



Charlotte Department of Transportation Memorandum

Date: April 24, 2015

To: Tammie Keplinger
Charlotte-Mecklenburg Planning Department

From: Michael A. Davis, PE *Mike Davis*
Development Services Division

Subject: Rezoning Petition 15-057: Approximately 1.1 acres located on the north side of East Woodlawn Road between Park Road and Brandywine Road.

CDOT has completed a review of the subject petition in order to ensure consistency with the Transportation Action Plan (TAP). The TAP seeks to ensure that Charlotte’s transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users. Based on our review of the petition, we offer the following comments.

Vehicle Trip Generation

This site could generate approximately 2,800 trips per day as currently zoned. Under the proposed zoning the site could generate approximately 1,900 trips per day.

We have the following comments regarding apparent conflicts between the proposed rezoning and existing ordinances or policies:

1. The Park Woodlawn Area Plan calls for the following typical section:

A6 — Woodlawn Road (Halstead Drive to Brandywine Road)											
	8'	8'	5'	11'	11'	10'*	11'	11'	5'	8'	8'
13' planting strip should be used to preserve future curb. <i>Specific setback requirements apply</i>											

The callout note on RZ-1 of the rezoning plan appears to be in conflict with the recommended area plan street cross-section. The rezoning plan note states that from the centerline, a 5’ (half) median, two 11’-wide lanes, 3’-bike lane and 2.5’ curb and gutter is the future street cross-section. According to the area plan the future back-of-curb is 34.5’ from the roadway centerline.

In addition to the comments above, CDOT requests the following changes to the rezoning plan:

1. CDOT requests the petitioner revise the pedestrian refuge island dimensions and associated pavement markings to the conceptual design shown in Figure 1 below.

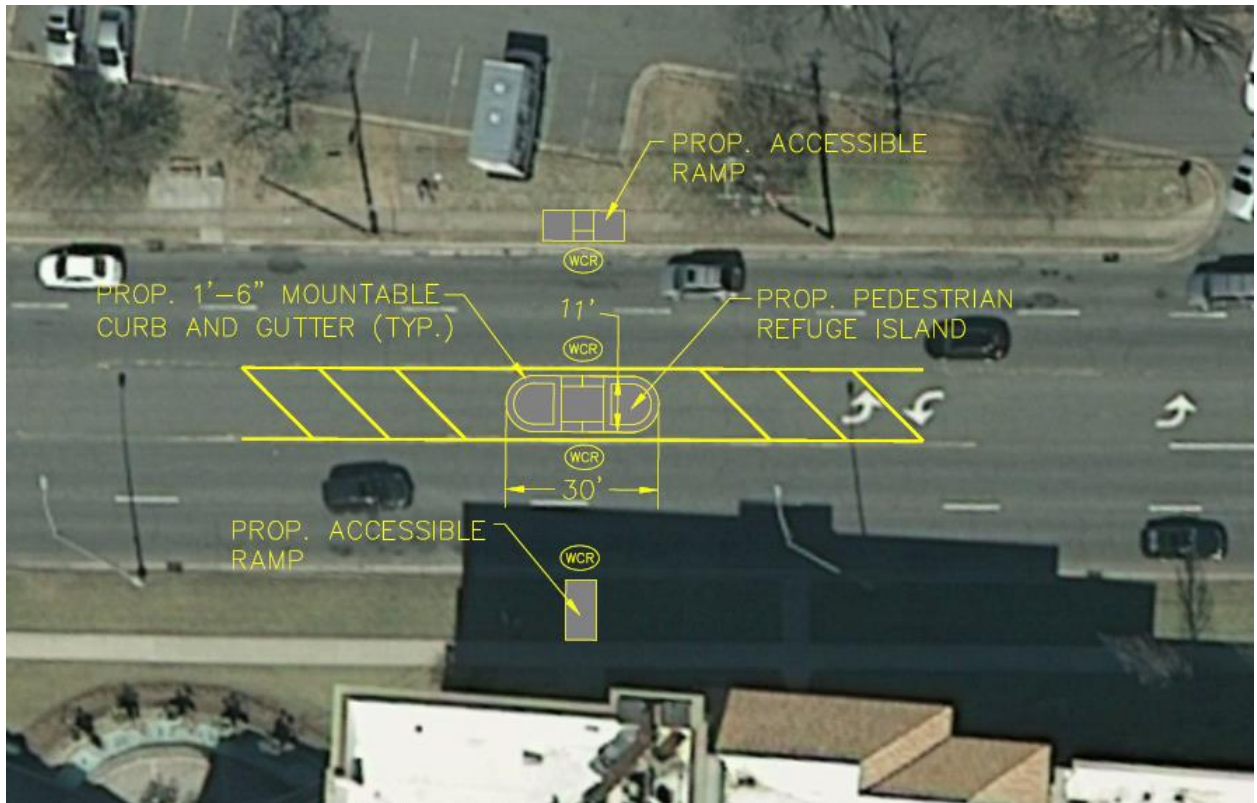


Figure 1

2. For the existing north/south private drive, CDOT requests the proposed back-of-curb sidewalk be relocated to create an 8'-wide planting strip and 6'-wide sidewalk adjacent to the westernmost proposed building (Space 525). CDOT also requests the sidewalk on the north side of the site get connected to the aforementioned sidewalk on the west side of the site (Figure 2).
3. It is our understanding that our Planning staff would like relocate the proposed buildings closer to Woodlawn Road to create a more urban design that is consistent with the recommendations of the Woodlawn area plan. In keeping with this goal, we recommend eliminating the southernmost driveway aisle and driveway connection to the existing 5th/3rd bank. See Figure 2 below.

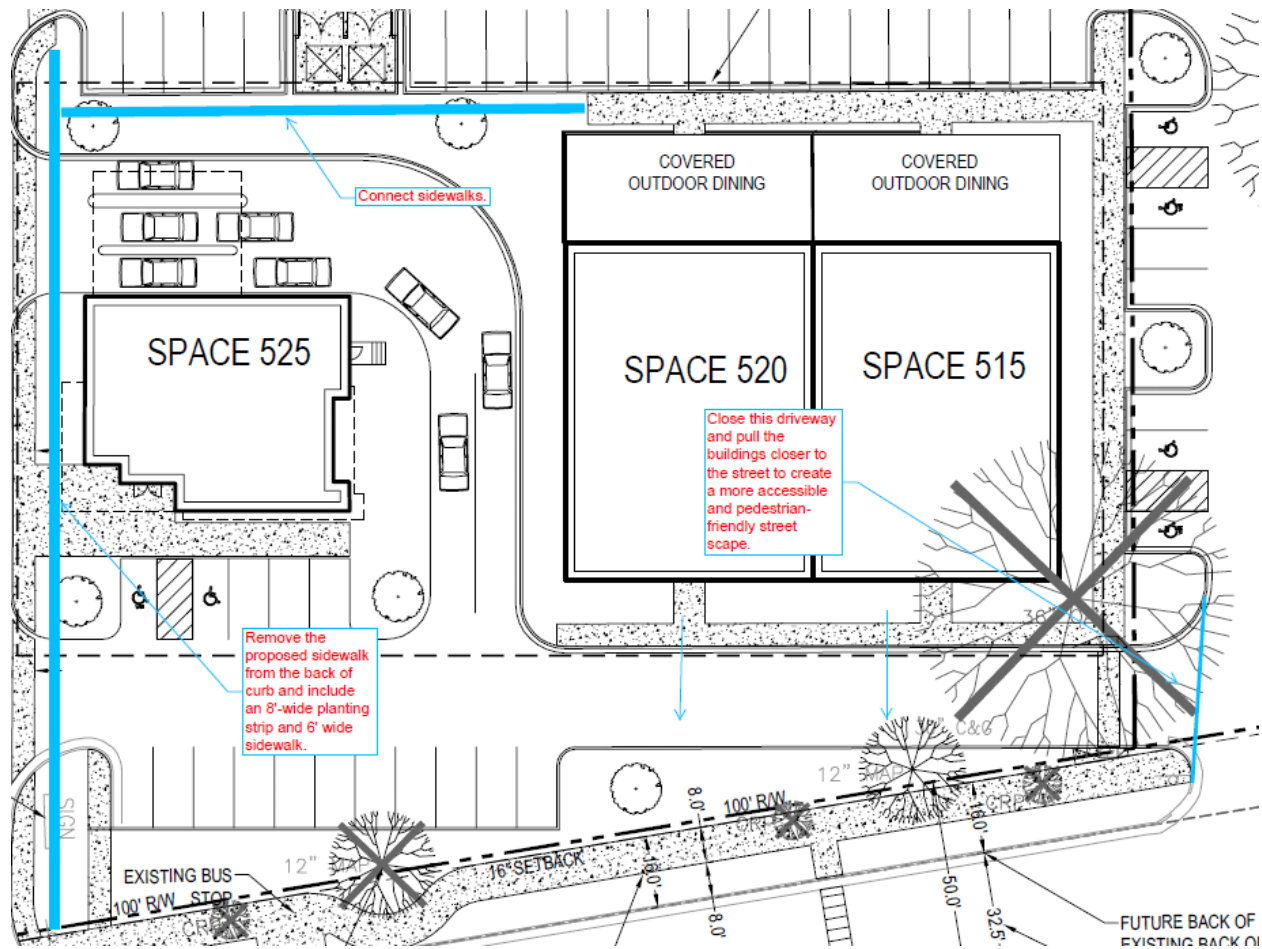


Figure 2

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrances. Two 35' x 35' and two 10' x 70' sight triangles are required for the entrances to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrances. Such items should be identified on the site plan.
3. The proposed driveway connection to East Woodlawn will require driveway permits to be submitted to CDOT and the North Carolina Department of Transportation for review and approval. The exact driveway locations and type/width of the driveways will be determined by CDOT during the driveway permit process. The locations of the driveways shown on the site plan are subject to change in order to align with driveways on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.

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4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.

If we can be of further assistance, please advise.

K. Bryant

cc: S. Correll
Rezoning File