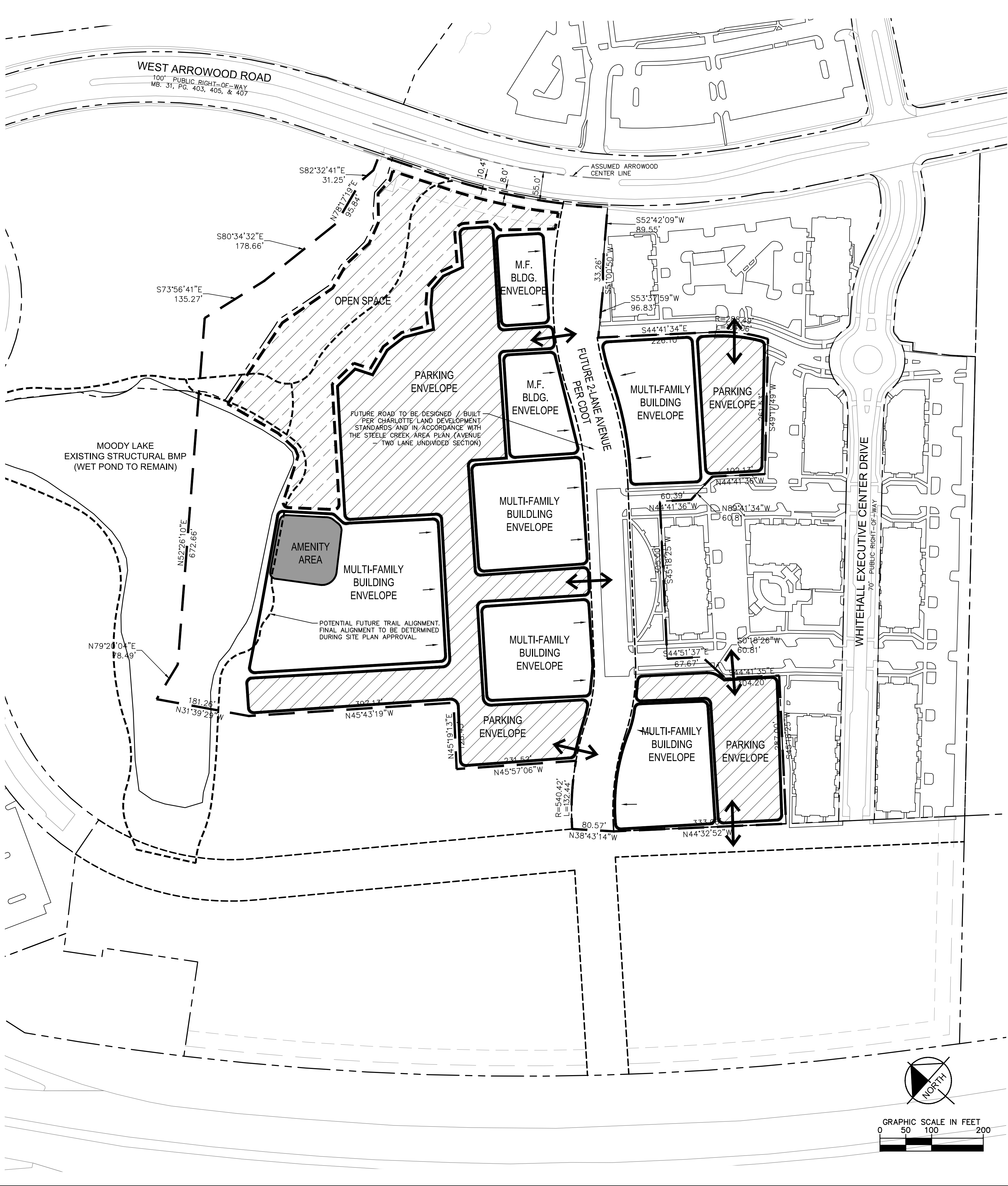


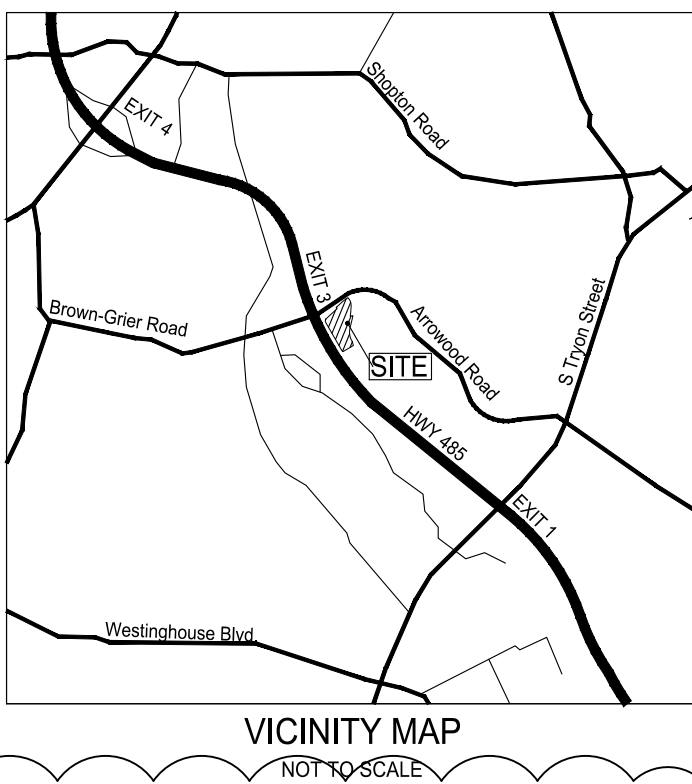
PROJECT NAME:	2019 MOODY LAKE REZONING UPDATE
OWNER:	WHITEHALL CORPORATE CENTER, LLC
PETITIONER NAME:	AMERICAN ASSET CORPORATION PHONE: (704) 295-4005
PLANS PREPARED BY:	KIMLEY-HORN AND ASSOCIATES, INC. PHONE: (704) 333-5131 PORTION OF 2015-61-05
TAX PARCEL ID:	MUDD-O
EXISTING ZONING:	MUDD-O S.P.A.
PROPOSED ZONING:	SEE THE DEVELOPMENT STANDARDS
PROPOSED BUILDING HEIGHT:	SEE DEVELOPMENT STANDARDS
PROPOSED USE:	APPROXIMATELY 23,840 SF
SITE AREA:	322 UNITS
RESIDENTIAL UNITS:	APPROXIMATELY 439,500 AC
RESIDENTIAL AREA:	

Kimley»»Horn

Plotted By: Handlinton, Laura Sheet Set: Moody Lake Rezoning Layout: RZ-2 REZONING PLAN October 14, 2019 02:23:05pm K:\CHL_FR\A\015272.dwg K:\CHL_FR\A\015272.dwg RZ-1 REZONING PLAN.dwg
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



LEGEND	
	EXISTING ZONING BOUNDARY
	REZONING BOUNDARY
	BUILDING AND PARKING ENVELOPE
	FULL MOVEMENT ACCESS POINTS
	BUILDING ORIENTATION ARROW



- 1. GENERAL PROVISIONS**
- A. These Development Standards form a part of the Rezoning Plan associated with the Rezoning Petition filed by American Asset Corporation (the "Petitioner") to request an amendment to the approved conditional rezoning plan associated with Rezoning Petition No. 2008-053 (the "Approved Plan"). More specifically, pursuant to this site plan amendment request, Petitioner seeks to amend the Approved Plan as it relates to an approximately 23.88 acre site located on West Arrowood Road near the West Arrowood Road - Interstate 485 interchange (the "Site"), which Site is more particularly depicted on the Rezoning Plan. The Site is a portion of Tax Parcel No. 201-451-05 and a portion of Tax Parcel No. 201-451-14.
- B. In the event that this Rezoning Petition is approved by the Charlotte City Council, the development and use of the Site will be governed by the Rezoning Plan and the Development Standards associated with this Rezoning Petition and the applicable provisions of the City of Charlotte Zoning Ordinance (the "Ordinance"). The Approved Plan relating to Rezoning Petition No. 2008-053 will not govern the development and use of the Site.
- C. Subject to the optional provisions set out below, the regulations established under the Ordinance for the Mixed Use Development District ("MUDD") zoning district shall govern the development and use of the Site.
- D. Building envelopes for the multi-family buildings proposed to be developed on the Site are each designated on the Rezoning Plan as a "Multi-Family Building Envelope." There are seven Multi-Family Building Envelopes on the Rezoning Plan for the proposed multi-family buildings. Each multi-family building developed on the Site shall be located in a Multi-Family Building Envelope. The locations of the Multi-Family Building Envelopes are subject to minor modifications during the permitting process.
- E. Parking envelopes for the parking facilities proposed to be developed on the Site are each designated on the Rezoning Plan as a "Parking Envelope." Parking facilities shall be located in a Parking Envelope. Notwithstanding the foregoing, parking facilities and vehicular maneuvering and circulation areas may also be located in a Multi-Family Building Envelope. The locations of the Parking Envelopes are subject to minor modifications during the permitting process.
- F. The development and uses depicted on the Rezoning Plan are schematic in nature and are intended to depict the general arrangement of uses and improvements on the Site. Accordingly, the layout, locations and sizes of the uses, improvements and site elements depicted on this Rezoning Plan (including the building and parking envelopes) as well as the internal streets, drives and parking areas are schematic in nature and, subject to the terms of these Development Standards and the Ordinance, are subject to minor alterations or modifications during the design development and construction document phases.
- G. The development of the Site proposed under this Rezoning Plan shall be considered to be a planned/unified development. Therefore, side and rear yards, building height separation requirements and other similar zoning standards shall not be required internally between improvements, uses and other elements located on the Site. Furthermore, the Petitioner and/or the owner(s) of the Site reserve the right to subdivide portions or all of the Site and to create lots within the interior of the Site without regard to any such internal separation standards, provided, however, that the development of the Site shall be required to meet any applicable setback, side yard and rear yard requirements with respect to the exterior boundaries of the Site.
- H. Pursuant to Section 1.110 of the Ordinance and Section 160A-385.1 of the North Carolina General Statutes, the Rezoning Plan, if approved, shall be vested for a period of 5 years due to the size and phasing of the development, the level of investment, economic cycles and market conditions.
- I. Future amendments to the Rezoning Plan and/or these Development Standards may be applied for by the then owner or owners of the Site in accordance with the provisions of Chapter 6 of the Ordinance. Alterations to the Rezoning Plan are subject to Section 6.207 of the Ordinance.
- 2. OPTIONAL PROVISIONS**
- The optional provisions set out below shall apply to the Site.
- A. Parking, vehicular circulation and vehicular maneuvering space may be located between the buildings located on the Site and the required setbacks from the adjacent public streets as depicted on the Rezoning Plan.
- 3. PERMITTED USES/DEVELOPMENT LIMITATIONS**
- A. The Site may only be devoted to a residential community containing a maximum of 352 multi-family dwelling units and to any incidental or accessory uses associated therewith that are permitted under the Ordinance in the MUDD zoning district. Incidental and accessory uses may include, without limitation, a leasing and management office and amenities.
- 4. TRANSPORTATION**
- A. Vehicular access shall be as generally depicted on the Rezoning Plan. The placement and configuration of the access points are subject to any minor modifications required by the Charlotte Department of Transportation ("CDOT") and/or the North Carolina Department of Transportation ("NCDOT").
- B. The alignments of the internal streets, drives and driveways and the vehicular circulation areas may be modified by Petitioner to accommodate changes in traffic patterns, parking layouts and any adjustments required for approval by CDOT and/or NCDOT in accordance with applicable published standards.
- C. Prior to the issuance of the first certificate of occupancy for a new building constructed on the Site, Petitioner shall dedicate and convey to the City of Charlotte (subject to a reservation for any necessary utility easements) those portions of the Site located immediately adjacent to West Arrowood Road as required to provide right of way measuring 55 feet from the centerline of existing West Arrowood Road, to the extent that such right of way does not already exist.
- D. Petitioner shall dedicate and convey all relevant right of way to the City of Charlotte prior to the issuance of the first certificate of occupancy for a new dwelling unit constructed on the Site. The right of way shall be setback 2 feet behind the back of sidewalk where feasible. If it is not feasible, the sidewalk, or portions thereof, may be located in a sidewalk utility easement.
- E. The Future 2-Lane Avenue to be constructed on the Site that is depicted on the Rezoning Plan shall be constructed in accordance with the Avenue - Two Lane Undivided Cross Section provided in the Steele Creek Area Plan.
- F. Petitioner shall install an eastbound right turn lane with 150 feet of storage on West Arrowood Road at the intersection of West Arrowood Road and the Future 2-Lane Avenue to be constructed on the Site and depicted on the Rezoning Plan.
- G. Petitioner shall install a bi-directional crossover with a minimum of 150 feet of storage westbound on West Arrowood Road at the intersection of West Arrowood Road and Future 2-Lane Avenue to be constructed on the Site and depicted on the Rezoning Plan.
- H. All transportation improvements will be approved and constructed prior to the issuance of the first certificate of occupancy for a new building constructed on the site except as otherwise provided below in paragraph G.
- I. In connection with the future development of any undeveloped portions of the site that will continue to be subject to Rezoning Petition No. 2008-053 after the approval of this Rezoning Petition, an updated traffic impact analysis shall be required to be submitted to CDOT and NCDOT (the "Updated TIA") in connection with such development. The purpose of the Updated TIA shall be to evaluate the phasing of the transportation improvements required to be installed in the transportation impact analysis relating to Rezoning Petition No. 2008-053 that was approved on September 12, 2011 (the "Approved TIA"). The transportation improvements required under the Updated TIA shall be consistent with and not exceed the transportation improvements required under the Approved TIA, or the Petitioner shall be required to install the required transportation improvements set out in the Approved TIA that have not been installed.
- 5. ARCHITECTURAL STANDARDS**
- A. The maximum height of any multi-family building developed on the Site shall be 85 feet as measured under the Ordinance.
- B. The architectural and design standards set out below shall apply to the Site.
- (1) Preferred Exterior Building Materials: All principal and accessory buildings abutting a network required public or private street shall comprise a minimum of 30% of that building's entire façade facing such network street using brick, natural stone (or its synthetic equivalent), stucco or other material approved by the Planning Director.
- (2) Prohibited Exterior Building Materials:
- (a) Vinyl siding (but not vinyl hand rails, windows, doors or door trim).
- (b) Concrete Masonry Units not architecturally finished.
- (3) Building Placement and Site Design shall focus on and enhance the pedestrian environment through the following:
- (a) Buildings shall be placed so as to present a front or side façade to all network required streets (public or private).
- (b) Buildings shall front a minimum of 50% of the total network required street frontage on the site (exclusive of driveways, pedestrian access points, accessible open space, tree save or natural areas, tree replanting areas and storm water facilities).
- (c) Parking lots shall not be located between any building and any network required public or private street except as generally depicted on the Rezoning Plan.
- (d) Driveways intended to serve single units shall be prohibited on all network required streets.
- (4) Building Massing and Height shall be designed to break up long monolithic building forms as follows:
- (a) Buildings exceeding 120 feet in length shall include modulations of the building massing/face plane (such as recesses, projections, and architectural details). Modulations shall be a minimum of 10 feet wide and shall project or recess a minimum of 6 feet extending through the building.
- (5) Architectural Elevation Design - elevations shall be designed to create visual interest as follows:
- (a) Building elevations shall be designed with vertical bays or articulated architectural façade features which may include but not be limited to a combination of exterior wall offsets, projections, recesses, pilasters, banding and change in materials or colors.
- (b) Buildings shall be designed with a recognizable architectural base on all facades facing network required public or private streets. Such base may be executed through use of Preferred Exterior Building Materials or articulated architectural façade features and color changes.
- (c) Building elevations facing network required public or private streets shall not have expanses of blank walls greater than 20 feet in all directions and architectural features such as but not limited to banding, medallions or design features or materials will be provided to avoid a sterile, unarticulated blank treatment of such walls.
- (6) Roof Form and Articulation - roof form and lines shall be designed to avoid the appearance of a large monolithic roof structure as follows:
- (a) Long pitched or flat roof lines shall avoid continuous expanses without variation by including changes in height and/or roof form, to include but not be limited to gables, hips, dormers or parapets.
- (b) For pitched roofs the minimum allowed is 4:12 excluding buildings with a flat roof and parapet walls.
- (c) Roof top HVAC and related mechanical equipment will be screened from public view at grade from the nearest street.
- (7) Service Area Screening - service areas such as dumpsters, refuse areas, recycling and storage shall be screened from view with materials and design to be compatible with principal structures. Such design shall include a minimum 20 percent Preferred Exterior Building Materials or a Class B buffer not less than 10' in depth at all above grade perimeter not paved for access.
- C. Building orientations are designated on the Rezoning Plan with arrows.
- 6. STREETScape, LANDSCAPING AND BUFFERS**
- A. A minimum 8 foot wide planting strip and a minimum 8 foot wide sidewalk shall be installed along the Site's public street frontages.
- 7. ENVIRONMENTAL FEATURES**
- A. Development of the Site shall comply with the requirements of the City of Charlotte Tree Ordinance.
- B. Petitioner shall comply with the Charlotte City Council approved and adopted Post-Construction Stormwater Ordinance.
- 8. AMENITIES**
- A. An amenity area shall be provided on the Site to serve the residential community, and the amenity area shall contain, at a minimum, a clubhouse and swimming pool. The actual location of the amenity area may vary from the location depicted on the Rezoning Plan.
- B. A walking trail shall be provided on the Site adjacent to the lake and through the Open Space Area to the public sidewalk along West Arrowood Road. The actual alignment of the walking trail shall be determined during the permitting process.
- 9. LIGHTING**
- A. All freestanding lighting fixtures installed on the Site (excluding street lights, landscape lighting and lower, decorative lighting that may be installed along the driveways, sidewalks and parking areas) shall be fully capped and shielded and the illumination downwardly directed so that direct illumination does not extend past any property line of the Site. The maximum height of any freestanding lighting fixtures installed on the Site shall be 21 feet.
- B. Any lighting fixtures attached to the buildings to be located on the Site shall be decorative, capped and downwardly directed.
- 10. BINDING EFFECT OF THE REZONING DOCUMENTS AND DEFINITIONS**
- A. If this Rezoning Petition is approved, all conditions applicable to the use and development of the Site imposed under these Development Standards and the Rezoning Plan will, unless amended in the manner provided under the Ordinance, be binding upon and inure to the benefit of Petitioner and the current and subsequent owners of the Site and their respective successors in interest and assigns.
- B. Throughout these Development Standards, the term "Petitioner" shall be deemed to include the heirs, devisees, personal representatives, successors in interest and assigns of Petitioner or the owner or owners of the Site from time to time who may be involved in any future development thereof.
- C. Any reference to the Ordinance herein shall be deemed to refer to the requirements of the Ordinance in effect as of the date this Rezoning Petition is approved.

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MOODY LAKE
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PREPARED FOR
AAC

REZONING PLAN

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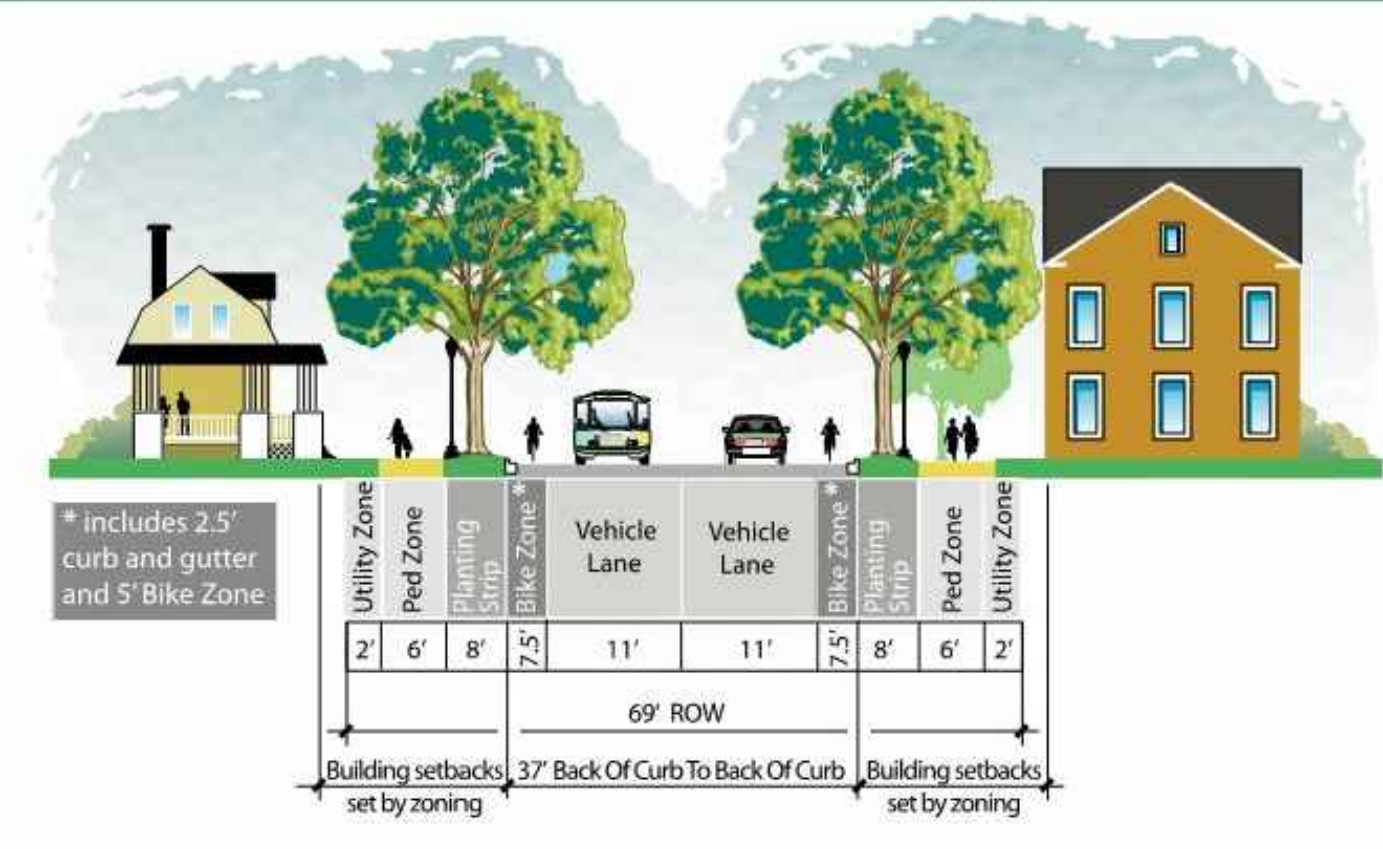
CHECKED BY

No.	REVISIONS	DATE	BY
1	REVISIONS PER CITY COMMENTS	08/19/19	

Avenue - Two Lane Undivided

Width: 37' from back of curb to back of curb. ROW is 69'.

Cross Section: One travel lane in each direction with bike zone, planting strip, sidewalk and utility zone. If a turn lane is added, an additional 11' is required. Building setbacks will vary by zoning districts.



SCSAP Avenue Section