC 28217

Sheet No. 2 of 9

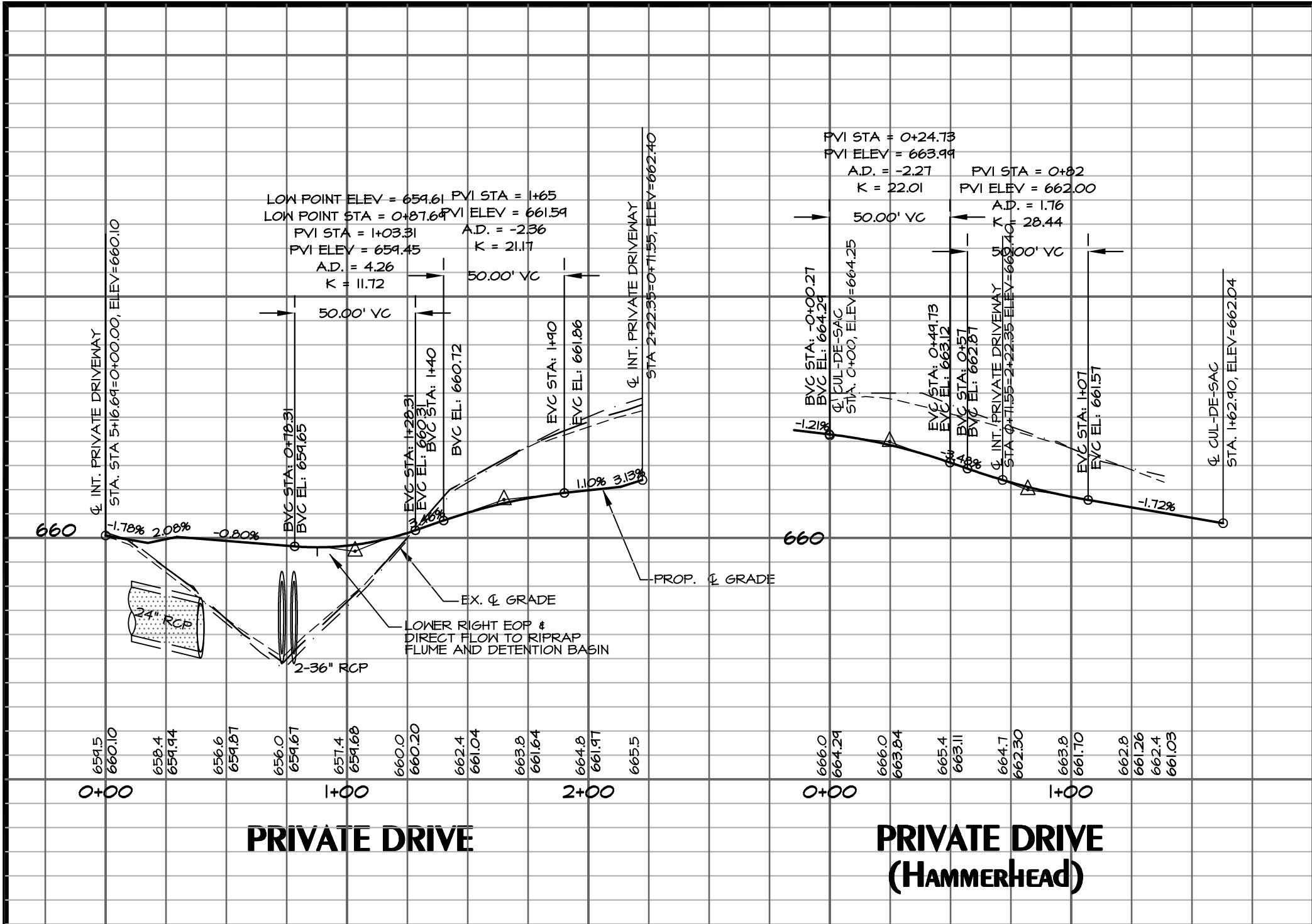






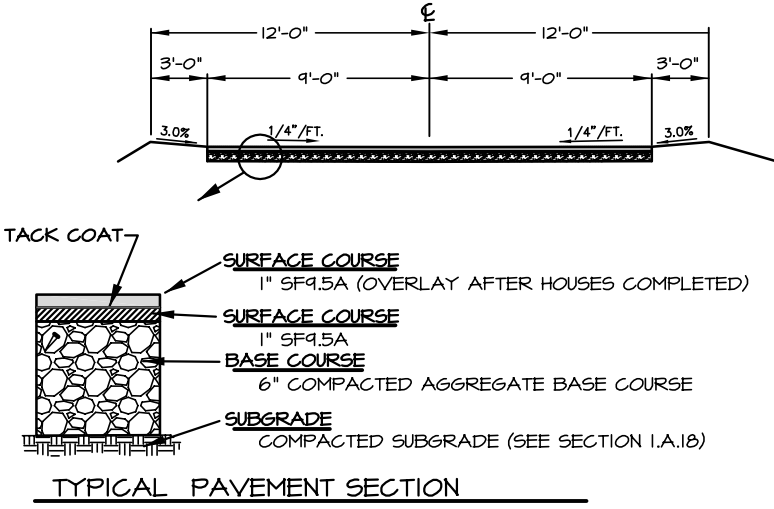




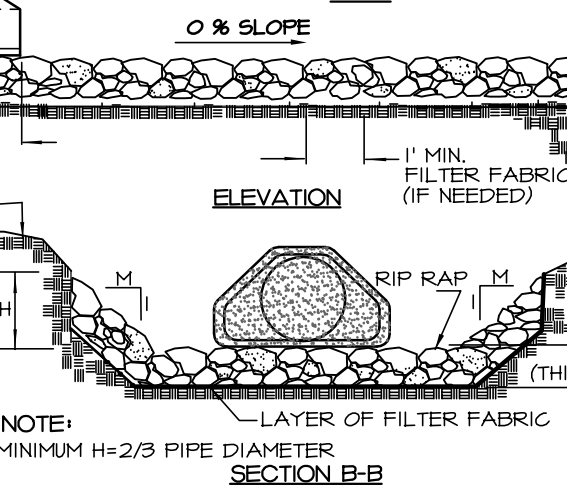
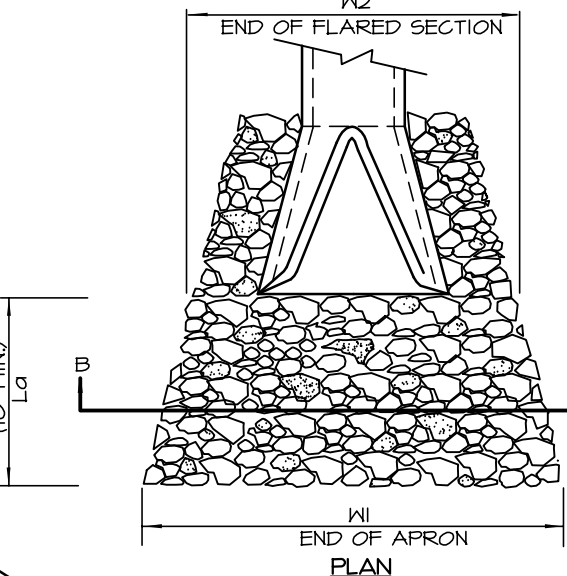


PRIVATE DRIVE

PRIVATE DRIVE (HAMMERHEAD)



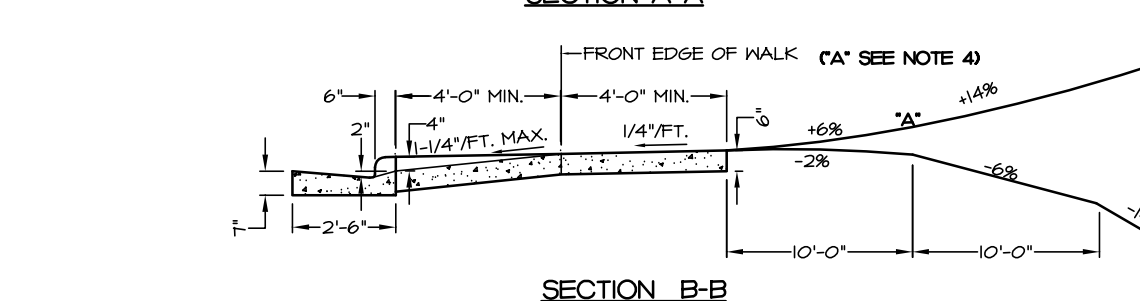
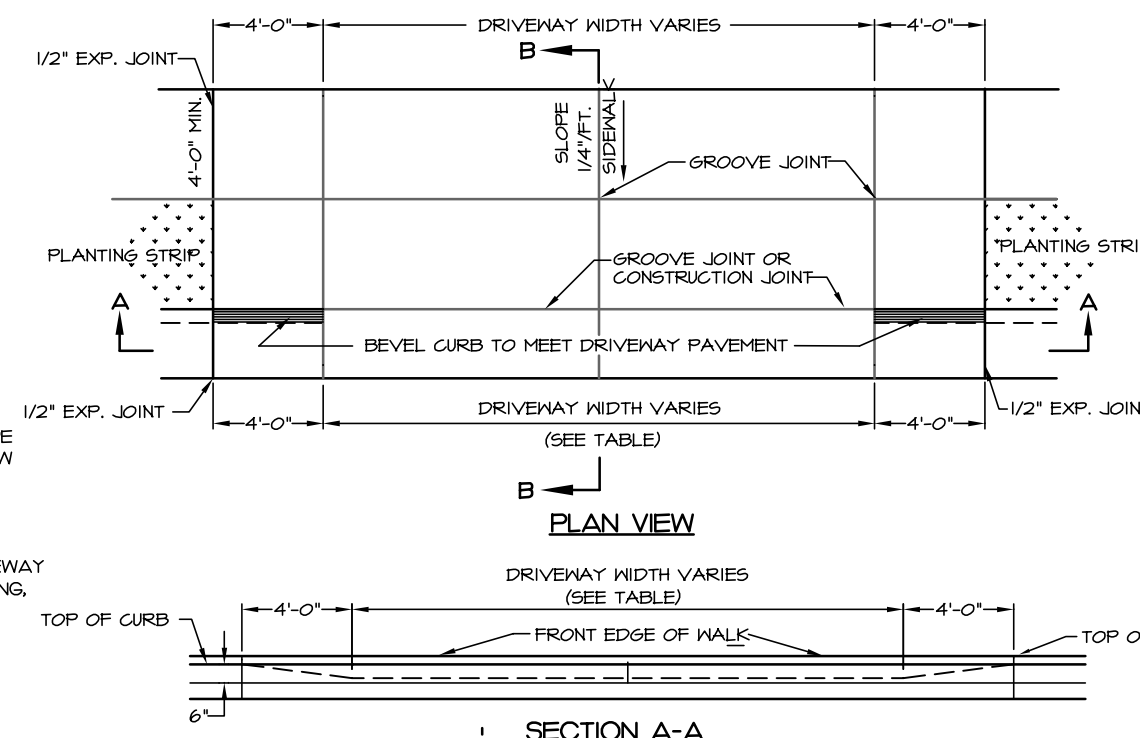
COMMON DRIVE  
NO SCALE



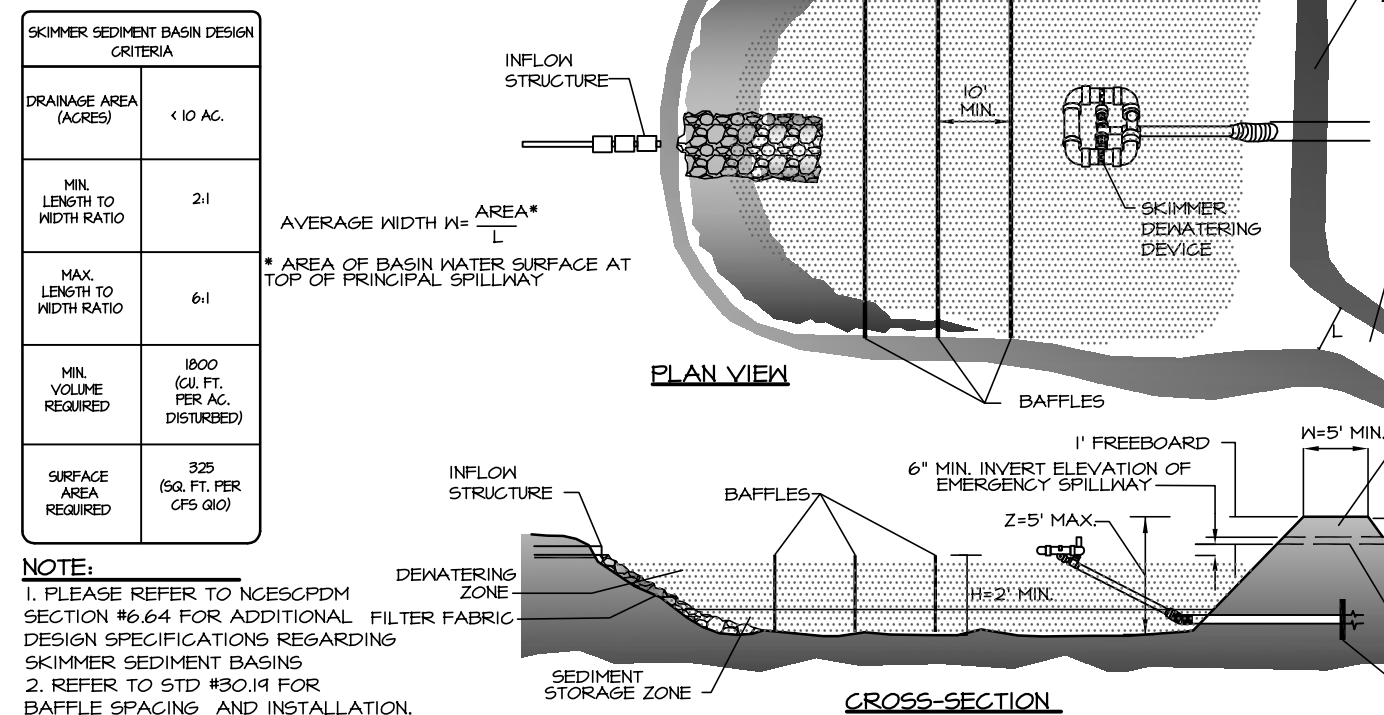
- GENERAL NOTES:**
1. CLASS OR MEDIAN SIZE OF RIPRAP AND LENGTH, WIDTH AND DEPTH OF APRON TO BE DESIGNED BY THE ENGINEER.
  2. REFER TO THE CHARLOTTE MECKLENBURG STORM WATER DESIGN MANUAL FOR RIPRAP APRON DESIGN STANDARDS.
  3. RIPRAP SHOULD EXTEND UP BOTH SIDES OF THE APRON AND AROUND THE END OF THE PIPE OR CULVERT AT THE DISCHARGE OUTLET AT A MAXIMUM SLOPE OF 2:1 AND A HEIGHT NOT LESS THAN TWO THIRDS THE PIPE DIAMETER OR CULVERT HEIGHT.
  4. THERE SHALL BE NO OVERFLOW FROM THE END OF THE APRON TO THE SURFACE OF THE RECEIVING CHANNEL. MAXIMUM TAPER TO RECEIVING CHANNEL: 5:1.
  5. THE WIDTH OF THE END OF THE APRON SHALL BE EQUAL TO THE BOTTOM WIDTH OF THE RECEIVING CHANNEL. MAXIMUM TAPER TO RECEIVING CHANNEL: 5:1.
  6. ALL SUBGRADE FOR STRUCTURE TO BE COMPACTED TO 15% OR GREATER.
  7. THE PLACING OF FILL, EITHER LOOSE OR COMPACTED, IN THE RECEIVING CHANNEL SHALL NOT BE ALLOWED.
  8. NO BENDS OR CURVES IN THE HORIZONTAL ALIGNMENT OF THE APRON WILL BE PERMITTED.
  9. FILTER FABRIC SHALL BE INSTALLED ON COMPACTED SUBGRADE PRIOR TO PLACEMENT OF RIP RAP.
  10. ANY DISTURBED AREA FROM END OF APRON TO RECEIVING CHANNEL MUST BE STABILIZED.
- USE USDA NON-ROCKY FILL FROM NC SEDIMENT AND EROSION CONTROL MANUAL OR CHARLOTTE MECKLENBURG STORM WATER DESIGN MANUAL FOR DESIGN DATA.

OUTLET	CFS	PIPE DIA. (IN)	APRON LENGTH "LO" (FT)	BOTTOM WIDTH "W" (FT)	TOP WIDTH "W2" (FT)	DEPTH "H" (IN)	HEIGHT "H" (IN)	STONE SIZE: d50 (IN)
FES 3	13.44	24"	22.0	6.0	28.6	12"	16"	6" STONE
FES 5	66.80	36" (2)	32.6	18.0	41.6	12"	24"	8" STONE
FES 7	1.73	15"	11.0	3.8	14.7	12"	10"	6" STONE
FES 11	1.73	15"	12.0	3.8	15.7	9"	10"	6" STONE

RIPRAP APRON AT PIPE OUTFALLS OTHER THAN AT SWIM  
NO SCALE  
CLD STD # 20.23



COMMERCIAL DROP CURB TYPE II DRIVEWAY WITH PLANTING STRIP  
NO SCALE  
CLD STD # 10.25B

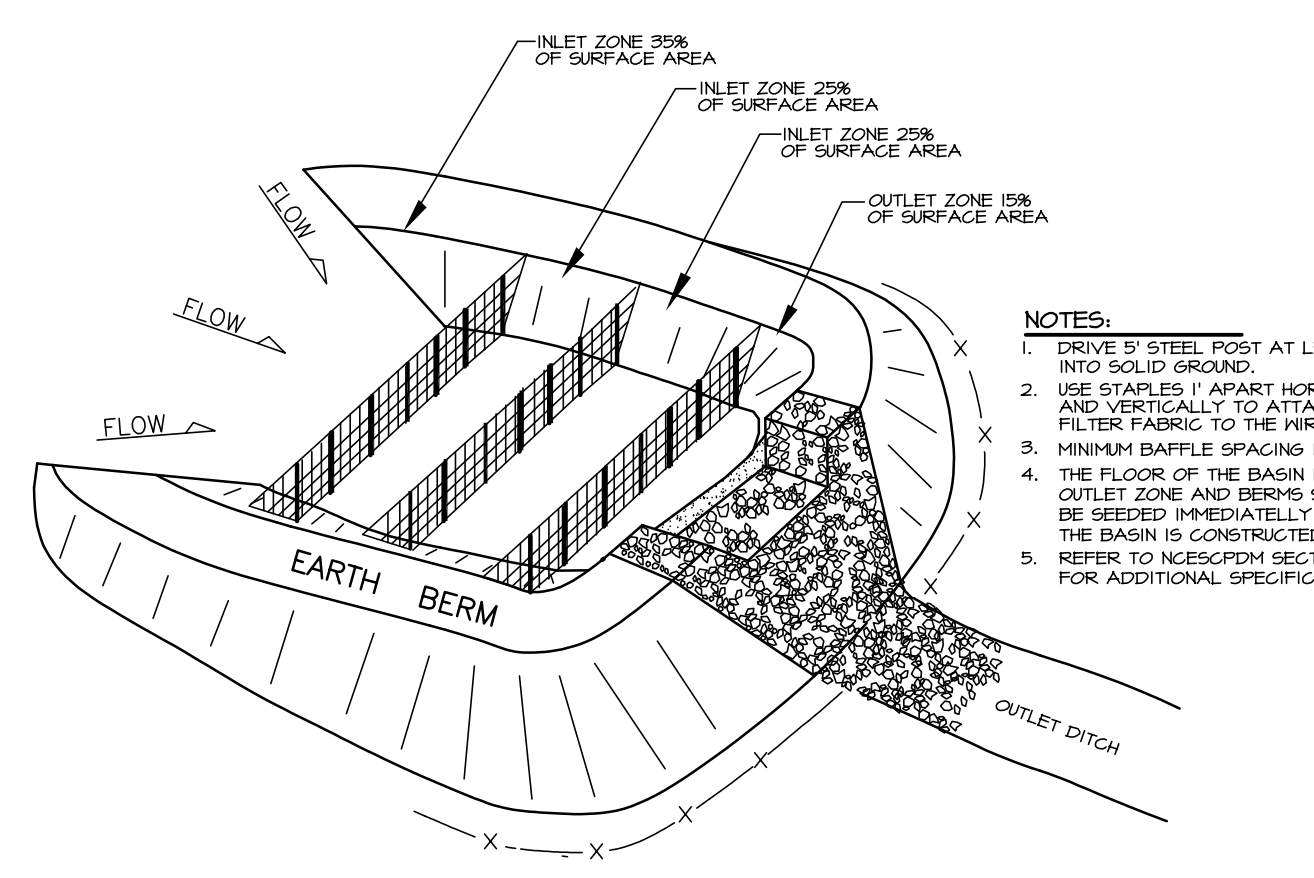


- NOTE:**
1. PLEASE REFER TO NCSCDPH SECTION 96.64 FOR ADDITIONAL FILTER FABRIC DESIGN SPECIFICATIONS REGARDING SKIMMER SEDIMENT BASINS.
  2. REFER TO STD 185.0 FOR Baffle Spacing and Installation.

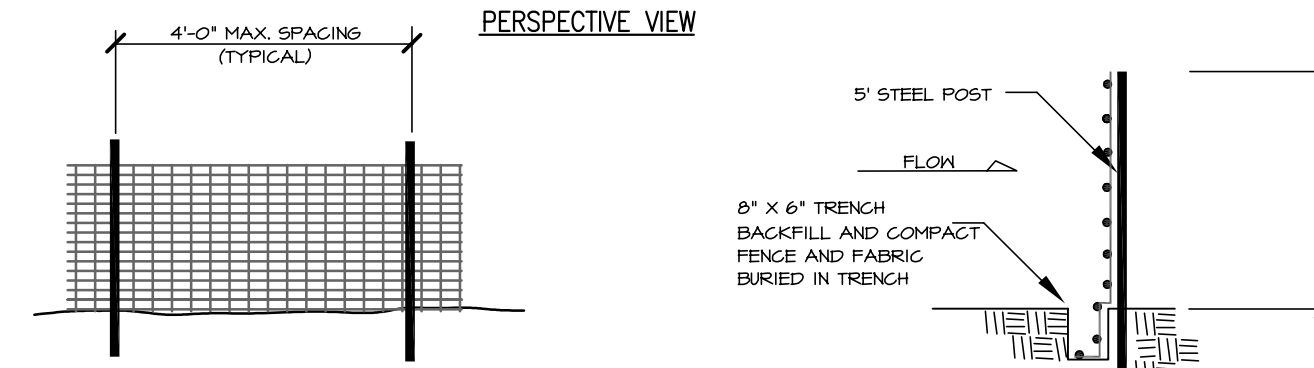
SKIMMER BASIN DATA BLOCK:

BASIN NO.	DRAINAGE AREA (AC)	DESIGNED AREA (AC)	Q10 (CFS)	TRAP SURFACE AREA REQUIRED PROVIDED (SQ FT)	TRAP VOLUME REQUIRED PROVIDED (CF)	CLEANOUT DEPTH (FT.)	H (FT)	L (FT)	T (FT)	M (FT)	X (FT)	SKIMMER PIPE DIAMETER	SKIMMER ORIFICE DIAMETER
1	2.0	2.0	6.8	2215 5066	3600 4332	1.1	2.3	10'	15'	5'	N/A	2"	FULL

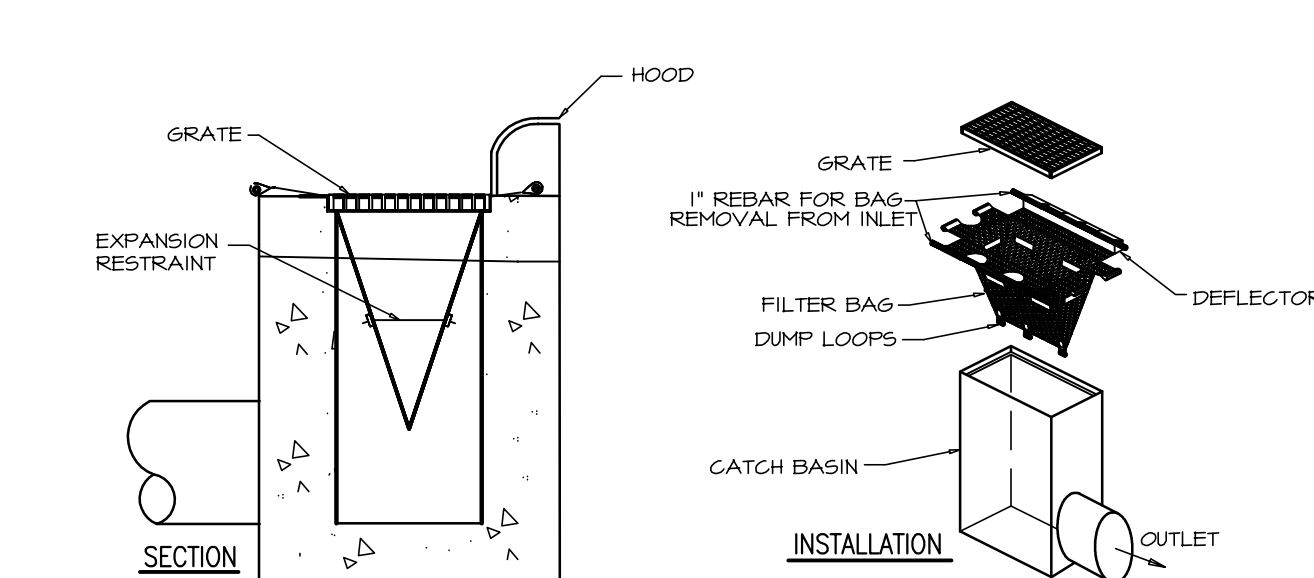
SKIMMER SEDIMENT BASIN  
NO SCALE  
CLD STD # 30.02A



- NOTES:**
1. DRIVE 5" STEEL POST AT LEAST 24" INTO SOLID GROUND.
  2. USE STAPLES 1" APART HORIZONTALLY AND VERTICALLY TO ATTACH THE FILTER FABRIC TO THE WIRE FENCE.
  3. MINIMUM Baffle SPACING IS 10'.
  4. THE FLOOR OF THE BASIN IN THE OUTLET ZONE AND BERTHS SHOULD BE SEEDS IMMEDIATELY AFTER THE BASIN IS CONSTRUCTED.
  5. REFER TO NCSCDPH SECTION 96.65 FOR ADDITIONAL SPECIFICATIONS.



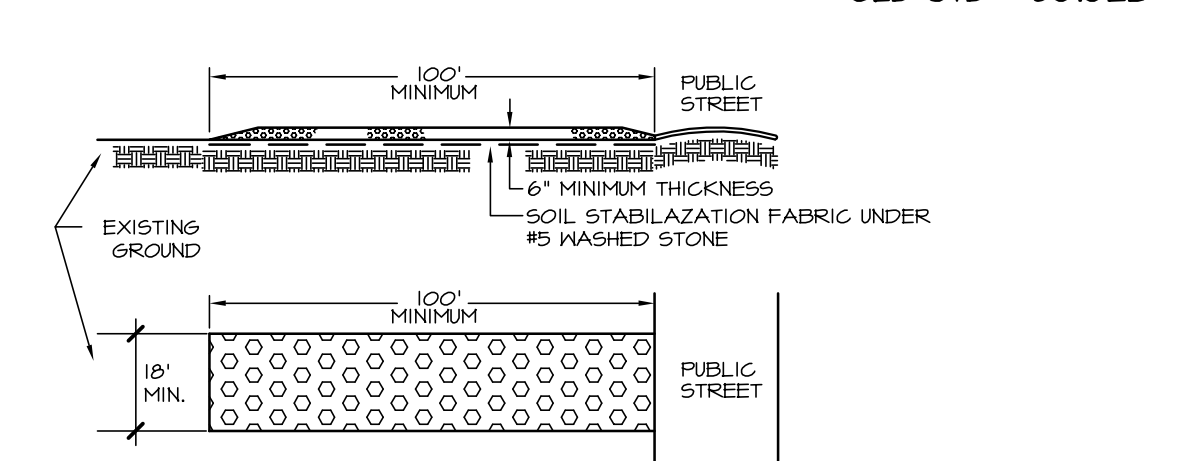
BAFFLE INSTALLATION  
NO SCALE  
CLD STD # 30.19



- NOTE:**
1. INLET MAINTENANCE SHALL BE DOCUMENTED IN PROJECT LOG BOOK.
  2. FILTER TYPES SHALL BE APPROVED BY THE CITY INSPECTOR PRIOR TO INSTALLATION.
  3. FILTER BAGS MAY BE REMOVED WHEN SITE IS STABILIZED AT THE DIRECTION OF THE ENGINEER.
  4. FILTER BAGS SHALL BE REMOVED PRIOR TO STREET ACCEPTANCE.
  5. FILTER BAGS SHALL BE CLEANED OR REPLACED ON A REGULAR BASIS (NOT BE MORE THAN HALF FULL AT ANY TIME).
  6. FILTER BAGS SHALL NOT BE ALLOWED IN EXISTING CITY OR NC DOT ROADS.

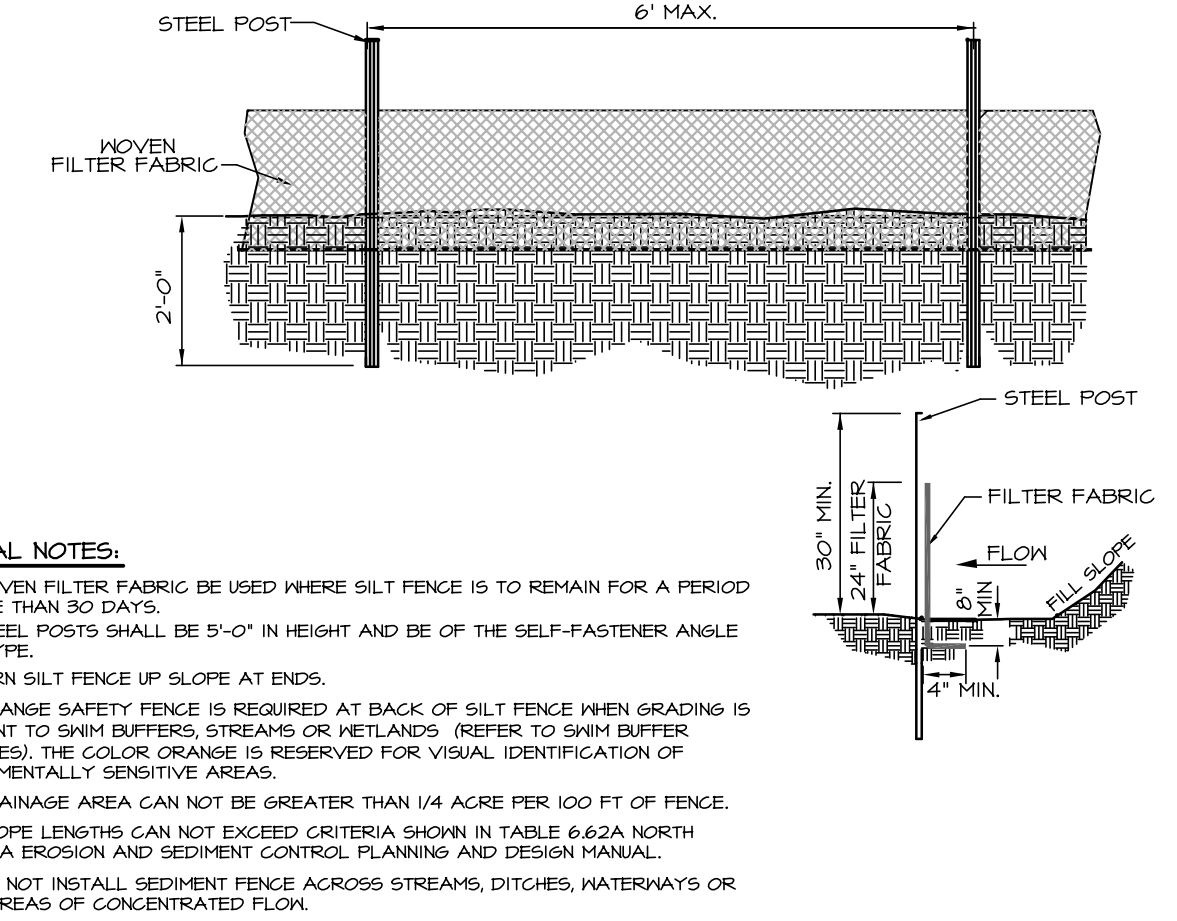
CATCH BASIN INLET PROTECTION  
NO SCALE  
CLD STD # 30.15

SKIMMER  
NO SCALE  
CLD STD # 30.02B



- GENERAL NOTES:**
1. A STABILIZED ENTRY PAD OF 15 WASHED STONE OR RAILROAD BALLAST SHALL BE LOCATED WHERE TRAFFIC WILL ENTER OR LEAVE THE CONSTRUCTION SITE ONTO A PUBLIC STREET.
  2. FILTER FABRIC OR COMPACTED CRUSHER RUN STONE MAY BE USED AS A BASE FOR THE CONSTRUCTION ENTRY.
  3. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC STREETS OR EXISTING PAVEMENT. THIS MAY REQUIRE PERIODIC TOP DRESSINGS WITH ADDITIONAL STONE AS CONDITIONS WARRANT AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.
  4. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC STREETS MUST BE REMOVED IMMEDIATELY.
  5. WHEN APPROPRIATE, WHEELS MUST BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTERING A PUBLIC WATERWAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE WHICH DRAINS INTO AN APPROVED SEDIMENT BASIN (SEE STD. 30.18).
  6. CLOT MAY REQUIRE A STANDARD COMMERCIAL DRIVEWAY (STD. 10.24/10.25) TO ACCESS THE CONSTRUCTION SITE IF THE DRIVEWAY IS ON A THOROUGHFARE.

STABILIZED CONSTRUCTION ENTRANCE  
NO SCALE  
CLD STD # 30.11A

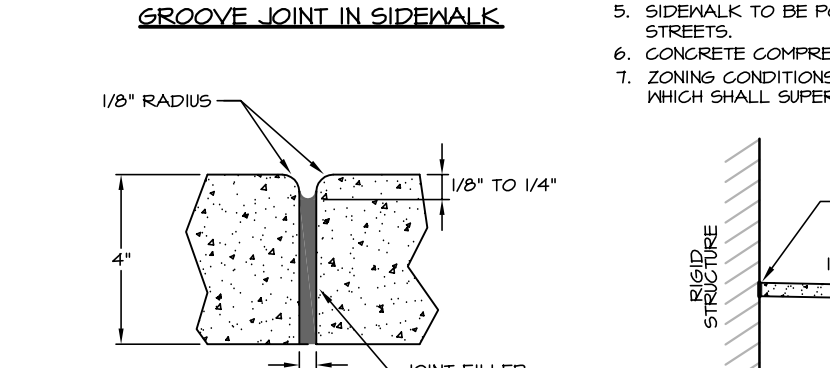
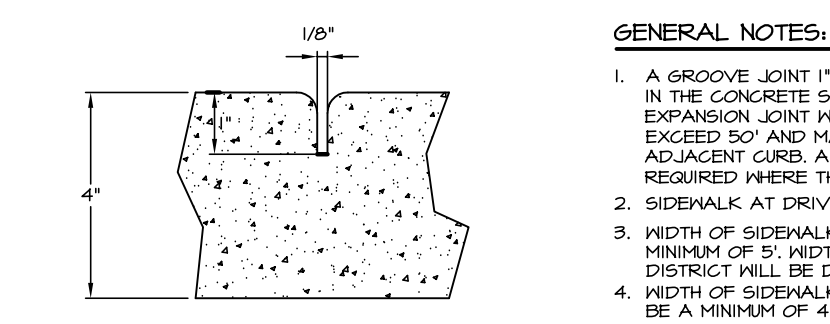


- GENERAL NOTES:**
1. MOVEN FILTER FABRIC BE USED WHERE SILT FENCE IS TO REMAIN FOR A PERIOD OF MORE THAN 30 DAYS.
  2. STEEL POSTS SHALL BE 5'-0" IN HEIGHT AND BE OF THE SELF-FASTENER ANGLE STEEL TYPE.
  3. TURN SILT FENCE UP SLOPE AT ENDS.
  4. ORANGE SAFETY FENCE IS REQUIRED AT BACK OF SILT FENCE WHEN GRADING IS ADJACENT TO SWIM BUFFERS, STREAMS OR RETAINERS. PREFER TO SWIM BUFFER GUIDELINES. THE COLOR ORANGE IS RESERVED FOR VISUAL IDENTIFICATION OF ENVIRONMENTALLY SENSITIVE AREAS.
  5. DRAINAGE AREA CAN NOT BE GREATER THAN 1/4 ACRE PER 100 FT OF FENCE.
  6. SLOPE LENGTHS CAN NOT EXCEED CRITERIA SHOWN IN TABLE 6.6.2A NORTH CAROLINA EROSION AND SEDIMENT CONTROL PLANNING AND DESIGN MANUAL.

- MAINTENANCE NOTES:**
1. FILTER BARRIERS SHALL BE INSPECTED BY THE FINANCIALLY RESPONSIBLE PARTY OR HIS AGENT IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REPAIRS NEEDED SHALL BE MADE IMMEDIATELY.
  2. SHOULD THE FABRIC DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE AND THE BARRIER STILL IS NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY.
  3. SEDIMENT DEPOSITS SHOULD BE REMOVED WHEN DEPOSITS REACH APPROX. HALF THE HEIGHT OF THE BARRIER. ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE IS REMOVED SHALL BE DRESSED TO CONFORM TO THE EXISTING GRADE, PREPARED AND SEEDS.

TEMPORARY SILT FENCE  
NO SCALE  
CLD STD # 30.06A

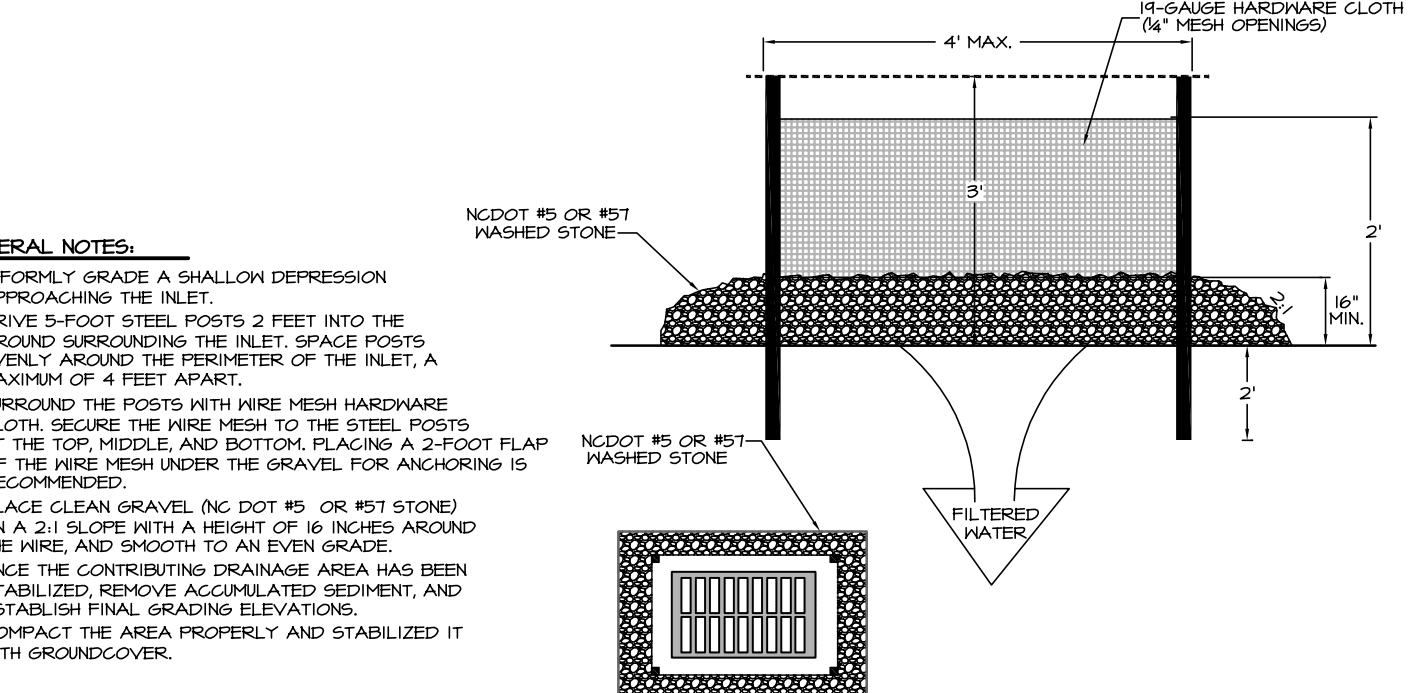
LOCAL COLLECTOR STREET  
NO SCALE  
CLD STD # U-07(MODIFIED)



TRANSVERSE EXPANSION JOINT IN SIDEWALK

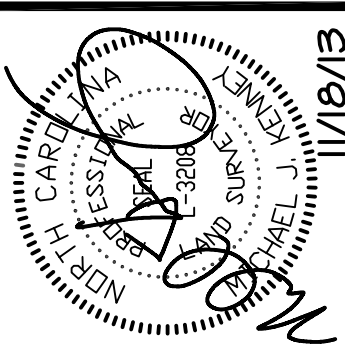
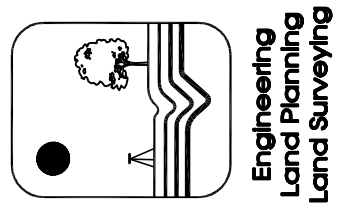
DETAILS SHOWING EXPANSION JOINTS IN CONCRETE SIDEWALK

CONCRETE SIDEWALKS  
NO SCALE  
CLD STD # 10.22



- GENERAL NOTES:**
1. UNIFORM GRADE A SHALLOW DEPRESSION APPROACHING THE INLET.
  2. DRIVE 5-FOOT STEEL POSTS 2 FEET INTO THE GROUND SURROUNDING THE INLET. SPACE POSTS EVENLY AROUND THE PERIMETER OF THE INLET, A MAXIMUM OF 4 FEET APART.
  3. SURROUND THE POSTS WITH WIRE MESH HARDWARE CLOTH. SECURE THE WIRE MESH TO THE STEEL POSTS AT THE TOP, MIDDLE AND BOTTOM USING A 3-FOOT FLAP OF THE WIRE MESH UNDER THE GRAVEL FOR ANCHORING IS RECOMMENDED.
  4. PLACE CLEAN GRAVEL NG DOT #5 OR #57 STONE ON A 21 SLOPE WITH A HEIGHT OF 16 INCHES AROUND THE WIRE AND 3/4 INCH TO AN EVEN GRADE.
  5. ONCE THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED, REMOVE AGGREGATED SEDIMENT, AND ESTABLISH FINAL GRADING ELEVATIONS.
  6. COMPACT THE AREA PROPERLY AND STABILIZED IT WITH GRASS COVER.

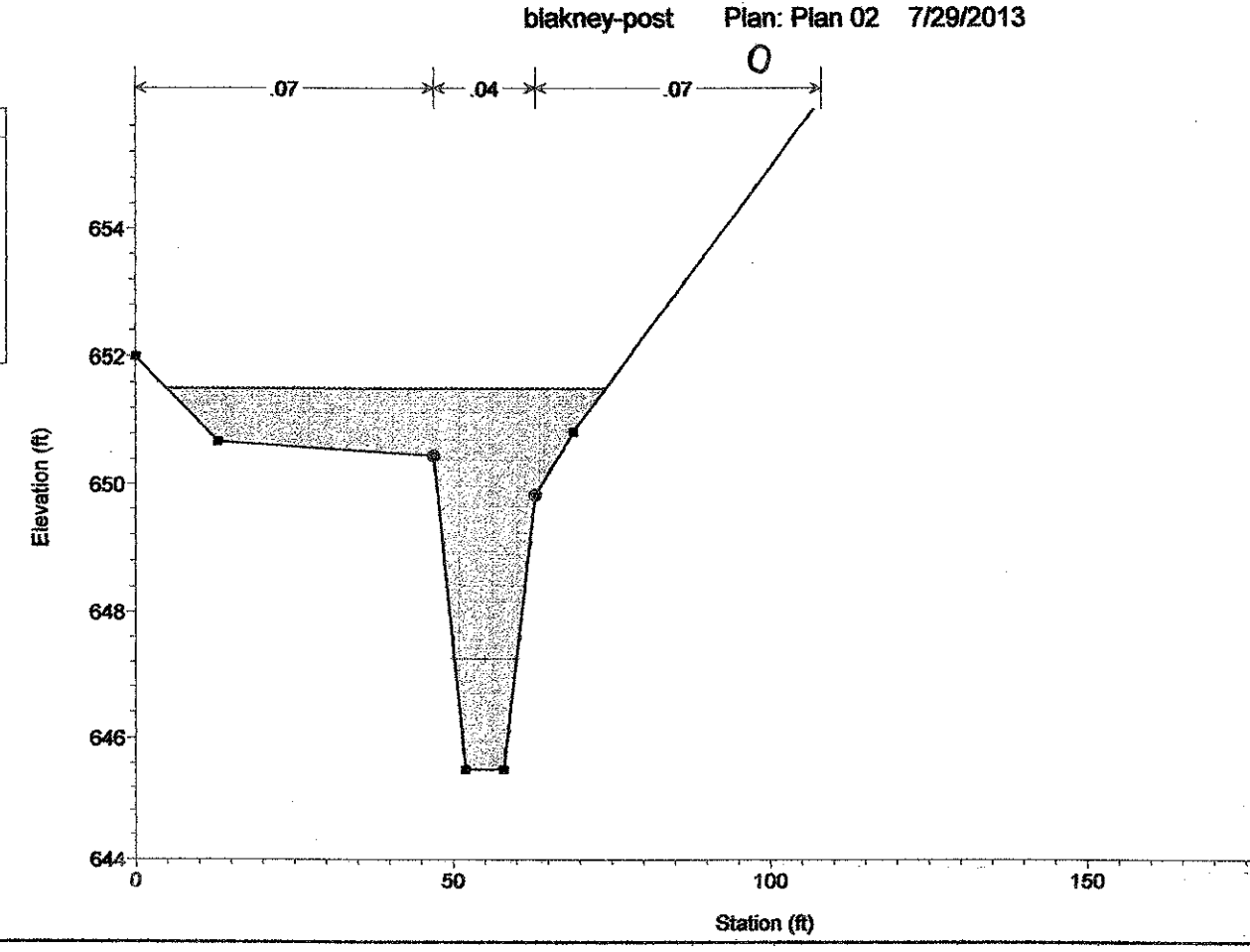
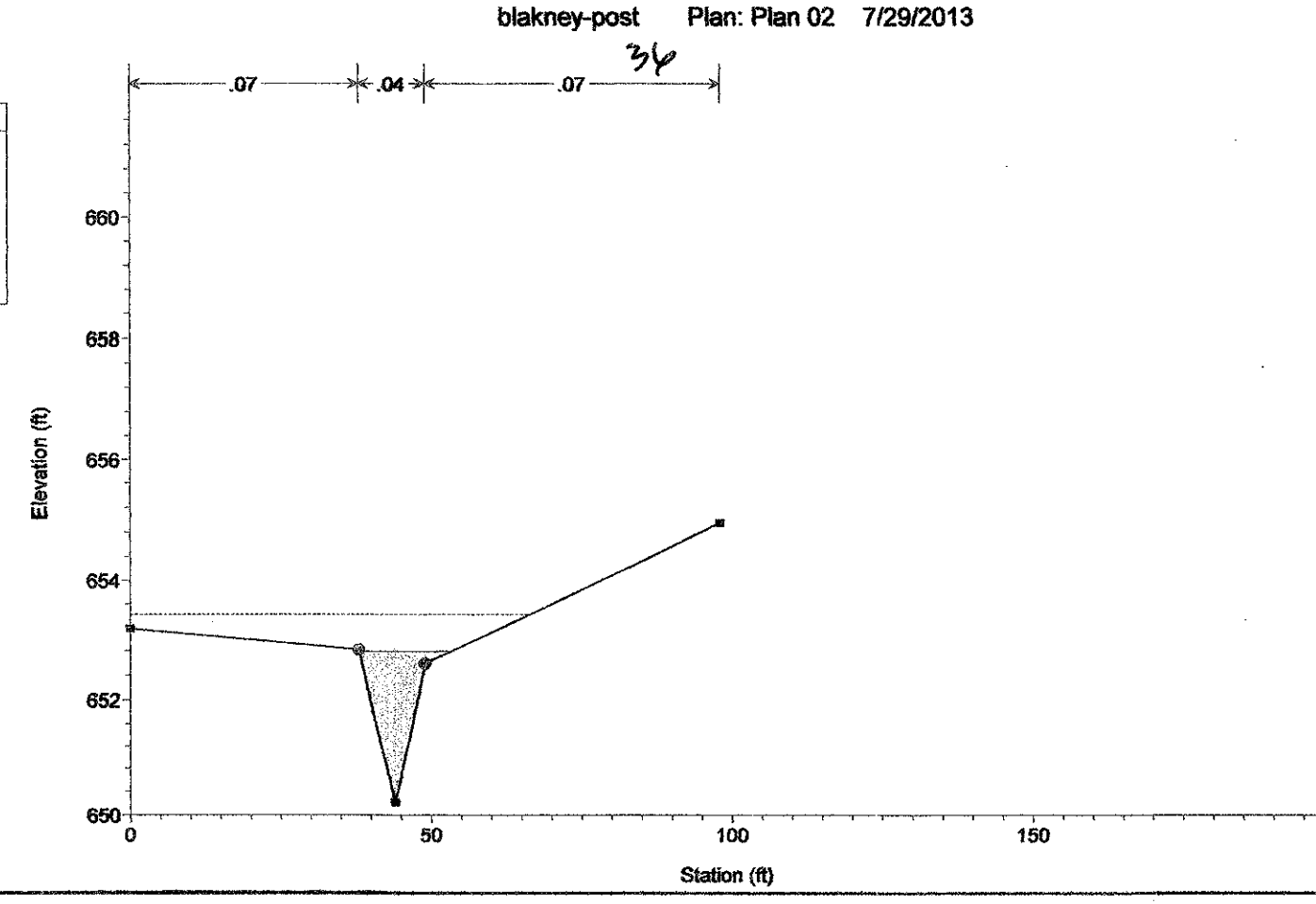
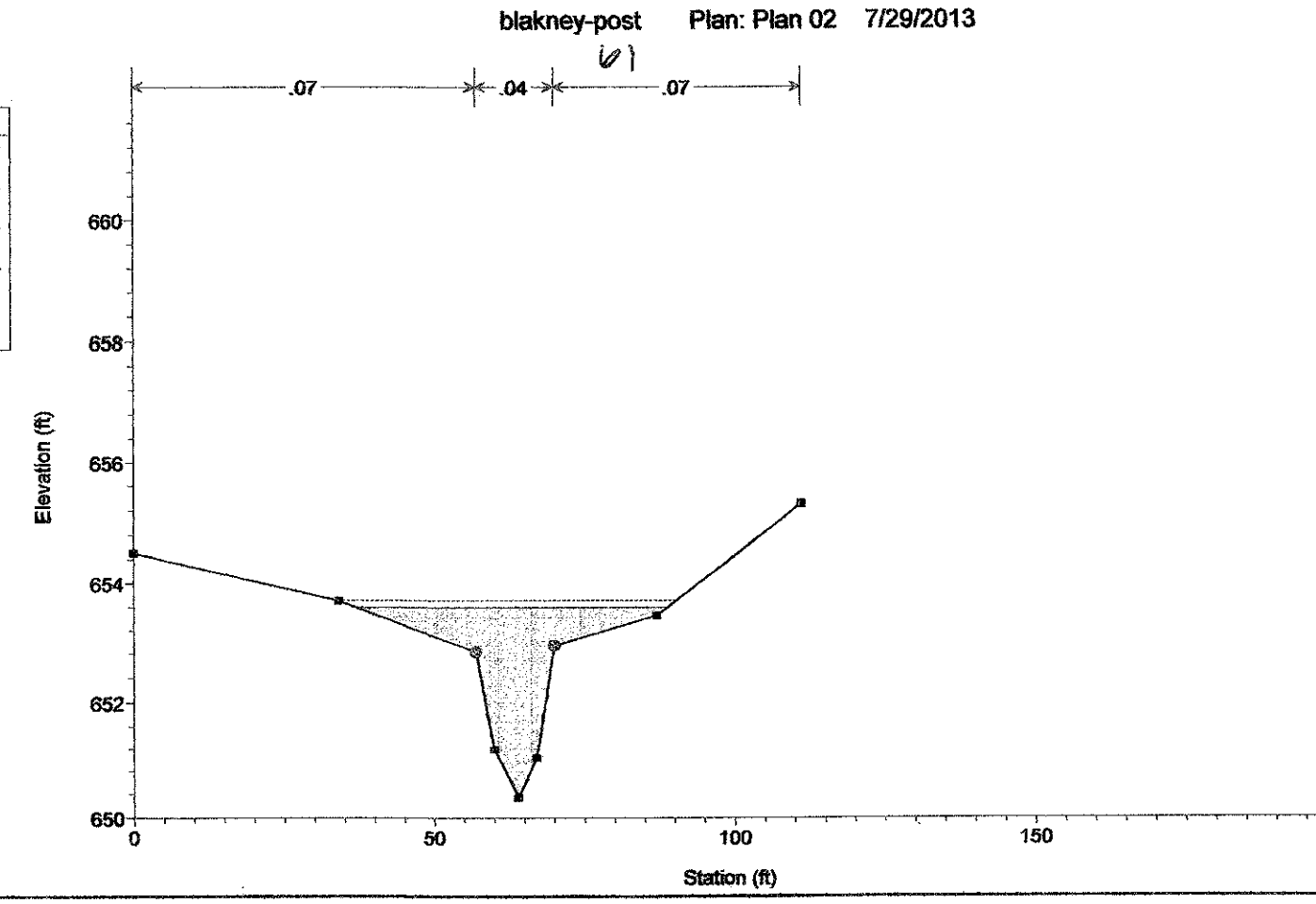
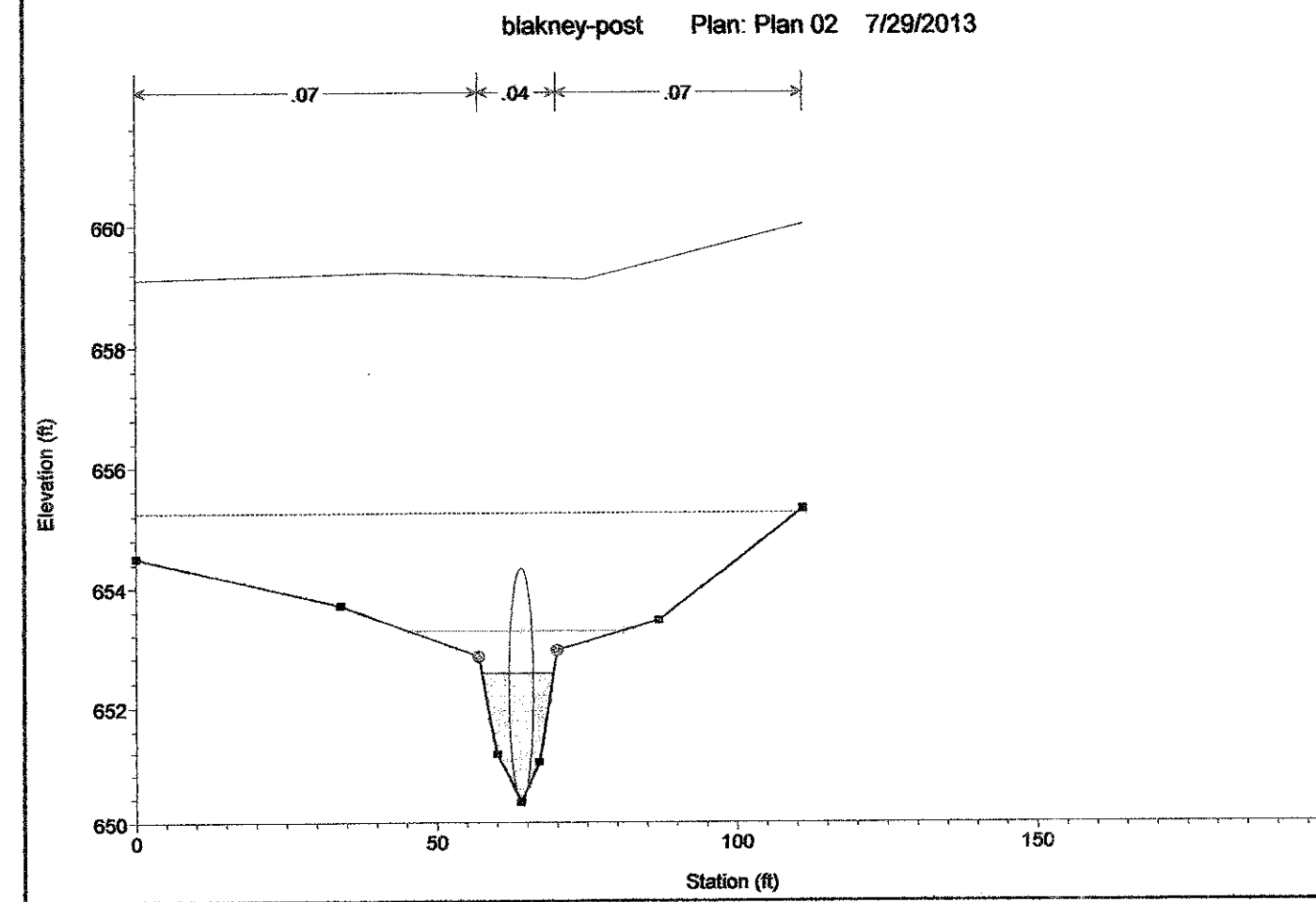
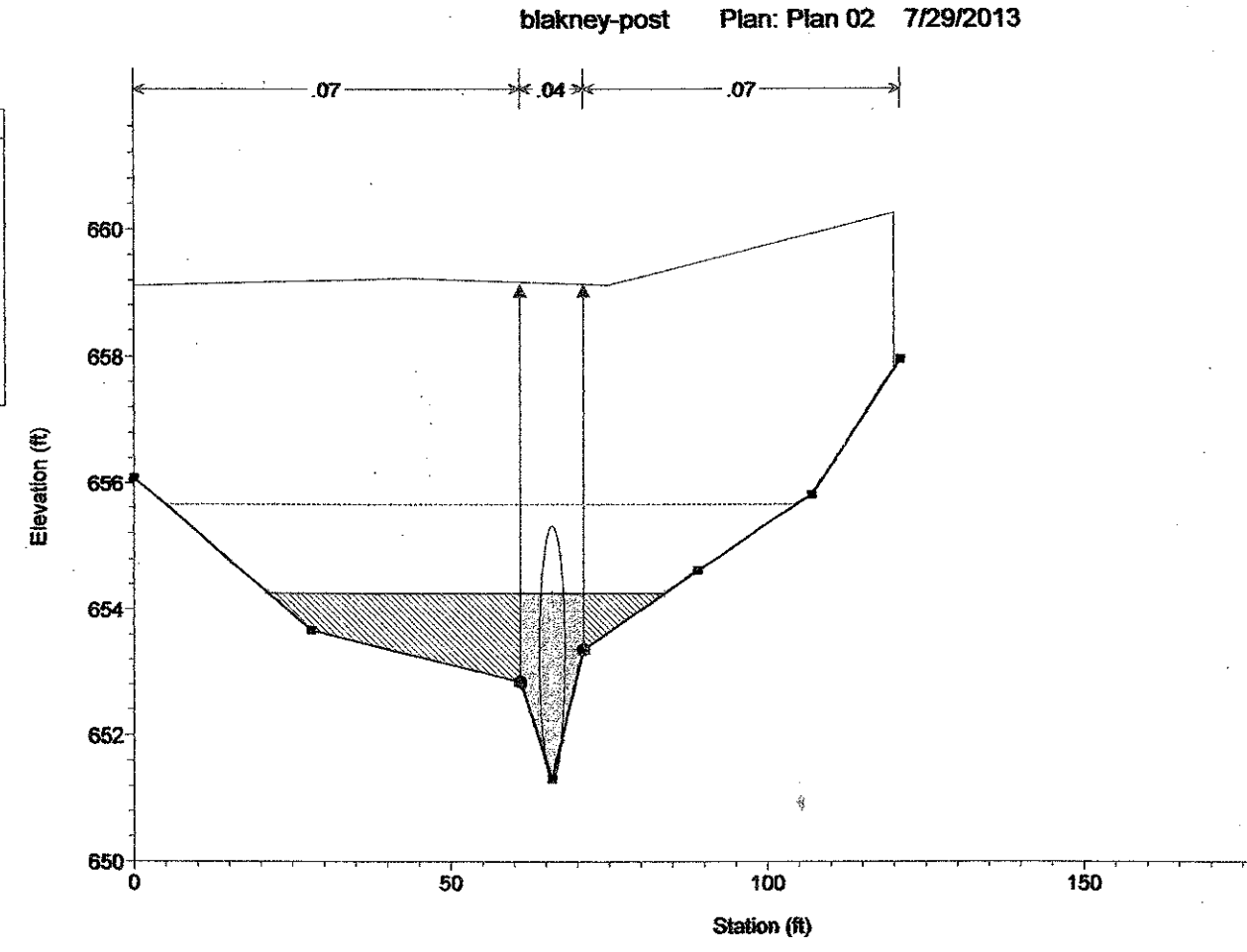
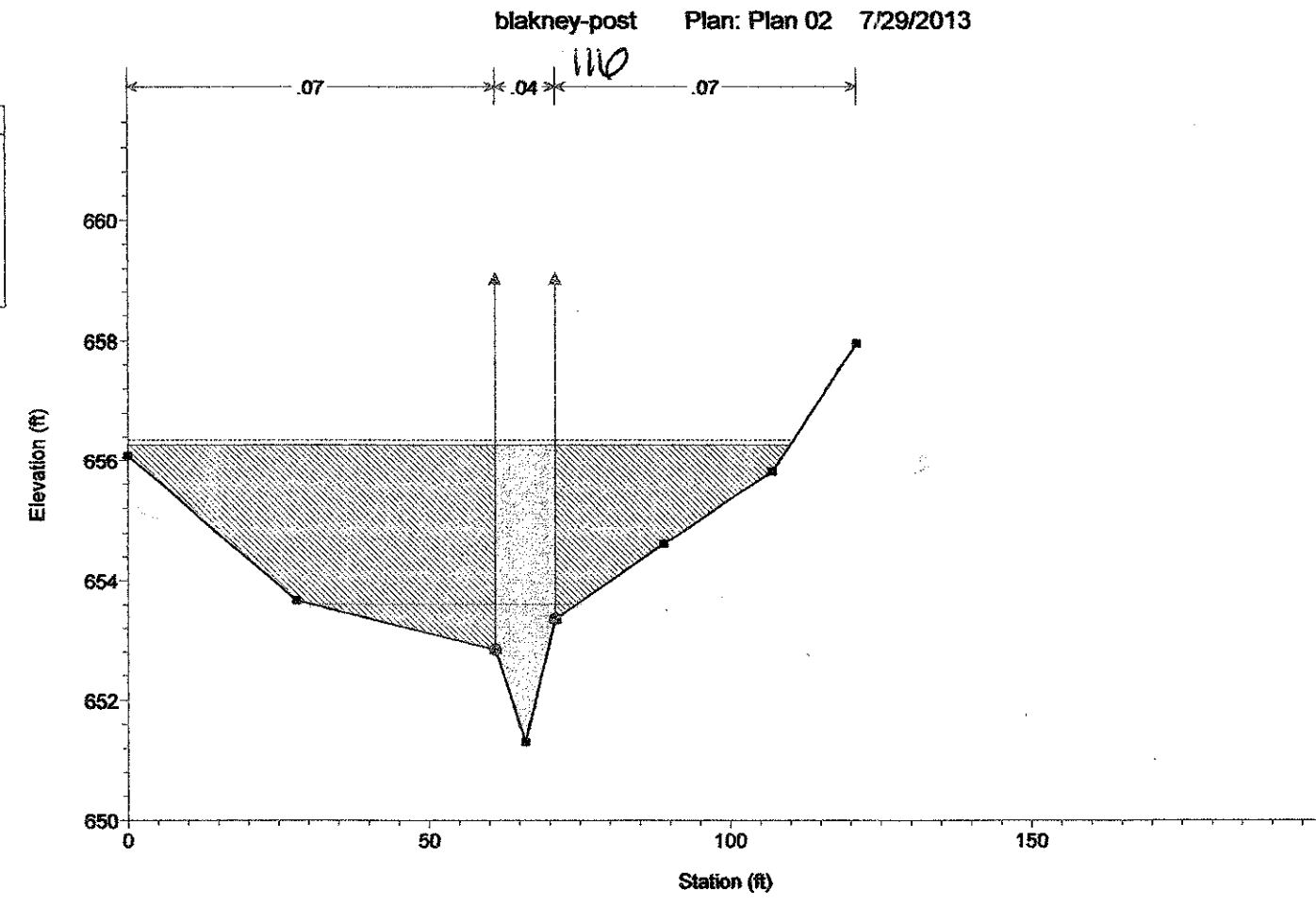
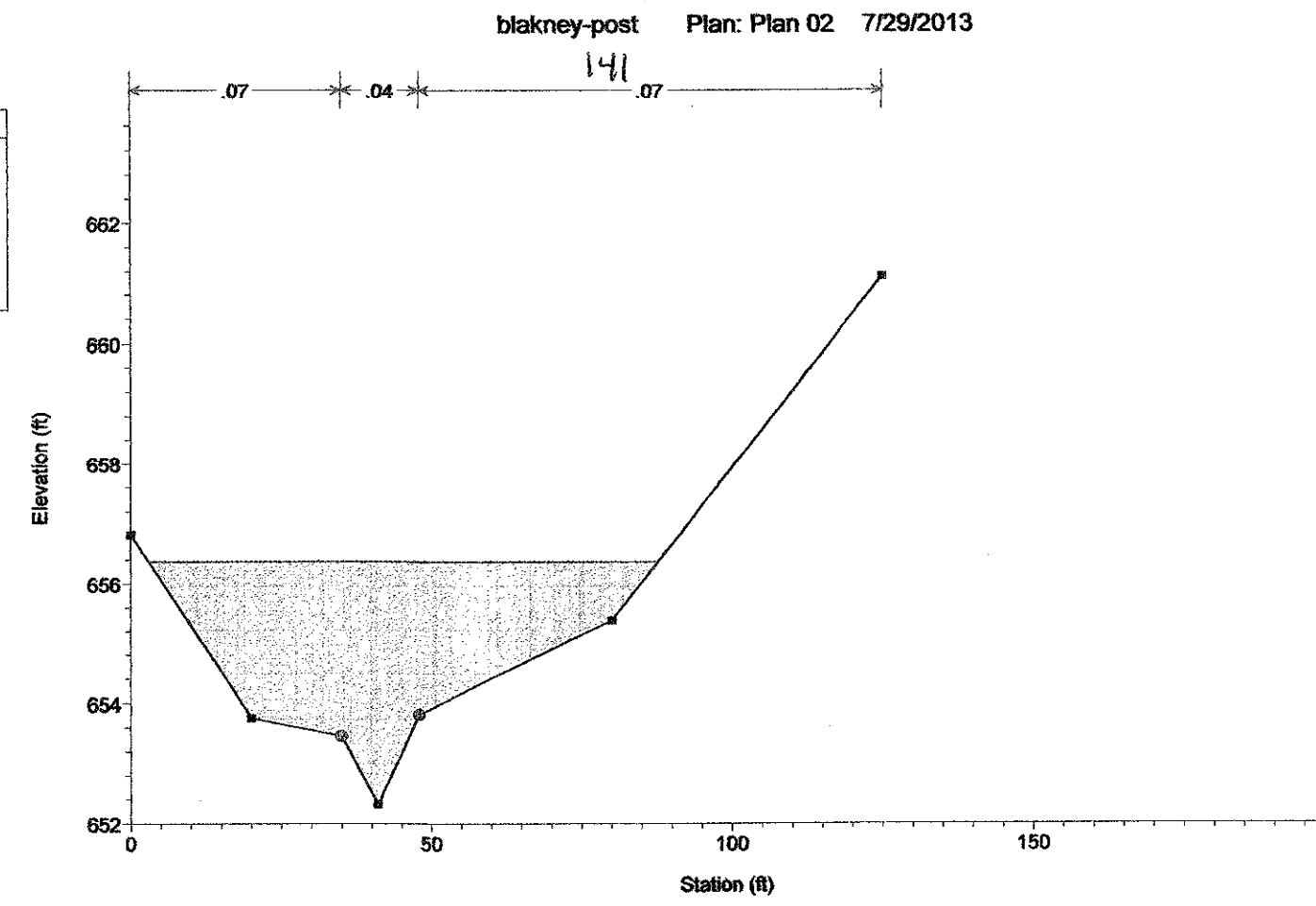
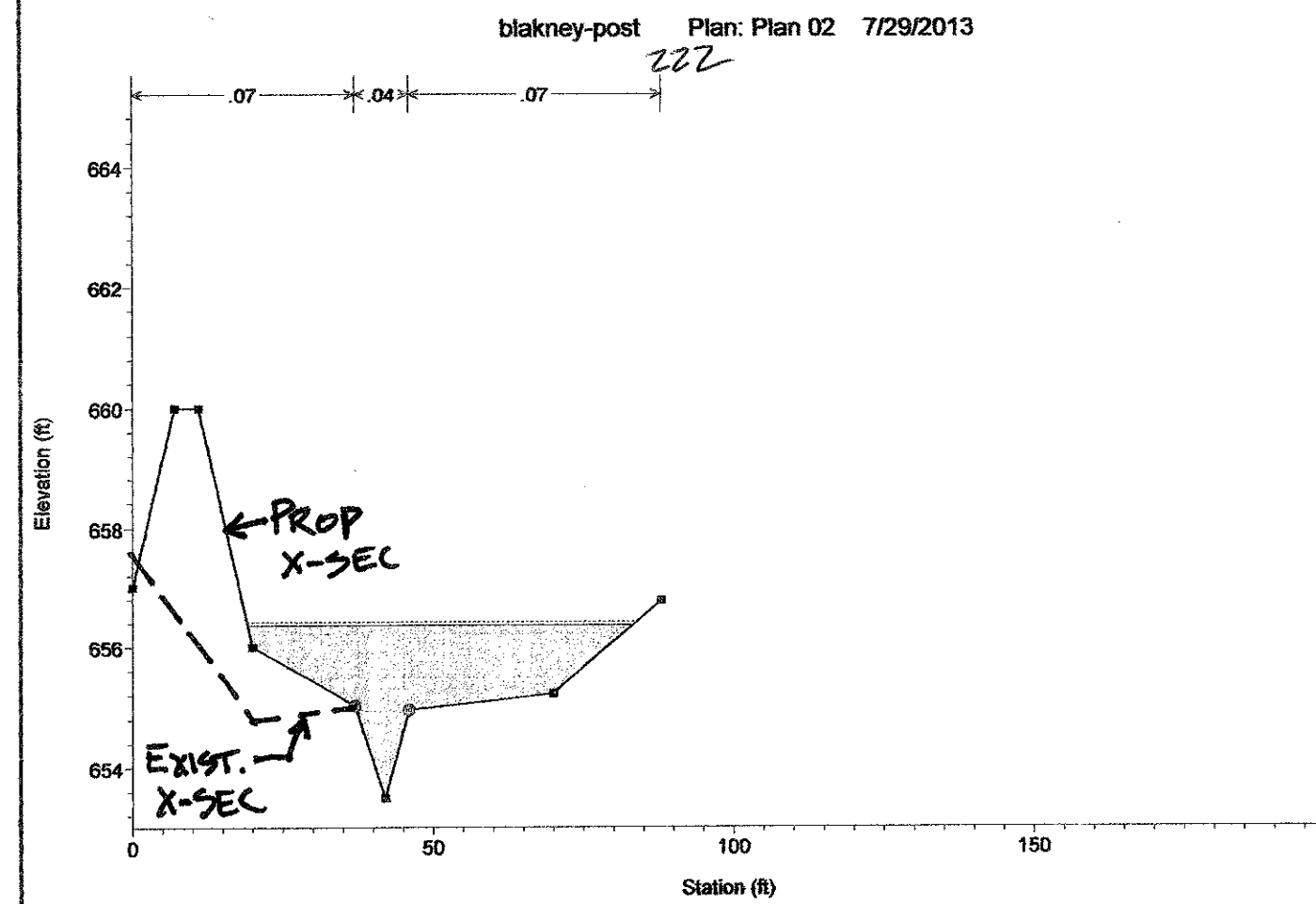
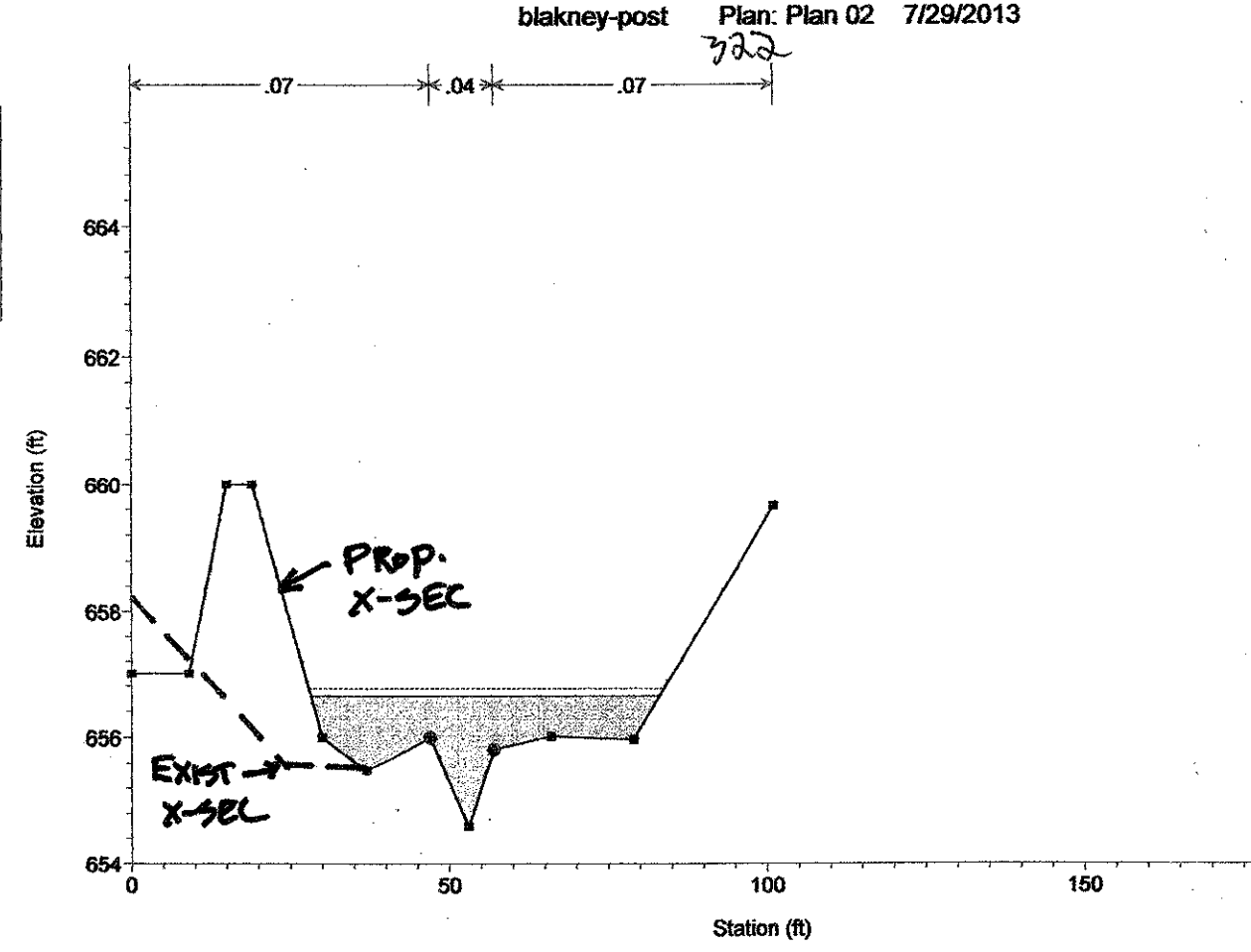
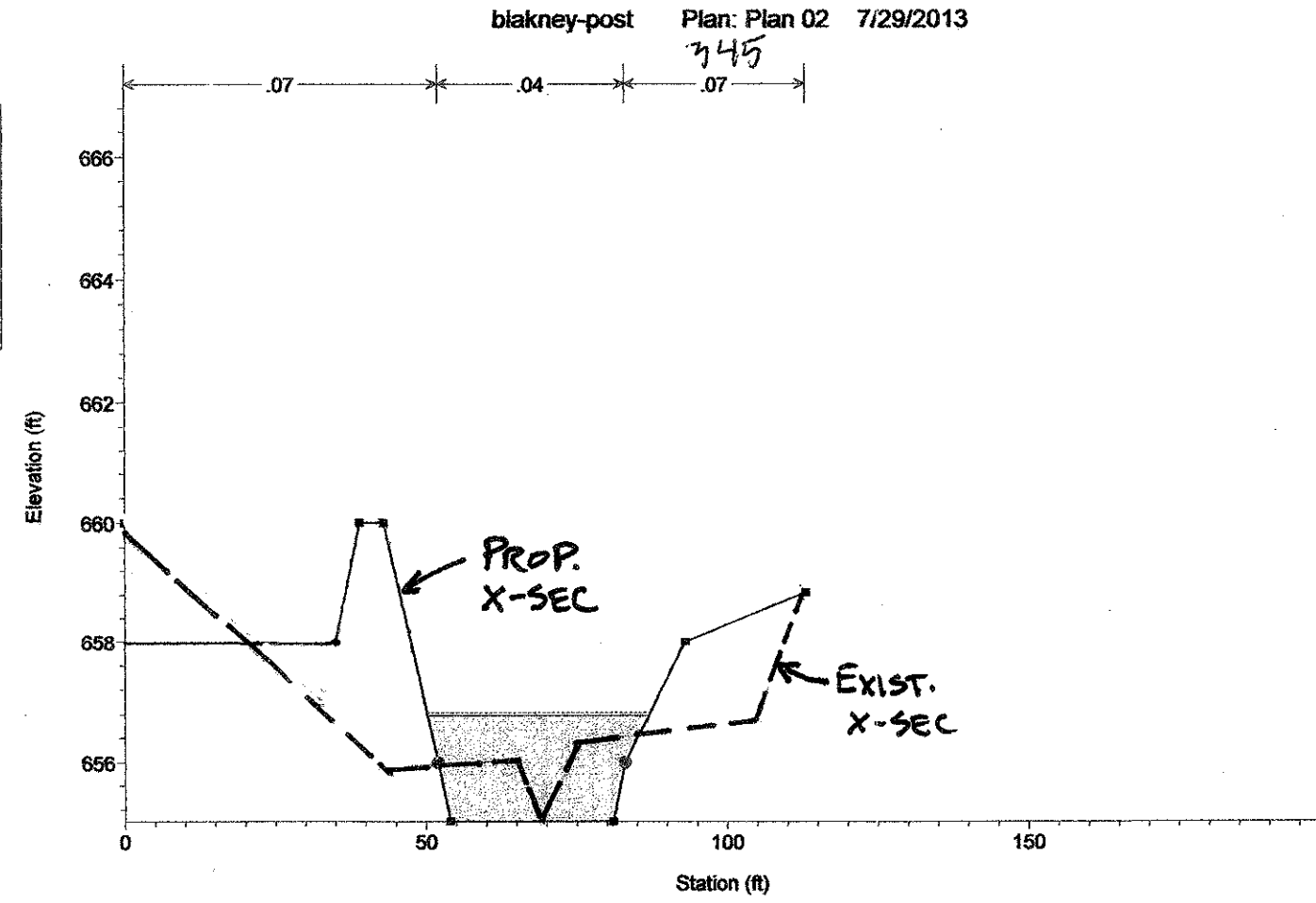
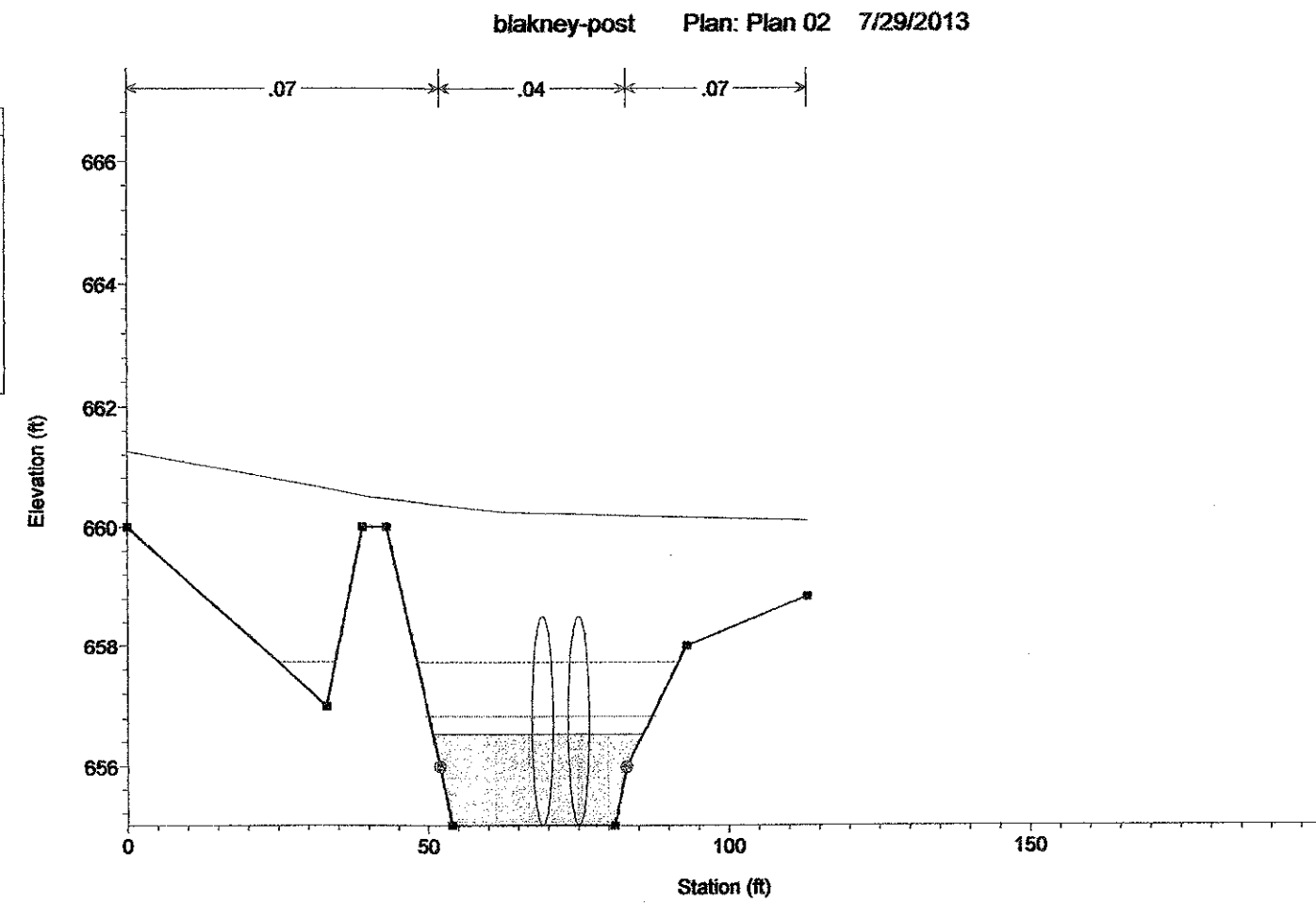
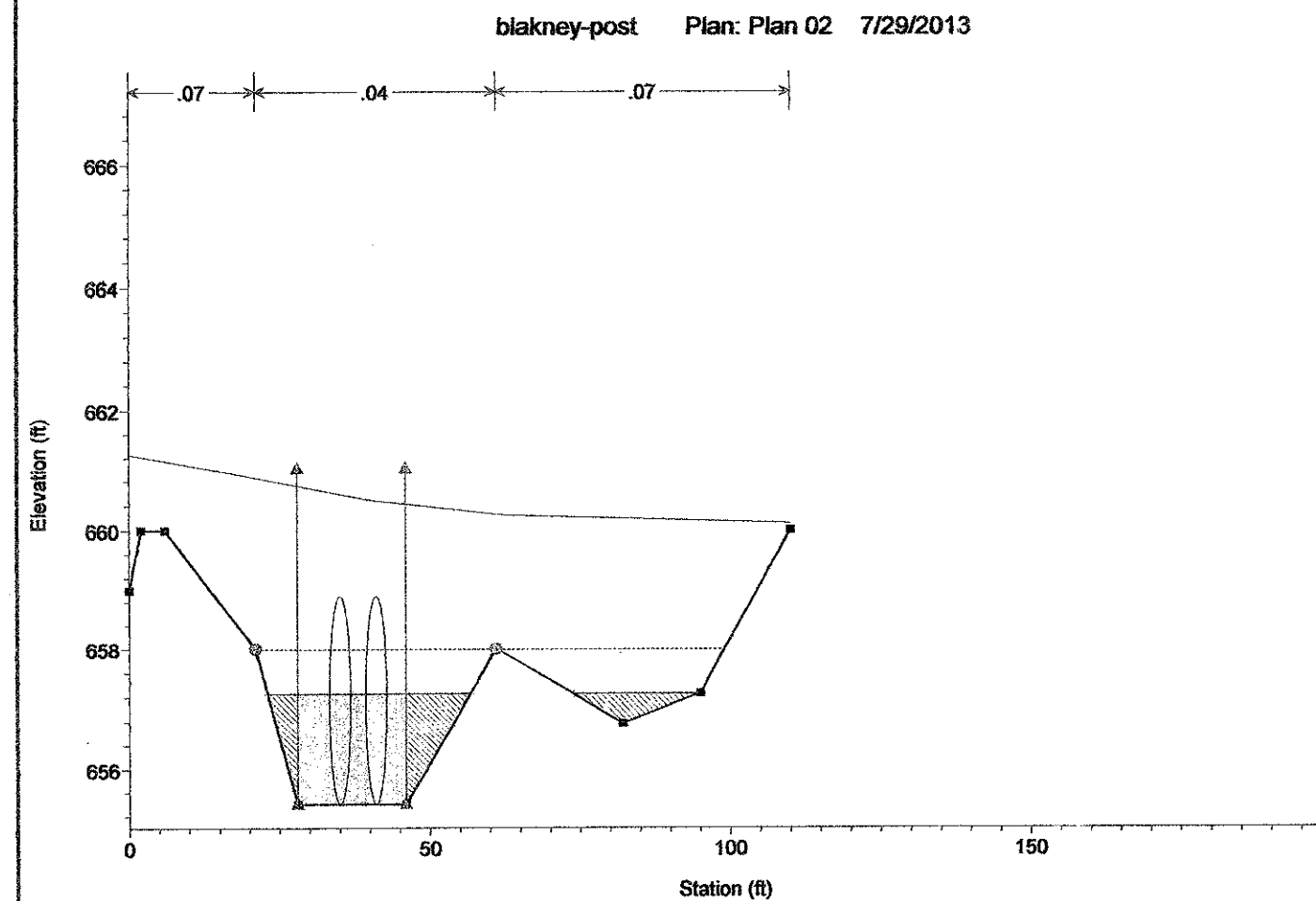
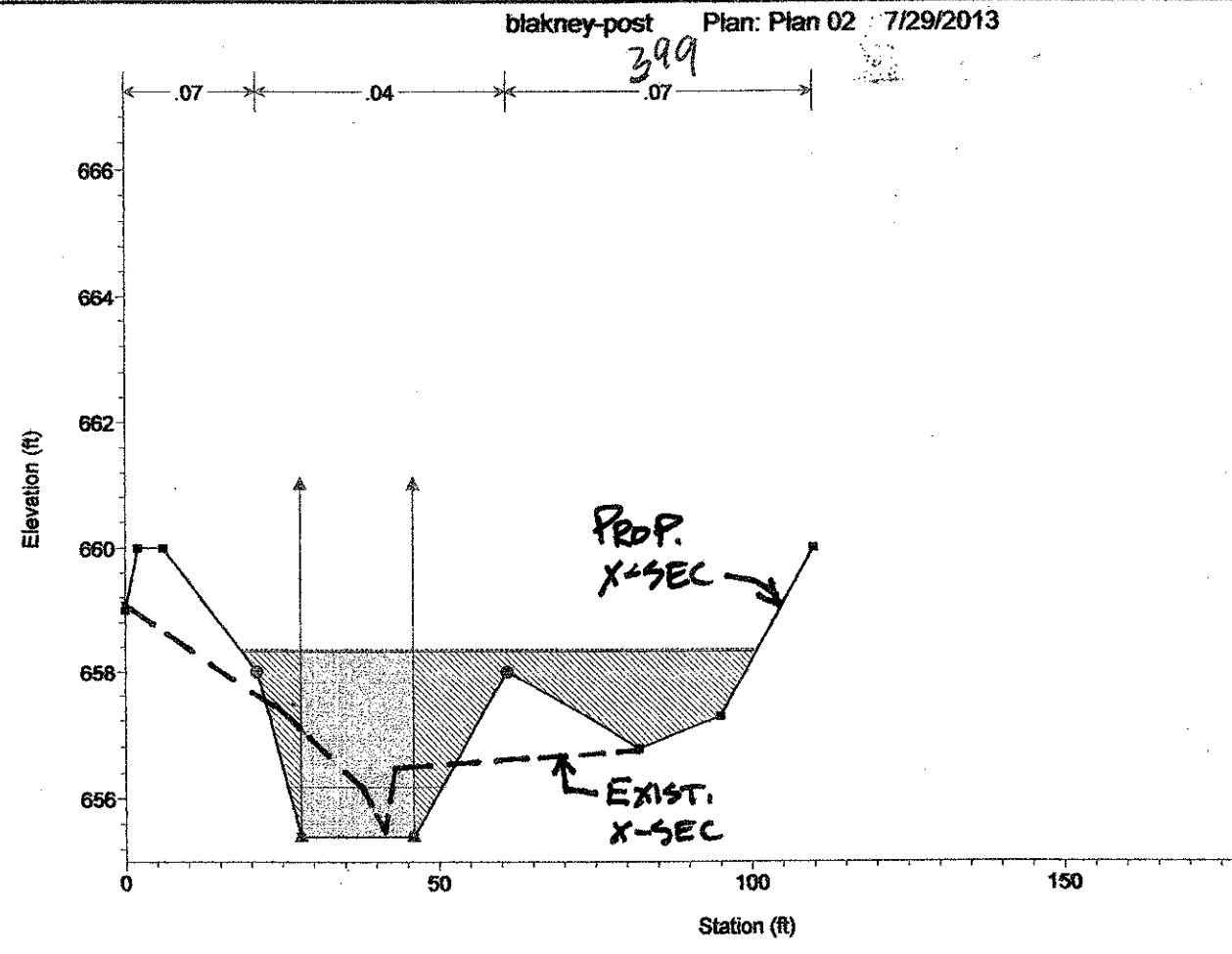
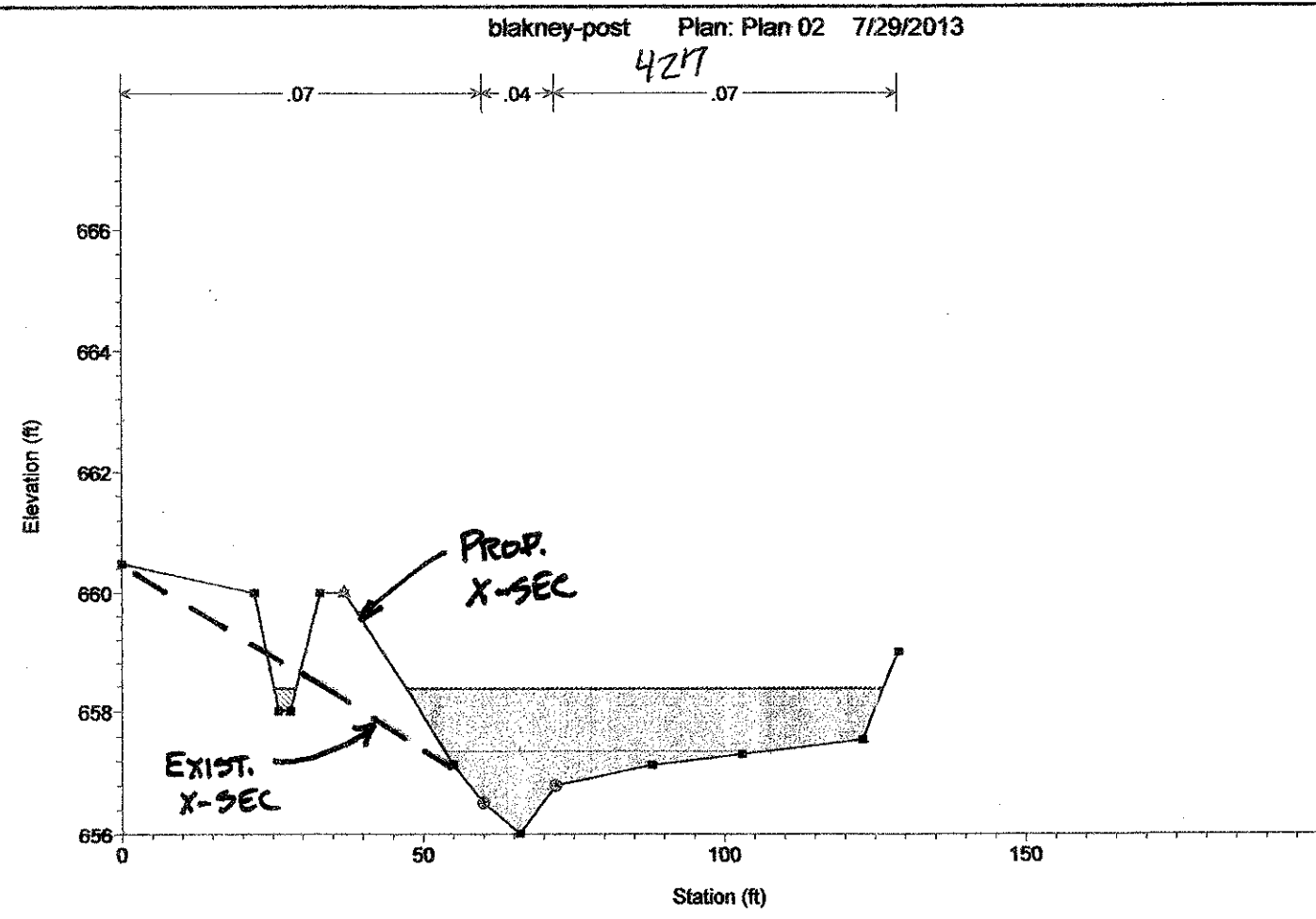
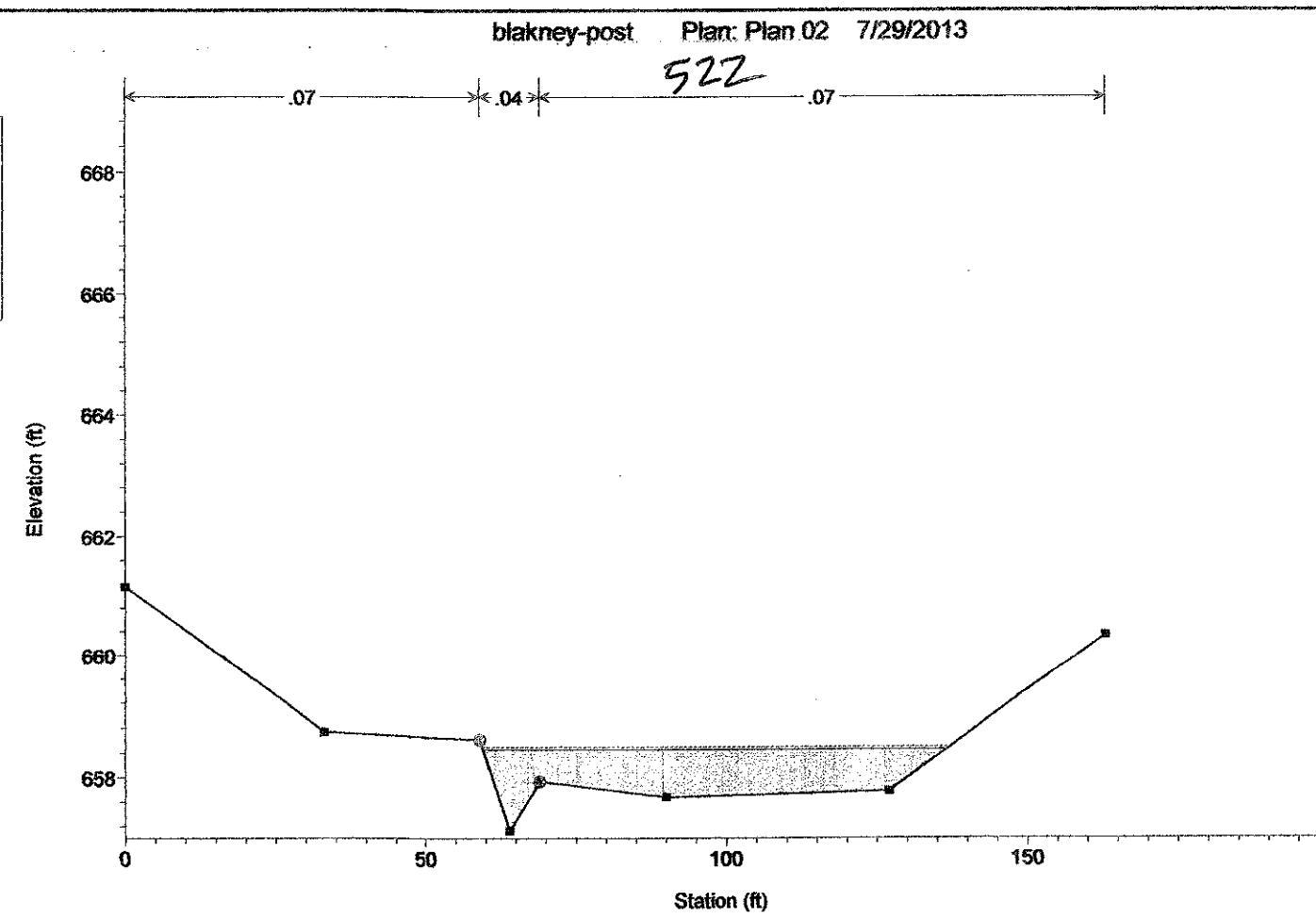
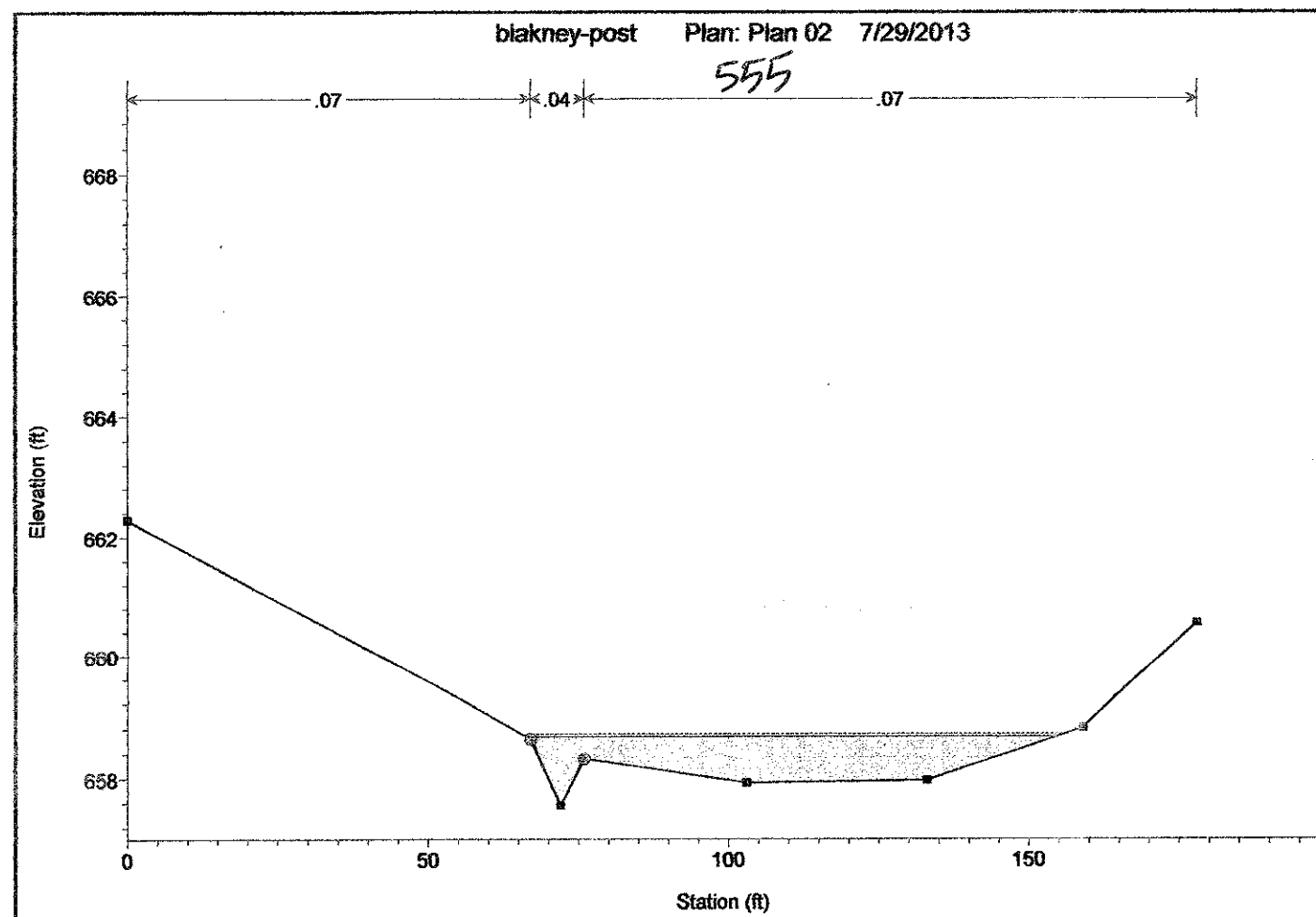
HARDWARE CLOTH & GRAVEL INLET PROTECTION  
NO SCALE  
CLD STD # 30.09





9 of





1 in Horiz. = 30 ft 1 in Vert. = 3 ft



Proposed X-sec  
and WSLS

**KENNEY DESIGN GROUP, PA**

1316 GREENWOOD CLIFF  
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C-1986

Engineering  
Land Surveying

Revisions  
9/29/13 X-SEC 222, 522, 745

HORIZ. 1" = 30'  
VERT. 1" = 3'

Date: 8/01/13  
Drawn By: MMK  
Designed By: MMK  
Job No.: 0712

**100+1 CROSS-SECTIONS**

**BLAKENEY RETREAT**

City of Charlotte, Mecklenburg County, North Carolina  
Classica Homes, 1101 Wood Ridge Center Dr, Suite #155, Charlotte, NC 28217

Sheet No. **7** of **9**



[illegible]

Diagram illustrating a Type III lane closure with a 4' edge of the excavation. The diagram shows the layout of the work area, including the downstream taper, variable section, shift taper, and the final taper. Key dimensions and components are labeled:

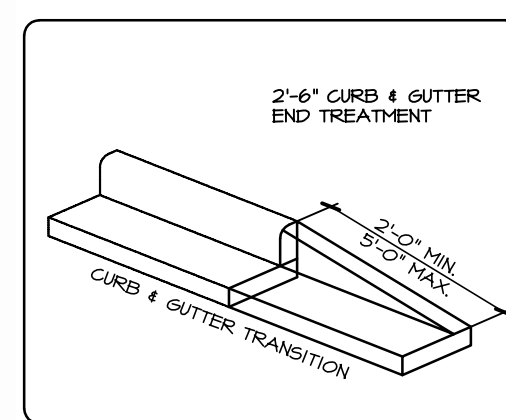
- Downstream Taper:** 100' (200')
- Variable Section:** 175' (205')
- Shift Taper:** 125' (250')
- Final Taper:** 200' (350')
- Work Area:** 26' (46')
- Clearance:** 10' MINIMUM CLEARANCE
- Signage:** "ROAD NARROWS AHEAD" and "ROAD CLOSURE AHEAD" signs are shown.
- Legend:**
  - Cones/Drums (Orange circles)
  - Signs (T symbol)
  - Type III Lane Closure (See Note 4) (Hatched area)
  - Work Area (Gray area)
  - Arrow Panel (Black rectangle with yellow dots)


**NOTES:**

- IF CLOSURE IS LONG-TERM (SEE SECTION 16), POINTED END DOWNSHIFT MACHINERY SHOULD BE USED.
- WHEN EXCAVATING WITHIN 4' OF THE EDGE OF THE EXISTING PAVEMENT, AN ARROW PANEL SHOULD BE USED.

NOTES:

1. IF CLOSURE IS LONG-TERM (SEE SECTION 16), CENTERLINE PAVEMENT MARKINGS SHALL BE REVISED TO MATCH TRAFFIC PATTERN. DRUMS ON CENTERLINE ARE NOT REQUIRED IF PAVEMENT MARKINGS ARE REVISED.
2. SPACING - 35 M.P.H.  
(SPACING) - ABOVE 35 M.P.H.
3. WHEN EXCAVATING WITHIN 4' OF THE EDGE OF PAVEMENT, THE ADJACENT LANE SHOULD BE CLOSED TO TRAFFIC USING OTHER APPLICABLE DIAGRAMS UNLESS BRACING OR SHORING IS USED.
4. TYPE III BARRICADE REQUIRED ONLY IF WORK ZONE IS LEFT UNATTENDED OVERNIGHT.



 CONSTRUCTION AREA

- NON-METALIC DRUM WITH TYPE C STEADY BURN WARNING LIGHTS
- \* (200') FOR SPEED LIMIT > 35 MPH

S = MAX. SPACING BETWEEN CHANNELIZING DEVICES SHOULD NOT EXCEED THE LEGAL SPEED LIMIT IN MILES PER HOUR. FOR EXAMPLE, IF THE SPEED LIMIT IS 45 MPH THE MAX. PERMITTED SPACING BETWEEN THESE DEVICES IS 45 FEET. HOWEVER, OTHER CONDITIONS MAY DICTATE THE NEED FOR REDUCING THIS SPACING.

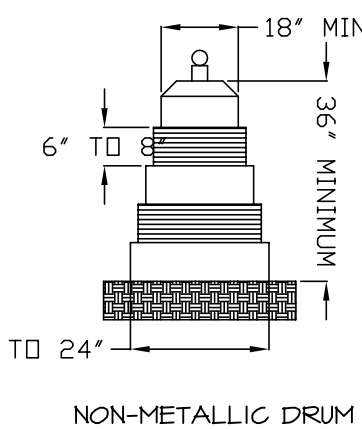
TYPE C STEADY BURN WARNING LIGHTS ARE MOST COMMONLY MOUNTED ON SEPARATE PORTABLE SUPPORTS OR ON TYPE I, OR TYPE II BARRICADES AND ARE INTENDED TO CONTINUALLY WARN THE DRIVER THAT HE IS APPROACHING OR ADJACENT TO A HAZARDOUS AREA.

BARRICADE WARNING LIGHTS ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE YELLOW.

BARRICADE WARNING LIGHTS SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE INSTITUTE OF TRANSPORTATION ENGINEERS (ITE), STANDARD FOR FLASHING AND STEADY BURN BARRICADE WARNING LIGHTS (1971) (SEE TABLE A)

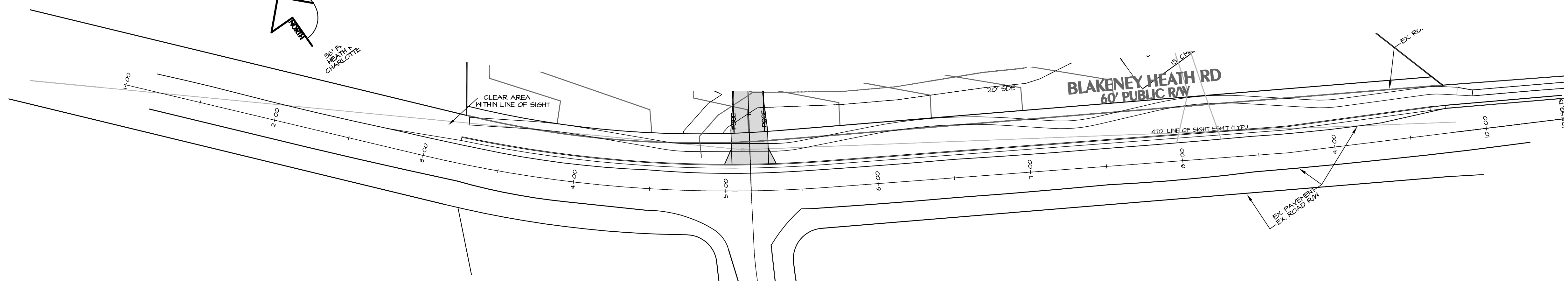
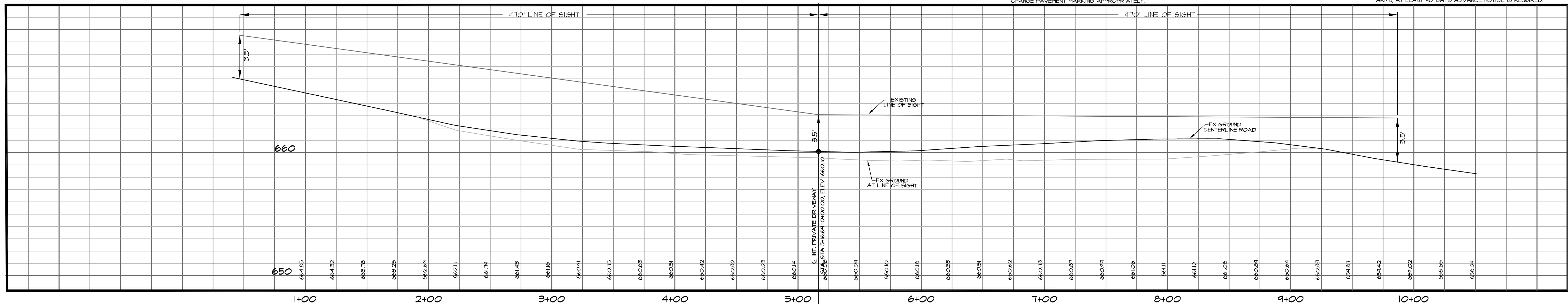
LENS DIRECTIONAL FACES	1 OR 2	1	1 OR 2
FLASH RATE/ MINUTE **	55 TO 75	55 TO 75	CONSTANT
FLASH DURATION	10%	8%	CONSTANT
MINIMUM EFFECTIVE INTENSITY ***	4.0 CANDELAS	35 CANDELAS	-
MINIMUM BEAM CANDLE POWER ***	-	-	2 CANDLES
HOURS OF OPERATION	DUSK TO DAWN	24 HRS/ DAY	DUSK TO DAWN

\*\*\* THESE VALUES MUST BE MAINTAINED WITHIN A FULL 9 DEGREES ON EACH SIDE OF THE VERTICAL AXIS, AND 5 DEGREES ABOVE AND 5 DEGREES BELOW THE HORIZONTAL AXIS.



## GENERAL REQUIREMENTS

- STREET SPACE IS AT A MINIMUM SO NO MORE SPACE SHOULD BE USED FOR CONSTRUCTION OR MAIN LANE THAN IS NECESSARY. THROUGH BARRICADING AND CANALIZATION, THE REMAINING STREET SPACE IS TO BE USED TO CARRY THE TRAFFIC AND THE WORK AREA IN THE BEST WAY POSSIBLE UNDER PREVAILING CONDITIONS.
- B. TRAFFIC CONTROL DEVICES SHALL BE SET UP PRIOR TO THE START OF CONSTRUCTION OR MAINTENANCE OPERATIONS AND SHALL BE MAINTAINED AND REPLACED AS THE WORK IS FINISHED OR WORK CONDITIONS CHANGE. THE AGENCY DOES THE WORK SHALL BE RESPONSIBLE TO ENSURE THAT ALL TRAFFIC CONTROL DEVICES ARE IN PLACE AND OPERATING AT ALL TIMES.
- A. ALL REFLECTORIZED SIGNS FOR THE WORK AREA SHALL BE REFLECTORIZED, THE REFLECTIVE MATERIALS SHALL BE EQUAL TO OR BETTER THAN THE TYPE TYPE 1 REFLECTIVE GLASS BEADS SPECIFIED IN SECTION 633 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF ROADS AND BRIDGES ON FEDERAL HIGHWAYS.
- D. AT NIGHT, ADEQUATE BARRICADES WITH REFLECTORIZED MATERIAL AND LIGHTS ARE REQUIRED TO IDENTIFICATION TO AND BE ADEQUATE TO ACTUAL LOCATION OF OBSTRUCTIONS AND HAZARDS.
- E. WHEN NOT IN USE DURING WORK HOURS OR CONSTRUCTION, THE VEHICLE SHALL BE PARKED A MINIMUM OF 10' AWAY FROM THE TRAVEL LANE IN SUCH A MANNER AS TO NOT CREATE A SIGHT OBSTRUCTION FOR TRAVELING TRAFFIC.
- F. THE MINIMUM WIDTH FOR TEMPORARY TRAVEL LANES IS 10'; HOWEVER, A 12' LANE IS ADVISABLE AND SHOULD BE PROVIDED WHEN PRACTICABLE.
- G. WHENEVER TRAFFIC MUST BE ROUTED AROUND THE CENTERLINE THE TWO DIRECTIONS OF TRAFFIC MUST BE SEPARATELY SEPARATED BY ADEQUATELY EFFECTIVE MEANS FOR THIS PURPOSE DURING DAYLIGHT HOURS; REFLECTORIZED DRUMS MUST BE USED WHEN NIGHT.
- H. TRAFFIC SHALL NOT BE ROUTED AROUND OR CENTERLINE WITH CONES OR DRUMS DURING NON-CONSTRUCTION HOURS. CONES OR DRUMS MUST BE CHANGED PAVEMENT MARKING APPROPRIATELY.
- BETWEEN THE HOURS OF 7-14 A.M. AND 4:30-6:30 PM MONDAY THROUGH FRIDAY, DURING THESE HOURS CONSTRUCTION ACTIVITY SHALL BE LIMITED TO CLOSURES WILL NOT BE ALLOWED ON THROUGHFARE STREETS EXCEPT IN EMERGENCY SITUATIONS.
- J. IF THE AGENCY DOES THE WORK, THE PROJECT LEADERS AND/OR POLICE CONTROL WHEN REQUIRED. THE CONTRACTOR MAY ALSO REQUEST A DESIGNATED A UNIFORMED OFFICER TO CONTROL TRAFFIC WHEN WORKING IN AND AROUND A SIGNALIZED INTERSECTION.
- K. EVERY ATTEMPT SHALL BE MADE TO SCHEDULE AND EXPEDITE THE WORK TO CAUSE THE LEAST DISRUPTION TO TRAVEL.
- L. IN SITUATIONS NOT COVERED IN THIS DESIGN, THE PROTECTION OF THE TRAVELING PUBLIC AND THE PROTECTION OF THE PROJECTOR'S INVESTMENT, DICTATE THE MEASURES TO BE TAKEN CONSISTENT WITH THE NCDOT TRAFFIC CONTROL STANDARDS.
- M. THE CONTRACTOR, UTILITY COMPANY, OR AGENCIES INVOLVED IN THE WORK IS RESPONSIBLE FOR NOTIFYING THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION.
- N. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONSTRUCTION OF THE CLOSURE AND ANY MAJOR WORK WHERE THE NUMBER OF CLOSED LANES ARE REDUCED TO ONE OR FEWER, CONDITIONS OR THE SITUATION IS REQUIRED TO BE CLOSED, EXCEPT IN EMERGENCIES, THE FOLLOWING NOTIFICATION IS REQUIRED:
1. CONSTRUCTION START DATE - 5 WORKING DAYS IN ADVANCE
2. CLOSING 1 OR MORE TRAVEL LANES - 5 WORKING DAYS PRIOR TO THE SCHEDULED CLOSURE
3. CLOSING A STREET - 10 WORKING DAYS PRIOR TO THE SCHEDULED CLOSURE
- THIS LEAD-TIME IS NECESSARY FOR PLANNING AND NOTIFYING THE PUBLIC OF EXPECTED CHANGES IN NORMAL TRAFFIC PATTERNS AND TO ALLOW THE USER WHEN COMPLETE STREET CLOSURE IS NECESSARY, THE DEPARTMENT OF TRANSPORTATION WILL COORDINATE WITH THE PROJECTOR AND, IF NECESSARY, FULLY SIGN THE DETOUR ROUTE.
- BY THIS STANDARD, THE WATCH AND/OR THE MUTED UNTIL SUCH REQUIREMENTS ARE MET.
- P. THIS DESIGN STANDARD IS NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION SITUATION IT IS INTENDED ONLY TO BE USED AS A GUIDELINE. SITUATIONS THAT REQUIRE SPECIAL JUDGEMENT IN THE PLACEMENT OF TRAFFIC CONTROL DEVICES BECAUSE OF LIMITED VERTICAL AND/OR HORIZONTAL SIGHT DISTANCE.
- Q. WHEN PERSONNEL AND/OR EQUIPMENT ARE WITHIN 2' OF THE EDGE OF AN OPEN TRAVEL LANE, THE CONTRACTOR SHALL REFER TO THE WORK FOR PROPER LANE CLOSURE.
- R. OPERATIONAL SIGNS ARE GENERALLY MOUNTED ON PORTABLE, ERECTABLE SIGNS. THESE ARE USED FOR SHORT-TERM OPERATIONS TO WARN AND GUIDE TRAVELERS THROUGH ADVANCE WARNING SIGNS (CONSTRUCTION AHEAD) SHALL BE MOUNTED ON STATIONARY SUPPORTS SEVEN DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- S. ALL DRUMS SHALL BE BALLASTED IN SUCH A MANNER THAT THEY WILL BE STABLE UNDER WIND AND OTHER UNDESIRABLE ACTS. BALLASTING SHALL BE WITH SAND/BALES OR OTHER YIELDING MATERIAL SITUATED IN THE BASE OF THE DRUMS.
- T. CONSTRUCTION TRAFFIC SHALL NOT ALLOWED ON BOTH SIDES OF THE ROAD SIMULTANEOUSLY WITHIN THE SAME AREA, UNLESS THE ROADWAY IS MEDIAN DIVIDED.
- U. AT THE END OF EACH WORK DAY THE CONTRACTOR SHALL BACKFILL UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT AREAS WITHIN THE WIDTH OF AN OPEN TRAVEL LANE THAT INCLUDED A DROP OFF OF MORE THAN 3 INCHES IN ACCORDANCE WITH THE WATCH (SEE DETAIL A).
- V. PAVEMENT MARKINGS ARE TO BE INSTALLED BY THE CONTRACTOR PRIOR TO THE START OF THE PROJECT PRIOR TO PLACEMENT OF THE PAVEMENT MARKINGS. THE CONTRACTOR SHALL NOTIFY JMY RHYNE (310-336-3703) IN ADVANCE OF PLACING THE PRE-LINES.
- W. PRELIMINARY SIGNAGE IS TO BE PERFORMED BY CDOT. THE CONTRACTOR SHALL NOTIFY JMY RHYNE (310-336-3703) AT LEAST 60 WORKING DAYS IN ADVANCE OF NEEDED SIGNAGE. IF A TRAFFIC ROUTE IS REQUIRED FOR A PROJECT, AT LEAST 40 DAYS ADVANCE NOTICE IS REQUIRED.



# Blakeney Retreat

City of Charlotte, Mecklenburg County, North Carolina  
Classica Homes, 1101 Wood Ridge Center Dr, Suite #155, Charlotte, NC 28217

Scale:	HORIZ 1"=40' VERT 1"=4'
Date:	6/28/13
Drawn By:	MJK
Designed By:	MJK
Job No.	0712

Revisions:  
1. 10/9/13 REVISED PER CPOD AND CED REVIEW COMMENTS

**KENNEY DESIGN  
GROUP, PA**

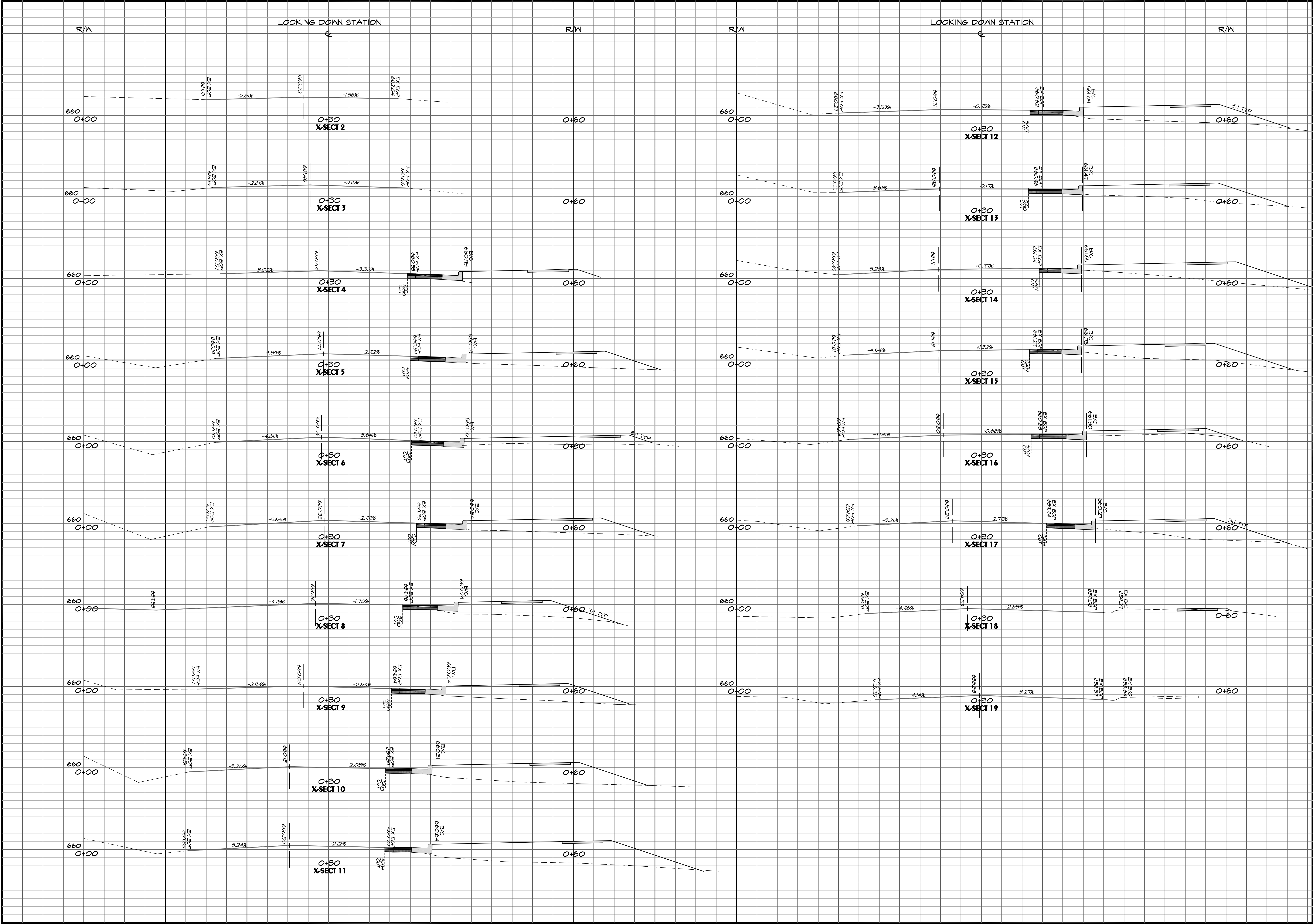
1316 GREENWOOD CLIFF  
CHARLOTTE, NORTH CAROLINA 28204  
PH: 704/377-6099 FAX: 704/377-6097  
EMAIL: KENNEY@KENNEYDESIGN.COM

Sheet No.

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9





Sheet No. 9 of 9

9

Blakeney Retreat

Roadway Cross-Sections 1-19

City of Charlotte, Mecklenburg County, North Carolina

Classica Homes, 1101 Wood Ridge Center Dr, Suite #155, Charlotte, NC 28217

Scale: HORIZ. 1" = 5'

VERT. 1" = 5'

Date: 7/22/13

Drawn By: MIK

Designed By: MIK

Job No.: 0712

REVISIONS:

1. 10/13 REVISED PER CIRC. CED 4 CDOT REVIEW COMMENTS.

2. 11/16/13 REVISED PER CDOT 1 CED

11/18/13

SEAL

STAMP

11/18/13

KENNEY DESIGN GROUP, PA

1316 GREENWOOD CLIFF

CHARLOTTE, NORTH CAROLINA 28204

PH. 704/377-6099 FAX: 704/377-6097

EMAIL: KENNEY@KENNEYDESIGN.COM

Engineering

Land Planning

Land Surveying