



# Charlotte Department of Transportation

## Memorandum

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**Date:** March 27, 2015

**To:** Tammie Keplinger  
Charlotte-Mecklenburg Planning Department

**From:** Michael A. Davis, PE *Mike Davis*  
Development Services Division

**Subject:** Rezoning Petition 15-052: Approximately 36.10 acres located on the west side of Providence Road between Cloister Drive and Knob Oak Lane and across from Strawberry Hill Drive.

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CDOT has completed a review of the subject petition in order to ensure consistency with the Transportation Action Plan (TAP). The TAP seeks to ensure that Charlotte's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users. Based on our review of the petition, we offer the following comments.

### Vehicle Trip Generation

This site could generate approximately 3,860 trips per day as currently zoned. Under the proposed zoning the site could generate approximately 3,860 trips per day.

We have the following comments regarding apparent conflicts between the proposed rezoning and existing ordinances or policies:

1. It appears that the proposed future street connection for the adjoining site to the south has not been fully coordinated with that of rezoning petition 2015-046. CDOT requests the petitioner coordinate efforts with the adjoining property owner to reflect a consistent street connection between the two properties. See Figures 1 and 2 below.

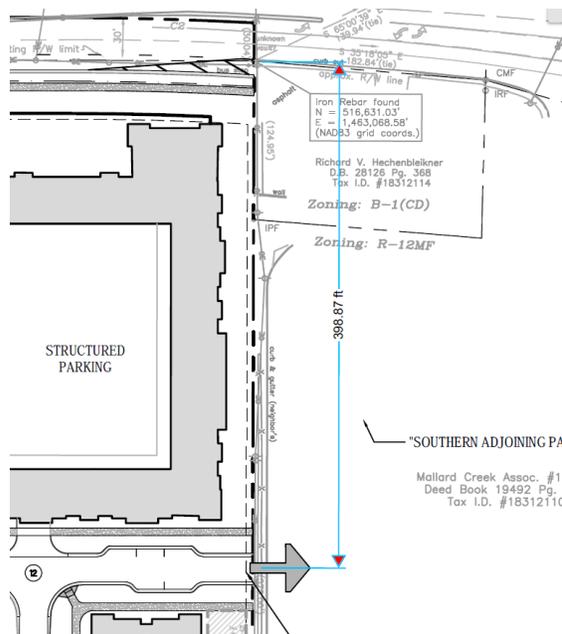


Figure 1

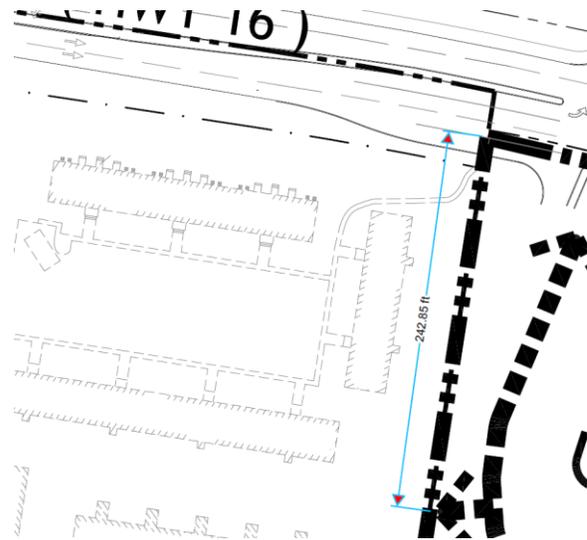


Figure 2

CDOT requests the following changes to the rezoning plan:

1. There are several locations on the site plan where sidewalk gaps are present. These gaps occur at internal intersections and also where proposed sidewalks stop short of proposed buildings. CDOT requests that all sidewalk gaps are eliminated and the site be made fully interconnected with sidewalks and curb ramps.
2. CDOT understands the petitioner wishes to eliminate the right-in-right-out driveway from the conditional rezoning plan. CDOT recommends that it remain on the plan as currently depicted.
3. CDOT requests that the timing aspects of transportation note 3.d. be revised to commit the funds for signalization to be made available when CDOT and NCDOT determine the signal is justified for installation beginning with the permitting of the first building on the site and ending 1 year after the permitting for the last building allowed on the site.

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrances. Two 35' x 35' and two 10' x 70' sight triangles are required for the entrances to meet requirements.

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All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrances. Such items should be identified on the site plan.

3. The proposed driveway connections to Providence Road will require driveway permits to be submitted to CDOT and the North Carolina Department of Transportation for review and approval. The exact driveway locations and type/width of the driveways will be determined by CDOT during the driveway permit process. The locations of the driveways shown on the site plan are subject to change in order to align with driveways on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.

If we can be of further assistance, please advise.

K. Bryant

cc: S. Correll  
Rezoning File