

# Rezoning Transportation Analysis

Petition Number: 2019-070

General Location Identifier: 19106107, 19106105, 19106121

**From:** Felix Obregon, PE  
[fobregon@charlottenc.gov](mailto:fobregon@charlottenc.gov)  
704-432-5729

**Reviewer:** Molly T Haynes, PE  
[molly.haynes@charlottenc.gov](mailto:molly.haynes@charlottenc.gov)  
704-336-1496

## Revision Log:

Date	Description
07-11-19	First Review
08-15-19	Second Review

## General Review Information

The site is at the unsignalized intersection of Independence Boulevard (proposed freeway, state maintained) and Wallace Road (minor thoroughfare, city maintained). The site is in a corridor outside Route 4 and is within the limits of the Independence Boulevard Area Plan.

## Active Projects Near the Site:

- South Pedestrian/Bike Independence Boulevard
  - This project will create a multi-modal facility south of and paralleling Independence Boulevard from the Briar Creek Greenway to Mason Wallace Park.
- Margaret Wallace Sidewalk
  - This project will construct new sidewalk along the south side of Margret Wallace Road from Campbell Creek to Old ate Drive to improve pedestrian access along the corridor.
- Independence Blvd Widening
  - NCDOT is widening Independence Blvd from Conference Drive to 485 to improve vehicular capacity. The project will also include bridges to improve neighborhood connectivity which will accommodate wide sidewalks and bicycle lanes.

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*CDOT's review of this rezoning petition is intended to ensure consistency with the Transportation Action Plan (TAP) which seeks to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.*

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*This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.*

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

The site is located along major thoroughfare and local road. The petitioner has committed to reserving right of way for the Wallace Road Realignment as part of the NCDOT Independence Boulevard Widening project. In addition, the site plan is dedicating land for the proposed CATS Silver Line alignment.

## Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Church Private School	8,490 sf 24,010 sf	290	Tax Record

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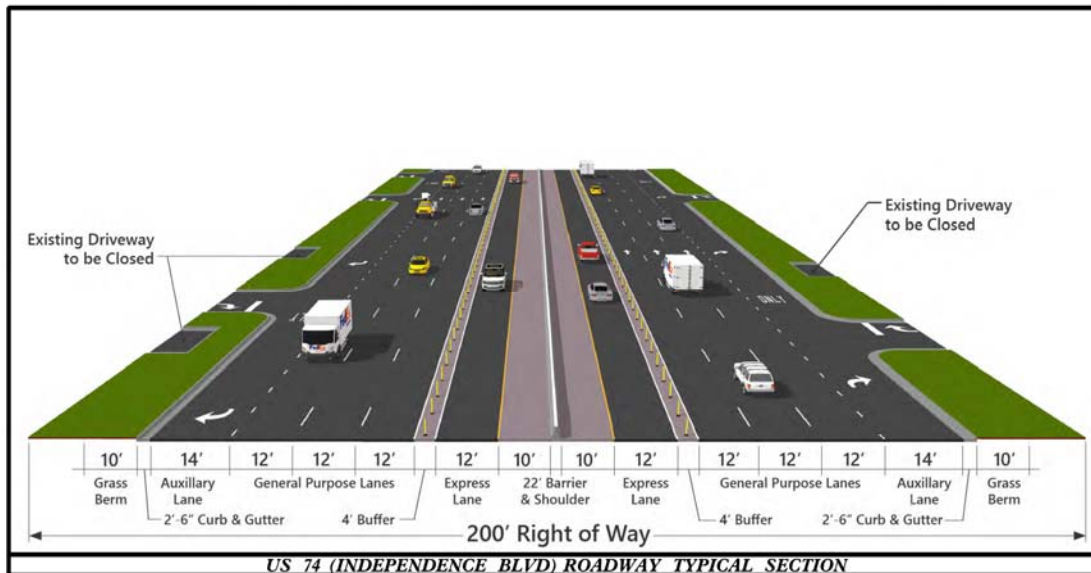
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Entitlement with Current Zoning	Automobile Sales	60,000 sf	1,670	RZ 2016-141
Proposed Zoning	Automobile Sales	100,000 sf	2,780	Site Plan: 08-12-19

## Outstanding Issues

~~Strikeout~~ = Not an outstanding issue

1. **Traffic Study** In lieu of traffic impact study, the petitioner is dedicating right-of-way for the Wallace Road realignment and Sharon Forest grade separated intersection as part of the NCDOT Independence Blvd widening project which will improve vehicular capacity. The grade separated intersection will also improve connectivity between South and North neighborhoods of Independence Blvd by also providing bicycle and sidewalk facilities.
- ~~2. The petitioner should send in an updated exhibit of the NCDOT Independence Blvd showing Wallace Road realignment overlaid on the rezoning site plan to better understand the relationship of both items.~~
- ~~3. The petitioner should update the conditional note(s) to commit to dedicate 80' right-of-way for the Wallace Road realignment.~~
- ~~4. The petitioner should coordinate with NCDOT on current US-74 Express Lane plans and CATS on an updated Silver Line Light Rail Transit alignment.~~



- ~~5. The petitioner should revise the site plan to add a note specifying dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible.~~

## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.

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2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.