

VICINITY MAP
NTS

SITE DEVELOPMENT DATA

Site Acreage:	82.0 Ac.
Tax Parcels:	199-241-03, 199-241-13, 199-241-15, 199-241-34, 199-241-44, 199-251-01
Proposed Zoning:	MUDD-O (LLW-PA); CC (LLW-PA); CC (SPA) LLW-PA
Existing Zoning:	CC (LLW-PA); CC SPA (LLW-PA); I-1 (CD) (LLW-PA - by Rezoning Petition No. 2010-11); and R-3 (LLW-PA)
Existing Uses:	Vacant and two single family homes
Proposed Uses:	Uses permitted by right and under prescribed conditions together with accessory uses, as allowed in the CC and MUDD zoning districts.

Maximum Gross Development: Up to 525,000 square feet of gross floor area and a hotel with up to 120 rooms (see Development Standards for detail).

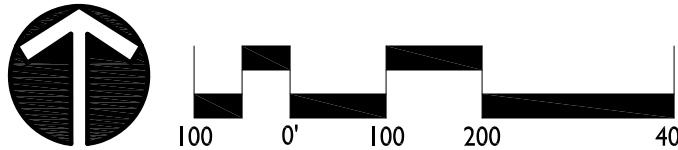
Maximum Building Height: As allowed by the Ordinance in the area zoned CC. In the area Zoned MUDD-O building height will be limited to 50 feet except for certain architectural features such as towers, gallery roofs and similar structures which may be up to 65 feet in height. Height to be measured as defined by the Ordinance.

Parking: As required by the Ordinance for the portion of the Site zoned CC and a minimum of one space per 250 gross s.f. of building area for the portion of the Site zoned MUDD-O

Open Space: A minimum of 15% of the Site will be established as open space as defined by the Ordinance.

SITE LEGEND

- SITE ACCESS
- PUBLIC STREET
- PRIVATE STREET
- BUILDING EDGE
- LOW WALL/ LANDSCAPE EDGE



STEELE CREEK RETAIL CENTER
REZONING PETITION No. 2013-001
STEELE CREEK (1997) Limited Partnership
TECHNICAL DATA SHEET

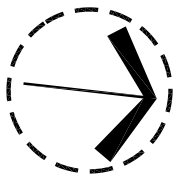
REVISIONS:
(1) 12.21.12 - REVISE PER STAFF COMMENTS
(2) 03.06.13 - REVISE PER STAFF COMMENTS
(3) 03.15.13 - REVISE PER STAFF COMMENTS

DATE: DECEMBER 3, 2012
DESIGNED BY: KST
DRAWN BY: KST
CHECKED BY: MFC
SCALE: 1"=200'
PROJECT #: 1012048
SHEET #:

RZ-1



SITE LEGEND



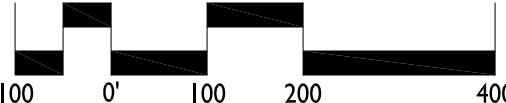
SITE ACCESS



PUBLIC STREET



PRIVATE STREET



DATE: DECEMBER 3, 2012
DESIGNED BY: KST
DRAWN BY: KST
CHECKED BY: MFC
SCALE: 1"=200'
PROJECT #: 1012048

STEELE CREEK RETAIL CENTER
REZONING PETITION No. 2013-001

STEELE CREEK (1997) Limited Partnership

SCHEMATIC SITE PLAN

Steele Creek 1997 Limited Partnership
Development Standards
3/15/17
Rezoning Petition No. 2013-001 (Tanger Retail Center)

Site Development Data:

- Acres:** 82.0
- Tax Parcel #:** 109-254-01, 13, 15, 34, and 44, 109-251-01
- Utility Agency:** Charlotte City and County Water Department (CCW&C) and Charlotte City and County Sewer Department (CCS&C)
- Proposed Zoning:** MUDD-O (LLWP-A, CC and CP) and LLW-F&A
- Existing Uses:** Vacant and single-family homes
- Proposed Uses:** Uses permitted by right and under prescribed conditions with accessory uses, as allowed in the CC and MUDD-O zoning districts (as more specifically described below)
- Maximum Gross Square Feet of Development:** Up to 525,000 square feet of gross floor area and a hotel with up to 120 rooms
- Maximum Building Height:** As allowed by the Ordinance in the area zoned CC. In the area zoned MUDD-O building height will be limited to 50 feet except for certain architectural features such as towers, gallery roofs and similar structures which may be up to 65 feet in height. Height to be measured as defined by the Ordinance.
- Parking:** As required by the Ordinance for the portion of the Site zoned CC and a minimum of one space per 250 gross square feet of building area for the portion of the Site zoned MUDD-O.
- Open Space:** A minimum of 15% of the Site will be established as open space as defined by the Ordinance.

I. General Provisions:

a. These Development Standards form a part of the Rezoning Plan associated with the Rezoning Petition filed by Steele Creek 1997 Limited Partnership to accommodate development of an upstate outlet retail center and accompanying retail, office and hotel uses on an approximately 82.0-acre site located south of Shopton Road, east of Dixie River Road, north of Steele Creek Road and west of I-485 (the "Site").

b. Development of the Site will be governed by the Rezoning Plan as well as the applicable provisions of the City of Charlotte Zoning Ordinance (the "Ordinance"). Unless the Rezoning Plan establishes more stringent standards, the regulations established under the Ordinance for portions of the Site to be designated on the Rezoning Plan, and the MUDD-O zoning district classification for the portions of the Site so designated on the Rezoning Plan (subject to the Optional Provisions provided below), shall govern all development taking place on the Site.

c. The schematic depictions of the uses, parking areas, sidewalks, structures and buildings, and other site elements set forth on the Rezoning Plan should be reviewed in conjunction with the provisions of the Development Standards. The ultimate layout, locations and sizes of the development and site elements depicted on the Rezoning Plan as well as any schematic building elevations or graphic representations of the development and site elements proposed, and they may be altered or modified in accordance with the setbacks, yard and buffer requirements set forth on this Rezoning Plan and the Development Standards, provided, however, any such alterations and modifications shall not materially change the overall design intent of the Rezoning Plan. Changes to the Rezoning Plan not permitted by the Rezoning Plan will be considered null and void.

d. Notwithstanding the number of buildings shown on the Rezoning Plan, the total number of principal buildings to be developed on the Site shall not exceed 78. Accessory buildings and structures located on the Site shall not be considered in any limitation on the number of buildings on the Site. Accessory buildings and structures will be constructed utilizing similar building materials, colors, architectural elements and designs as the principal building(s) located within the same Development Area. The accessory structure/building. Other than parking structures, the size of accessory structures/buildings located within Development Area A will be limited to 20% of the building area of the principal buildings constructed within Development Area A.

II. Optional Provisions for MUDD-O Areas:

a. Subject to the restrictions and limitations listed below, the principal buildings constructed on the Site may be developed with up to 525,000 square feet of gross floor area of uses permitted by right and under prescribed conditions and a hotel with up to 120 rooms (as shown on the Rezoning Plan and the Development Standards) and uses permitted by right and under prescribed conditions in the Commercial Center (CC) and MUDD-O zoning districts, as applicable.

For purposes of the development limitations set forth in these Development Standards that do not have to be construed as a limitation on FAR requirements, the term "gross floor area" or "GFA" shall mean and refer to the sum of the gross horizontal area of each surface of a principal building on the Site measured from the outside of the exterior walls on the center line of party walls; provided, however, such term shall exclude any surface of structural parking facilities, areas used for building and equipment access (such as stairs, elevator shafts and other necessary road access), all loading dock areas (open or enclosed), and outdoor dining areas whether on the roof of the building(s) or at street level (parking for outdoor dining areas will be provided as required by the Ordinance).

b. For ease of reference, the Rezoning Plan sets forth eight (8) development areas as generally depicted on the Technical Data Sheet as Development Areas A, B, C, D, E, F, G and H (each a "Development Area" and collectively the "Development Areas"). Development Areas A, B, C and D will be zoned MUDD-O (LLWP-A, CC and CP), and Development Areas E, F, G and H will be zoned CC (LLWP-A).

c. Within Development Areas E, F, G and H the total allowed square foot of the principal buildings will be limited to 500,000 square feet of gross floor area and a hotel with up to 120 rooms (as shown on the Rezoning Plan and the Development Standards). Of the allowed 500,000 square feet of gross floor area, a maximum of 27,500 square feet may be utilized for retail and/or restaurant uses. Up to three (3) uses, in total, with an accessory drive-through window may be developed within Development Areas E, F, G and H. A gas station with or without a convenience store may be substituted for one of the three allowed uses with an accessory drive-through window (e.g. if a gas station with or without a convenience store is constructed on one of the Development Areas referenced in this Section, then only two uses with accessory drive-through windows will be allowed on the remaining Development Areas referenced).

d. Up to two (2) uses, in total, with an accessory drive-through window will be allowed within Development Areas B, C, and D. One gas station with or without a convenience store may be constructed on Development Area B.

The number of allowed hotel rooms may be increased by up to 30 rooms by the amount of commercial uses at the rate of 500 square feet for each hotel room added.

f. "Building Edges" have been provided within portions of Development Areas B and C as generally depicted on the Technical Data Sheet. *[Note: The Building Edges shown consist of two (2) edges, one for each abutting street].* The buildings constructed within Development Areas B and C must adhere to the Building Edges to which they abut as generally depicted on the Technical Data Sheet (plaza/outdoor dining areas will be considered part of the buildings for the purposes of compliance with this provision) such that vehicular parking and maneuvering may not be located between the proposed buildings and the Building Edges indicated within these Development Areas that do not abut the Building Edges indicated shall not have to comply with this restriction. Without limiting the provisions of Sections 1 and 2 above, the schematic representations of the design treatment for the development within Development Areas B and C generally depicted on Sheet RZ-2 are merely schematic in nature and may be altered in a manner consistent with the Building Edge(s) provisions described above. In addition, the provisions of this Section may be altered to permit greater flexibility in development as allowed by the Administrative Amendment provisions of the Ordinance.

g. "Modified Building Edges" have been provided within Development Area D as generally depicted on the Technical Data Sheet. The Petitioner will provide along the Modified Building Edges an improved open space area that will contain seating, concrete and/or masonry walls, landscaping and specialty pavers; if the Petitioner is able to locate a building along the Modified Building Edges these requirements shall not apply.

h. "Building Edges" have been provided within portions of Development Areas E and F as generally depicted on the Technical Data Sheet. The buildings constructed within Development Areas E and F must adhere to one of the two Building Edges generally depicted on the Technical Data Sheet but not both (plaza/outdoor dining areas will be considered part of the buildings for the purposes of compliance with this provision), such that vehicular parking and maneuvering may not be located between the proposed buildings and one of the two Building Edges indicated. Along the other Building Edge vehicular parking and maneuvering may be located between the proposed buildings and such Building Edge, provided that: (i) the amount of vehicular parking and maneuvering allowed between the building and the Building Edge will be limited to one (1) parking and/or one drive thru lane; (ii) a low wall (2 to 2.5 feet high) and low accent planting will be provided between the drive thru lane or parking area and the adjacent street; and (iii) such wall will be constructed of building materials generally compatible with the buildings within the applicable Development Area. Other buildings constructed within Development Areas E and F that do not abut a Building Edge shall not have to comply with these restrictions. Without limiting the provisions of Sections 1 and 2 above, the schematic representations of the design treatment for the development within Development Areas E and F generally depicted on Sheet RZ-2 are merely schematic in nature and may be altered in a manner consistent with the Building Edge provisions described above. In addition, the provisions of this Section may be altered to permit greater flexibility in development as allowed by the Administrative Amendment provisions of the Ordinance.

i. Buildings constructed within Development Area G abutting Dixie River Road, Steele Creek Road or Trojan Drive, and the portion of Development Area F abutting Dixie River Road that does not have a Building Edge requirement, will limit the amount of parking between the proposed building and these streets to one of parking and/or a drive thru lane. A low wall (2 to 2.5 feet high) and low accent planting will be provided between the drive thru lane or parking area and the adjacent street, and such wall will be constructed of building materials generally compatible with the buildings within Development Area G and H as applicable.

Optional Provisions for MUDD-O Areas:

The following optional provisions shall apply to the portions of the Site designated MUDD-O on the Technical Data Sheet (i.e. Development Areas A, B, C and D) in a more particularly described below.

a. To allow vehicular parking, maneuvering and service between the proposed buildings and: (i) I-485; (ii) Dixie River Road; (iii) Shopton Road; and (iv) the interior public streets, in the manner generally depicted on the Rezoning Plan and in accordance with the "Building Edges" provisions set forth above in Sections 2(f) and (g).

b. To allow up to two (2) uses with an accessory drive-through window within Development Areas B, C, or D.

c. To allow two (2) detached, product identification signs along I-485 within Development Area A. Each of these two (2) signs may be up to 50 feet high and may contain up to 300 square feet of sign area. To allow along the internal public street (i.e. the extension of Trojan Drive through Site to Shopton Road) up to three (3) ground mounted signs identifying the shopping center within Development Area A, but not the individual tenants; these signs will be generally located at the intersection of the internal private streets and Trojan Drive extension and will be up to eight (8) feet high and may contain up to 64 square feet of sign area, the overall width of the allowed sign plus any structural supports or other none sign face features of the signs may not exceed 12 feet.

d. To allow along the other public street frontages of Development Areas A, B, C, and D, one (1) shopping center sign per street frontage, these signs may be up to 30 feet high and contain up to 150 square feet of sign area.

e. To allow one (1) detached identification signs for each building located within Development Areas B, C and D. These detached identification signs may be up to four (4) feet high and contain up to 50 square feet of sign area.

f. To allow wall signs to have up to 200 square feet of sign surface area per wall or 10% of the wall area to which they are attached, whichever is less, within Development Areas A, B, C, and D.

g. To allow any of the detached signs allowed by the Ordinance or these Optional Provisions to be placed within the setbacks or landscape areas indicated on the Rezoning Plan, provided, however, such detached signs are located a minimum of 14 feet behind the back of curb of the abutting public street.

h. To allow tenant identification signs within Development Area A to be located on exterior building walls and certain architectural features (e.g. tower elements and other such features) that are not part of the leasehold area of the tenant identified. The total amount of signage on these architectural features will not exceed 25% of the wall area, and the signs located on the exterior building walls will not exceed 10% of the wall area to which the signs are attached.

i. To allow the buildings constructed within Development Area A to provide alternative blank wall treatments for the building walls that face I-485, Dixie River Road, Shopton Road and the interior public street. Examples of these alternative treatments are generally depicted on Sheets RZ-5a and RZ-5b of the Rezoning Plan, but other similar treatments may be allowed.

j. To not require driveway to be recessed into the face of the building(s) within Development Areas B, C, and D.

Note: The optional provisions regarding signs are additions to the standards for signs in the MUDD district and should be used in conjunction with and in addition to the normal MUDD sign regulations.

Access:

a. Access to the Site will be from Steele Creek Road, Dixie River Road, Trojan Drive and Shopton Road as generally depicted on the Rezoning Plan.

b. The number and location of access points to the internal public streets will be determined during the building permit process.

c. The private streets generally depicted on the Technical Data Sheet will be designed to meet the "Office-Narrow" cross-section as defined in City of Charlotte Subdivision Ordinance in existence at the time of approval of this Petition.

d. A sidewalk network as generally depicted by the pedestrian circulation plan set forth on Sheet RZ-4 of the Rezoning Plan will be provided. The minimum width of the sidewalks indicated will be six (6) feet.

e. The alignment of the internal public and private streets, vehicular circulation and driveways may be modified by the Petitioner to accommodate changes in traffic patterns, parking layouts and any adjustments required for approval by the Charlotte Department of Transportation (CDOT) in accordance with published standards.

Transportation Improvements:

Proposed Improvements:

The Petitioner plans to provide or cause to be provided on its own or in cooperation with other parties who may implement portions of the improvements, the improvements set forth below to benefit overall traffic patterns throughout the area in accordance with the following implementation provisions:

The following Transportation Improvements are also illustrated on Figures 11a and 11b on Sheet RZ-4 of the Rezoning Plan. The figures on Sheet RZ-4 are to be used in conjunction with the following notes to determine the extent of the proposed improvements (reference to a number or letter when describing an improvement corresponds to the number or letter found on Figures 11a and 11b for the proposed improvement).

Improve Dixie River Road Between Steele Creek Road (NC 160) and Shopton Road (section # 1) as follows:

i. Construct a second westbound through lane on Dixie River Road within the existing cross-section (median reduction/pavement marking modifications) for the majority of its length that commences as a combined through-right turn lane at Proposed Access "M" and terminates at the existing westbound left turn lane at Shopton Road; and

ii. Construct a second eastbound through lane on Dixie River Road within the existing cross-section (median reduction/pavement marking modifications) for the majority of its length that commences as a combined through-right turn lane at Proposed Access "M" and terminates at the existing westbound left turn lane at Shopton Road.

Improve Steele Creek Road Between I-485 Outer Ramps & Dixie River Road (section # 2) as follows:

i. Construct a 3rd southbound through lane on Steele Creek Road that commences at the I-485 Outer Exit Ramp and terminates as the existing right turn lane on Dixie River Road (approximately 1,175 feet in length).

Improve Steele Creek Road (NC 160) & Brown-Grier Road (intersection # 3) as follows:

i. Construct a northbound right turn lane on Steele Creek Road with a minimum of 200 feet of storage and a 150-foot bay taper.

Improve Shopton Road & Private Street (intersection # 3) as follows:

i. Re-mark the existing painted out northbound left turn lane on Steele Creek Road to a 2nd left turn lane (300 feet of storage and a 250-foot bay taper);

ii. Construct a 2nd eastbound left turn on Dixie River Road (within the existing median) with 400 feet of storage (matching the existing left turn lane) and a 200-foot bay taper;

iii. Mark-out the existing outer eastbound left turn lane for an eventual through lane for the future Berwick Commons offsite development on the east side of Steele Creek Road; and

iv. Extend the existing southbound left turn lane storage on Steele Creek Road from 150 feet to 225 feet with a 150-foot bay taper.

Improve Steele Creek Road (NC 160) & Trojan Drive/Rigby Road (intersection # 4) as follows:

i. Extend the existing raised median on Steele Creek Road a minimum of 100 feet north of Trojan Drive/Rigby Road to eliminate the possibility of left entering vehicles or left exiting vehicles.

ii. The northbound left turn lane at Trojan Drive will be removed; and

iii. Construct a southbound right turn lane on Steele Creek Road with 100 feet of storage and a 150-foot bay taper.

Improve Steele Creek Road (NC 160) & I-485 Outer Ramps (intersection # 5) as follows:

i. Construct an eastbound right turn lane on the I-485 Outer Exit Ramp with a minimum of 1,150 feet of storage and a 200-foot bay taper (the existing eastbound combined through-right turn lane will remain as is).

Improve Steele Creek Road (NC 160) & Inner Ramps (intersection # 6) as follows:

i. Re-mark the existing westbound combined through-right turn exit lane to a combined through-left turn lane on the I-485 Inner Exit Ramp; and

ii. Construct a westbound right turn lane on the I-485 Inner Exit Ramp with a minimum of 275 feet of storage and a 200-foot bay taper.

Improve Steele Creek Road (NC 160) & Shopton Road (intersection # 7) as follows:

i. Re-mark the existing westbound through lane to a 2nd left turn lane on Shopton Road with 225 feet of storage and a 200-foot bay taper; and

ii. Extend the existing westbound right turn lane 150 feet of Shopton Road (to the existing retail driveway) and re-mark to a combined through-right turn lane; and

iii. With the introduction of this 2nd left turn lane the southwest corner of the intersection will need to be modified with new pavement (to safely maneuver larger vehicles without crossing into the adjacent travel lane) that terminates at the existing right turn lane on Steele Creek Road (CVS pharmacy - approximately 125 feet in length).

Contribute Funds for the future Signalization of the Intersection Dixie River Road & Shopton Road (intersection # 8) as follows:

Provide up to \$86,000.00 toward the cost of the future installation of a traffic signal at this intersection (after a traffic signal warrant analysis is performed by CDOT or NCDOT and the traffic warrants for the intersection are met). Petitioner's contribution will not exceed the amount stated above.

Improve Dixie River Road & Proposed Access A (intersection 10A) as follows:

i. Construct Proposed Access "A" with one northbound ingress lane and one southbound egress lane that terminates as a right turn lane. The existing median on Dixie River Road will eliminate the possibility of left entering vehicles or left exiting vehicles; and

ii. Re-marked as a combined through-right turn lane the existing westbound outturn through lane on Dixie River Road.

Improve Dixie River Road & Proposed Access B (intersection 10B) as follows:

i. Signalize the intersection, the cost of the proposed signal to be shared with the developer of Berwick Town Center;

ii. The existing eastbound left turn lane with 150 feet of storage on Dixie River Road is acceptable as is;

iii. Construct dual southbound left turn exit lanes on Proposed Access "B" with 150 feet of storage and a 100-foot bay taper; and

iv. Construct a southbound (future combined through-right turn) exit lane on Proposed Access "B";

v. Construct Proposed Access "B" with one northbound entering lane;

vi. The existing westbound right turn lane on Dixie River Road shall be extended as a second through lane (the outer lane will be a combined through-right turn lane at Proposed Access "B") where the new through lane constructed in the existing median terminates at the existing westbound left turn lane at Shopton Road (for all intersections on Dixie River Road);

vii. Construct a second eastbound through lane on Dixie River Road that commences at a point east of Shopton Road and terminates as the existing eastbound outturn left turn lane at Steele Creek Road (at all intersections on Dixie River Road).

Improve Dixie River Road & Proposed Access "C" (intersection 10C) as follows:

i. Petitioner to fund the full cost of the installation of a traffic signal at this intersection;

ii. The existing eastbound left turn lane with 260 feet of storage and the existing westbound left turn lane with 560 feet of storage on Dixie River Road are acceptable as is;

iii. Construct a westbound right turn lane on Dixie River Road with 100 feet of storage and a 100-foot bay taper; and

iv. Construct dual southbound left turn exit lanes on Proposed Access "C". The inner left turn lane will include 200 feet of storage and a 100-foot bay taper and the outer left turn lane will terminate from the inner through lane from the Trojan Drive/Proposed Access "C" intersection. The southeast corner of the intersection will be widened with a 100-foot taper in order to safely maneuver larger vehicles without crossing into the adjacent travel lane;

Improve Dixie River Road & Proposed Access "D" (intersection 10D) as follows:

i. Construct Proposed Access "D" with one northbound ingress lane and one southbound egress lane. The existing median on Dixie River Road will eliminate the possibility of left entering vehicles or left exiting vehicles; and

ii. The existing westbound through lane on Dixie River Road will be re-marked as a combined through-right turn lane;

Improve Dixie River Road & Proposed Access "E" (intersection 10E) as follows:

i. Construct a roundabout with the following layout: 14-foot wide ingress lanes (two on each of the two Dixie River legs and one on the Proposed Access "E" leg) and two 14-foot wide egress lanes (two on each of the two Dixie River legs and one on the Proposed Access "E" leg). The roundabout will generally consist of a minimum 65-foot wide diameter island, a 1-foot wide truck apron, two one-foot wide circulating lanes, and 5-foot wide bike lanes on both sides of Dixie River Road (160-foot wide inscribed diameter).

Improve Dixie River Road & Proposed Access "F" (intersection 10F) as follows:

i. Construct Proposed Access "F" with one northbound ingress lane and one southbound egress lane. The existing median on Dixie River Road will eliminate the possibility of left entering vehicles or left exiting vehicles; and

ii. The existing westbound through lane on Dixie River Road will be re-marked as a combined through-right turn lane.

Improve Trojan Drive & Proposed Access "G" (intersection 10G) as follows:

i. With one eastbound and westbound through lane on Trojan Drive with a two-way left-turn lane (TWTL) (three-lane section) commencing at Steele Creek Road and terminating at Shopton Road (the TWTL will terminate as a westbound left turn lane at Proposed Access "N").

ii. Create with pavement markings and eastbound and westbound left turn lane on Trojan Drive with 100 feet of storage each within the existing TWTL described above;

iii. Construct Proposed Access "G" with a northbound left turn egress lane with 100 feet of storage;

iv. Construct Proposed Access "G" with a northbound combined through-right egress lane (opposite Old Trojan Drive); and

v. Construct Proposed Access "G" with one southbound ingress lane.

Improve Trojan Drive & Proposed Access "H" (intersection 10H) as follows:

i. With one eastbound and westbound through lane on Trojan Drive with a two-way left-turn lane (TWTL) (three-lane section) commencing at Steele Creek Road and terminating at Shopton Road (the TWTL will terminate as a westbound left turn lane at Proposed Access "N").

ii. A eastbound right turn lane on Private Street with 100 feet of storage and a 100-foot bay taper;

iii. Construct Proposed Access "H" with 150 feet of stem length with one northbound entering lane and two northbound exit lanes (separate left and right lanes).

Improve Trojan Drive & Proposed Access "I" (intersection 10I) as follows:

i. With one eastbound and westbound through lane on Trojan Drive with a two-way left-turn lane (TWTL) (three-lane section) commencing at Steele Creek Road and terminating at Shopton Road (the TWTL will terminate as a westbound left turn lane at Proposed Access "N").

ii. A eastbound right turn lane on Private Street with 100 feet of storage and a 100-foot bay taper;

iii. Construct Proposed Access "I" with 150 feet of stem length with one southbound entering lane and two northbound exit lanes (separate left and right lanes).

Improve Trojan Drive & Proposed Access "J" (intersection 10J) as follows:

i. With one eastbound and westbound through lane on Trojan Drive with a two-way left-turn lane (TWTL) (three-lane section) commencing at Steele Creek Road and terminating at Shopton Road (the TWTL will terminate as a westbound left turn lane at Proposed Access "N").

ii. A westbound right turn lane on Trojan Drive with 100 feet of storage and a 100-foot bay taper;

iii. A eastbound right turn lane on Private Street with 100 feet of storage and a 100-foot bay taper;

iv. The south leg of Proposed Access "K" will include the inner northbound lane as a combined through-left turn lane and the outer northbound leg will be a combined through-right turn lane; and

v. Proposed Access "K" with 300 feet of stem length, with one northbound entering lane and two southbound exit lanes (the inner lane is a combined through-left turn lane and the outer lane is a combined through-right turn lane).

Improve Trojan Drive & Proposed Access "L" (intersection 10L) as follows:

i. With one eastbound and westbound through lane on Trojan Drive with a two-way left-turn lane (TWTL) (three-lane section) commencing at Steele Creek Road and terminating at Shopton Road (the TWTL will terminate as a westbound left turn lane at Proposed Access "N").

ii. A eastbound right turn lane on Private Street with 100 feet of storage and a 100-foot bay taper;

iii. A westbound right turn lane on Trojan Drive with 100 feet of storage and a 100-foot bay taper;

iv. The south leg of Proposed Access "K" will include the inner northbound lane as a combined through-left turn lane and the outer northbound leg will be a combined through-right turn lane; and

v. Proposed Access "K" with 300 feet of stem length, with one northbound entering lane and two southbound exit lanes (the inner lane is a combined through-left turn lane and the outer lane is a combined through-right turn lane).

Improve Private Street & Proposed Access "L" (intersection 10L) as follows:

i. With one eastbound and westbound through lane on Trojan Drive with a two-way left-turn lane (TWTL) (three-lane section) commencing at Steele Creek Road and terminating at Shopton Road (the TWTL will terminate as a westbound left turn lane at Proposed Access "N").

ii. A eastbound right turn lane on Private Street with 100 feet of storage and a 100-foot bay taper;

iii. Proposed Access "L" with 150 feet of stem length with one southbound entering lane and two northbound exit lanes (separate left and right lanes).

Improve Private Street & Proposed Access "M" (intersection 10M) as follows:

i. With one eastbound and westbound through lane on Private Street with a two-way left-turn lane (TWTL) (three-lane section) commencing at Steele Creek Road and terminating at Shopton Road (the TWTL will terminate as a westbound left turn lane at Proposed Access "N").

ii. A eastbound and westbound left turn lane with 100 feet of storage should be marked within the TWTL. Private Street will be under stop sign control at this intersection (Proposed Access "M" will be a free-flow movement);

iii. A westbound right turn lane on Private Street with 100 feet of storage and a 100-foot bay taper;

iv. The south leg of Proposed Access "M" will include a northbound left turn lane with 100 feet of storage and a combined through-right turn lane; and

v. Proposed Access "M" with a minimum of 175 feet of stem length, with one northbound entering lane and two southbound exit lanes (the inner lane terminates as a left turn lane and the outer lane is a combined through-right turn lane).

Improve Private Street & Proposed Access "N" (intersection 10N) as follows:

i. With one eastbound and westbound through lane on Private Street with a two-way left-turn lane (TWTL) (three-lane section) commencing at Steele Creek Road and terminating at Proposed Access "N" (mark for 100 feet of storage); and

ii. Proposed Access "N" with 150 feet of stem length with one southbound entering lane and two northbound exit lanes (separate left and right lanes).

Improve Shopton Road & Private Street (intersection 10O) as follows:

i. Access "O" with one eastbound ingress lane and one westbound egress lane that terminates as a right turn lane;

ii. Construct a raised median on Shopton Road a minimum of four-foot wide that extends 100 feet north and south of the Private Street to eliminate the possibility of left entering vehicles or left exiting vehicles; and

iii. A northbound right-turn lane on Shopton Road with 100 feet of storage and a 150-foot bay taper.

Improve Shopton Road & Proposed Access "P" (intersection 10P) as follows:

i. Petitioner to fund the full cost of the installation of a traffic signal at this intersection;

ii. A northbound right turn lane on Shopton Road with 100 feet of storage and a 150-foot bay taper;

iii. A southbound left turn exit lane on Shopton Road with 150 feet of storage, 100-foot bay taper, and a 500-foot through lane taper;

iv. An eastbound entering lane on Proposed Access "P"; and

v. Separate westbound left (150 feet of storage) and right (termination) exit lanes on Proposed Access "P".

Improve Steele Creek Road & Proposed Access "R" (intersection 10R) as follows:

i. Construct proposed access "R" with one westbound ingress lane (right-in only);

ii. Standards, Phasing and Other Provisions.

a. CDOT and NCDOT. All of the foregoing public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within the project road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad southwest Mecklenburg area, by way of a private-public partnership effort or other public sector project support.]

Phasing and Substantial Completion:

Development Areas E, F, G and H. The Petitioner will be allowed to obtain a certificate of occupancy for any one or all of the buildings located within Development Areas E, F, G and H upon the substantial completion of the following improvements:

Improve Steele Creek Road (NC 160) & Dixie River Road (intersection # 3) as follows:

i. Re-mark the existing painted out northbound left turn lane on Steele Creek Road to a 2nd left turn lane with 300 feet of storage and 250-foot bay taper.

Improve Steele Creek Road (NC 160) & Trojan Drive/Rigby Road (intersection # 4) as follows:

i. Extend the existing raised median on Steele Creek Road a minimum of 100 feet north of Trojan Drive/Rigby Road to eliminate the possibility of left entering vehicles or left exiting vehicles (the northbound left turn lane at Trojan Drive will be removed); and

ii. Construct a southbound right turn lane on Steele Creek Road with 100 feet of storage and a 150-foot bay taper.

Improve Dixie River Road & Proposed Access "A" (intersection 10A) as follows:

i. Construct Proposed Access "A" with one northbound ingress lane and one southbound egress lane that terminates as a right turn lane. The existing median on Dixie River Road will eliminate the possibility of left entering vehicles or left exiting vehicles; and

ii. The existing westbound outturn through lane on Dixie River Road shall be re-marked as a combined through-right turn lane.

Improve Dixie River Road & Proposed Access "B" (intersection 10B) as follows:

i. [Signalize the intersection, the cost of the proposed signal to be shared with the developer of Berwick Town Center];

ii. The existing eastbound left turn lane with 150 feet of storage on Dixie River Road is acceptable as is;

iii. Construct dual southbound left turn exit lanes on Proposed Access "B" with 150 feet of storage and a 100-foot bay taper;

iv. Construct a second receiving lane on eastbound Dixie River Road from Proposed Access "B" within the existing median to be at the start of the storage of the existing eastbound inside left turn on Dixie River Road;

v. Construct a southbound (future combined through-right) right turn exit lane on Proposed Access "B";

vi. Construct one northbound entering lane on Proposed Access "B"; and

vii. The existing lane that terminates as a westbound right turn lane on Dixie River Road is acceptable as is.

Improve Dixie River Road & Proposed Access "C" (intersection 10C) as follows:

i. With one eastbound and westbound through lane on Trojan Drive with a two-way left turn lane (TWTL) (three lane section) commencing at Steele Creek Road and terminating at Shopton Road (the TWTL will be extended to Shopton Road as part of phase two of the development);

ii. Create with pavement markings and eastbound and westbound left turn lane on Trojan Drive with 100 feet of storage each within the existing TWTL described above;

iii. Construct Proposed Access "C" with a northbound left turn egress lane with 100 feet of storage;

iv. Construct Proposed Access "C" with a northbound combined through-right egress lane (opposite Old Trojan Drive); and

v. Construct Proposed Access "C" with one southbound ingress lane.

Improve Steele Creek Road & Proposed Access "D" (intersection 10D) as follows:

i. Construct Proposed Access "D" with one westbound ingress lane (right-in only);

ii. Standards, Phasing and Other Provisions.

Improve Private Street & Proposed Access "E" (intersection 10E) as follows:

i. With one eastbound and westbound through lane on Trojan Drive with a two-way left turn lane (TWTL) (three lane section) commencing at Steele Creek Road and terminating at Shopton Road (the TWTL will be extended to Shopton Road as part of phase two of the development);

ii. Create with pavement markings and eastbound and westbound left turn lane on Trojan Drive with 100 feet of storage each within the existing TWTL described above;

iii. Construct Proposed Access "E" with a northbound left turn egress lane with 100 feet of storage;

iv. Construct Proposed Access "E" with a northbound combined through-right egress lane (opposite Old Trojan Drive); and

v. Construct Proposed Access "E" with one southbound ingress lane.

Improve Steele Creek Road & Proposed Access "F" (intersection 10F) as follows:

<

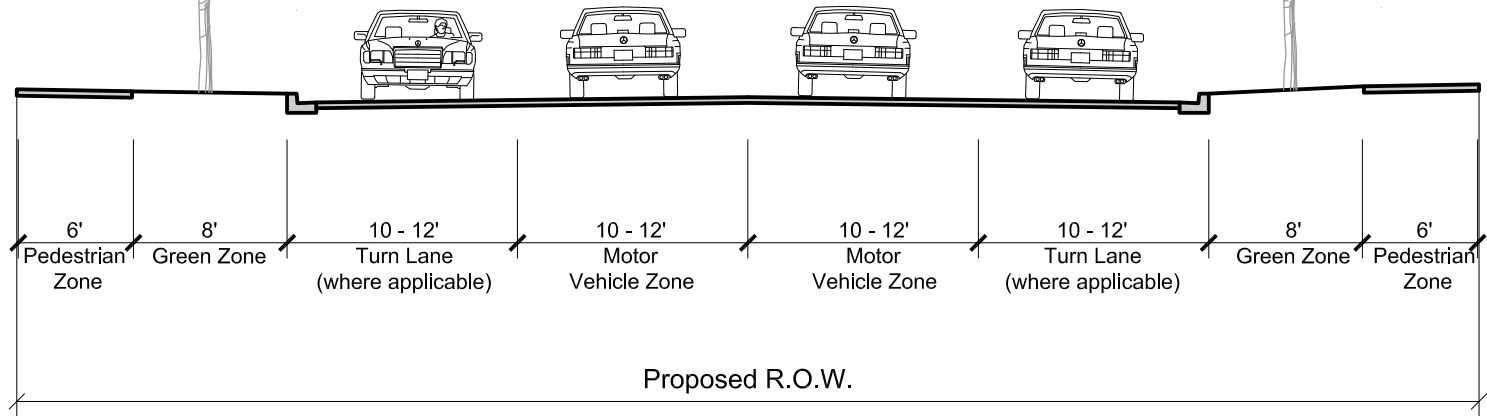


SITE LEGEND

- PEDESTRIAN ACCESSPLAZAS
- PEDESTRIAN ACCESS
- PEDESTRIAN CONNECTIONS

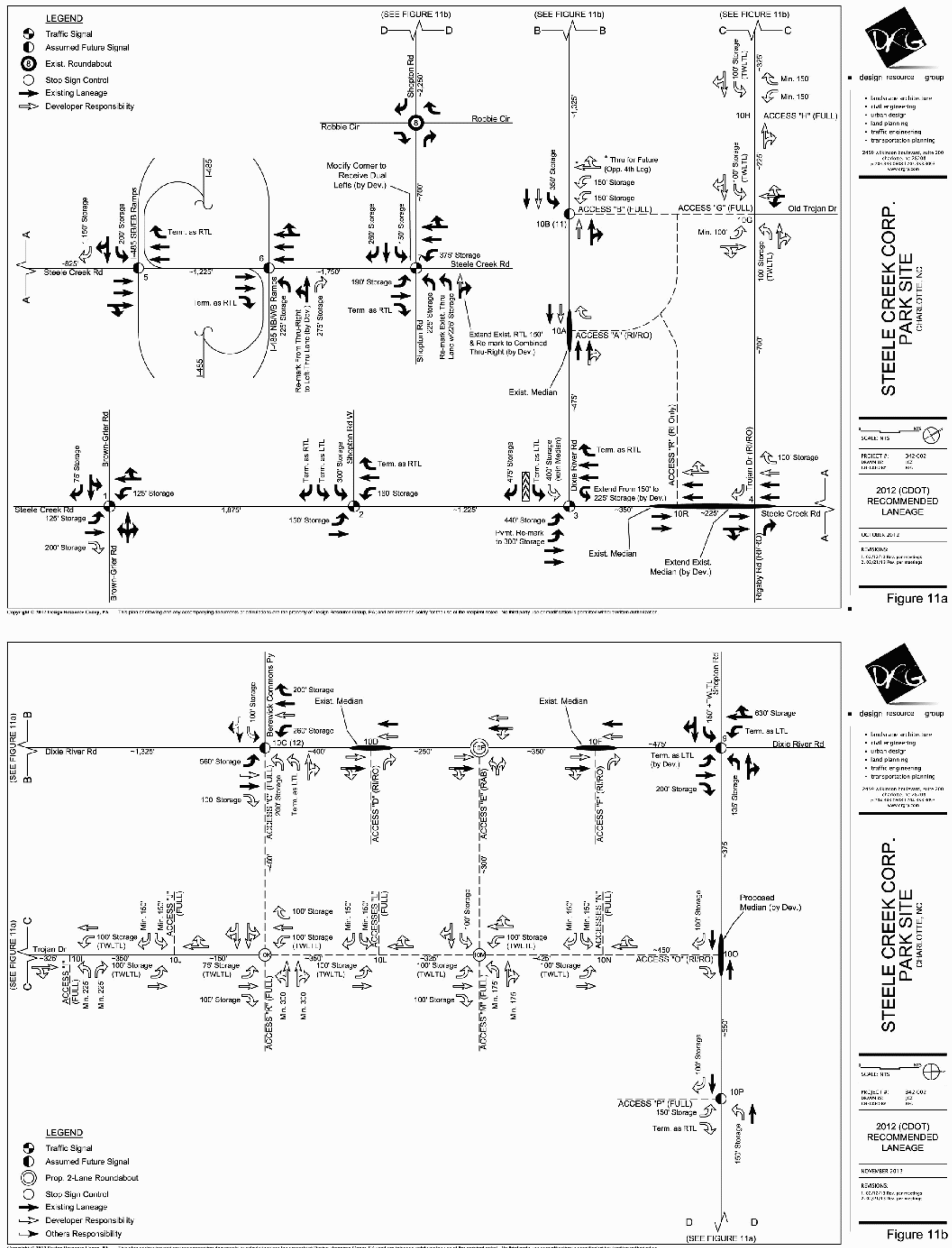
SECTION SCALE 1" = 10'

Section A Avenue



Note: Optional medians allowed at petitioner's discretion

TRAFFIC FIGURES - LANEAGE

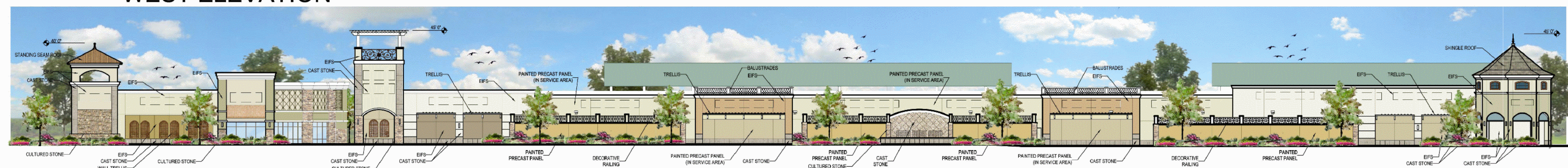


STEELE CREEK RETAIL CENTER REZONING PETITION No. 2013-001 STEELE CREEK (1997) Limited Partnership PEDESTRIAN CONNECTIVITY

DATE: DECEMBER 3, 2012
DESIGNED BY: KST
DRAWN BY: KST
CHECKED BY: MFC
SCALE: 1"=200'
PROJECT #: 1012048

SHEET #:
RZ-4

WEST ELEVATION



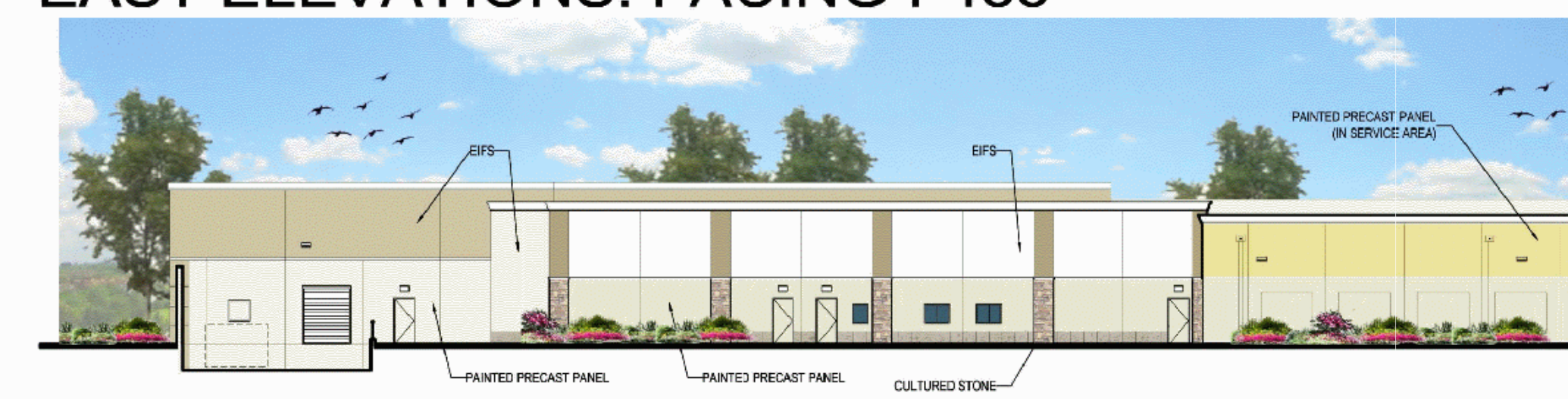
BUILDING 03

SOUTH ELEVATION



BUILDING 01

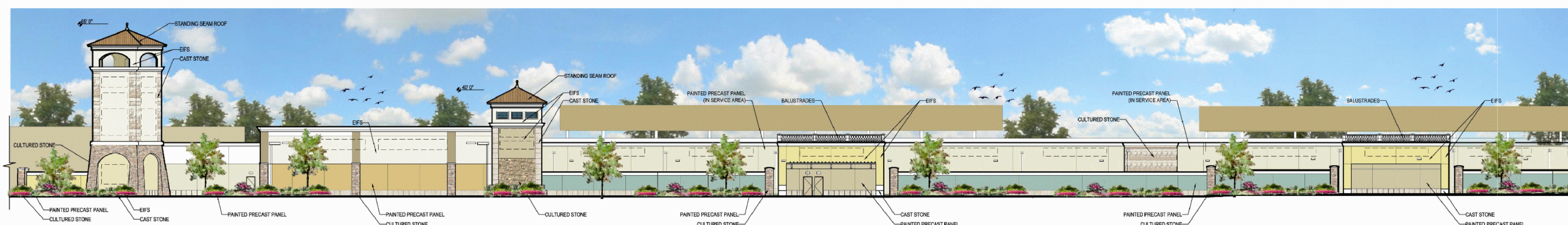
EAST ELEVATIONS: FACING I-485



BUILDING 01

BUILDING 07

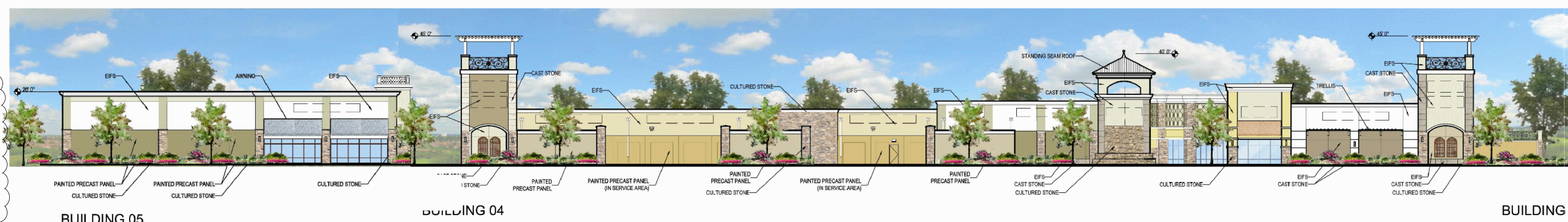
EAST ELEVATIONS: FACING I-485



BUILDING 06

BUILDING 05

NORTH ELEVATIONS



BUILDING 05

BUILDING 04

BUILDING 03

NOTE:

ARCHITECTURE WILL INCLUDE A MIXTURE OF VARIED PARAPET HEIGHTS, SHINGLE AND STANDING SEAM ROOFING, BALUSTRADE, DECORATIVE RELIEFS, TRELLIS AND A VARIETY OF WINDOW, DOOR AND ADVERTISING OPENINGS/ PANELS, MODULATED BUILDING FRONTAGES AND AWNINGS TO CREATE VISUAL INTEREST AS ILLUSTRATED ON THIS SHEET.

DATE: DECEMBER 3, 2012

DESIGNED BY: KST

DRAWN BY: KST

CHECKED BY: MRC

Q.C. BY: MRC

SCALE: N/A

PROJECT #: 1012048

REVISIONS:

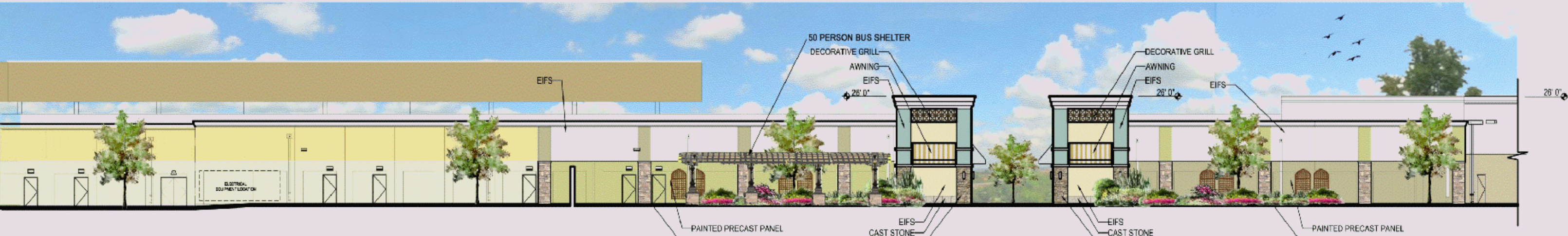
(1)	12.21.12	REVISE PER STAFF COMMENTS
(2)	03.06.13	REVISE PER STAFF COMMENTS
(3)	03.15.13	REVISE PER STAFF COMMENTS

SHEET #:
RZ-5a

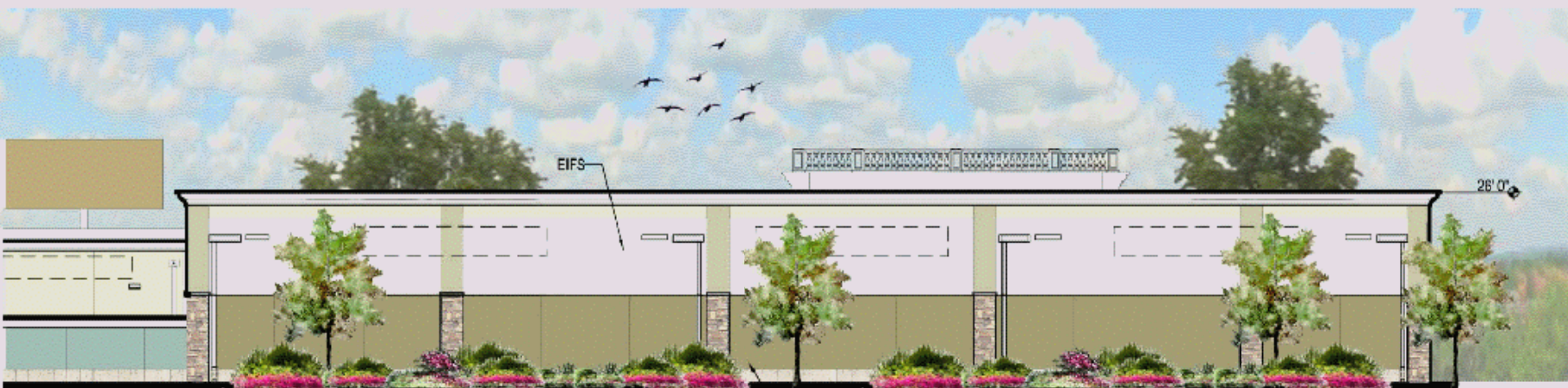
STEEL CREEK RETAIL CENTER
REZONING PETITION No. 2013-001
STEEL CREEK (1997) Limited Partnership
CONCEPTUAL ARCHITECTURAL ELEVATIONS



BUILDING 02 "FRONT DOOR"



BUILDING 07



BUILDING 05



BUILDING 06

NOTE:

ARCHITECTURE WILL INCLUDE A MIXTURE OF VARIED PARAPET HEIGHTS, SHINGLE AND STANDING SEAM ROOFING, BALUSTRADE, DECORATIVE RELIEFS, TRELLIS AND A VARIETY OF WINDOW, DOOR AND ADVERTISING OPENINGS/ PANELS, MODULATED BUILDING FRONTAGES AND AWNINGS TO CREATE VISUAL INTEREST AS ILLUSTRATED ON THIS SHEET.



THESE ELEVATIONS ARE PROVIDED TO REFLECT THE ARCHITECTURAL STYLE AND QUALITY OF THE BUILDINGS THAT MAY BE CONSTRUCTED ON THE SITE (THE ACTUAL BUILDINGS CONSTRUCTED ON THE SITE MAY VARY FROM THESE ILLUSTRATIONS AS LONG AS THE GENERAL ARCHITECTURAL CONCEPTS AND INTENT ILLUSTRATED IS MAINTAINED).

