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**STEWART**

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ASHLEY COURT APARTMENTS
DEVELOPED BY: ASHLEY COURT APARTMENTS, LLC
CHARLOTTE, NORTH CAROLINA

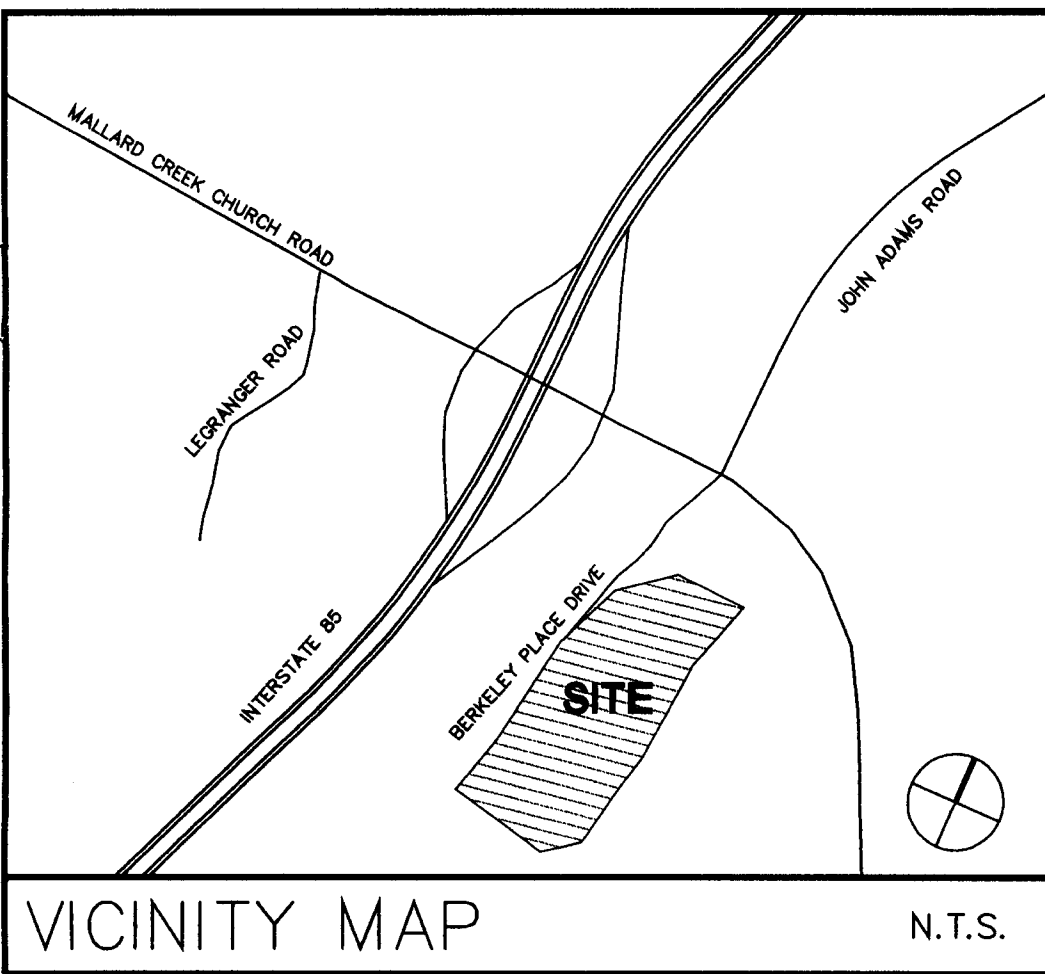
ENLARGED
SITE
PLAN

DATE: 5-31-07

SCALE: HORIZ : 1" = 40'

JOB NO. X6031.00

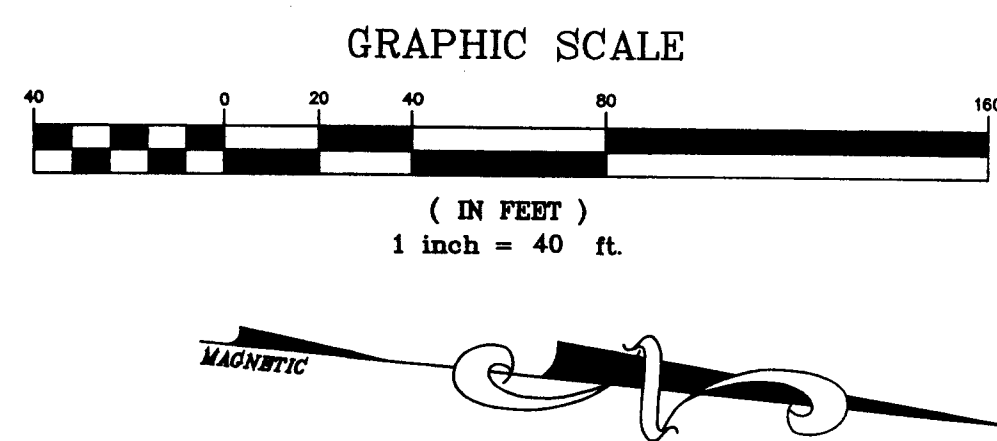
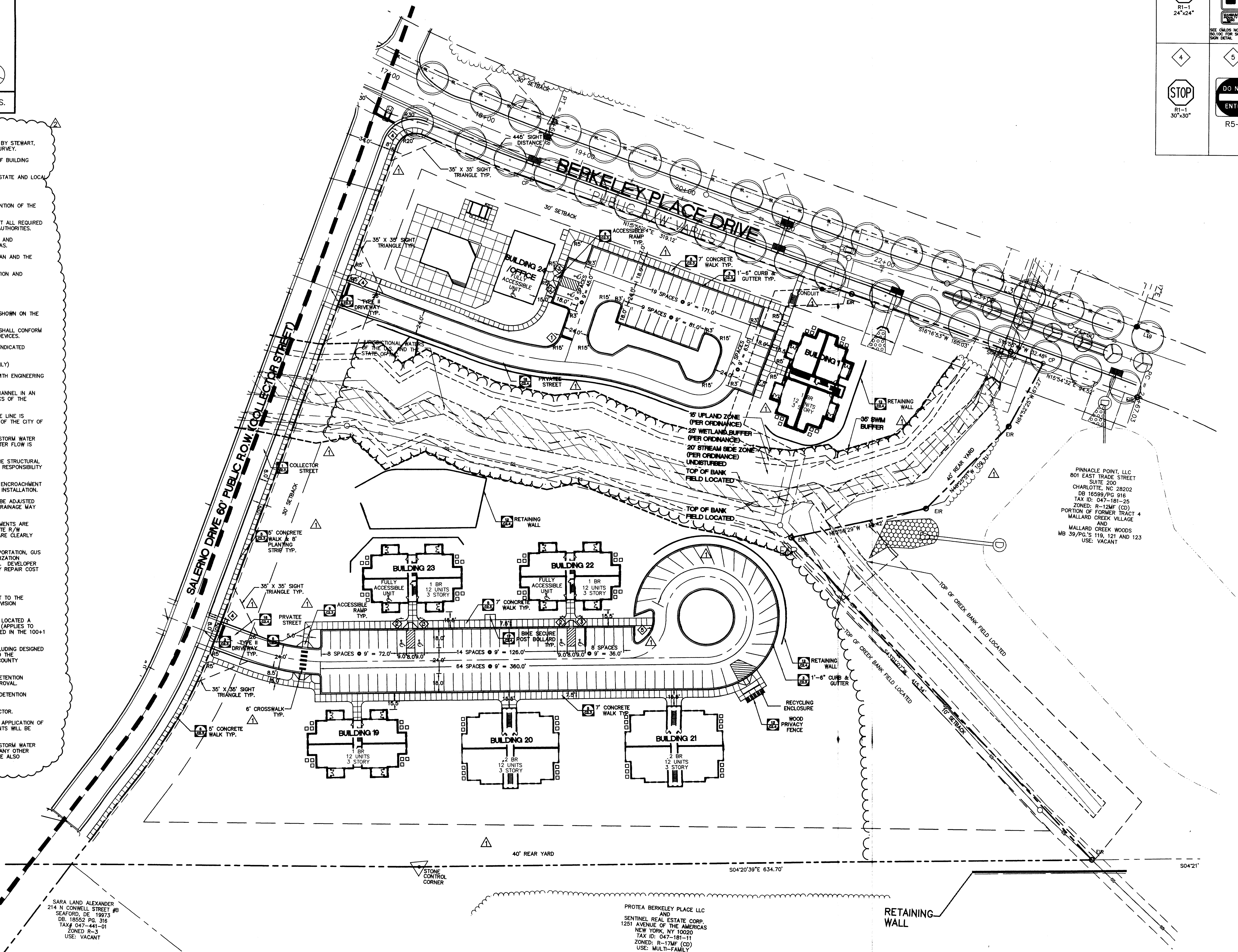
SHEET: C2.1



SITE NOTES

1. BOUNDARY AND SURVEY INFORMATION TAKEN FROM A SURVEY PREPARED BY STEWART, DATED 3-13-07. REFER TO SHEET C1.0 FOR INFORMATION REGARDING SURVEY.
2. ALL DIMENSIONS ARE TO FACE OF CURB, EDGE OF PAVEMENT, OR FACE OF BUILDING UNLESS OTHERWISE NOTED.
3. ALL IMPROVEMENTS AND CONSTRUCTION SHALL BE IN ACCORDANCE WITH STATE AND LOCAL STANDARDS.
4. SIGHT TRIANGLES SHOWN ARE THE MINIMUM REQUIRED.
5. ANY DISCREPANCIES FOUND IN THE FIELD SHALL BE CALLED TO THE ATTENTION OF THE OWNER PRIOR TO PROCEEDING WITH THE WORK.
6. PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THAT ALL REQUIRED PERMITS AND APPROVALS HAVE BEEN OBTAINED FROM ALL REGULATORY AUTHORITIES.
7. THE CONTRACTOR SHALL CONTACT ALL OWNERS OF EASEMENTS, UTILITIES, AND RIGHT-OF-WAYS, PUBLIC AND PRIVATE, PRIOR TO WORKING IN THESE AREAS.
8. CONTRACTOR SHALL MAINTAIN THE SITE IN A MANNER SUCH THAT WORKMAN AND THE GENERAL PUBLIC SHALL BE PROTECTED FROM INJURY.
9. DO NOT SCALE DRAWING FOR ACTUAL DIMENSIONS, AS IT IS A REPRODUCTION AND SUBJECT TO DISTORTION.
10. REFER TO ARCHITECTURAL PLANS FOR ACTUAL BUILDING DIMENSIONS.
11. FOR EXTERIOR LIGHTING, REFER TO ELECTRICAL PLANS.
12. CONTRACTOR SHALL FURNISH AND INSTALL ALL PAVEMENT MARKINGS AS SHOWN ON THE PLANS.
13. ALL SIGNS, PAVEMENT MARKINGS, AND OTHER TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
14. ALL PAVEMENT MARKINGS SHALL BE FOUR (4) INCH WHITE LINES UNLESS INDICATED OTHERWISE ON THE DRAWINGS.
15. DEVELOPER WILL PROVIDE STREET SIGNS PER CMDS #50.05, (9" SIGNS ONLY).
16. ALL ROAD IMPROVEMENTS AT BERKELEY PLACE DRIVE TO BE COORDINATE WITH ENGINEERING DEPARTMENT PRIOR TO CONSTRUCTION.
17. THE DEVELOPER SHALL MAINTAIN EACH STREAM, CREEK, OR BACKWASH CHANNEL IN AN UNOBTSTRUCTED STATE AND SHALL REMOVE FROM THE CHANNEL AND BANKS OF THE STREAM ALL DEBRIS, LOGS, TIMBER, JUNK, AND OTHER ACCUMULATIONS.
18. ANY CONSTRUCTION OR USE WITHIN THE FUTURE CONDITIONS FLOOD FRINGE LINE IS SUBJECT TO THE RESTRICTIONS IMPOSED BY THE FLOODWAY REGULATIONS OF THE CITY OF CHARLOTTE AND MECKLENBURG COUNTY.
19. THE PURPOSE OF THE STORM DRAINAGE EASEMENT (SDE) IS TO PROVIDE STORM WATER CONVEYANCE AND ANY STRUCTURES AND/OR OBSTRUCTION TO STORM WATER FLOW IS PROHIBITED.
20. THE CITY OF CHARLOTTE ENGINEERING DEPARTMENT HAS NOT REVIEWED THE STRUCTURAL STABILITY OF ANY RETAINING WALLS ON THE SITE AND DOES NOT ASSUME RESPONSIBILITY FOR THEM.
21. NON-STANDARD ITEMS IN THE RIGHT-OF-WAY REQUIRE A RIGHT-OF-WAY ENCROACHMENT AGREEMENT WITH CHARLOTTE DEPARTMENT OF TRANSPORTATION PRIOR TO INSTALLATION.
22. CURB AND GUTTER SHOWN ON PLANS ALONG BERKELEY PLACE DRIVE MAY BE ADJUSTED BASED UPON FIELD STAKING BY CITY ENGINEERING. ASSOCIATED STORM DRAINAGE MAY ALSO REQUIRE MODIFICATION BASED UPON FIELD CONDITIONS.
23. PRIOR TO PLAT RECORDATION, OFFSITE R/W AND/OR CONSTRUCTION EASEMENTS ARE REQUIRED TO BE OBTAINED ACCORDING TO THE GUIDELINES OF THE OFFSITE R/W ACQUISITION PROCESS. THESE NEEDED R/W AND CONSTRUCTION LIMITS ARE CLEARLY SHOWN ON THE ROADWAY IMPROVEMENT PLAN.
24. THE DEVELOPER SHALL CONTACT THE CHARLOTTE DEPARTMENT OF TRANSPORTATION, GUS JORDI (704-336-7086) TO IDENTIFY ANY CONFLICTS WITH TRAFFIC SIGNALIZATION EQUIPMENT. 60-90 DAYS WILL BE REQUIRED TO COORDINATE RELOCATION. DEVELOPER SHALL BE RESPONSIBLE FOR ALL RELATED RELOCATION COST AND/OR ANY REPAIR COST CAUSED BY THE CONTRACTOR/DEVELOPER.
25. REFER TO SHEET C3.0 FOR GRADING AND DRAINAGE.
26. ANY BUILDING WITHIN THE 100+1 STORMWATER ELEVATION LINE IS SUBJECT TO THE RESTRICTIONS OF THE CITY OF CHARLOTTE/MECKLENBURG COUNTY SUBDIVISION ORDINANCE, SECTION 7.200.6.
27. ALL OPENINGS (E.G. DOORS, WINDOWS, VENTS) IN STRUCTURES SHOULD BE LOCATED A MINIMUM OF ONE FOOT ABOVE THE ADJACENT FINISHED GROUND SURFACE (APPLIES TO LOTS WHICH MAY EXPERIENCE SIGNIFICANT OVERLAND FLOW NOT CONSIDERED IN THE 100+1 FLOOD ANALYSIS).
28. "AS-BUILT" DRAWINGS AND PLANS OF THE STORM DRAINAGE SYSTEM, INCLUDING DESIGNED DITCHES, MUST BE SUBMITTED PRIOR TO SUBDIVISION FINAL INSPECTION TO THE CITY/COUNTY ENGINEERING DEPARTMENT IN ACCORDANCE WITH THE CITY/COUNTY SUBDIVISION ORDINANCE.
29. PRIOR TO INSTALLATION, PE SEALED SHOP DRAWINGS OF UNDERGROUND DETENTION DETENTION SYSTEMS MUST BE FURNISHED TO CITY OF CHARLOTTE OF APPROVAL.
30. PRIOR TO CO. SURVEYOR SEALED AS-BUILT DRAWINGS OF UNDERGROUND DETENTION SYSTEMS MUST BE PROVIDED.
31. COORDINATE ALL CURB AND STREET GRADES IN INTERSECTION WITH INSPECTOR.
32. IN ROLLING AND HILLY TERRAINS, SWEEPING OF THE STONE BASE AND/OR APPLICATION OF A TACK COAT MAY BE REQUIRED NEAR INTERSECTIONS. THESE REQUIREMENTS WILL BE ESTABLISHED BY THE INSPECTOR AND BASED ON FIELD CONDITIONS.
33. THE PURPOSE OF THE STORM DRAINAGE EASEMENT (SDE) IS TO PROVIDE STORM WATER CONVEYANCE. BUILDINGS ARE NOT PERMITTED IN THE EASEMENT AREA. ANY OTHER OBJECTS WHICH IMPEDE STORM WATER FLOW OR SYSTEM MAINTENANCE ARE ALSO PROHIBITED.

SIGN LEGEND		
1 STOP R1-1 24"x24"	2 VANY RESERVED PARKING R7-BE R7-BA R7-BD SEE CHART NO. 90.108 & 90.109 FOR SUPPLEMENTAL SIGN DETAIL.	3 RESERVED PARKING R7-BA R7-BD SEE CHART NO. 90.108 & 90.109 FOR SUPPLEMENTAL SIGN DETAIL.
4 STOP R1-1 30"x30"	5 DO NOT ENTER R5-1	



REVISOR PER CITY OF CHARLOTTE COMMENTS

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1	8/17/07	

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STEWART

ENGINEERING

10/10/07

ASHLEY COURT APARTMENTS

DEVELOPED BY: ASHLEY COURT APARTMENTS, LLC

CHARLOTTE, NORTH CAROLINA

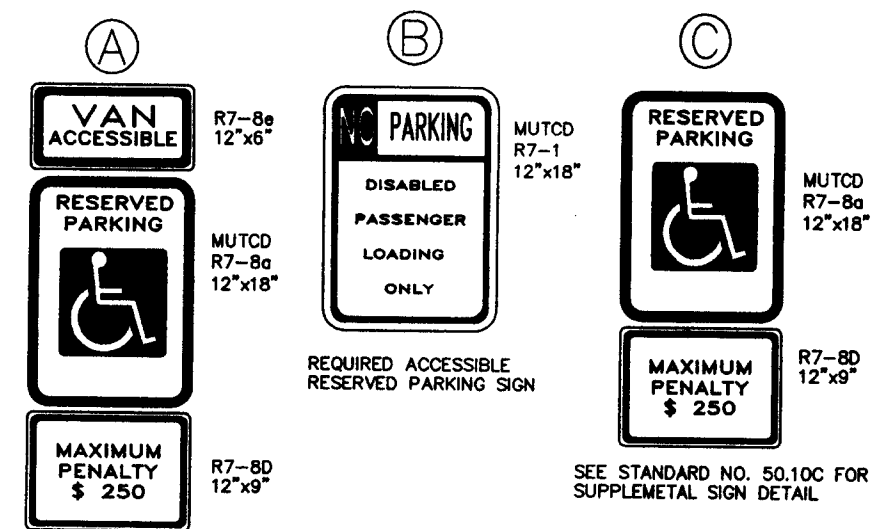
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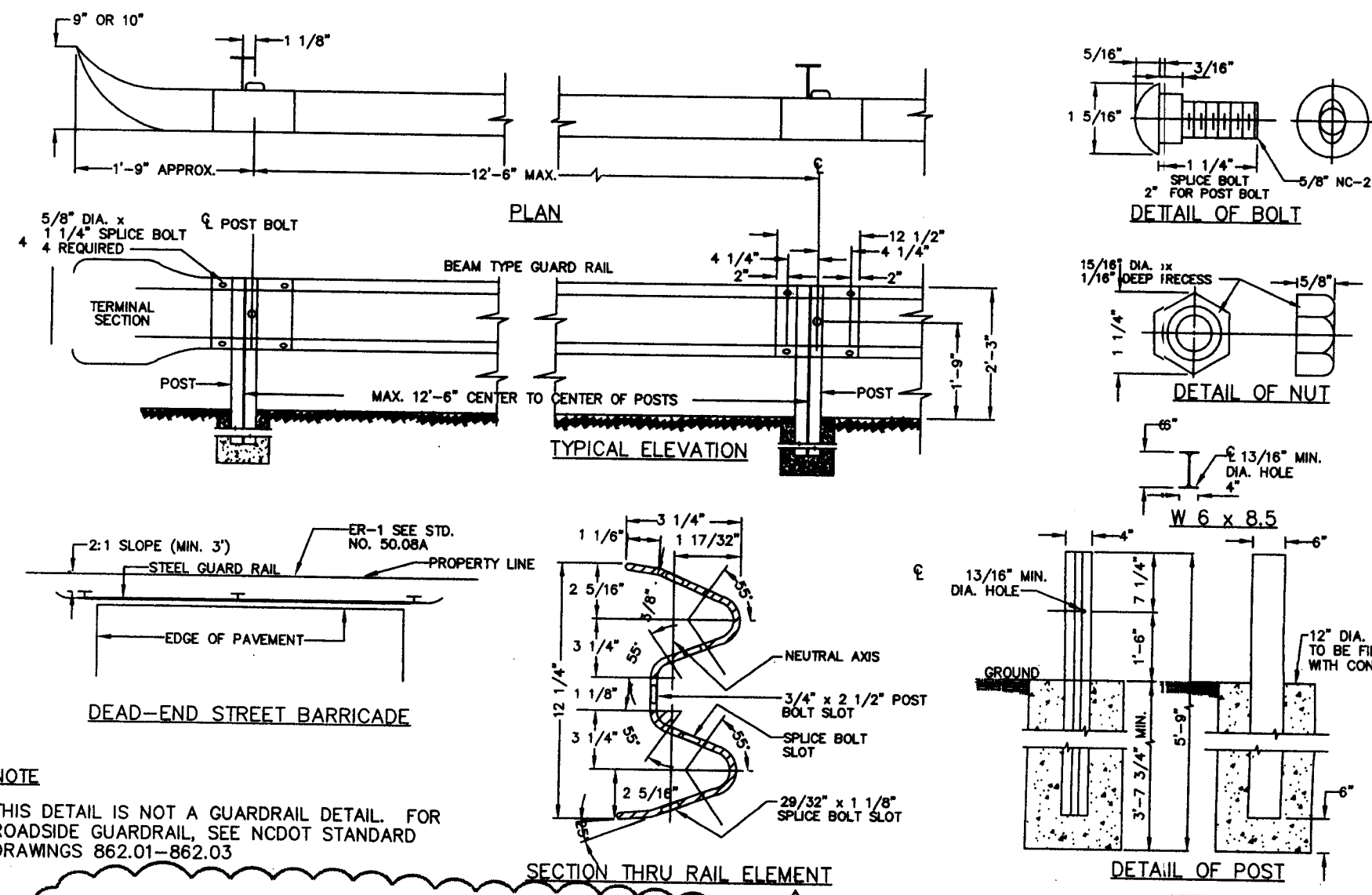
SHEET: **C2.2**



- NOTES:
- ALL 12"x18" ACCESSIBLE SIGNS (R7-8a & R7-1) SHALL BE MOUNTED AT 7 FEET FROM GRADY BOTTOM EDGE OF SIGN FACE (MUTD). MOUNTING HEIGHT CAN BE REDUCED TO 5 FEET IF PLACED IN AN AREA BETWEEN SIDEWALK AND BUILDING FACE IN WHICH PEDESTRIANS ARE NOT EXPECTED TO USE.
 - REFER TO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTD) U.S. DEPARTMENT OF TRANSPORTATION AND NORTH CAROLINA DEPARTMENT OF TRANSPORTATION SUPPLEMENT.
 - IF ACCESSIBLE ROUTE IS A RAISED SIDEWALK AREA, THEN RAMP ARE REQUIRED AT LOADING ZONE AREA.

11 ACCESSIBLE PARKING & SIGNAGE STANDARDS N.T.S.

CLDS STD. DTL. 50.10A(MODIFIED)



NOTE:

- THIS DETAIL IS NOT A GUARDRAIL DETAIL FOR ROADSIDE GUARDRAIL. SEE NC DOT STANDARD DRAWINGS 862.01-862.03.

14 DEAD END STREET BARRICADE N.T.S.

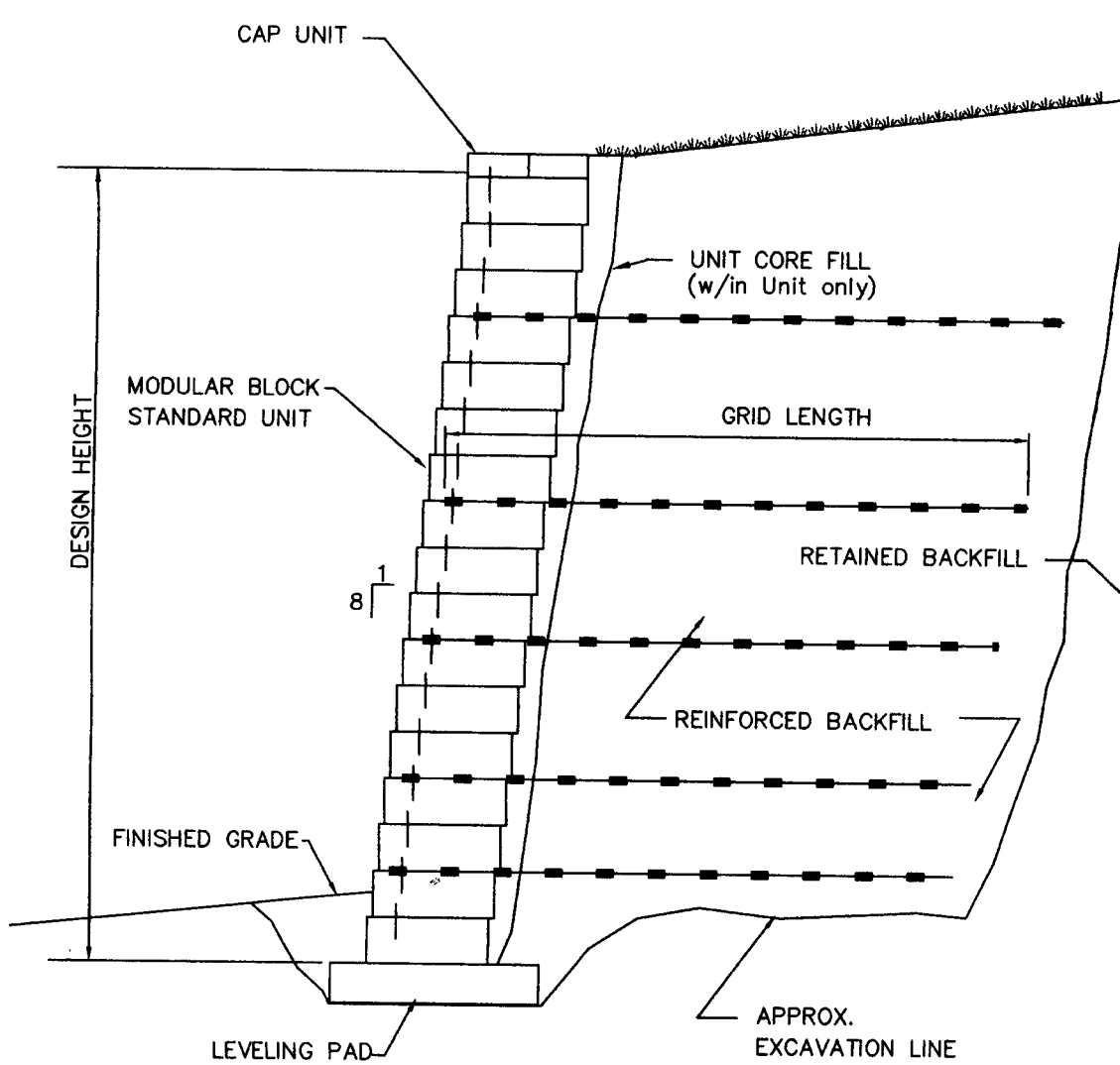
CLDS STD. DTL. 50.07A

GENERAL NOTES:

- STEEL BEAM TYPE GUARD RAILS SHALL BE INSTALLED AT THE END OF ALL DEAD-END STREETS, EXCEPT CUL-DE-SAC STREETS WHICH HAVE BEEN IMPROVED WITH A PERMANENT TURN-AROUND.
- FOR STREETS 26' IN WIDTH THE GUARD RAIL SHALL CONSIST OF TWO(2) 12'-6" SECTIONS OR ONE(1) 25' SECTION, THREE (3) STEEL POSTS, AND TWO (2) TERMINAL SECTIONS. FOR STREETS GREATER THAN 25' IN WIDTH THE GUARD RAIL SHALL SPAN THE ENTIRE WIDTH OF THE STREET.
- GUARD RAIL SHALL CONSIST OF RAIL ELEMENTS FABRICATED TO DEVELOP CONTINUOUS BEAM STRENGTH AND INSTALLED AS SHOWN.
- MINIMUM THICKNESS OF GUARD RAIL SHALL BE 12 GAGE U.S. STANDARD. THE RAIL ELEMENT INCLUDING SPLICES, SHALL HAVE A MINIMUM ULTIMATE TENSILE STRENGTH OF 80,000 LBS. GUARD RAIL PARTS FURNISHED SHALL BE INTERCHANGEABLE WITH SIMILAR PARTS REGARDLESS OF THE SOURCE OF MANUFACTURER. THE HOLES FOR CONNECTING BOLTS SHALL BE PUNCHED OR DRILLED, BURNING WILL NOT BE PERMITTED.
- THE GUARD, BOLTS, NUTS, STEEL POSTS, AND ALL OTHER METAL PARTS SHALL BE GALVANIZED TO CONFORM TO THE REQUIREMENTS FOR THE COATING CLASS, (2.50 OUNCES PER SQUARE FOOT) OF THE CURRENT SPECIFICATIONS FOR ZINC-COATED (GALVANIZED) IRON, AND STEEL SHEETS, COILS, AND CUT LENGTHS, IN ACCORDANCE WITH ASTM 123A.
- IF THE AVERAGE SPELTER COATING AS DETERMINED FROM THE REQUIRED SAMPLES IS LESS THAN TWO (2) OUNCES OF SPELTER PER SQUARE FOOT, OR IF ANY ONE SPECIMEN HAS LESS THAN 1.8 OUNCES OF SPELTER PER SQUARE FOOT OF DOUBLE EXPOSED SURFACE, THE LOT SAMPLED SHALL BE REJECTED, THE FINISHED SHEETS SHALL BE OF FIRST CLASS COMMERCIAL QUALITY, FREE FROM INJURIOUS DEFECTS, SUCH AS BUSTERS, FLUX, AND UNCOATED SPOTS.
- THE GUARD RAIL SHALL BE INSPECTED TO DETERMINE THAT THE MATERIAL, DIMENSIONS, AND WORKMANSHIP ARE IN ACCORDANCE WITH THIS PLAN.
- WHERE A DEAD-END STREET REQUIRES GUARD RAIL, END OF ROADWAY MARKER SIGNS SHALL ALSO BE REQUIRED. (SEE STD. 50.08A & 50.08B) (ER-1).

14 DEAD END STREET BARRICADE - GENERAL NOTES N.T.S.

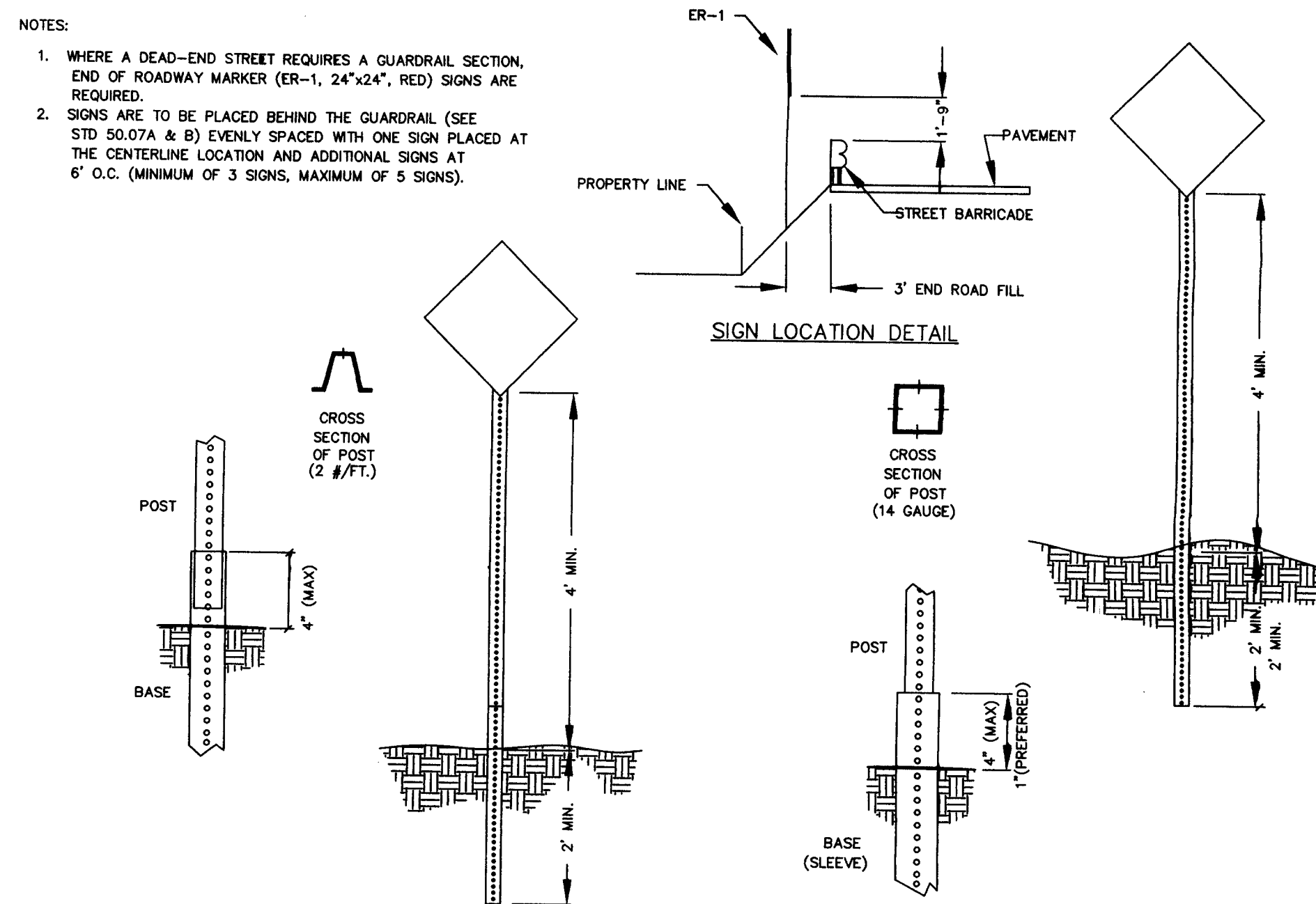
CLDS STD. DTL. 50.07B



NOTES:

- RETAINING WALL DESIGN BY OTHERS.
- WALLS OVER 30" TALL REQUIRE SAFETY RAIL.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR RETAINING THE SERVICES OF A QUALIFIED ENGINEER TO COMPLETE THE DESIGN OF ALL PROPOSED RETAINING WALLS SHOWN ON THE DRAWINGS. THE DESIGN OF ALL RETAINING WALLS IS TO BE CONDUCTED IN ACCORDANCE WITH THE NORTH CAROLINA BUILDING CODE, SECTION 1610.3. DETAILED RETAINING WALL DESIGN DRAWINGS, SEALED BY A NORTH CAROLINA LICENSED ENGINEER, SHALL BE SUBMITTED TO THE LOCAL PLAN REVIEW AUTHORITY PRIOR TO CONSTRUCTION. A NORTH CAROLINA LICENSED ENGINEER MUST PERFORM CONSTRUCTION OBSERVATION, VERIFYING IN A SEALED LETTER TO THE LOCAL INSPECTION AUTHORITY THAT THE RETAINING WALLS WERE CONSTRUCTED IN ACCORDANCE WITH THE APPROVED ENGINEERED DRAWINGS, IN COMPLIANCE WITH SECTION 1610.3 OF THE NORTH CAROLINA BUILDING CODE.

12 TYPICAL MODULAR BLOCK RETAINING WALL SECTION N.T.S.

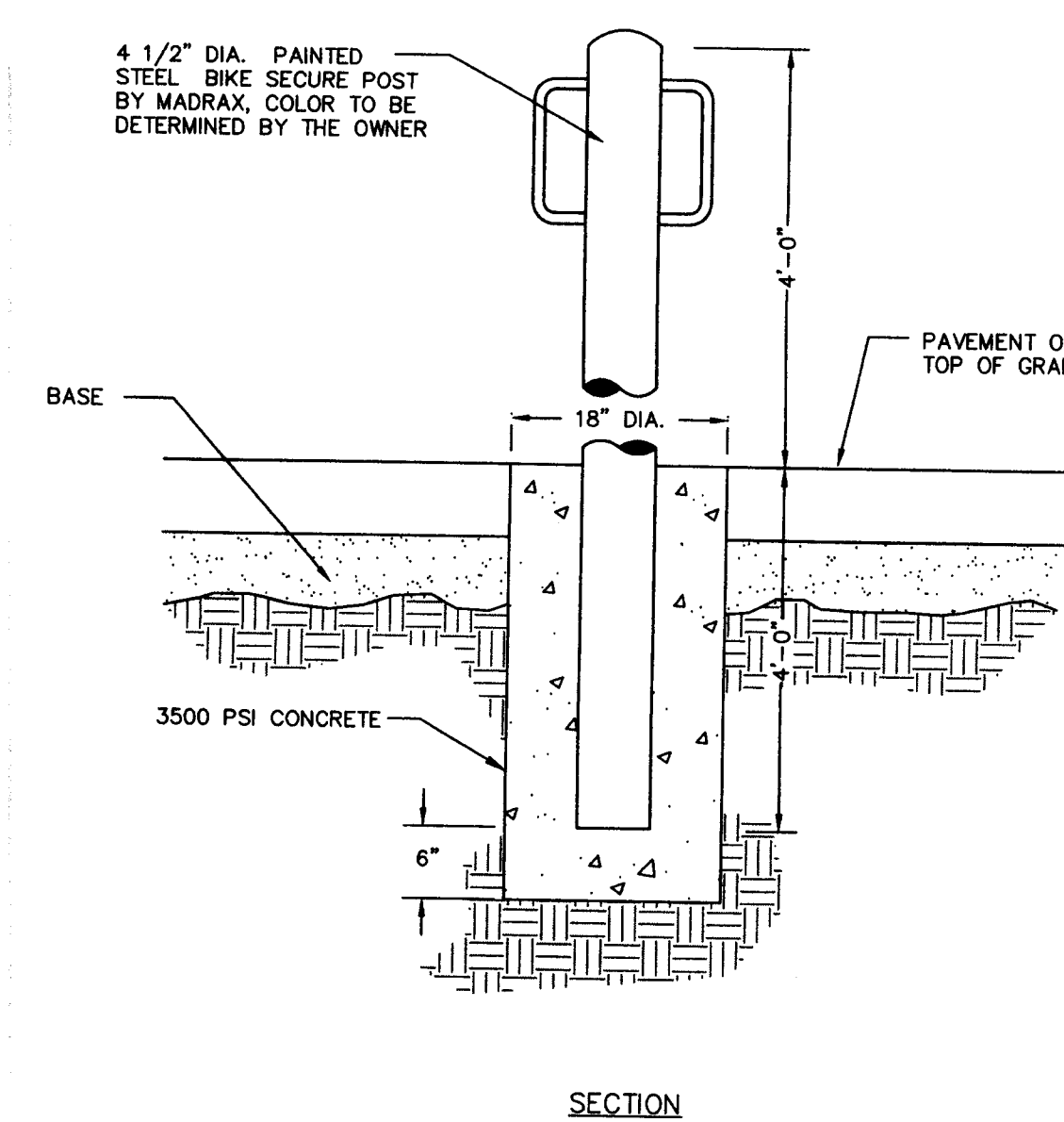


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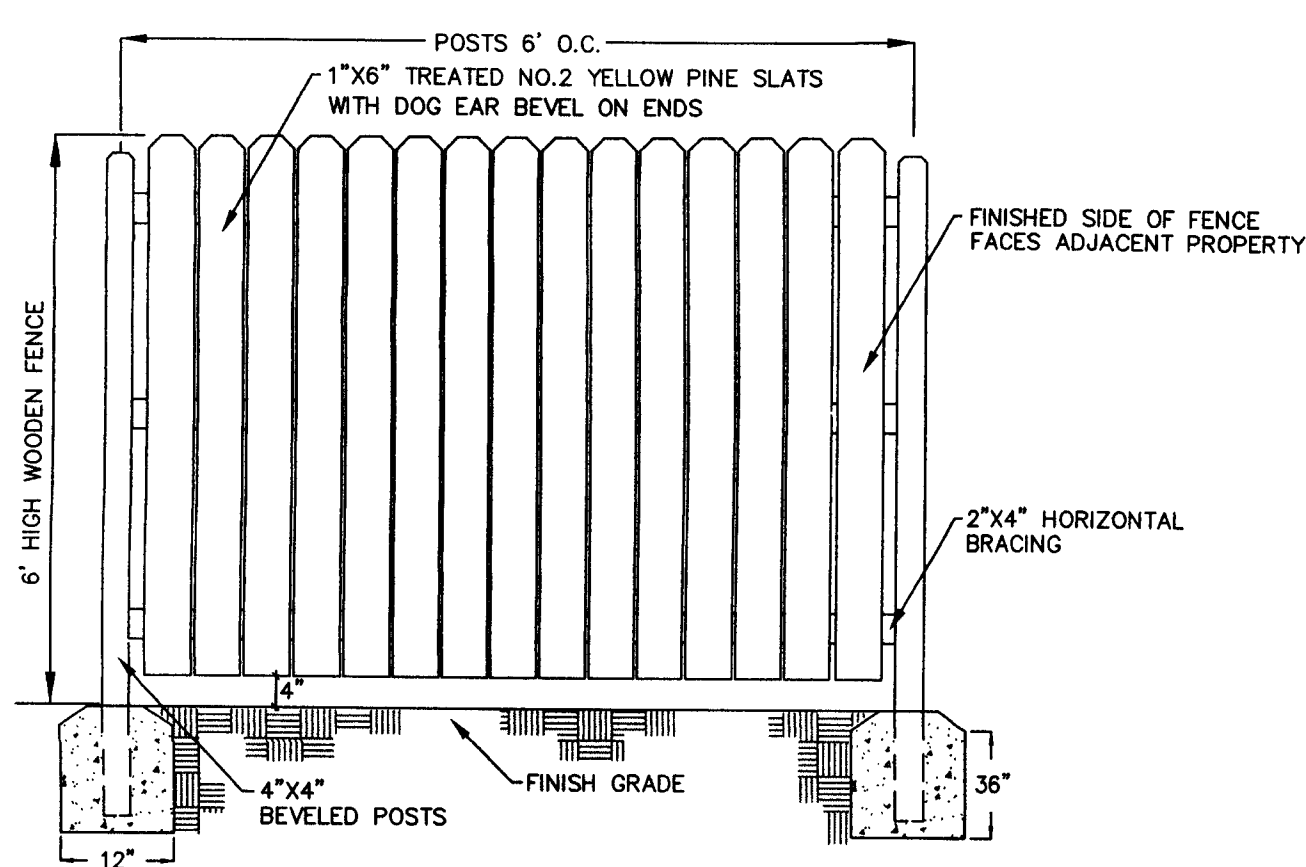
- WHERE A DEAD-END STREET REQUIRES A GUARDRAIL SECTION, END OF ROADWAY MARKER (ER-1, 24"x24", RED) SIGNS ARE REQUIRED.
- SIGNS ARE TO BE PLACED BEHIND THE GUARDRAIL (SEE STD 50.07A & B) EVENLY SPACED WITH ONE SIGN PLACED AT THE CENTERLINE LOCATION AND ADDITIONAL SIGNS AT 6' O.C. (MINIMUM OF 3 SIGNS, MAXIMUM OF 5 SIGNS).

15 END OF ROADWAY MARKER (ER-1) N.T.S.

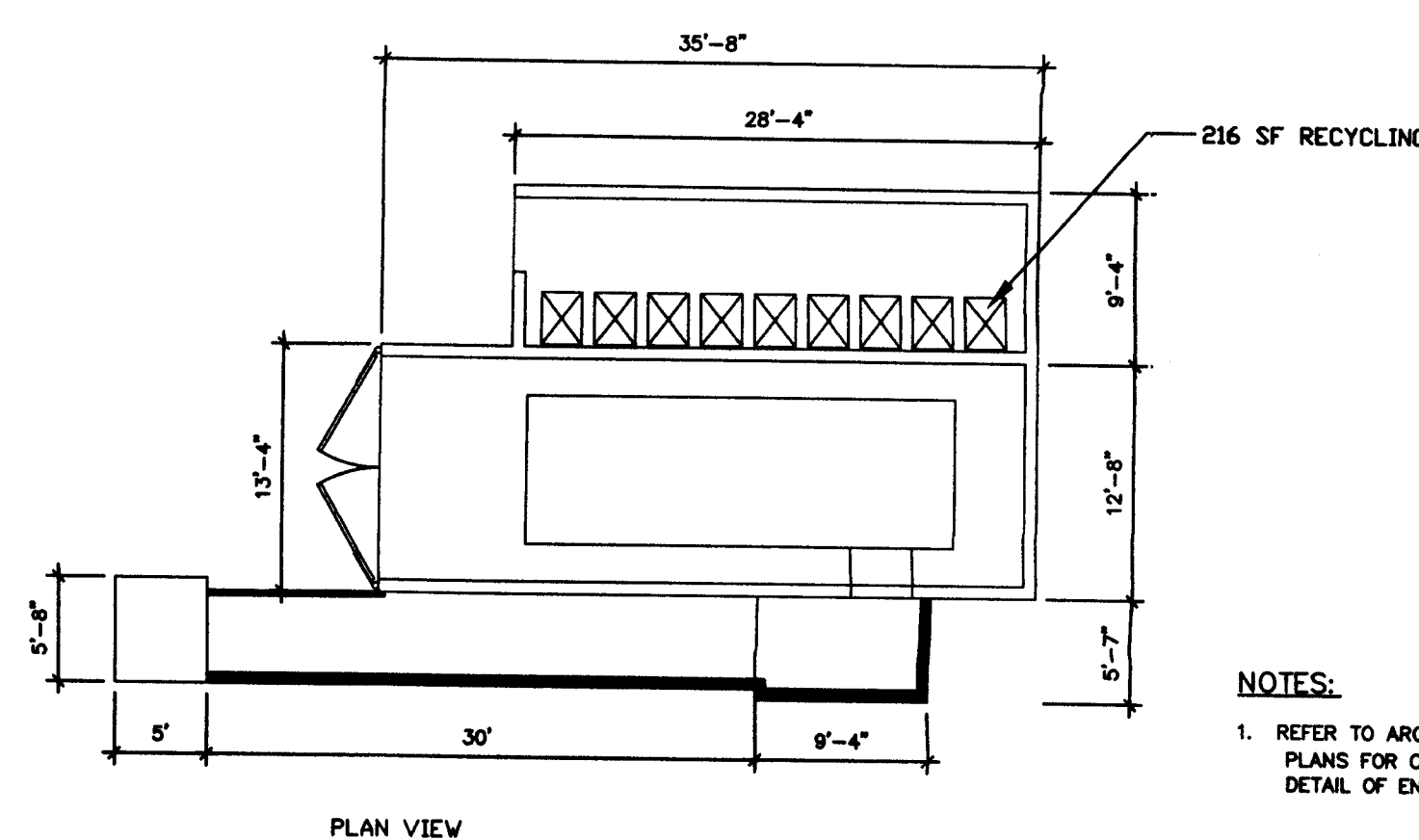
CMCLS STD. DTL. 50.08A



17 BIKE SECURE POST BOLLARD N.T.S.



13 WOOD PRIVACY FENCE N.T.S.



NOTES:

- REFER TO ARCHITECTURAL PLANS FOR CONSTRUCTION DETAIL OF ENCLOSURE.

16 COMPACTOR AND RECYCLING ENCLOSURE N.T.S.

GUIDELINES FOR PRIVATE STREET DESIGN:

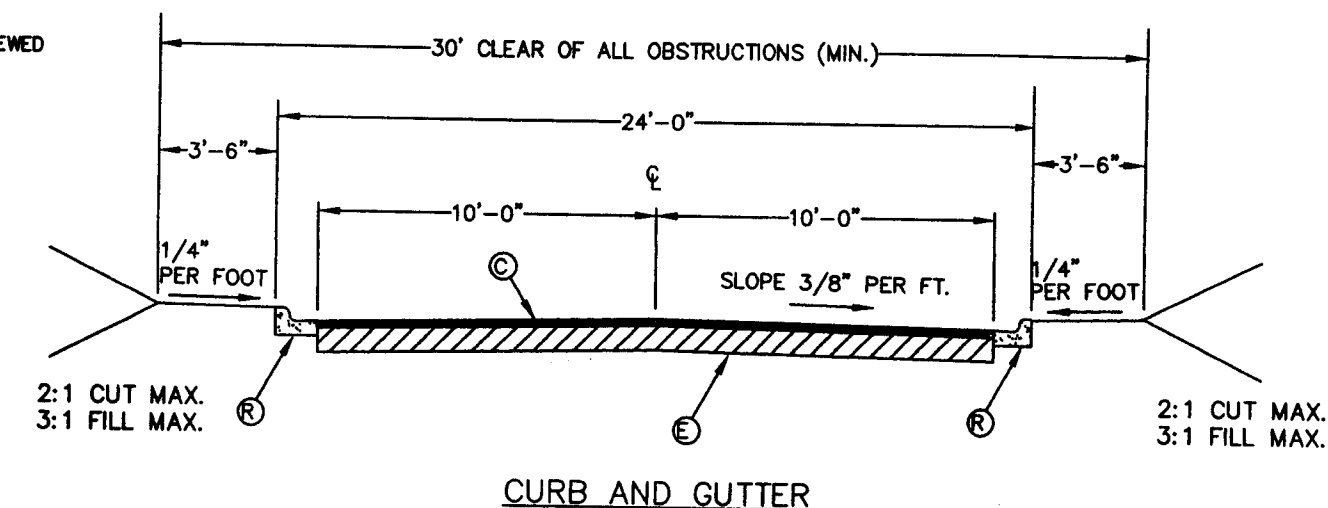
- INTERNAL STREET ALIGNMENT:
MAXIMUM GRADE: 10%
MINIMUM VERTICAL CURVE "K" VALUES: 10/20 (CREST/SAG)
MINIMUM HORIZONTAL CURVE CENTERLINE RADIUS: 50 FT.
- INTERSECTION WITH PUBLIC STREET:
SAME AS FOR PUBLIC STREET. SEE GENERAL NOTES, SECTION 1.8.2.
OR MAXIMUM GRADE WITHIN 40 FEET OF PUBLIC STREET INTERSECTION BEGINNING FROM EDGE OF PAVEMENT LINE.
NOTE: VARIATIONS ON THESE GUIDELINES WILL BE REVIEWED ON A CASE BY CASE BASIS BY CITY STAFF.

NOTES:

- DETAILS SHOWN SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY STANDARDS.
- DITCH TYPE STREET REQUIRES APPROVAL OF CITY ENGINEER.
- MINIMUM CURB RADIUS ON INTERIOR DRIVES AND PARKING AREAS IS 10'

PAVEMENT SCHEDULE

- SEE DETAIL 4/C2.3
- SEE DETAIL 4/C2.3
- CURB AND GUTTER (REFERENCE 10.17A AND B)



18 TYPICAL SECTIONS PRIVATE STREET N.T.S.

CLDS STD. DTL. 11.13 (MODIFIED)

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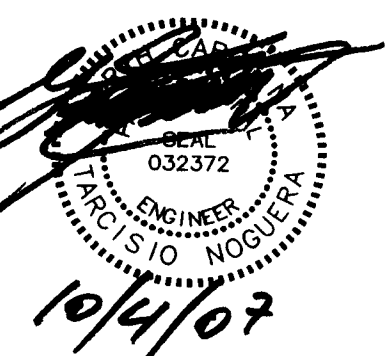
SITE
DETAILS

DATE: 05.31.07

SCALE: N/A

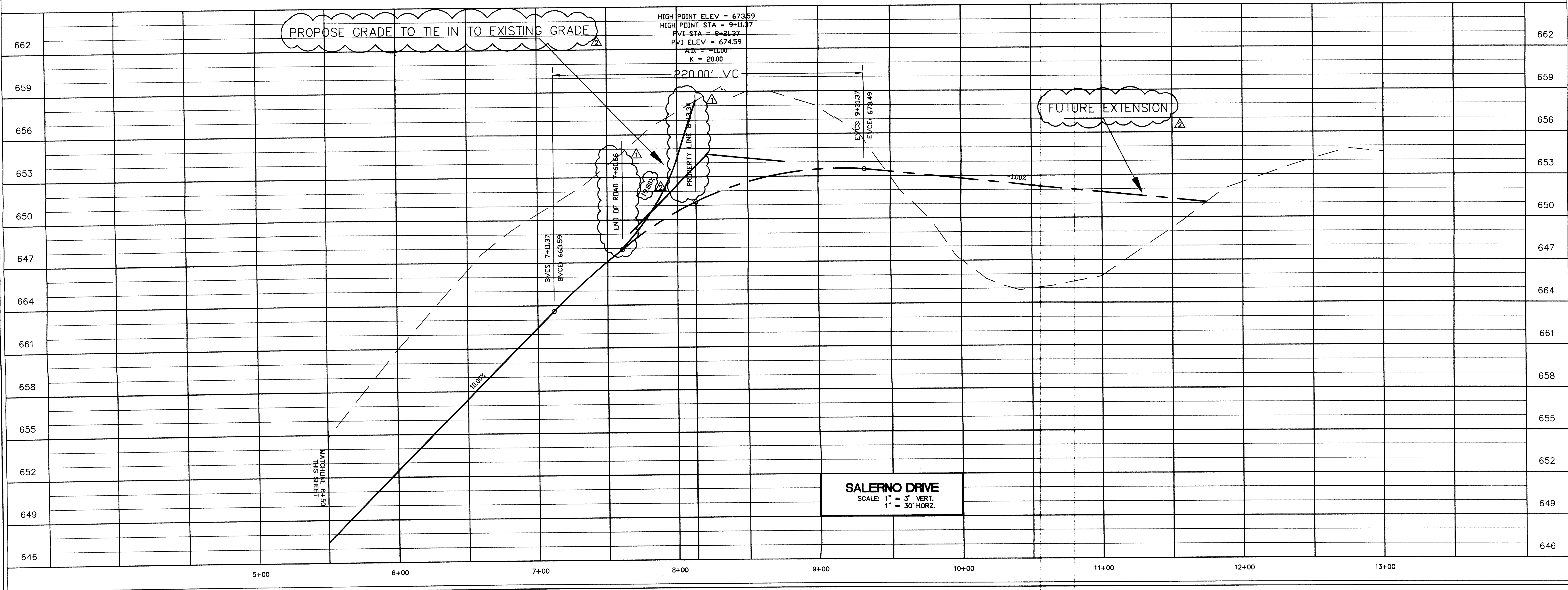
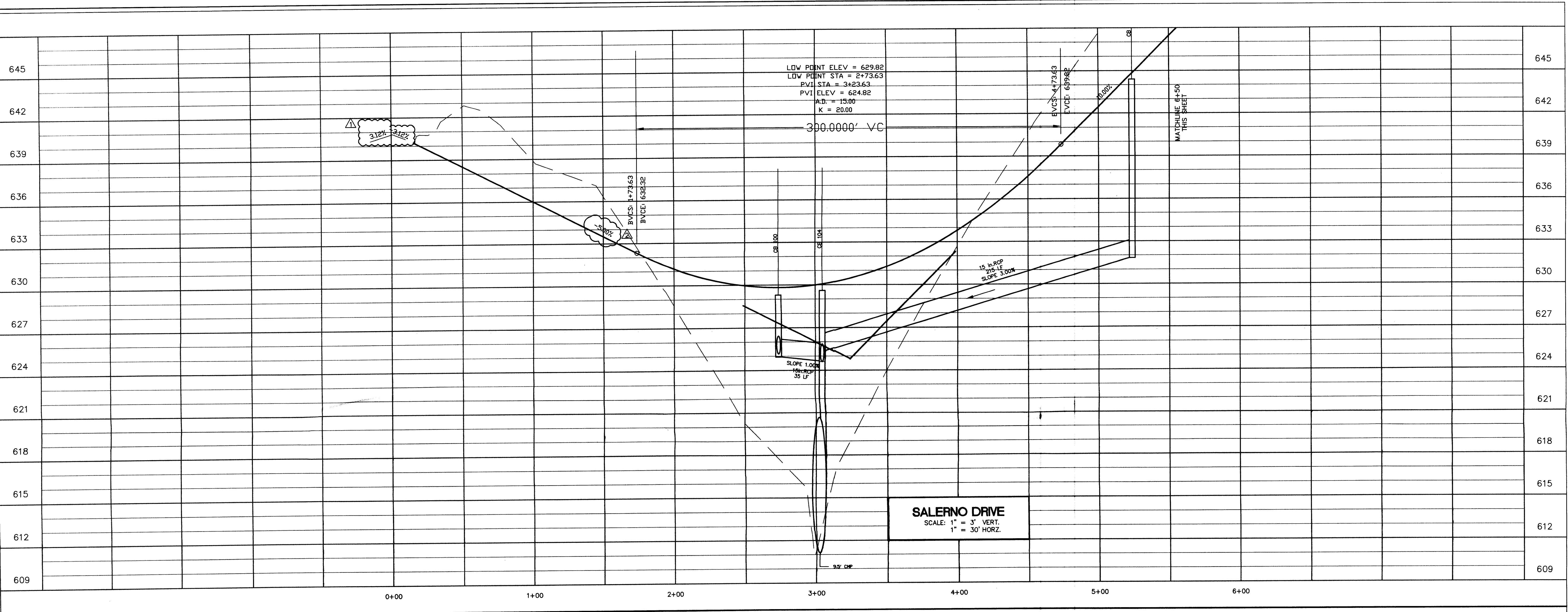
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SHEET: C2.4



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2	10/4/07
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2 10/4/07

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10/4/07

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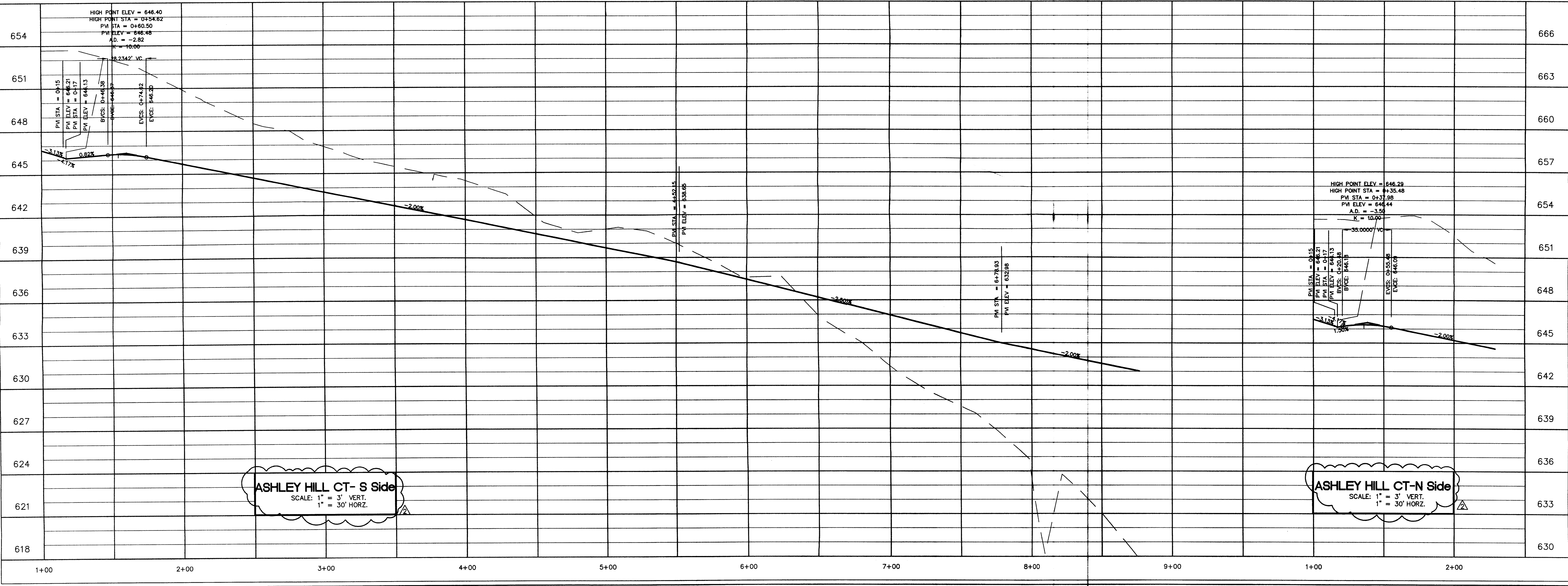
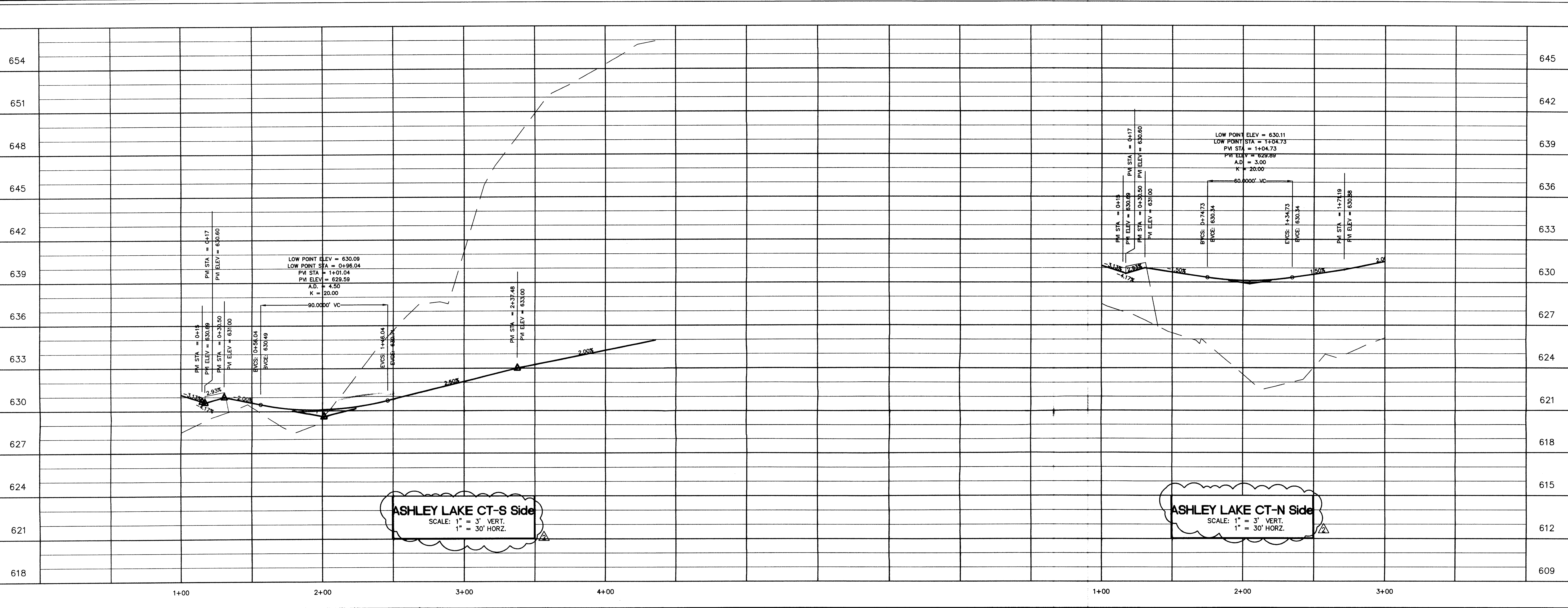
STREET PROFILES

DATE: 05.31.07

SCALE: 1" = 30'

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SHEET: **C2.5**



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10/4/07

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