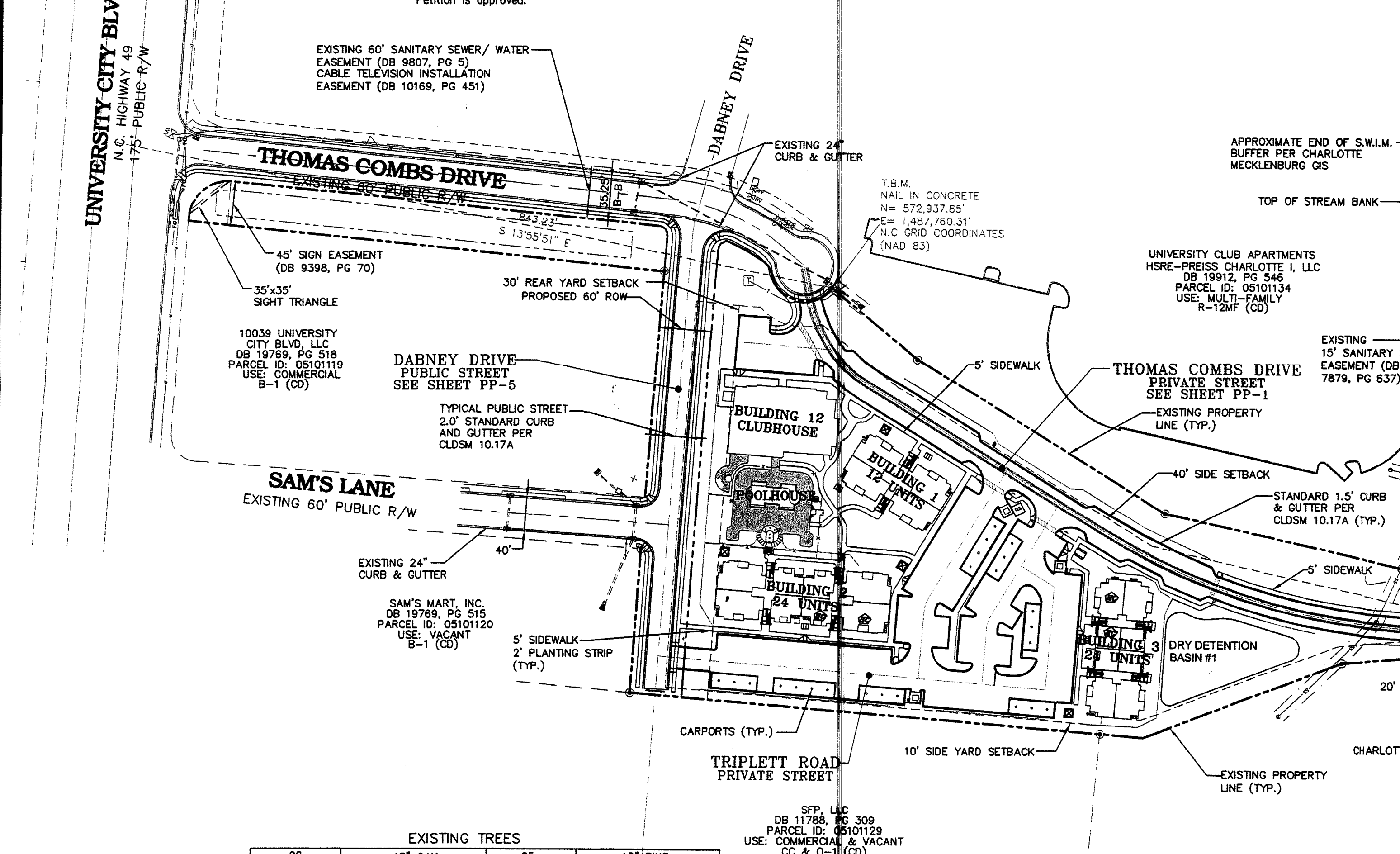


REZONING PETITION 97-62
CONDITIONAL DEVELOPMENT NOTES
APPROVED BY CITY COUNCIL - SEPTEMBER 15, 1997
ADMINISTRATIVE APPROVAL - AUGUST 2, 2002

- The building configurations, placements and sizes shown on the schematic site plan are illustrative in nature and may be altered or modified during design development and construction phases within the maximum building limit lines established on the rezoning plan. The rezoning plan also establishes the maximum building limit lines. The rezoning plan also establishes the maximum building limit lines. The rezoning plan also establishes the maximum building limit lines.
- All buildings constructed within the site shall meet or exceed the setback, rear yard and side yard requirements established under the R-12MF district.
- Buffers established on the Technical Data Sheet shall conform to the standards of Section 12.302 of the Zoning Ordinance subject, however, to the provisions of Section 12.304. Buffer areas are to remain as undisturbed open space except to the extent necessary to accommodate utility construction access points, walls, fences and sidewalks. In all buffer areas where existing trees and natural vegetation have been cleared to accommodate walls, fences and utility construction, the cleared, unimproved areas will be landscaped as required by Section 12.302 of the Zoning Ordinance. Buffer widths will not be reduced in width from that shown on the plan except in areas designated on the plan and further described in development note 18.
- Screening shall conform to the standards and treatments specified in Section 12.303 of the Ordinance. Landscaped areas within the site will be planted and improved in sequences which are key to the development taking place on each portion of the property. Each dumpster and service area will be screened from public streets and from adjacent properties in a manner which satisfies or exceeds the requirements in the Ordinance. Dumpsters will be located in a solid enclosure with a gate.
- All signs placed on the site will be erected in accordance with the requirements of the Ordinance.
- Maximum building heights shall not exceed three stories or 40 feet in height.
- The number of vehicular points shall be limited to the one shown on this rezoning plan and will be located in the general area depicted on the Technical Data Sheet. The configurations and final locations of driveways and access points within the site are subject to any minor modifications required to accommodate site and architectural construction plans and designs are further subject to approval by NCDOT and the Charlotte Department of Transportation.
- The parking spaces depicted on the schematic site plan may vary, but in all events, will be sufficient to satisfy or exceed the minimum off-street parking standards established under the Ordinance. No parking will be permitted within designated buffer and landscaped areas or setbacks.
- Adequate fire protection in the form of fire hydrants will be provided to the City's Fire Marshal's specifications. Fire hydrants shall be located within 750 feet as the fire truck travels of the most remote and accessible point of all buildings.
- The proposed public street shown on the plan shall be completed prior to issuance of occupancy permits. The exact location of access from HWY 49 will be subject to approval by NCDOT and Charlotte Department of Transportation.
- Lighting fixtures installed within the site, including bases, shall not exceed 20 feet in height.
- The following agencies must be contacted regarding water quality permits: 401/404 Permit NCDENHR-Raleigh Office (919) 733-1786 and Wetlands Permit US Corp of Engineers (704) 271-4854.
- Architecture for Phase II will be different from Phase I by the use of different materials and colors in order to create separately distinct projects.
- Phase I will consist of the development of Parcel 1, construction of the proposed collector street and any HWY 49 roadway improvements deemed necessary at that time the traffic impact study. Construction of Phase I may commence at any time after this Rezoning Petition has been approved. Phase II will consist of the development of Parcel 2 and any remaining HWY 49 roadway improvements. Construction of Phase II may commence at any time after the second anniversary of the date in which this Rezoning Petition is approved.



EXISTING TREES			
28	15" OAK	65	13" PINE
29	11" OAK	66	26" OAK
30	9" OAK	67	33" OAK
31	11" OAK	68	24" OAK
32	9" OAK	69	12" OAK
33	8" CEDAR	70	10" PINE
34	12" GUM	71	12" PINE
35	8" GUM	72	12" PINE
36	8" CEDAR	73	8" PINE
37	8" PINE	74	8" MAPLE
38	12" PINE	75	14" GUM
39	9" PINE	76	12" GUM
40	10" PINE	77	8" PINE
41	14" CEDAR	78	14" GUM
42	18" OAK	79	12" OAK
43	16" OAK	80	18" OAK
44	13" OAK	81	23" OAK
45	13" OAK	82	13" PINE
46	16" OAK	83	12" GUM
47	20" OAK	84	23" OAK
48	19" OAK	85	12" PINE
49	19" OAK	86	18" PINE
50	24" OAK	87	14" OAK
51	40" OAK	88	23" OAK
52	33" OAK	89	27" OAK
53	33" OAK	90	20" OAK
54	24" OAK	91	18" MAPLE
55	24" OAK	92	10" OAK
56	23" OAK	93	8" CEDAR
57	17" OAK	94	12" OAK
58	16" OAK	95	12" OAK
59	16" OAK	96	10" OAK
60	16" OAK	97	12" OAK
61	20" OAK	98	8" OAK
62	24" OAK	99	8" OAK
63	22" OAK	100	12" OAK
64	12" OAK	101	12" OAK

102	14" OAK	130	8" OAK
103	14" OAK	131	9" OAK
104	12" OAK	132	18" PINE
105	8" PINE	133	19" OAK
106	18" OAK	134	8" PINE
107	13" OAK	135	11" PINE
108	13" OAK	136	12" OAK
109	8" OAK	137	22" PINE
110	8" OAK	138	11" OAK
111	11" OAK	139	22" OAK
112	13" GUM	140	8" OAK
113	14" OAK	141	9" PINE
114	19" OAK	142	8" PINE
115	12" OAK	143	8" PINE
116	12" OAK	144	20" OAK
117	14" PINE	145	17" OAK
118	18" OAK	146	18" OAK
119	12" OAK	147	10" OAK
120	13" OAK	148	10" OAK
121	9" OAK	149	22" OAK
122	8" OAK	150	24" OAK
123	12" OAK	151	16" OAK
124	12" OAK	152	24" OAK
125	15" PINE	153	15" OAK
126	12" OAK	154	12" OAK
127	13" OAK	155	24" OAK
128	16" OAK		
129	16" OAK		

- The owner of Parcel 1 shall be obligated to construct, at its expense, a bicycle path leading from this point to UNC Charlotte's existing bicycle path system if the owner is able to acquire the necessary easements from the owners of property between Parcel 2 and the University's existing bike path system and if the University approves this connection. If the Rezoning Petition is approved, the Petitioner commits to exert its best efforts to obtain the necessary easements and consents.
- Alternative alignment for this relocation is schematic in nature but is intended to illustrate the type of alignment the Petitioner would be willing to implement if requested to do so and the necessary off-site right-of-way is acquired.
- Development:
 - Phase I:

The Developer agrees to the design and construction of a southbound right turn lane at the proposed public collector street for right turn access to the property from HWY 49. The right turn shall include 150 feet of storage and a 25:1 taper (180 feet). In addition, the Developer agrees, at their expense, to continue construction of the southbound third lane along the entire site frontage on HWY 49 if requested to do so by either NCDOT or Charlotte Department of Transportation.
 - Phase II:

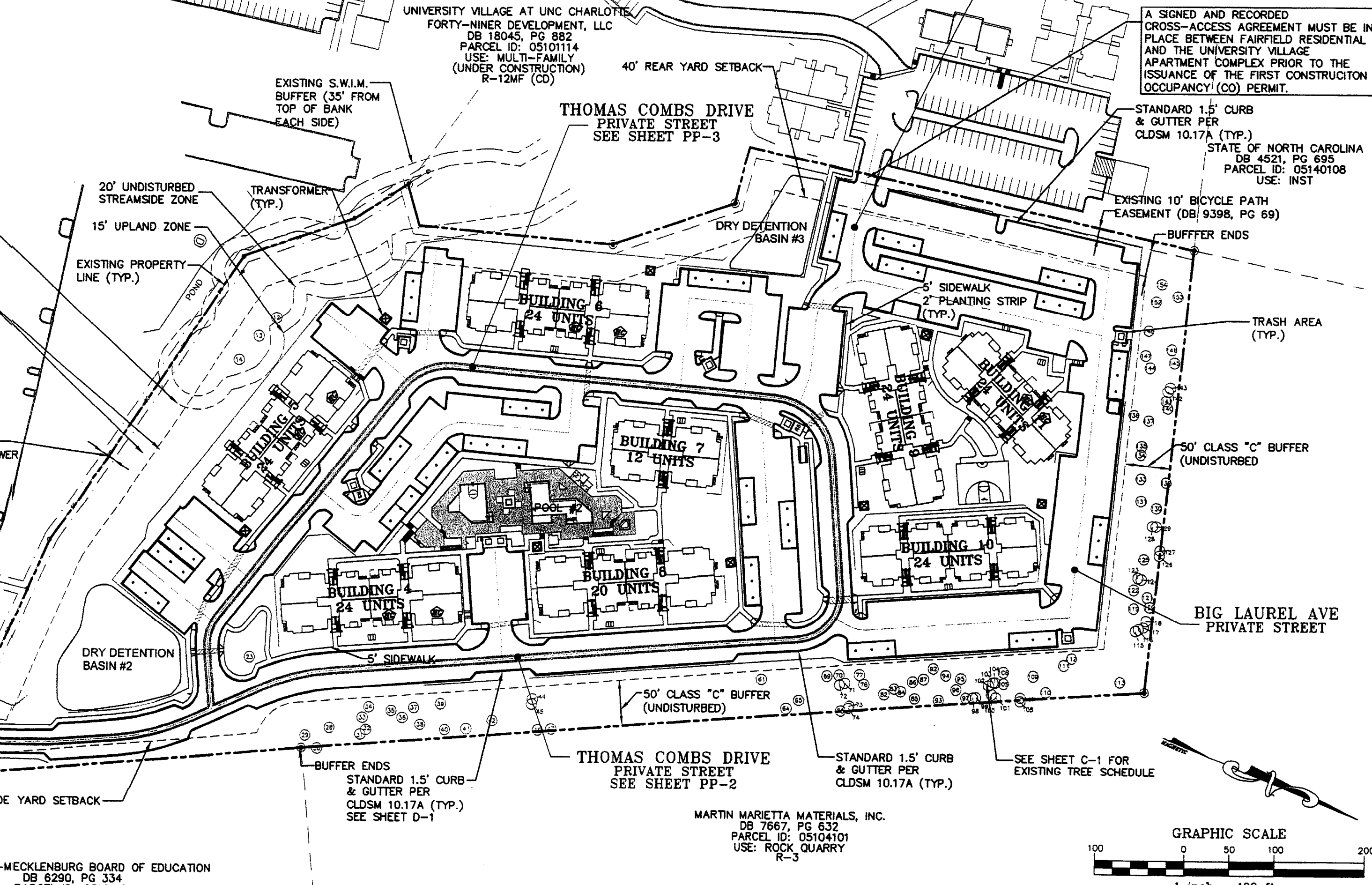
The Developer acknowledges that until additional transportation improvements beyond those cited under paragraph (a) above are added, Phase II development shall be limited to densities of no more than those currently allowed in a R-4 zoning district and land uses allowed in that district. Should Developer wish to develop Phase II for multi-family use, or densities higher than current R-4 limits, one of the following improvements shall be constructed at Developer's expense prior to or simultaneously with such development:

1. A northbound left turn crossover on HWY 49 with a minimum of 200 feet of storage and a 180 foot taper at a location to be approved by NCDOT and Charlotte Department of Transportation, and which may be at or located between the proposed collector street which provides a northbound left turn directly to the proposed public collector street (or the Alternate Alignment, if interchange is constructed), up to a location before the beginning of the current left turn intersection of HWY 49 at Pavilion Boulevard OR,

2. If such a crossover is not approved by NCDOT, Developer shall be responsible for design and construction of an additional left turn lane (northbound) at the intersection of HWY 49 and Pavilion Boulevard. The Developer shall also be responsible for design and construction of a second westbound lane on Pavilion Boulevard for a left turn of approximately 900 feet to the driveway providing access to Parcel D (see Diagram A) and either drop the lane or provide a transition to the existing pavement beyond the driveway as determined by Charlotte Department of Transportation at that time. Any traffic signal/equipment modifications that are necessary by these improvements will be the responsibility of the Developer.

18. As depicted on this site plan, certain portions of the previously required Class C Buffer within Parcel 2 have been eliminated based on an Administrative Approval authorized by the Charlotte-Mecklenburg Planning Commission on August 2, 2002.

This Administrative Approval was authorized based on an analysis of certain changes that have occurred on adjacent properties that will satisfy buffer requirements in these areas along common property boundaries abutting the subject site. These changes are a result of development approvals or development activity that has occurred on adjacent properties since the effective date of when Rezoning Petition 97-62 was approved on September 15, 1997.



SITE DATA			
AIRFIELD RESIDENTIAL, LLC			
N 118	TAX MAP NUMBER 05101118	ACREAGE 21.6± Ac.	
R-12MF (CD) (97-62)			
VACANT			
MULTI-FAMILY-APARTMENTS			
259 UNITS PER 97-62			
10.9 DU/AC.			
236 UNITS			
30' (THOMAS COMBS DR.)			
15' (NEW INTERNAL PUBLIC AND PRIVATE RD.)			
0' PER 97-62; 20' ABUTTING S.F. ZONED PROPERTY			
40'; 50' ABUTTING S.F. ZONED PROPERTY			
50%			
53.2%			
40' PER 97-62 (3 STORIES)			
354 (1.5 PER UNIT)			
722±			
15±			
20±			
2±			
6±			
12			
12			
NONE			
55.0%			
16'			
TO BEGIN SUMMER 2008 AND FINISH JULY 2010.			

- SITE LEGEND**
- SIGNAGE
 - YARD LIGHTS
 - LIGHT POLE
 - POWER POLE
 - TRAFFIC DIRECTIONAL ARROW
 - ACCESSIBLE PARKING STALL
 - ACCESSIBLE RAMPS
 - SITE BOUNDARY LINE
 - RIGHT-OF-WAY LINE
 - ADJOINING PROPERTY LINE
 - EASEMENT LINE
 - CENTERLINE
 - FENCE
 - PRIVATE STREET LIMITS

- GENERAL NOTES:**
- ALL DIMENSIONS AND RADII ARE TO OUTSIDE FACE OF BUILDING OR TO FACE OF CURB UNLESS NOTED OTHERWISE.
 - ALL CONSTRUCTION TO BE IN ACCORDANCE WITH PLANS AND SPECIFICATIONS APPROVED BY THE LOCAL GOVERNING AUTHORITY/ AUTHORITIES.
 - ALL CONSTRUCTION TO BE IN ACCORDANCE WITH PERMITS ISSUED AND APPLICABLE STATE, COUNTY AND LOCAL CODES.
 - PAVEMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPLICABLE SPECIFICATIONS AND IN ACCORDANCE WITH RECOMMENDATIONS BY THE GEOTECHNICAL ENGINEER.
 - CONTRACTOR SHALL REPLACE/RESTORE EXISTING IMPROVEMENTS DAMAGED DURING CONSTRUCTION TO THEIR ORIGINAL CONDITION AND TO THE SATISFACTION OF THE OWNER OF THE IMPROVEMENTS.
 - COMPACTION OF THE SOIL UNDER PAVED AREAS AND BUILDING AND THE MAINTENANCE OF PROPER MOISTURE CONTENT SHALL BE IN ACCORDANCE WITH THE GEOTECHNICAL ENGINEER OR LOCAL CODES.
 - THE CONTRACTOR SHALL OBTAIN AND PAY FOR ALL INSPECTIONS, CERTIFICATIONS, EQUIPMENT, ETC. THAT MAY BE REQUIRED.
 - THE CONTRACTOR SHALL NOTE THAT THE DRAWINGS ARE SCHEMATIC IN NATURE AND DO NOT SHOW EVERY OFFSET, TRANSITION, FITTING, ETC. THAT MAY BE REQUIRED FOR A COMPLETE AND WORKING SYSTEM.
 - THE ENGINEER AND/OR OWNER DISCLAIM ANY ROLE IN THE CONSTRUCTION MEANS AND METHODS ASSOCIATED WITH THE PROJECT AS SET FORTH IN THESE PLANS.
 - CONTRACTOR SHALL MAINTAIN AN "AS BUILT" SET OF DRAWINGS TO RECORD THE EXACT LOCATION OF ALL PIPING PRIOR TO CONCEALMENT. DRAWINGS SHALL BE GIVEN TO THE OWNER UPON COMPLETION OF THE PROJECT WITH A COPY OF THE TRANSMITTAL LETTER TO THE ENGINEER.
 - IF DEPARTURES FROM THE SPECIFICATIONS OR DRAWINGS ARE DEEMED NECESSARY BY THE CONTRACTOR, DETAILS OF SUCH DEPARTURES AND REASONS THEREOF SHALL BE SUBMITTED TO THE OWNER FOR REVIEW. NO DEPARTURES FROM THE CONTRACT DOCUMENTS SHALL BE MADE WITHOUT THE WRITTEN PERMISSION OF THE OWNER.
 - THE CONTRACTOR SHALL, AT ALL TIMES, KEEP THE PREMISES FREE FROM ACCUMULATIONS OF WASTE MATERIALS OR RUBBISH CAUSED BY HIM, HIS EMPLOYEES OR HIS WORK. ALL DEBRIS SHALL BE REMOVED FROM THE PROJECT SITE ON A DAILY BASIS.
 - HANDRAILS OR FENCING REQUIRED FOR ALL WALLS HIGHER THAN 30"

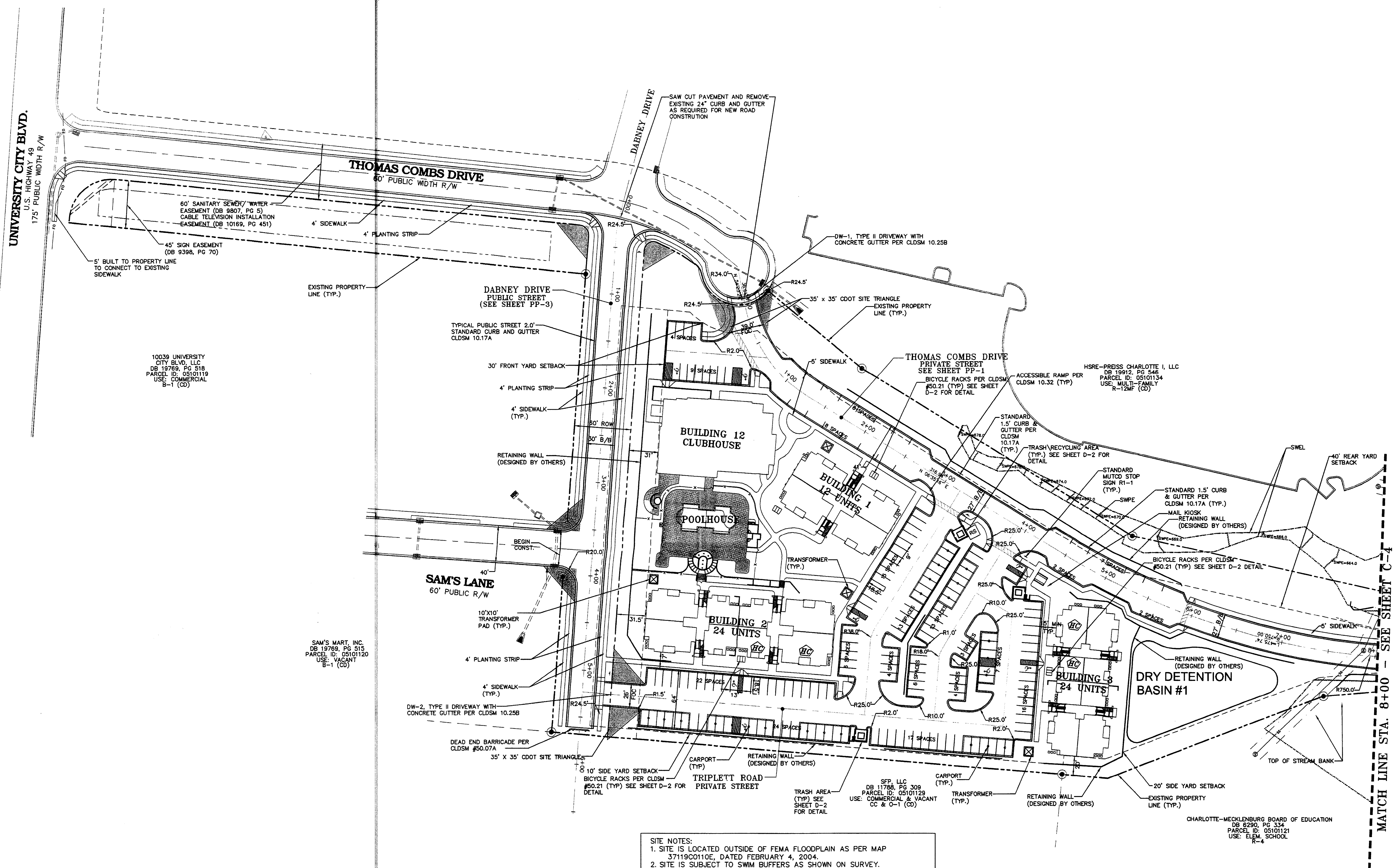
FINAL DRAWING - RELEASED FOR CONSTRUCTION

THE JOHN R. MCADAMS COMPANY, INC.
ENGINEERS/PLANNERS/SURVEYORS
6701 CARMEL ROAD, SUITE 205
CHARLOTTE, NORTH CAROLINA 28226
(704) 527-0800 WWW.JOHNMCADAMS.COM

PROJECT NO. FFD-07020
FILENAME: FFD-07020-S1
DESIGNED BY: RMR
DRAWN BY: TAB
SCALE: 1"=100'
DATE: 11-14-2007
SHEET NO. C-2

THE EXCHANGE AT CHARLOTTE
THOMAS COMBS DRIVE
CHARLOTTE, NORTH CAROLINA
OVERALL SITE PLAN

OWNER: FAIRFIELD RESIDENTIAL, LLC
7200 WISCONSIN AVENUE
SUITE 1108
BETHESDA, MARYLAND 20814











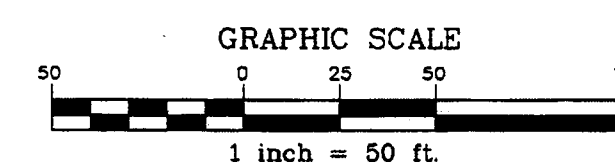
- SITE NOTES:**
1. SITE IS LOCATED OUTSIDE OF FEMA FLOODPLAIN AS PER MAP 37119C0110E, DATED FEBRUARY 4, 2004.
 2. SITE IS SUBJECT TO SWIM BUFFERS AS SHOWN ON SURVEY.
 3. STORMWATER DETENTION WILL BE REQUIRED FOR SITE.
 4. 50' CLASS C BUFFER REQUIRED ADJOINING SINGLE FAMILY RESIDENTIAL ZONED PROPERTY.
 5. STREET LIGHTS SHALL NOT EXCEED 20' IN HEIGHT.
 6. STANDARD MUTCD STOP SIGNS (R1-1) SHALL BE INSTALLED BY THE DEVELOPER AT THE MINOR LEGS OF ALL INTERNAL INTERSECTIONS.
 7. 5' WIDE MINIMUM INTERNAL SIDEWALK EXCEPT 7' ABUTTING 90' PARKING OR 2' PLANTING STRIP AND 5' SIDEWALK ABUTTING 90' PARKING.
 8. THOMAS COMBS DRIVE WITHIN THE SITE IS A PRIVATE STREET AND ENSURES THAT ALL BUILDINGS ARE WITHIN 400' OF EITHER A PUBLIC OR PRIVATE STREET. DABNEY DRIVE IS PUBLIC WITH A 60' ROW.

SOLID WASTE/RECYCLING DATA
SOLID WASTE
REQUIREMENTS: EIGHT (8) 8 CUBIC YARD DUMPSTERS (1/30 UNITS)
PROVIDED: EIGHT (8) 8 CUBIC YARD DUMPSTERS

RECYCLING
REQUIREMENTS: THREE (3) 144 SQ FT STATIONS
PROVIDED: THREE (3) 144 SQ FT STATIONS

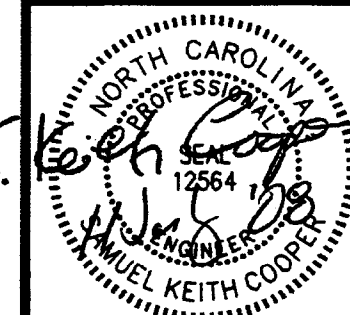
SITE LEGEND

	SIGNAGE
	ACC ACCESSIBLE PARKING STALL
	HANDICAP RAMPS
	SITE BOUNDARY LINE
	RIGHT-OF-WAY LINE
	ADJOINING PROPERTY LINE
	EASEMENT LINE
	CENTERLINE



FINAL DRAWING - ~~NOT~~ RELEASED FOR CONSTRUCTION

**THE JOHN R. McADAMS
COMPANY, INC.**



REVISIONS:

OWNER: FAIRFIELD RESIDENTIAL, LLC

OWNER:

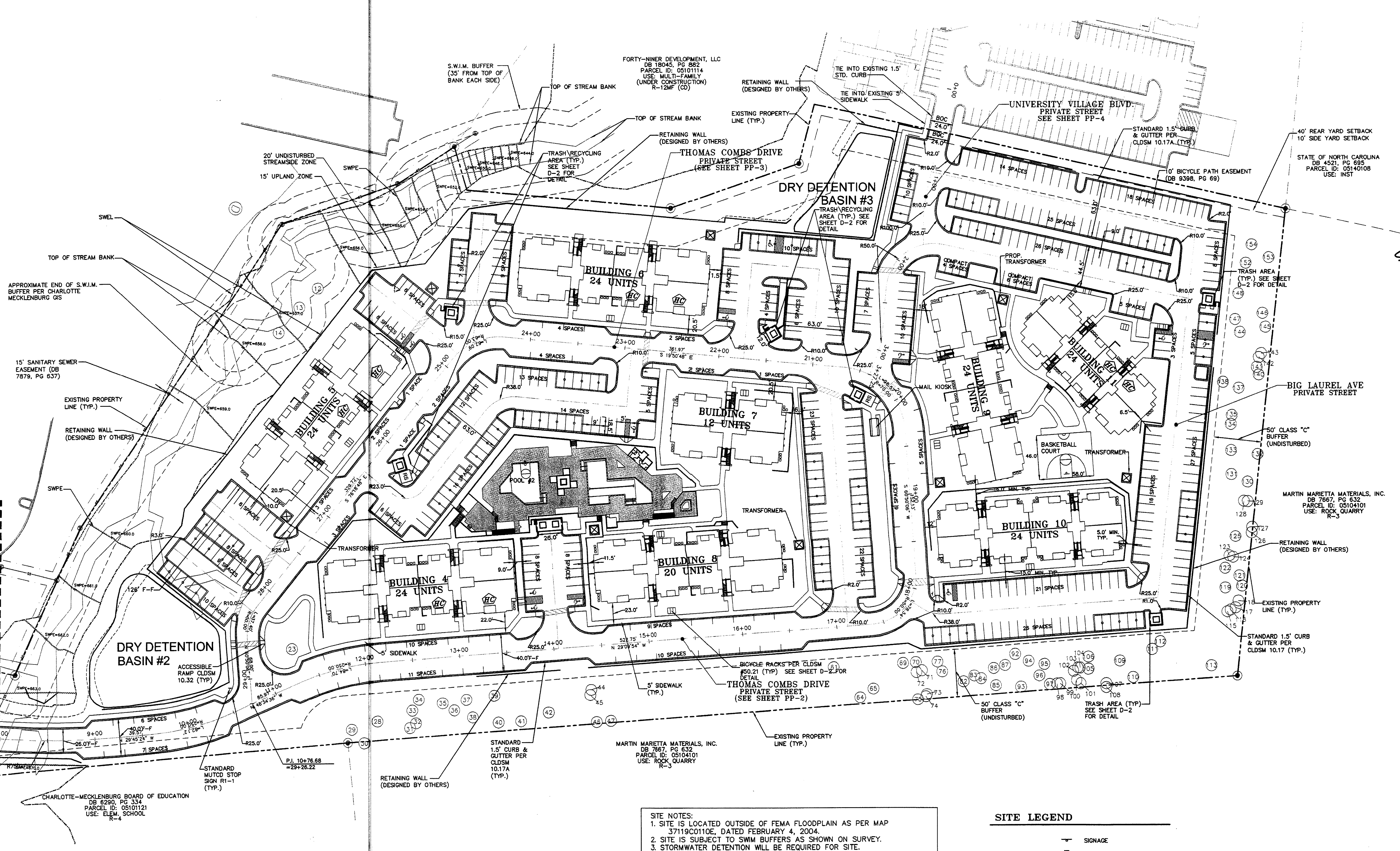
THE EXCHANGE AT CHARLOTTE

CHANGE AT CHA
THOMAS COMBS DRIVE
CHARLOTTE, NORTH CAROLINA

PROJECT NO. FFD-07020
 FILENAME: FFD-07020-S2
 DESIGNED BY: RMR
 DRAWN BY: TAB
 SCALE: 1"=50'
 DATE: 11-14-2007
 SHEET NO. C-3

McADAMS

MATCH LINE STA. 8+00 - SEE SHEET C-3



- SITE NOTES:**
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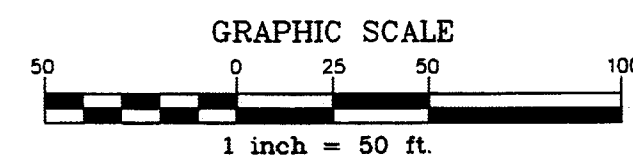
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REQUIREMENTS: THREE (3) 144 SQ FT STATIONS
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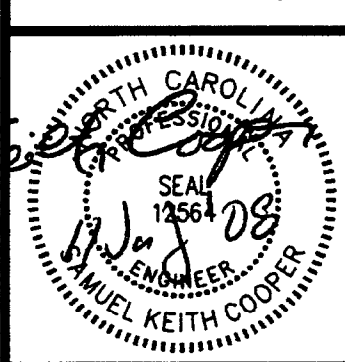
SITE LEGEND

- SIGNAGE
- YARD LIGHTS
- LIGHT POLE
- POWER POLE
- TRAFFIC DIRECTIONAL ARROW
- HANDICAP PARKING STALL
- HANDICAP RAMP
- PHASE LINE
- SITE BOUNDARY LINE
- RIGHT-OF-WAY LINE
- ADJOINING PROPERTY LINE
- EASEMENT LINE
- CENTERLINE
- CONCRETE
- PAINT STRIPING
- FENCE



FINAL DRAWING - NOT RELEASED FOR CONSTRUCTION

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ENGINEERS/PLANNERS/SURVEYORS
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SITE PLAN - NORTH

PROJECT NO. FFD-07020
FILENAME: FFD-07020-S3
DESIGNED BY: RMR
DRAWN BY: TAB
SCALE: 1"=50'
DATE: 11-14-2007
SHEET NO. C-4
McADAMS