

Rezoning Transportation Analysis

Petition Number: 2019-062

General Location Identifier: 10501231

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Revision Log:

Date	Description
05-15-19	First Review

General Review Information

The site is at the unsignalized intersection of Rocky River Road (minor thoroughfare) and Newell Farm Road (major collector). The site is in a corridor outside Route 4.

The City Grier Road and Rocky River Road Intersection Project is constructing a left-turn lane on Grier Road to improve congestion.

CDOT's review of this rezoning petition is intended to ensure consistency with the Transportation Action Plan (TAP) which seeks to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

To be provided prior to public hearing.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family	1 dwelling	10	Tax Record
Entitlement with Current Zoning	Retail Office	5,000 sf 95,000 sf	1,790	RZ 2012-084 & RZ 1995-040(C)
Proposed Zoning	Duplexes	126 dwellings	910	Site Plan: 04-22-19

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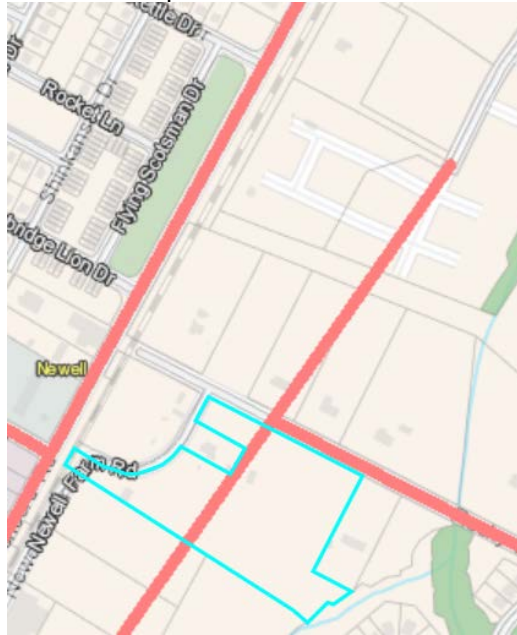
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Outstanding Issues

Strikeout = Not an outstanding issue

1. **Curblin** The proposed zoning district has a setback measured from back of the existing or proposed future curblin. The location of the future back of curb is 26' from centerline along Rocky River Road to account for minor thoroughfare with buffered bike lane to point shown below (Approximate location, CDOT can work with petitioner with where the intersection will occur). After that point the future back of curb is 13' from center line in accordance to CLDS U-02. Per chapter 19 the developer should install curb and gutter as part of this development.

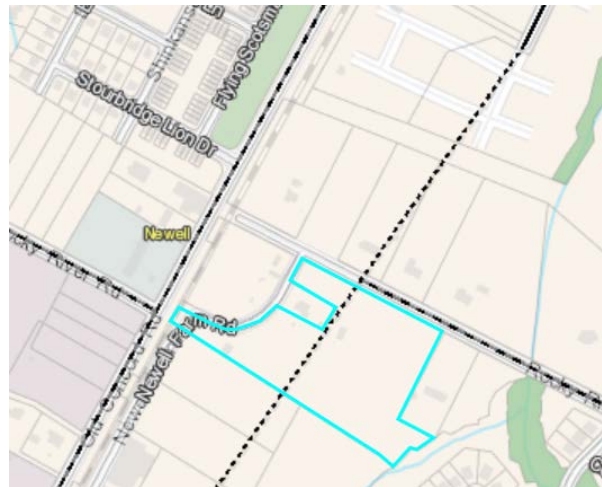
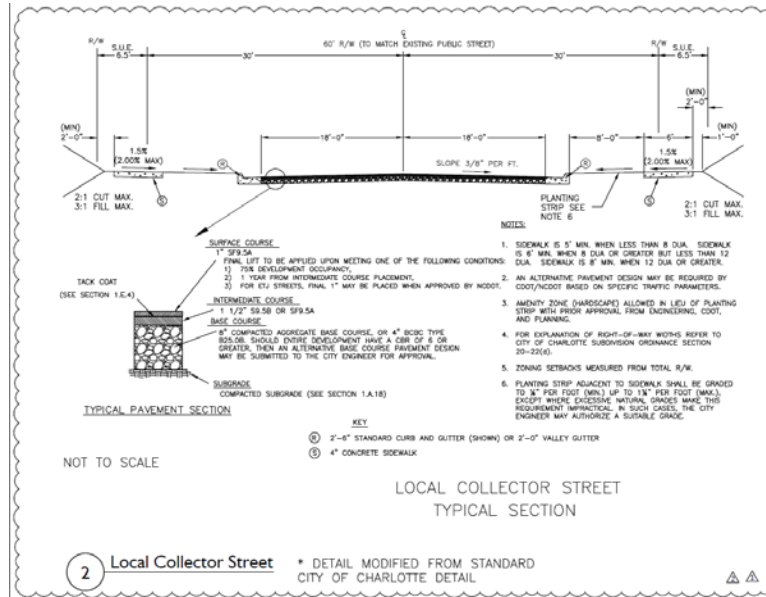


2. **Traffic Study** A Traffic Impact Study is not necessary for the complete review of this petition.
3. The petitioner should revise the site plan and conditional note(s) to commit to constructing a public road to meet the intent of the Minor Thoroughfare road right-of-way through the site as prescribed by the Comprehensive Transportation Plan approved by the Charlotte Regional Transportation Planning Organization. The cross-section should match what was previously approved by development north of site as shown below.

Rezoning Transportation Analysis

Petition Number: 2019-062

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4. The petitioner should revise the site plan to add a note specifying dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible.
5. The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.
6. **Resolved**-Sample format for comment that is resolved

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

Rezoning Transportation Analysis

Petition Number: 2019-062

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1. The setback for this district is measured from the back of the existing or future curbline as determined by CDOT and Planning during the permitting process.
2. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
5. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.