

# Rezoning Transportation Analysis

Petition Number: 2019-058

General Location Identifier: 08501401

**From:** Felix Obregon, PE  
[fobregon@charlottenc.gov](mailto:fobregon@charlottenc.gov)  
704-432-5729

**Reviewer:** Isaiah Washington  
Isaiah.washington@charlottenc.gov  
704-432-6511

## Revision Log:

Date	Description
05-10-19	First Review

## General Review Information

The site is on W 30<sup>th</sup> Street (minor thoroughfare) and is in a corridor inside Route 4. The site is within the limits of the North Tryon Area Plan within proximity to a couple of Community Investment Projects.

The North Business Corridor Project will promote economic development for this area through infrastructure improvements by providing safer pedestrian environment by installing traffic calming features, crosswalks, sidewalks, planting strips, decorative lighting, and bike lanes.

The Matheson Bridge Streetscape Project is providing improved pedestrian and bicycle facilities from North Tryon to Jordan Place.

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*CDOT's review of this rezoning petition is intended to ensure consistency with the Transportation Action Plan (TAP) which seeks to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.*

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*This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.*

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

To be provided prior to public hearing.

## Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Apartments	36 dwellings	230	Tax Record
Entitlement with Current Zoning	Apartments (4.27 ac of R-22MF)	93 dwellings	660	General Guidance from Planning
Proposed Zoning	Apartments	140 dwellings	760	Site Plan: 03-22-19

## Outstanding Issues

~~Strikeout~~ = Not an outstanding issue

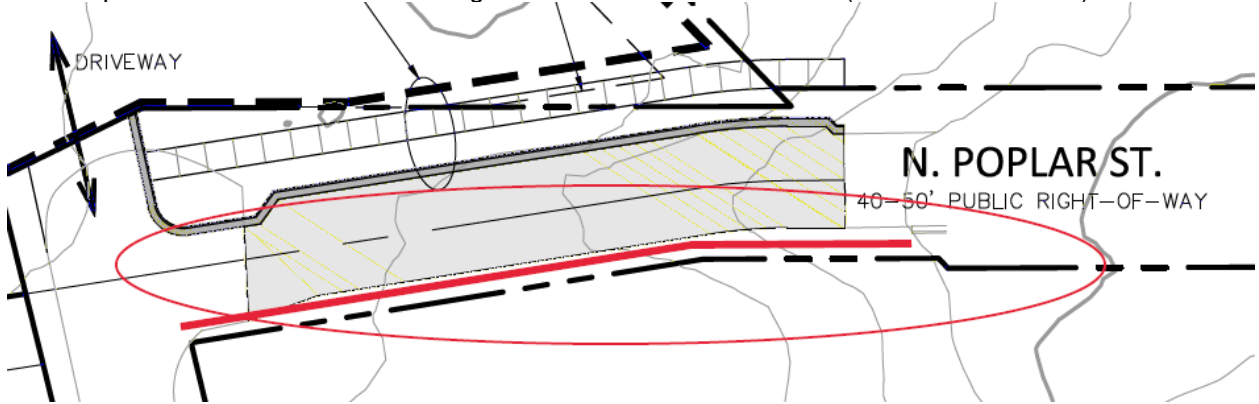
1. **Curbline** The proposed zoning district has a setback measured from back of the existing or proposed future curbline. The location of the future back of curb is in it's existing location, and doesn't need to be relocated. (Site Plan: 03-22-19)

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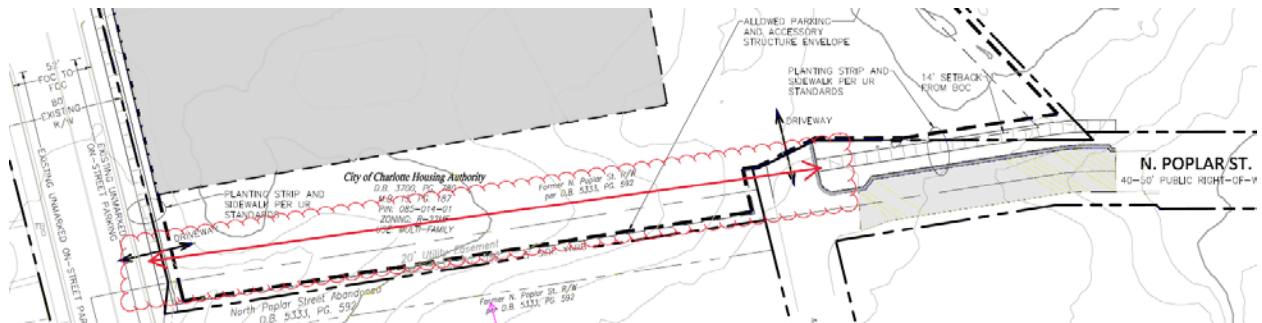
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2. **Traffic Study** A Traffic Impact Study is not necessary for the complete review of this petition. (Site Plan: 03-22-19)
3. The petitioner should revise the site plan and conditional note(s) to add sidewalk on southern portion of N. Poplar Street to connect to existing sidewalk on that side of road. (Site Plan: 03-22-19)



4. The petitioner should revise the site plan and reserve half of the right-of-way for a future extension of North Poplar Street from West 30<sup>th</sup> Street to West 31<sup>st</sup> Street. (Site Plan: 03-22-19)
5. The petitioner should revise the site plan and conditional note(s) to commit to adding sidewalk along southern portion of site to create pedestrian connection between W. 30<sup>th</sup> Street and N. Poplar Street. (Site Plan: 03-22-19)



6. The petitioner should revise the site plan and conditional note(s) to commit to installing an 8' planting strip and a 6' sidewalk to better meet current city standards for thoroughfares. (Site Plan: 03-22-19)
7. The petitioner should revise the site plan and conditional note(s) to commit to installing a buffered bike lane along property frontage on W. 30<sup>th</sup> street within the existing pavement. (Site Plan: 03-22-19)
8. The petitioner should revise the site plan to add a note specifying dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible. (Site Plan: 03-22-19)
9. The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes. (Site Plan: 03-22-19)

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~~10. Resolved-Sample format for comment that is resolved~~

## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. The setback for this district is measured from the back of the existing or future curbline as determined by CDOT and Planning during the permitting process.
2. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
5. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.