

Rezoning Transportation Analysis

Petition Number: 2018-160

General Location Identifier: 05111103, 05111109, 05111109, 05122178

From: Felix Obregon, PE
fobregon@charlottenc.gov
704-432-5729

Reviewer: Isaiah Washington
isaiah.washington@charlottenc.gov
704-432-6511

Revision Log:

Date	Description
01-15-19	First Review

General Review Information

The site is on University City Boulevard (major thoroughfare) and is located in a corridor outside Route 4.

CDOT's review of this rezoning petition is intended to ensure consistency with the Transportation Action Plan (TAP) which seeks to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

To be provided prior to public hearing.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family	1 dwelling	10	Tax Record
Entitlement with Current Zoning	Single Family (93.86 ac of R-3)	281 dwellings	2,690	General Guidance from Planning
Proposed Zoning	Single Family	603 dwellings	5,430	Site Plan: 10-22-18

Outstanding Issues

~~Strikeout~~ = Not an outstanding issue

- Traffic Study** A Traffic Impact Study is necessary for the complete review of this petition.
 - The study will need to be reviewed and approved by CDOT, prior to the case going to public hearing. The site plan submitted for public hearing should include conditional notes to commit to construct roadway improvements determined CDOT. This means the traffic study should be approved at least 30 days prior to scheduled hearing, in order to incorporate transportation improvements notes in the submitted site plan.
 - The petitioner should include a conceptual scale of the intersection for access to verify geometry and understand median location.

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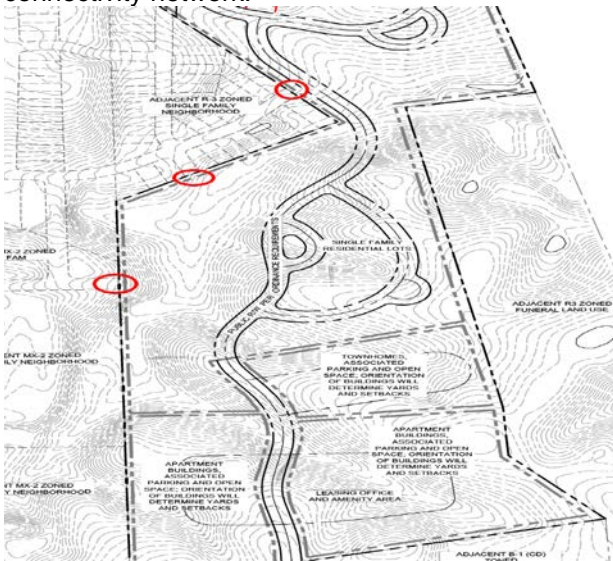
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- The petitioner should revise the site plan and conditional note(s) to commit to construct North-South Road to stub to norther property line.



- The petitioner should revise the site plan and conditional note(s) to commit to construct a North-South street to collector street standards (CLDSM U-07).
 - The petitioner should revise the site plan and conditional note(s) to commit to have reverse frontage access on the collector road.
- The petitioner should revise the site plan and conditional note(s) to commit to create connections to all existing stubs bordering the property per Subdivision Ordinance to provide better street connectivity network.



- The petitioner should revise the site plan and conditional note(s) to commit to create stub(s) to eastern property line on northern half of site for future street connectivity to provide a better street network.
- The petitioner should add conditional note(s) to commit to have alley loaded townhomes.
- The petitioner should revise the site plan and conditional note(s) to commit to an 8' planting strip and 12' multi-use-path along frontage on University City Boulevard.

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8. The petitioner should revise the site plan and conditional note(s) to state access location to be finalized during permitting along University City Boulevard.
9. The petitioner should revise the site plan and conditional note(s) to commit to installing 25mph signs within development.
10. The petitioner should revise the site plan and conditional note(s) to specify which roads are to be public or private.
11. The petitioner should revise the site plan to add a note specifying dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible.
12. The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.

~~13. Resolved-Sample format for comment that is resolved~~

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.