

NEW BERN STATION AREA PLAN

Plan Amendments
Draft October 2017

EXISTING

Based on the City's *Urban Street Design Guidelines*, future cross-sections have been determined for streets, as well as the rail frontage, within the study area. Map 5 shows the location for each type. The following street types are recommended for the plan area:

- **Avenue:** 4-lane divided, & 2-lane divided
- **Main Street**
- **Office/Commercial Street-** wide
- **Local Residential Street-** wide
- **Rail Frontage/Multi-Use Trail**

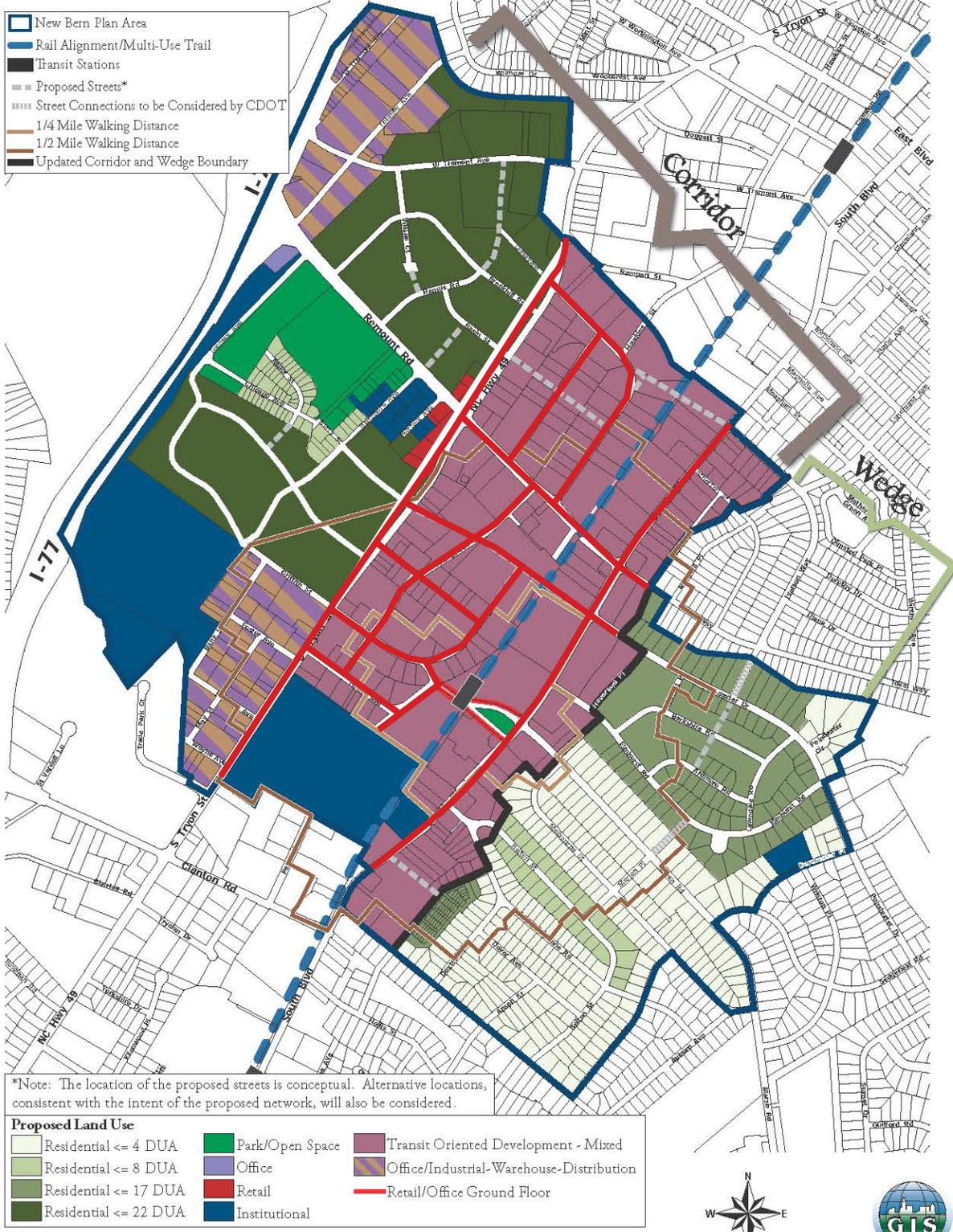
PROPOSED

Based on the City's *Urban Street Design Guidelines*, future cross-sections have been determined for streets, as well as the rail frontage, within the study area. Map 5 shows the location for each type. The following street types are recommended for the plan area:

- **Avenue:** 4-lane divided, & 2-lane divided
- **Main Street**
- **Signature Street**
- **Mixed Use Street (Office/Commercial Street)-** wide
- **Local Residential Street -** wide
- **Rail Frontage/Multi-Use Trail**

New Bern Transit Station Area Plan

Map 3: Recommended Future Land Use



Produced by the Charlotte-Mecklenburg Planning Department.

Date: February 06, 2008

EXISTING

Street Cross-Sections

Based on the City's *Urban Street Design Guidelines*, the future cross-sections have been determined for streets, as well as the rail frontage, within the New Bern study area, with the exception of the Sedgefield neighborhood, where little change to existing streets is expected. The following street types are recommended for the plan area:

- **Avenue:** Four lane divided, and two-lane divided
- **Main Street**
- **Office/Commercial Street - Wide**
- **Local Residential Street - Wide**
- **Rail Frontage/Multi-Use Trail**

Map 5 shows the desired location for each of these street types. Consult this map to identify the recommendation for a specific street, then refer to the matching cross-section on the following pages. Streets within neighborhood areas slated for preservation are intended for preservation as well, so new cross sections for these areas are not provided.

PROPOSED

Street Cross-Sections

Based on the City's *Urban Street Design Guidelines*, the future cross-sections have been determined for streets, as well as the rail frontage, within the New Bern study area, with the exception of the Sedgefield neighborhood, where little change to existing streets is expected. The following street types are recommended for the plan area:

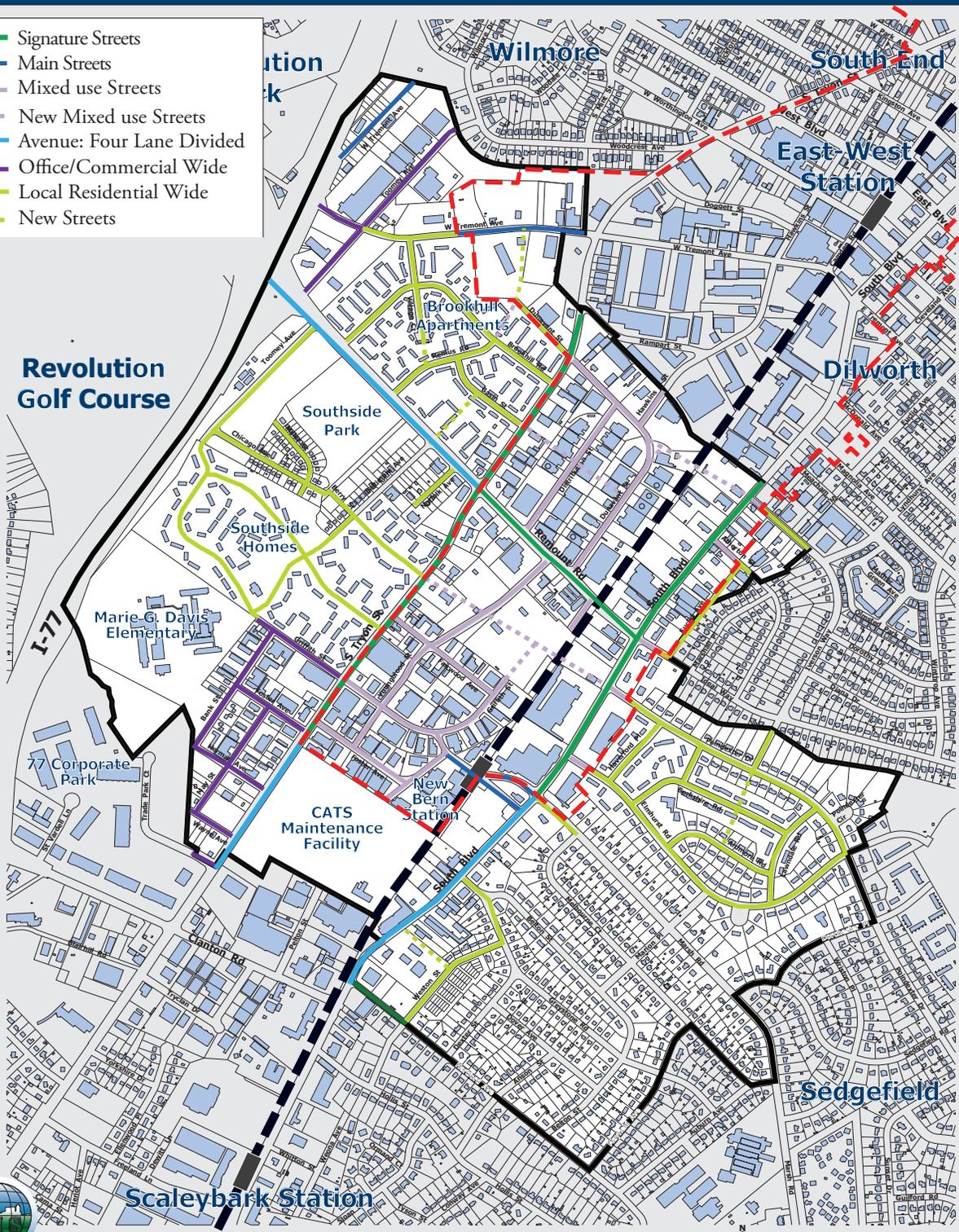
- **Avenue:** Four lane divided, and two-lane divided
- **Main Street**
- **Signature Street**
- **Mixed Use Streets (Office/Commercial Street - Wide)**
- **Local Residential Street - Wide**
- **Rail Frontage/Multi-Use Trail**

Map 5 shows the desired location for each of these street types. Consult this map to identify the recommendation for a specific street, then refer to the matching cross-section on the following pages. Streets within neighborhood areas slated for preservation are intended for preservation as well, so new cross sections for these areas are not provided.

New Bern Transit Station Area Plan

Map 5: Streetscape Cross Section Key

- Signature Streets
- Main Streets
- Mixed use Streets
- - - New Mixed use Streets
- Avenue: Four Lane Divided
- Office/Commercial Wide
- Local Residential Wide
- - - New Streets



Produced by the Charlotte-Mecklenburg Planning Department.



Date: Oct. 2017

(Avenue - Four Lane Divided, continued)

Land Use: The land use will vary; within the plan area the desired uses will be typically medium to high density mixed use and residential.

Situation: The major thoroughfares in the study area are classified as Avenues. The four lane divided type is recommended for South Boulevard, South Tryon Street, and the segment of Remount Road extending from South Tryon Street to I-77. The proposed cross-section will allow these streets to continue to perform an important mobility function for motorists, as well as to support safe and comfortable pedestrian and bicycle travel.

Existing Condition: These streets typically have two lanes in each direction; some are widened for left turn lanes near intersections. Right of way is typically 60 feet. South Tryon Street is 80 feet south of Foster Avenue; Remount Road has wider variable right of way.

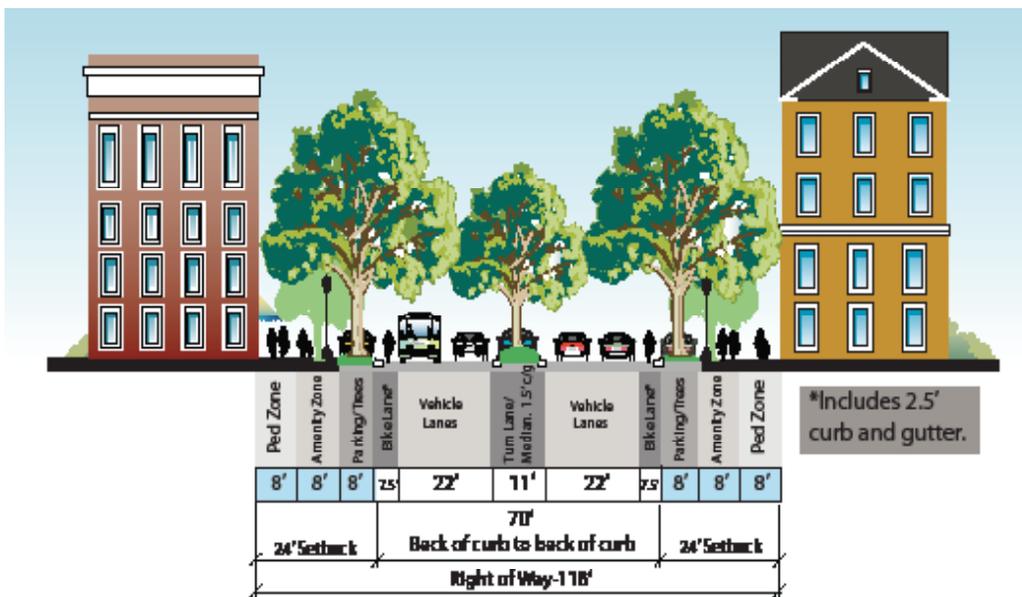
Proposed Curb to Curb: Recommended width is 70 feet from back of curb to back of curb; right of way is 118 feet.

- Two travel lanes and bike lane in each direction.
- Continuous center lane for left turns and pedestrian refuge; to include mid-block landscaped pedestrian refuge islands in some locations.

Behind the Curb: Minimum building setback is 24 feet from back of (unrecessed) recommended curb. Tree planting is required with spacing, irrigation,

subdrainage, and adequate soil space for roots per the Charlotte Tree Ordinance.

- ~~Planting strip or recessed on-street parallel parking serve as buffer from traffic to pedestrians on sidewalk. The recessed parking is required in all locations feasible under CDOT standards, with intermittent planter islands to break up parking into bays no more than 100 feet in length. Planting strip with trees is required in all other situations. South Tryon Street is a state highway, and current state regulations do not permit on-street parking. If circumstances change in the future such that on-street parking would be allowable, on-street parking would be required for further development projects per zoning standards.~~
- Amenity zone provides supplemental tree planting location. Trees in the amenity zone should be planted in curbed planters. The amenity zone also provides additional paved area for street furniture, paved access to on-street parking, and merchandising purposes.
- Ped zone is the usual location for the clear sidewalk. Where there is no on-street parking and planting strips are in place, the clear sidewalk can be pushed into the amenity zone location and the ped zone can be used for landscaping, sidewalk dining, or paved merchandising purposes. Encroachments into the ped zone for features such as steps and open porches are allowed in accordance with the zoning ordinance, but encroachments at grade may not reduce the clear sidewalk to less than 8 feet.



New Bern Transit Station Area Plan

**PAGE 19
PROPOSED**

(Avenue - Four Lane Divided, continued)

Land Use: The land use will vary; within the plan area the desired uses will be typically medium to high density mixed use and residential.

Situation: The major thoroughfares in the study area are classified as Avenues. The four lane divided type is recommended for South Boulevard, South Tryon Street, and the segment of Remount Road extending from South Tryon Street to I-77. The proposed cross-section will allow these streets to continue to perform an important mobility function for motorists, as well as to support safe and comfortable pedestrian and bicycle travel.

Existing Condition: These streets typically have two lanes in each direction; some are widened for left turn lanes near intersections. Right of way is typically 60 feet. South Tryon Street is 80 feet south of Foster Avenue; Remount Road has wider variable right of way.

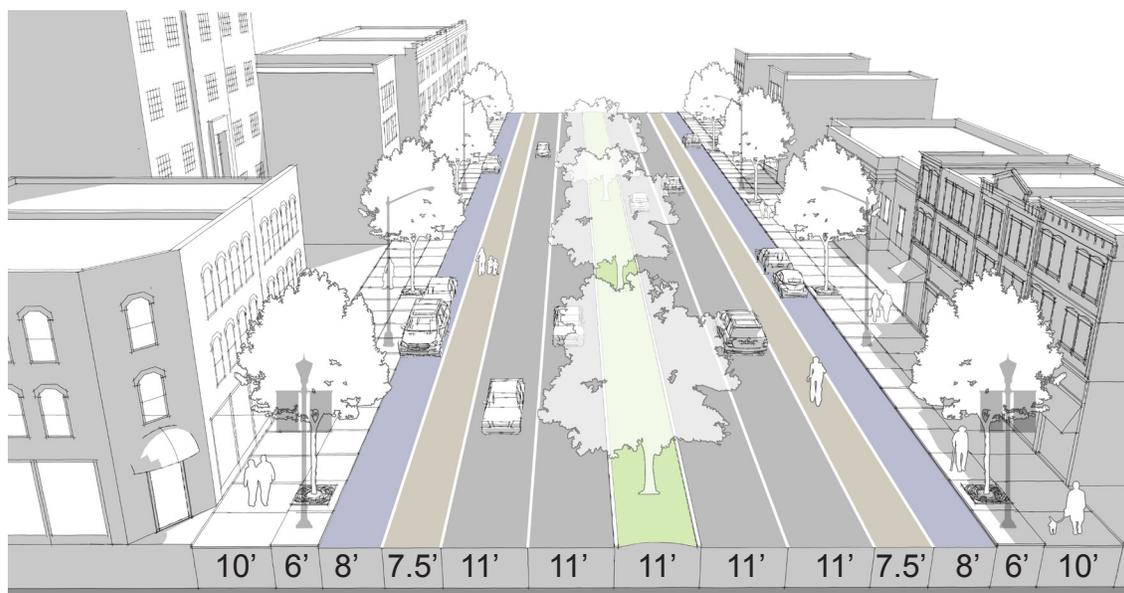
Proposed Curb to Curb: Recommended width is 70 feet from back of curb to back of curb; right of way is 118 feet.

- Two travel lanes and bike lane in each direction.
- Continuous center lane for left turns and pedestrian refuge; to include mid-block landscaped pedestrian refuge islands in some locations.

Behind the Curb: Minimum building setback is 24 feet from back of (unrecessed) recommended curb. Tree planting is required with spacing, irrigation,

subdrainage, and adequate soil space for roots per the Charlotte Tree Ordinance.

- Recessed on-street parallel parking serve as buffer from traffic to pedestrians on sidewalk. The recessed parking is required in all locations feasible under CDOT standards, with intermittent planter islands to break up parking into bays no more than 100 feet in length. **An amenity zone with curbed planters or tree grates is required between the on-street parking and sidewalk.** If circumstances change in the future such that on-street parking would be allowable, on-street parking would be required for further development projects per zoning standards.
- Amenity zone provides supplemental tree planting location. Trees in the amenity zone should be planted in curbed planters **or tree grates (tree grates are required with ground floor commercial uses).** The amenity zone also provides additional paved area for street furniture, paved access to on-street parking, and merchandising purposes.
- Ped zone is the usual location for the clear sidewalk. Where there is no on-street parking and planting strips are in place, the clear sidewalk can be pushed into the amenity zone location and the ped zone can be used for landscaping, sidewalk dining, or paved merchandising purposes. Encroachments into the ped zone for features such as steps and open porches are allowed in accordance with the zoning ordinance, but encroachments at grade may not reduce the clear sidewalk to less than 8 feet.



New Bern Transit Station Area Plan

Avenue - Two Lane Divided

Description: The Avenue is the most common (non-local) street providing access from neighborhoods to commercial areas, between areas of the city, and, in some cases, through neighborhoods. It is designed to provide a balance of service for all modes of transportation, including accessibility for transit, pedestrians, and bicyclists in addition to carrying significant automobile traffic.

Land Use: The land use will vary; within the plan area the desired uses will be typically medium to high density mixed use and residential.

Situation: This Avenue cross section is narrower than the four-lane divided version used on other streets in the study area. It is recommended for the portion of Remount Road between South Boulevard and South Tryon Street. The proposed cross-section will allow this street type to continue to perform an important mobility function for motorists, as well as to support safe and comfortable pedestrian and bicycle travel.

Existing Condition: This street has two lanes in each direction, with left turn lanes near intersections, and some landscaped median. Right of way is variable, generally 80 feet or wider.

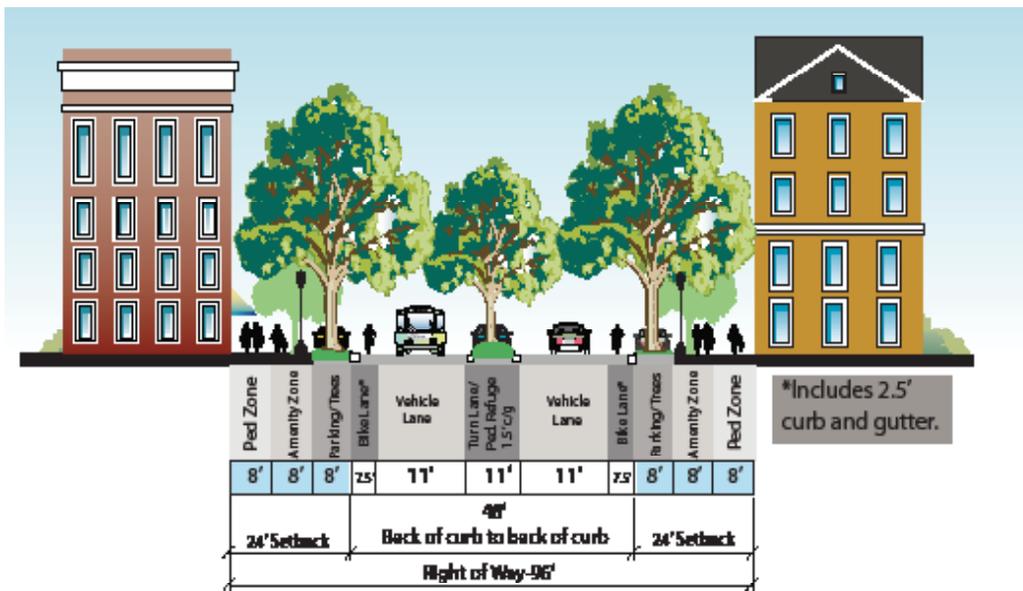
Proposed Curb to Curb: Recommended width is 48 feet from back of curb to back of curb; right of way is 96 feet.

- One travel lane and bike lane in each direction.

- Continuous center lane for left turns and pedestrian refuge; to include mid-block landscaped pedestrian refuge islands in some locations.

Proposed Behind the Curb: Minimum building setback is 24 feet from back of (unrecessed) recommended curb. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots per the Charlotte Tree Ordinance.

- ~~Planting strip or~~ recessed on-street parallel parking serve as buffer from traffic to pedestrians on sidewalk. The recessed parking is required in all locations feasible under CDOT standards, with intermittent planter islands to break up parking into bays no more than 100 feet in length. ~~Planting strip with trees is required in all other situations.~~
- Amenity zone provides supplemental tree planting location. Trees in the amenity zone should be planted in ~~curbed planters~~. The amenity zone also provides additional paved area for street furniture, paved access to on-street parking, and merchandising purposes.
- Ped Zone is the usual location for the clear sidewalk. Where there is no on-street parking and ~~planting strips are in place~~, the clear sidewalk can be pushed into the amenity zone location, and the ped zone can be used for landscaping, sidewalk dining, or paved merchandising purposes. Encroachments into the ped zone for features such as steps and open porches are allowed in accordance with the zoning ordinance, but encroachments at grade may not reduce the clear sidewalk to less than 8 feet.



Signature Street | Remount Rd. |
Avenue - Two Lane Divided

Description: The Avenue is the most common (non-local) street providing access from neighborhoods to commercial areas, between areas of the city, and, in some cases, through neighborhoods. It is designed to provide a balance of service for all modes of transportation, including accessibility for transit, pedestrians, and bicyclists in addition to carrying significant automobile traffic.

Land Use: The land use will vary; within the plan area the desired uses will be typically medium to high density mixed use and residential.

Situation: This Avenue cross section is narrower than the four-lane divided version used on other streets in the study area. It is recommended for the portion of Remount Road between South Boulevard and South Tryon Street. The proposed cross-section will allow this street type to continue to perform an important mobility function for motorists, as well as to support safe and comfortable pedestrian and bicycle travel.

Existing Condition: This street has two lanes in each direction, with left turn lanes near intersections, and some landscaped median. Right of way is variable, generally 80 feet or wider.

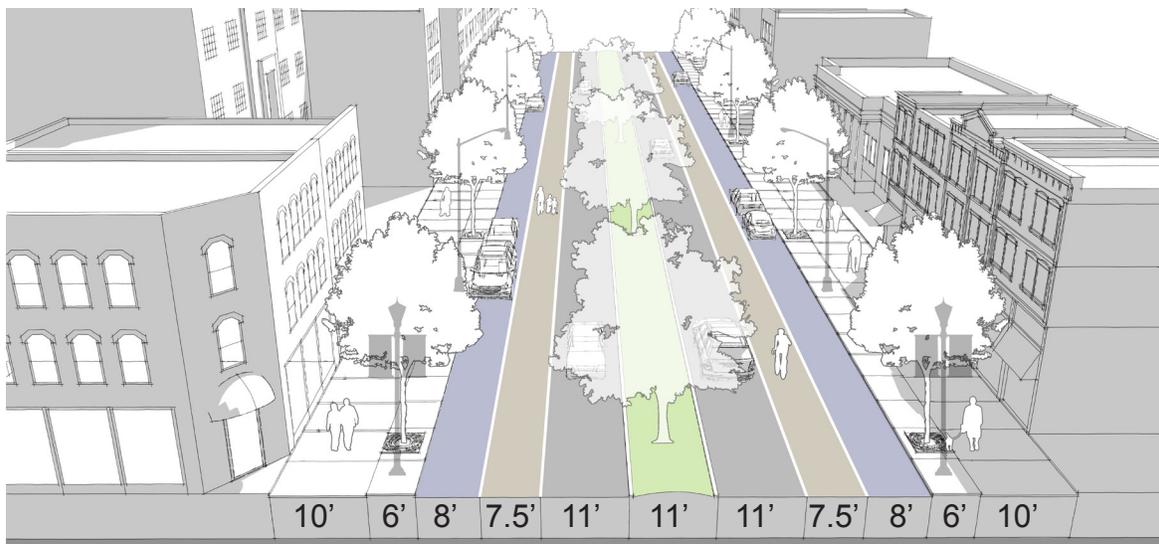
Proposed Curb to Curb: Recommended width is 48 feet from back of curb to back of curb; right of way is 96 feet.

- One travel lane and bike lane in each direction.

- Continuous center lane for left turns and pedestrian refuge; to include mid-block landscaped pedestrian refuge islands in some locations.

Proposed Behind the Curb: Minimum building setback is 24 feet from back of (unrecessed) recommended curb. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots per the Charlotte Tree Ordinance.

- Recessed on-street parallel parking serve as buffer from traffic to pedestrians on sidewalk. The recessed parking is required in all locations feasible under CDOT standards, with intermittent planter islands to break up parking into bays no more than 100 feet in length. **Tree grates per CLDSM** are required in all other scenarios.
- Amenity zone provides supplemental tree planting location. Trees in the amenity zone should be planted in **tree grates per Charlotte Land Development Standards Manual (CLDSM)** The amenity zone also provides additional paved area for street furniture, paved access to on-street parking, and merchandising purposes.
- Ped zone is the usual location for the clear sidewalk. Where there is no on-street parking, the clear sidewalk can be pushed into the amenity zone location, and the ped zone can be used for landscaping, sidewalk dining, or paved merchandising purposes. Encroachments into the ped zone for features such as steps and open porches are allowed in accordance with the zoning ordinance, but encroachments at grade may not reduce the clear sidewalk to less than 8 feet.



Main Street

Description: Main Street is a “destination” street that provides access to and functions as a center of civic, social, and commercial activity. Development along main streets is dense and focused toward the pedestrian realm.

Land Use: In the plan area, the desired land use is typically medium to high density mixed use with ground level retail.

Situation: Main streets are recommended in the heart of the station area, at New Bern Street and Rail Crossing Lane between South Boulevard and Griffith Street. Here, pedestrians receive the highest priority of all the transport modes. To accommodate pedestrian traffic, a wide pedestrian zone and an amenity zone are recommended.

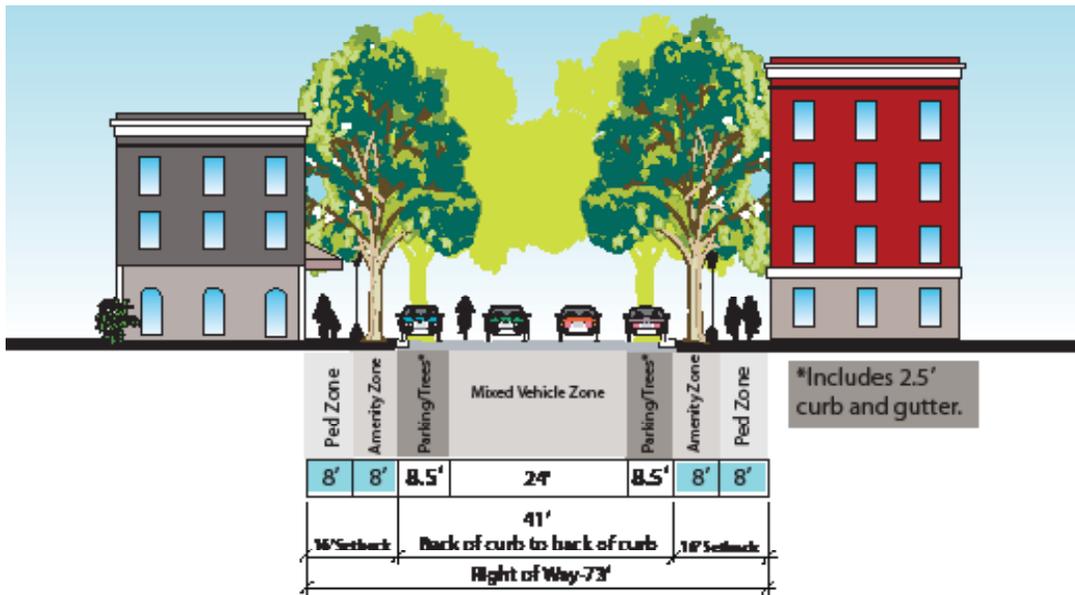
Existing Condition: These streets have one lane in each direction with variation in turn lane and parking arrangement. Right of way varies from 50 feet upward.

Proposed Curb to Curb: Recommended width is 41 feet from back of curb to back of curb; right of way is 73 feet.

- One travel lane in each direction shared with bicyclists.
- Widening for left turn lane on New Bern Street intersection with South Boulevard, consistent with existing condition.

Proposed Behind the Curb: Minimum building setback is 16 feet from back of recommended recessed curb. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots per the Charlotte Tree Ordinance.

- ~~Planting strip or recessed on-street parallel parking serve as buffer from traffic to pedestrians on sidewalk. The recessed parking is required in all locations feasible under CDOT standards, with intermittent planter islands to break up parking into bays no more than 100 feet in length.~~
- Amenity zone provides supplemental tree planting location. Trees in the amenity zone should be planted in ~~curbed planters where planting strip is not in place.~~ The amenity zone also provides additional paved area for street furniture, paved access to on-street parking, and merchandising purposes.
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Main Street

Description: Main Street is a “destination” street that provides access to and functions as a center of civic, social, and commercial activity. Development along main streets is dense and focused toward the pedestrian realm.

Land Use: In the plan area, the desired land use is typically medium to high density mixed use with ground level retail.

Situation: Main streets are recommended in the heart of the station area, at New Bern Street and Rail Crossing Lane between South Boulevard and Griffith Street, as well as the western portion of Tremont Avenue. Here, pedestrians receive the highest priority of all the transport modes. To accommodate pedestrian traffic, a wide pedestrian zone and an amenity zone are recommended.

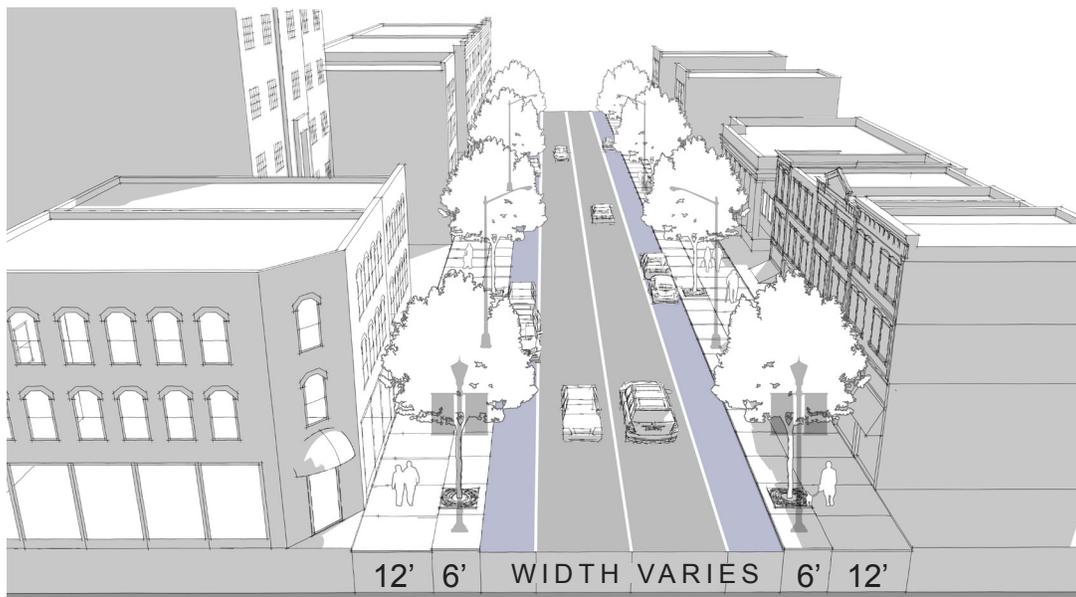
Existing Condition: These streets have one lane in each direction with variation in turn lane and parking arrangement. Right of way varies from 50 feet upward.

Proposed Curb to Curb: Recommended width is 41 feet from back of curb to back of curb; right of way is 73 feet.

- One travel lane in each direction shared with bicyclists.
- Widening for left turn lane on New Bern Street intersection with South Boulevard, consistent with existing condition.

Proposed Behind the Curb: Minimum building setback is 18 feet from back of recommended recessed curb. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots per the Charlotte Tree Ordinance.

- On-street parallel parking serves as buffer from traffic to pedestrians on sidewalk. The recessed parking is required in all locations feasible under CDOT standards, with intermittent planter islands to break up parking into bays no more than 100 feet in length.
- Amenity zone provides supplemental tree planting location. Trees in the amenity zone should be planted in tree grates per CLDSM. The amenity zone also provides additional paved area for street furniture, paved access to on-street parking, and merchandising purposes.
- Ped zone is the usual location for the clear sidewalk. Where there is no on-street parking, the clear sidewalk can be pushed into the amenity zone, and the ped zone can be used for landscaping, sidewalk dining, or paved merchandising purposes. Encroachments into the ped zone for features such as steps and open porches are allowed in accordance with the zoning ordinance, but encroachments at grade may not reduce the clear sidewalk to less than 8 feet.



Office / Commercial Street - Wide

Description: Local streets provide access to residential, industrial, commercial, or mixed-use development. The majority of Charlotte’s streets are classified as local streets and are typically built through the land development process.

Land Use: In the study area, the land use along these streets is typically office, warehouse/industrial, and retail development with an auto orientation. It may include medium density mixed use development.

Situation: The office/commercial streets are recommended in the portion of the General Corridor Area that is intended for non-residential uses. Office/commercial streets are appropriate because they provide basic pedestrian amenities while allowing for reasonable vehicular access and speeds.

Existing Condition: These streets have one lane in each direction; some have on-street parking. Right of way is typically 50 feet.

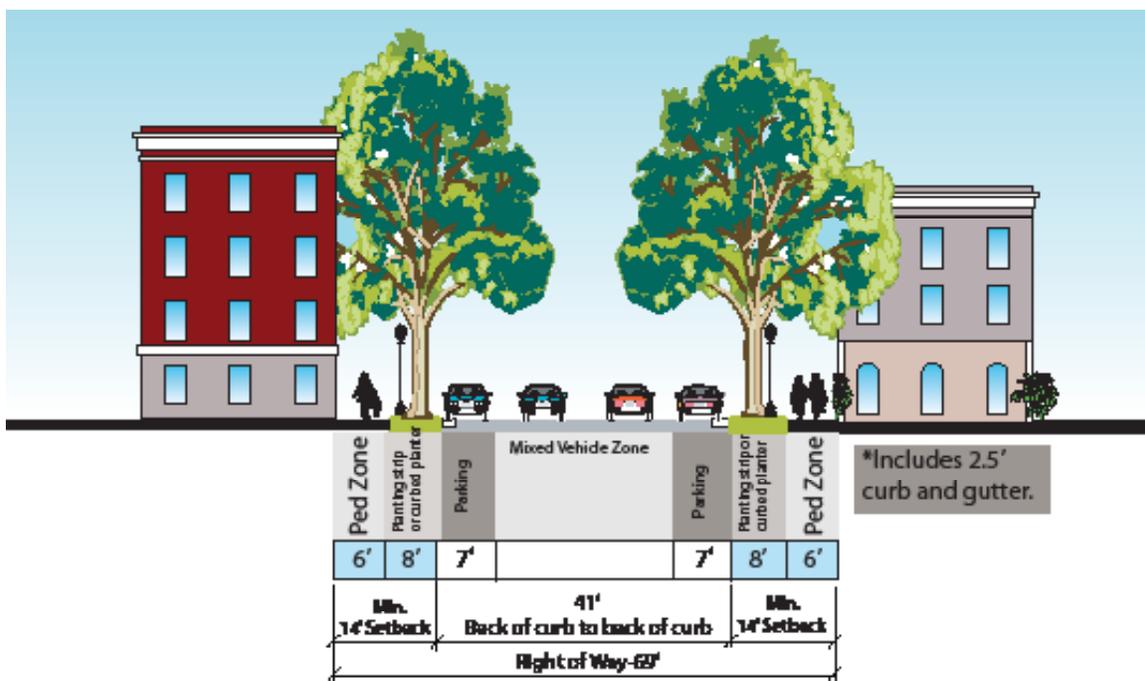
Proposed Curb to Curb: Recommended width is 41 feet from back of curb to back of curb; right of way is 69 feet.

- One travel lane in each direction shared with bicyclists.

- On-street parking on both sides. Curb extensions may be used to narrow street width at intersections and other locations where on-street parking is not appropriate.
- Widening for left turn lanes onto South Tryon Street may be required in accordance with CDOT standards.

Proposed Behind the Curb: Minimum building setback is 14 feet from back of (recessed) recommended curb, or 21 feet from back of any extended curb. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots per the Charlotte Tree Ordinance.

- Planting strip provides buffer from traffic to pedestrians on the sidewalk, and a landscaping opportunity. In locations with retail frontage or other high density applications, tree planting in curbed planters, with paved amenity zone for street furniture, paved access to on-street parking, and merchandising purposes should be substituted for the planting strip.
- Ped Zone is the usual location for the clear sidewalk. Encroachments into the ped zone for features such as steps and open porches are allowed in accordance with the zoning ordinance, but encroachments at grade may not reduce the clear sidewalk to less than 6 feet.



New Bern Transit Station Area Plan

Office / Commercial Street - Wide

Description: Local streets provide access to residential, industrial, commercial, or mixed-use development. The majority of Charlotte's streets are classified as local streets and are typically built through the land development process.

Land Use: In the study area, the land use along these streets is typically office, warehouse/industrial, and retail development with an auto orientation. It may include medium density mixed use development.

Situation: The office/commercial streets are recommended in the portion of the General Corridor Area that is intended for non-residential uses. Office/commercial streets are appropriate because they provide basic pedestrian amenities while allowing for reasonable vehicular access and speeds.

Existing Condition: These streets have one lane in each direction; some have on-street parking. Right of way is typically 50 feet.

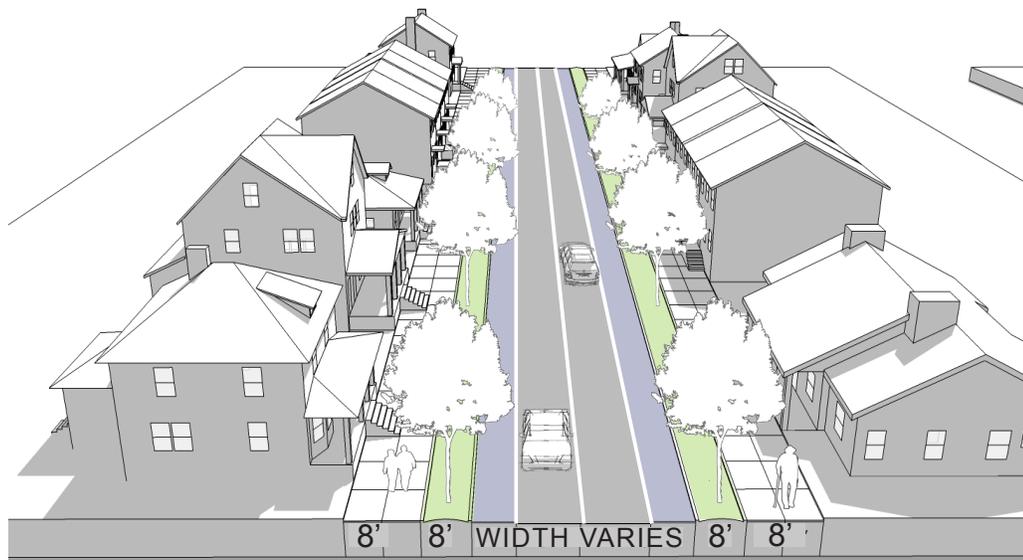
Proposed Curb to Curb: Recommended width is 41 feet from back of curb to back of curb; right of way is 69 feet.

- One travel lane in each direction shared with bicyclists.

- On-street parking on both sides. Curb extensions may be used to narrow street width at intersections and other locations where on-street parking is not appropriate.
- Widening for left turn lanes onto South Tryon Street may be required in accordance with CDOT standards.

Proposed Behind the Curb: Minimum building setback is 16 feet from back of (recessed) recommended curb, or 21 feet from back of any extended curb. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots per the Charlotte Tree Ordinance.

- Planting strip provides buffer from traffic to pedestrians on the sidewalk, and a landscaping opportunity. In locations with retail frontage or other high density applications, tree planting in curbed planters, with paved amenity zone for street furniture, paved access to on-street parking, and merchandising purposes should be substituted for the planting strip.
- Ped Zone is the usual location for the clear sidewalk. Encroachments into the ped zone for features such as steps and open porches are allowed in accordance with the zoning ordinance, but encroachments at grade may not reduce the clear sidewalk to less than 6 feet.



Local Residential Street - Wide

Description: Local streets provide access to residential, industrial, commercial or mixed-use development. The majority of Charlotte's streets are classified as local streets and are typically built through the land development process.

Land Use: The land use is typically medium to high density residential, with some mixed use development.

Situation: Local residential streets are recommended in portions of the study area where residential uses higher in density than single-family are in place or expected. They also are recommended in portions of the transit station area off of the main thoroughfares where a predominance of residential development is expected. Redevelopment is anticipated, and with that high-quality neighborhood street design is desired. The local residential street cross-section is designed for low traffic speeds and a comfortable walking, cycling and living environment.

Existing Condition: These streets have one lane in each direction. Some have on-street parking. Right of way varies widely, but is typically 50 feet.

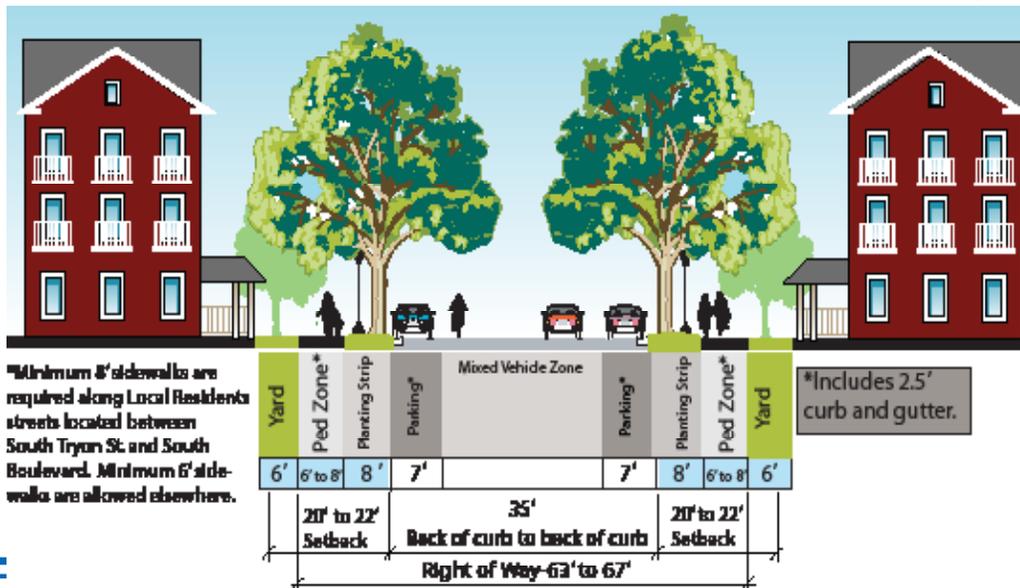
Proposed Curb to Curb: Recommended width is 35 feet from back of curb to back of curb; right of way is 63 to 67 feet, depending on the sidewalk requirement.

- One travel lane in each direction shared with bicyclists.
- On-street parking on both sides. Curb extensions may be used to narrow street width at intersections and other locations where on-street parking is not appropriate.

- Widening for left turn lanes onto South Tryon Street, South Boulevard, or Remount Road may be required in accordance with CDOT standards.

Proposed Behind the Curb: Minimum building setback is 20 to 22 feet from back of (recessed) recommended curb, or 27 to 29 feet from back of any extended curb, depending on the sidewalk width requirement. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots per the Charlotte Tree Ordinance.

- Planting strip provides buffer from traffic to pedestrians on the sidewalk, and a landscaping opportunity. In locations with retail frontage or other high density applications, tree planting in curbed planters, with paved amenity zone for street furniture, paved access to on-street parking, and merchandising purposes should be substituted for the planting strip.
- Ped zone is the usual location for the clear sidewalk. For streets located between South Tryon Street and South Boulevard, the minimum sidewalk width is 8 feet. Elsewhere in the plan area the minimum width is 6 feet.
- The Yard area is intended to provide additional landscaping, and a buffer between sidewalk and residential uses. Encroachments for features such as steps and open porches are allowed in accordance with the zoning ordinance, but encroachments at grade may not reduce the clear sidewalk to less than the required width.
- Parcels located on streets that have existing single-family zoning designations (R-3, R-4, R-5, R-6, and R-8) either across the street, or abutting on the same side of the street, shall have a minimum setback of 30 feet.



Local Residential Street - Wide

Description: Local streets provide access to residential, industrial, commercial or mixed-use development. The majority of Charlotte's streets are classified as local streets and are typically built through the land development process.

Land Use: The land use is typically medium to high density residential, with some mixed use development.

Situation: Local residential streets are recommended in portions of the study area where residential uses higher in density than single-family are in place or expected. They also are recommended in portions of the transit station area off of the main thoroughfares where a predominance of residential development is expected. Redevelopment is anticipated, and with that high-quality neighborhood street design is desired. The local residential street cross-section is designed for low traffic speeds and a comfortable walking, cycling and living environment.

Existing Condition: These streets have one lane in each direction. Some have on-street parking. Right of way varies widely, but is typically 50 feet.

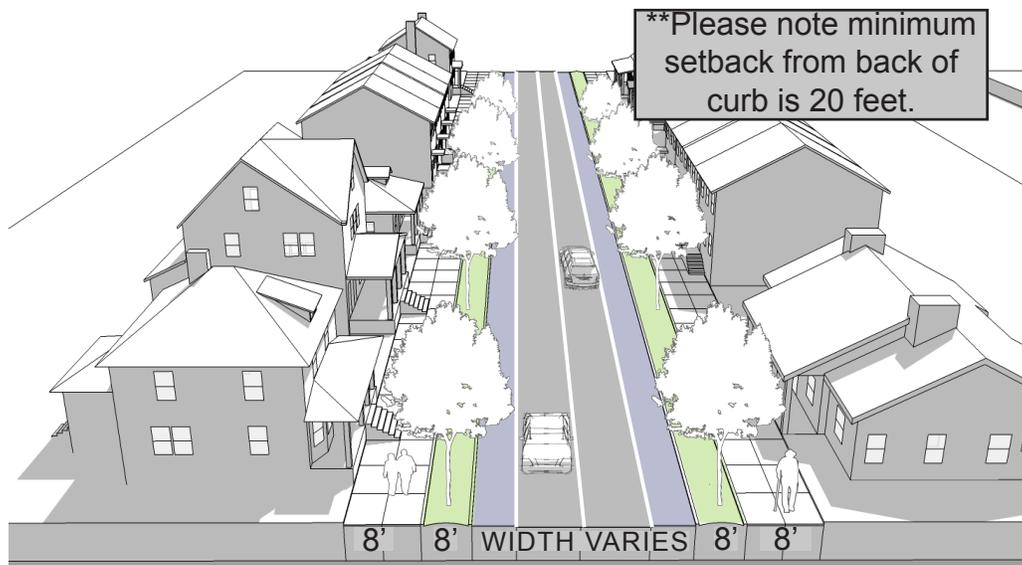
Proposed Curb to Curb: Recommended width is 35 feet from back of curb to back of curb; right of way is 63 to 67 feet, depending on the sidewalk requirement.

- One travel lane in each direction shared with bicyclists.
- On-street parking on both sides. Curb extensions may be used to narrow street width at intersections and other locations where on-street parking is not appropriate.

- Widening for left turn lanes onto South Tryon Street, South Boulevard, or Remount Road may be required in accordance with CDOT standards.

Proposed Behind the Curb: Minimum building setback is 20 to 22 feet from back of (recessed) recommended curb, or 27 to 29 feet from back of any extended curb, depending on the sidewalk width requirement. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots per the Charlotte Tree Ordinance.

- Planting strip provides buffer from traffic to pedestrians on the sidewalk, and a landscaping opportunity. In locations with retail frontage or other high density applications, **tree grates will be required.**
- Ped zone is the usual location for the clear sidewalk. For streets located between South Tryon Street and South Boulevard, the minimum sidewalk width is 8 feet. Elsewhere in the plan area the minimum width is 6 feet.
- The Yard area is intended to provide additional landscaping, and a buffer between sidewalk and residential uses. Encroachments for features such as steps and open porches are allowed in accordance with the zoning ordinance, but encroachments at grade may not reduce the clear sidewalk to less than the required width.
- Parcels located on streets that have existing single-family zoning designations (R-3, R-4, R-5, R-6, and R-8) either across the street, or abutting on the same side of the street, shall have a minimum setback of 30 feet.



New Bern Transit Station Area Plan

Rail Frontage / Multi-Use Trail

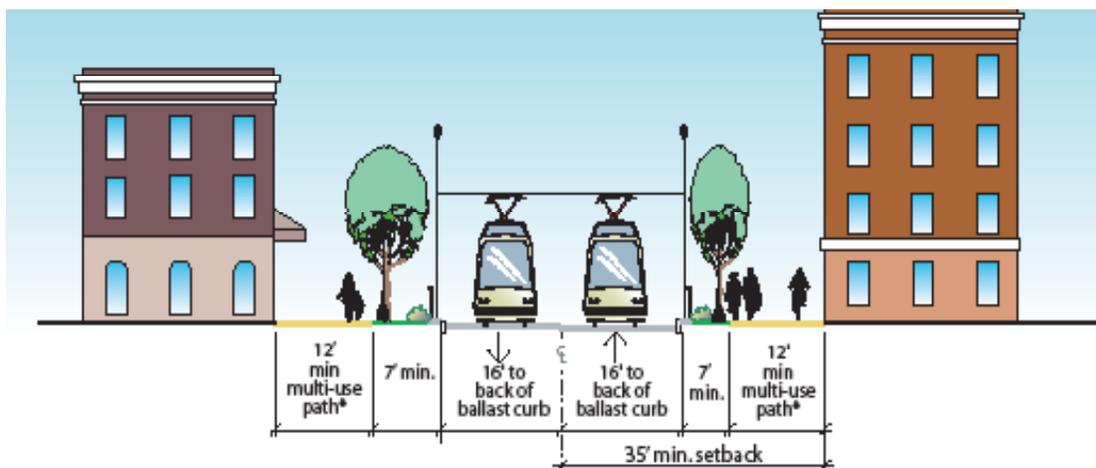
Description: The rail line is the centerpiece of the station area. The City has constructed a multi-use path running parallel to the rail line, typically on the west side. To spark activity along this pathway, adjacent development is required to front it. New development is required to enhance the path where it exists adjacent to the development site. Where the trail does not exist or is across the tracks, new development will be required to construct the trail.

Land Use: The land use is typically medium to high density mixed use, especially residential, with some ground level retail.

Existing Condition: An asphalt multi-use path has been constructed along portions of the right of way, typically along the west side of the tracks. Right of way varies, but is typically 130 feet wide. Rail corridor frontage circumstances vary widely, and generally reflect the warehouse/industrial uses presently and formerly occupying the adjoining property.

Proposed Cross Section: The innermost portion of the right of way is reserved for light rail tracks. A landscaped buffer and multi-use trail are planned beyond the tracks on both east and west sides. The trail is for use by pedestrians and bicyclists.

- The ballast curb for the light rail line typically extends 16 feet from the center line in each direction. A decorative fence should be located adjacent to the ballast curb/trackway.
- A minimum of 7 feet outward from the ballast curb is reserved for a landscaped buffer to the track. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots. Trees must be a columnar form, planted a minimum of 5 feet out from the ballast curb, and approved by CATS. Tree spacing should be consistent with spacing for street trees, as required by the Charlotte Tree Ordinance.
- The multi-use trail, a minimum of 12 feet wide, should be constructed beyond the landscaped buffer. If there is an existing asphalt trail, it should be replaced by an enhanced trail. The trail location may be required to be more than 7 feet from the back of the ballast curb due to site features such as topography, drainage, and CATS equipment. The location of this trail must be approved by CATS. The trail material must be approved by Planning and CDOT. The trail can be widened to allow emergency fire access, but should not be used as a driveway for adjacent development.
- The minimum setback from the centerline of the tracks is 35 feet or the location of the right of way line, whichever is greater. However, where the right of way line extends beyond the minimum setback required, development may be allowed to encroach into the right of way as a method of encouraging development which is consistent with the City's land use vision and if the required written agreements with CATS are completed to occupy portions of the right of way.



*Material of multi-use trail to be approved by Planning CDOT

*Location of multi-use trail to be approved by CATS

Rail Frontage / Multi-Use Trail

Description: The rail line is the centerpiece of the station area. The City has constructed a multi-use path running parallel to the rail line, typically on the west side. To spark activity along this pathway, adjacent development is required to front it. New development is required to enhance the path where it exists adjacent to the development site. Where the trail does not exist or is across the tracks, new development will be required to construct the trail.

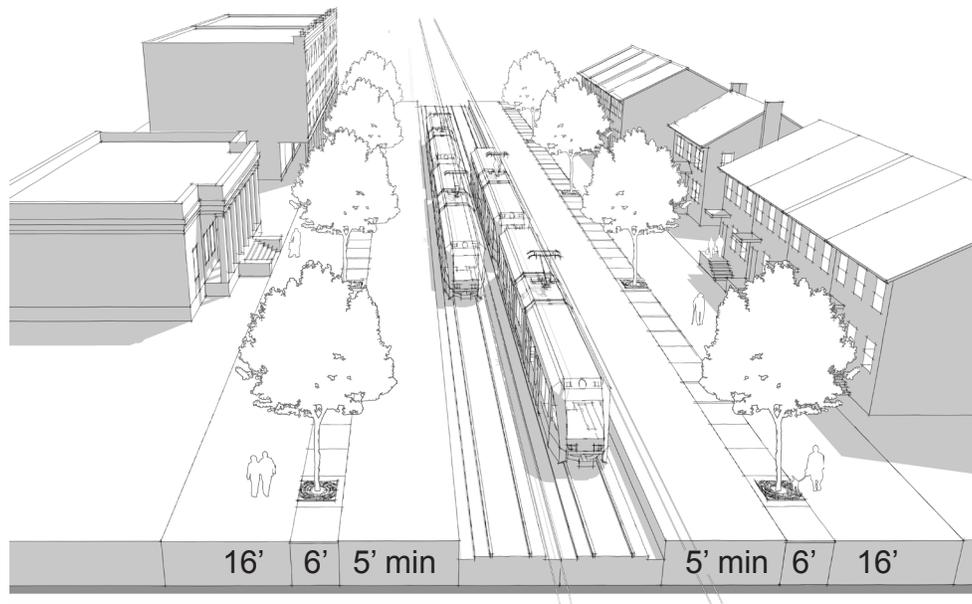
Land Use: The land use is typically medium to high density mixed use, especially residential, with some ground level retail.

Existing Condition: An asphalt multi-use path has been constructed along portions of the right of way, typically along the west side of the tracks. Right of way varies, but is typically 130 feet wide. Rail corridor frontage circumstances vary widely, and generally reflect the warehouse/industrial uses presently and formerly occupying the adjoining property.

Proposed Cross Section: The innermost portion of the right of way is reserved for light rail tracks. A landscaped buffer and multi-use trail are planned beyond the tracks on both east and west sides. The trail is for use by pedestrians and bicyclists.

- The ballast curb for the light rail line typically extends 16 feet from the center line in each direction. A decorative fence should be located adjacent to the ballast curb/trackway.

- A minimum of 5 feet outward from the ballast curb is reserved for a landscaped buffer to the track. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots. Trees must be a columnar form, planted a minimum of 5 feet out from the ballast curb, and approved by CATS. Tree spacing should be consistent with spacing for street trees, as required by the Charlotte Tree Ordinance.
- The multi-use trail, a minimum of 16 feet wide, should be constructed beyond the landscaped buffer. If there is an existing asphalt trail, it should be replaced by an enhanced trail. The trail location may be required to be more than 7 feet from the back of the ballast curb due to site features such as topography, drainage, and CATS equipment. The location of this trail must be approved by CATS. The trail material must be approved by Planning and CDOT. The trail can be widened to allow emergency fire access, but should not be used as a driveway for adjacent development.
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Mixed Use Street

Description: A Mixed Use Street is a walkable, pedestrian friendly street that provides access to and functions for transit oriented development. Development along mixed use streets is dense and focused toward the pedestrian realm.

Land Use: In the plan area, the desired land use is typically medium to high density mixed use with ground level residential, office, and retail.

Situation: Mixed Use streets are recommended for all streets adjacent to the station area and in the South End District. Here, pedestrians receive the highest priority of all the transport modes. To accommodate pedestrian traffic, a wide pedestrian zone and an amenity zone are recommended.

Existing Condition: These streets have one lane in each direction with variation in turn lane and parking arrangement. Right of way varies from 50 feet upward.

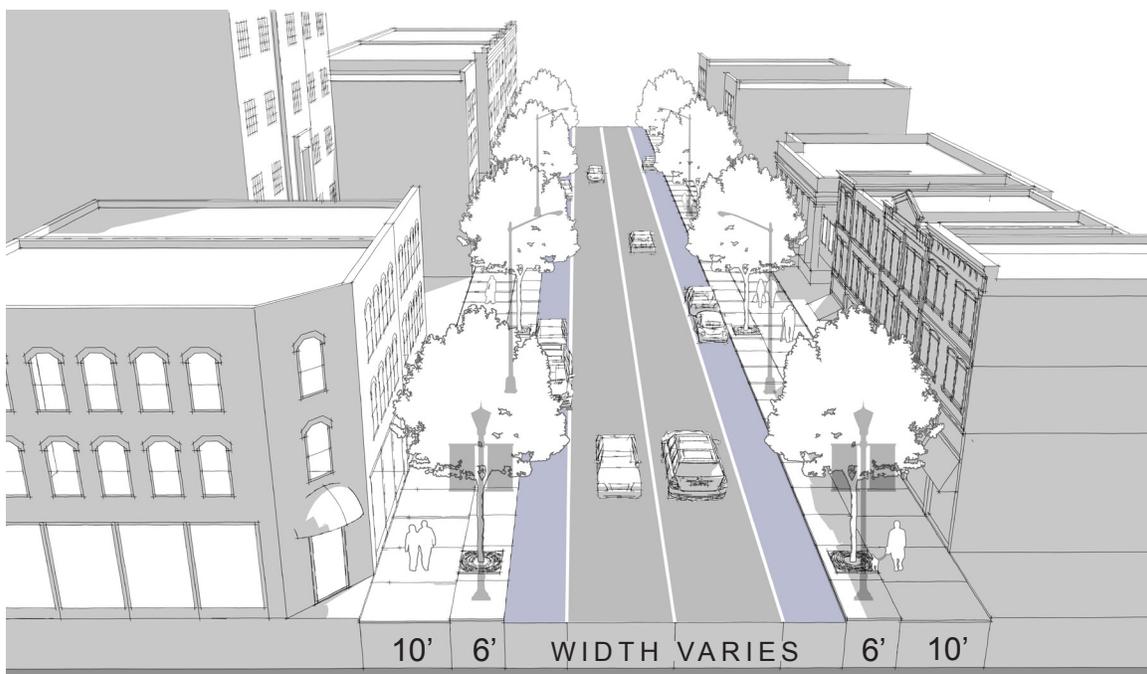
Proposed Curb to Curb: Recommended width is 41 feet from back of curb to back of curb; right of way is 73 feet.

- One travel lane in each direction shared with bicyclists.

Proposed Behind the Curb: Minimum building setback is 16 feet from back of recommended recessed curb. Tree planting is required with spacing, irrigation, subdrainage, and adequate soil space for roots

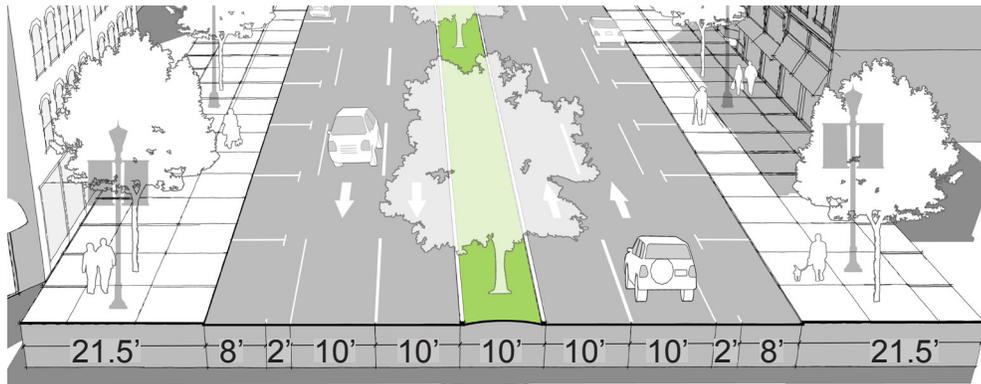
per the Charlotte Tree Ordinance. Tree grates are required per the CLDSM.

- Recessed on-street parallel parking serve as buffer from traffic to pedestrians on sidewalk. The recessed parking is required in all locations feasible under CDOT standards, with intermittent planter islands to break up parking into bays no more than 100 feet in length.
- Amenity zone provides supplemental tree planting location. Trees in the amenity zone should be planted in tree grates. The amenity zone also provides additional paved area for street furniture, paved access to on-street parking, and merchandising purposes.
- Ped zone is the usual location for the clear sidewalk. Where there is no on-street parking, the clear sidewalk can be pushed into the amenity zone, and the ped zone can be used for landscaping, sidewalk dining, or paved merchandising purposes. Encroachments into the ped zone for features such as steps and open porches are allowed in accordance with the zoning ordinance, but encroachments at grade may not reduce the clear sidewalk to less than 8 feet.

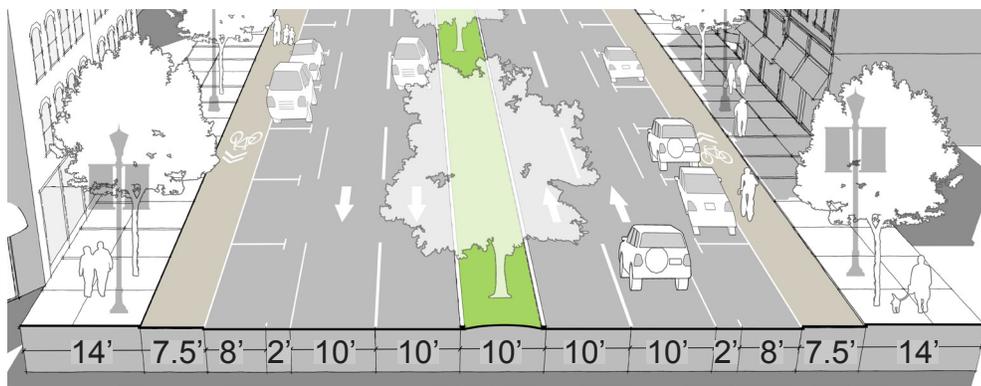


New Bern Transit Station Area Plan

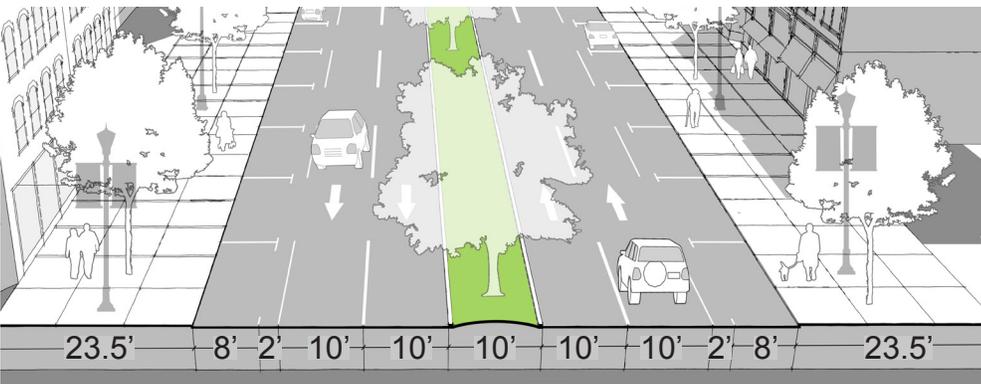
Signature Street - South Boulevard



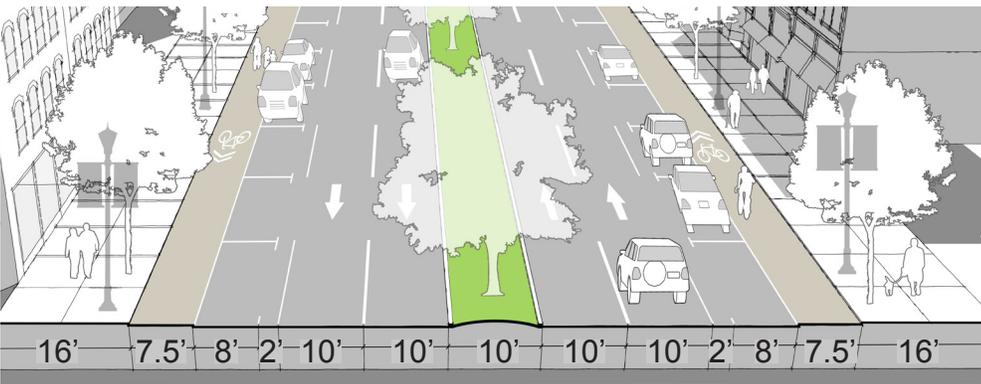
Segment #1
Proposed Future
Cross Section - Interim
(Carson Boulevard to
Iverson Way)



Proposed Future Cross
Section - Final
(Carson Boulevard to
Iverson Way)

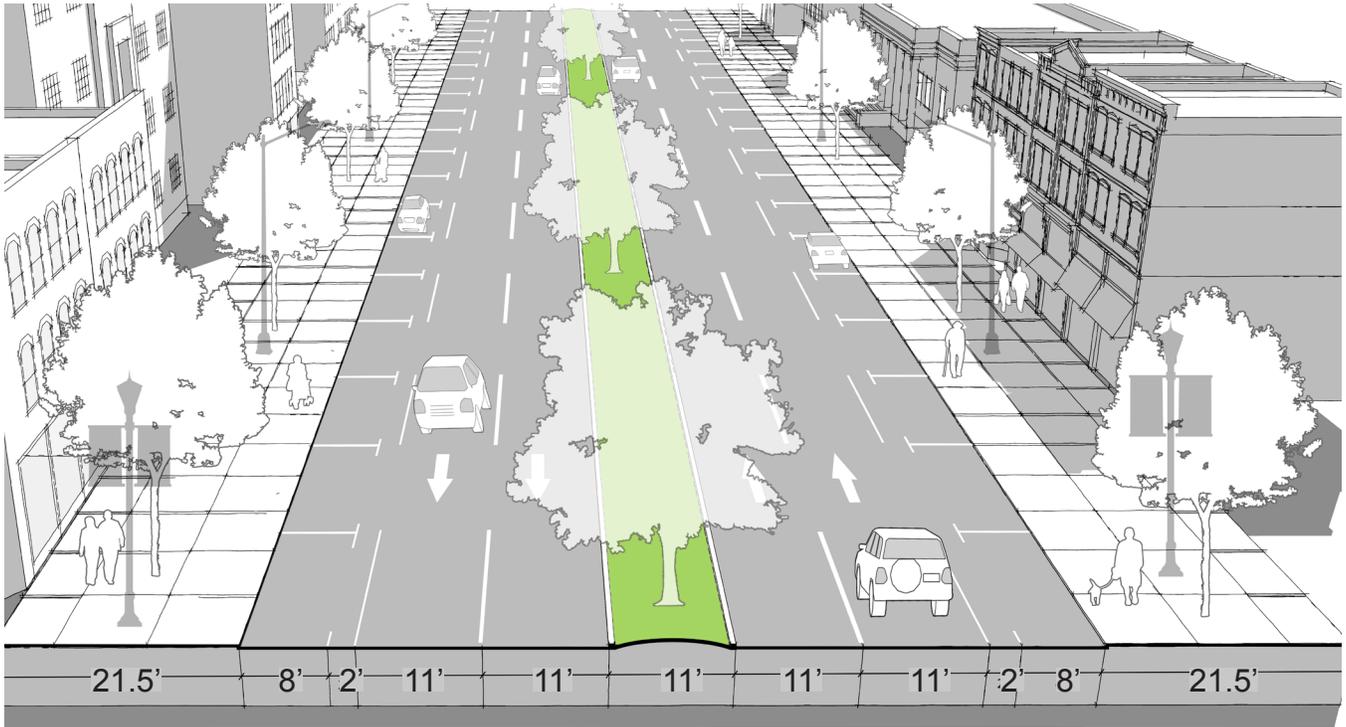


Segment #2
Proposed Future Cross
Section - Interim
(Iverson Way to Clanton
Drive)

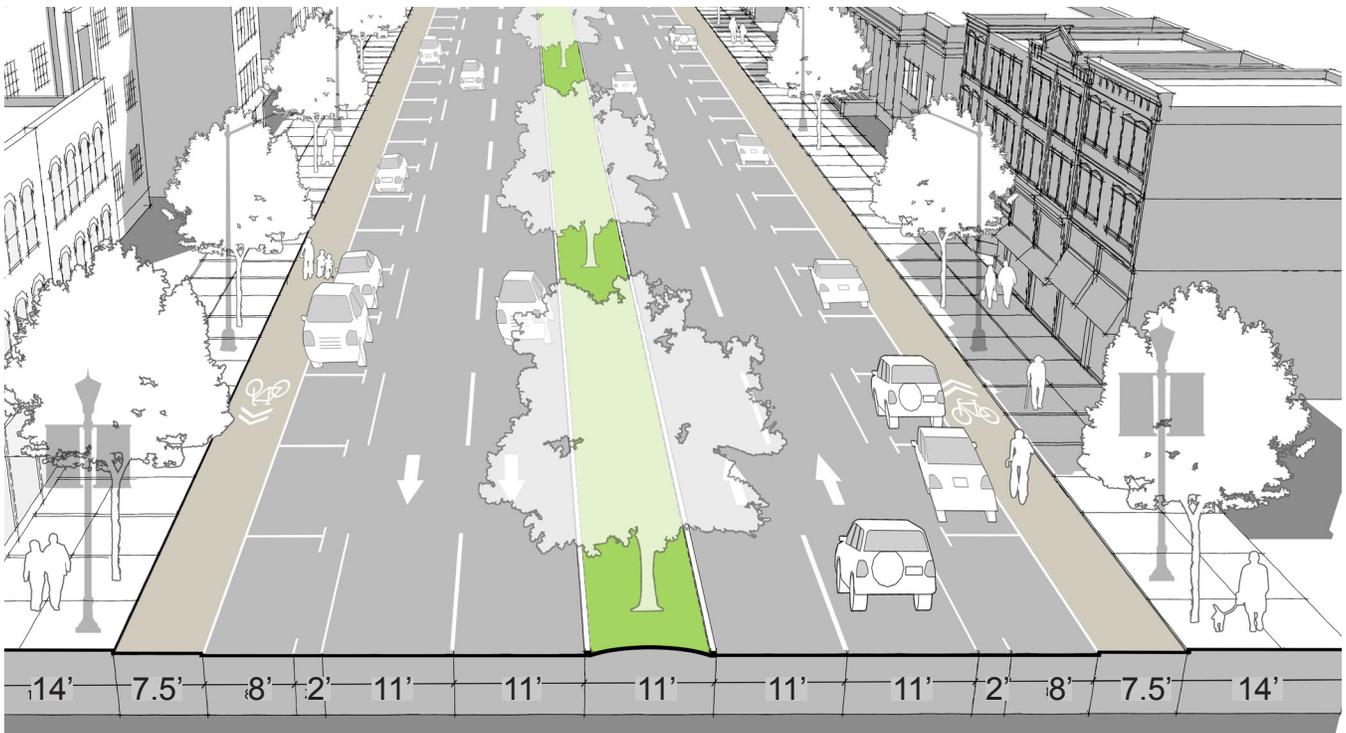


Proposed Future Cross
Section - Final
(Iverson Way to Clanton
Drive)

Signature Street - South Tryon Street



Proposed Future Cross Section - Interim
(Tremont Avenue to Clanton Drive)



Proposed Future Cross Section - Final
(Tremont Avenue to Clanton Drive)

Community Design Policies:

Community design policies help ensure that new development and redevelopment complements the desired character of SouthEnd. While design policies alone do not dictate land use or zoning, they are used to strengthen how various land uses/developments fit together. These policies consider not only the built environment in conjunction with the natural environment, but also how people move through these spaces as pedestrians, bicyclists, or drivers and will be used to help guide future development.

Preserve Flexible Workspace Buildings

This SouthEnd Plan recognizes the importance of preserving existing buildings as affordable workplaces. One way to accomplish this is through renovation and/or adaptive re-use. Older buildings in the Gold District, and on Distribution Street, Dunavant Street, and Griffith Street should be preserved whenever possible. Assembly of smaller parcels and removal of existing usable structures to make way for new development should be discouraged in these areas. Some ways to encourage preservation of existing buildings include:

- Limiting the maximum Floor Area Ratio (FAR) to 1.0 in specific districts.
- Promoting retail, office, and light manufacturing rather than residential development.
- Working with a non-profit or government entity to develop programs to promote the affordable use of newly constructed retail space as interim or bridge uses.
- Restricting building heights and limiting new development to three stories.

Treat the Rail Trail as a Main Street

The Rail Trail should be treated as a Signature street to ensure a safe and vibrant multi-use path. Buildings that abut the Rail Trail should be designed in a manner that provides usable and attractive frontage on the trail as if it were a retail main street. The ground floor of new buildings should include leasable commercial spaces with an appropriate depth and utility stubs to encourage use.

New Bern Transit Station Area Plan



Top to bottom / 1 Adaptive reuse in LoDo, Denver, CO / 2 Art Mural in SoMo, Tallahassee, FL / 3 Retail gateway from the Beltline at the Ponce City Market, Atlanta, GA

Building Length and Proportions

Long, uninterrupted facades have a negative impact on the public realm and should be avoided. They offer less visual interest and variety for pedestrians, diminish available natural light and sky views, discourage through-block pedestrian connectivity, and limit valuable retail space.

Design techniques to prevent long, uninterrupted building facades include:

- Façade modulation that provides variation in the wall plane.
- Building mass separation between all or part of a single building to create the appearance of multiple buildings.
- Use of varying architectural styles, building heights, and/or roof pitches to reduce the apparent size of a building.

Façade/Material Detailing

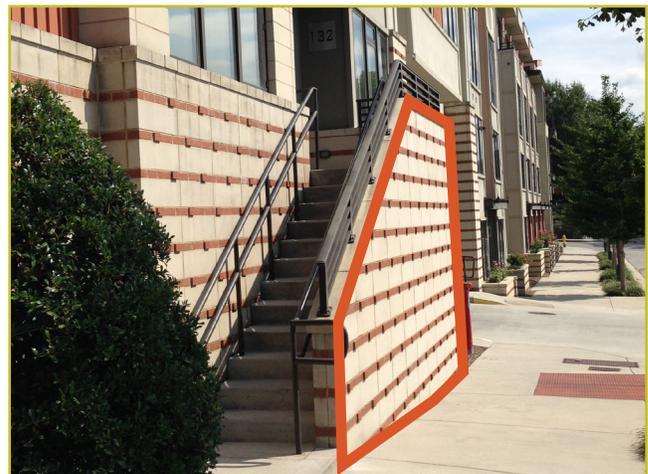
To encourage the good design quality of new buildings, façades should be detailed to avoid a monolithic appearance. Strategies to accomplish this include a clear delineation between the first floor base and the upper stories with changes in building materials and architectural style. In addition to a horizontal delineation, there should be a vertical delineation for facades over 50 feet in length through changes in architectural treatment to reduce the impact of large buildings.

Blank walls should be enhanced with banding, medallions, and other architectural design features when the walls are over 10 feet in length.

Ground Floor Activity and Uses

Streets that are comprised predominantly of active uses such as retail shops and restaurants on the ground floor are perceived as being safer and more interesting, thus encouraging more pedestrian activity throughout the neighborhood. The presence of such active uses are an important neighborhood amenity. Depending on the type of street and frontage, there are a varying level of preferred uses and designs.

- Main Street frontages (such as Camden Road and New Bern Street) should be primarily occupied by retail and restaurant uses with finished tenant spaces that are publicly accessible. Large office lobbies and apartment community spaces are more appropriate for upper stories on Main Street frontages.



Top to bottom / 1 Façade detailing with attention paid to the corner and modern elements, Chattanooga, TN / **2** Blank wall detailing, Chattanooga, TN

NEW PAGE ADDED

- Signature street frontages (like South Tryon Street and South Boulevard) should be designed in a similar fashion as Main Street frontages, but may have a wider range of uses including community spaces and lobby spaces.
- Mixed Use/Transitional Streets (such as Worthington Avenue, Bland Street, Distribution Street, and Dunavant Street along with many others) may include a wider range of design details that are associated with the type of building. Commercial buildings should include the same design standards as those for Main Street frontages, while residential frontages should adhere to the residential frontage policies below.

Ground Floor Design

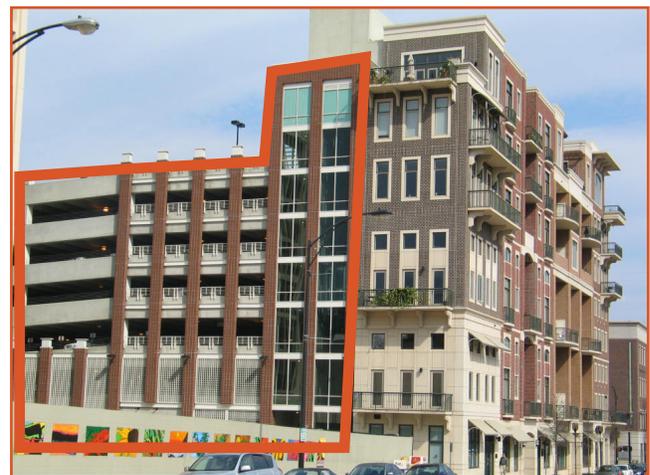
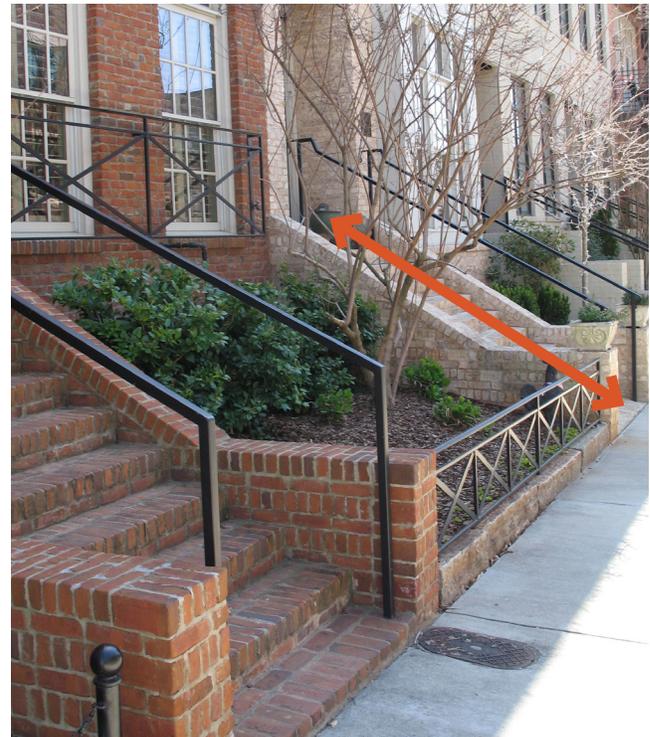
Large, transparent storefront windows provide a sense of connection between the public realm and the merchandise or activity inside. They also provide a source of lighting of the sidewalk at night. This connection encourages pedestrian activity along the frontage sidewalk.

Ground floor transparency should include visibility into interior spaces to a minimum depth of 20 feet. Ground floor building facades should be over 60% transparent between 2 ft. and 10 ft. above the adjacent sidewalk for the length of the first floor. This improves the connection between the sidewalk and the shop. High quality materials should also be used on the ground floor to withstand the wear and tear of a high pedestrian traffic environment. Finally, above ground utilities should not be located between the building and the primary frontage, as they intrude into the pedestrian space.

Entrances

Prominent and easily discernable entrances provide pedestrians with a clear sense of direction and connection with the tenant spaces. The more prominent the street, the more frequent the entrances into the building should be, located to encourage pedestrian activity and smaller shops and offices.

On Signature Streets it is appropriate for residential lobbies to be located on the ground floor with prominent entrances, as long as the building is wrapped with other active retail, restaurant, and/or office uses.



Top to bottom / 1 Ground floor transparency exceeding 75%, Birmingham, MI | 2 Raised stoop entries for housing at Glenwood Park, Atlanta, GA | 3 'Bookend' apartment concealing parking deck beyond, Greenville, SC

NEW PAGE ADDED

Residential Frontage Details along Mixed-Use/Transitional Streets

Multi-family residential development should provide direct connections to the sidewalk for ground floor units on Mixed-Use/Transitional Streets. See above for entrance details on Signature Streets in residential buildings. Raised stoops and entries provide a sense of privacy for residential occupants and differentiate them from commercial entries. Stoops should be designed as primary entrances and not as back porches.

Parking and Driveways

Vehicular movement and parking should be minimized so as to not negatively impact the pedestrian environment, improve aesthetics, and encourage the use of transit and other alternative transportation, including autonomous and shared vehicles.

Parking decks should be wrapped with active uses, specifically on Main Streets and Signature Streets. Active uses are defined as spaces that are leasable and occupiable - with a minimum depth of 20 feet and include the ground floor uses listed above in the “Ground Floor Activity” section. Spaces should be connected to the sidewalk and include a prominent entrance. On Main Streets, parking decks should be wrapped on all floors, whereas on Signature Streets parking decks should, at a minimum, be wrapped on the ground floor.

Trees, Landscaping, and Lighting

Trees, landscaping, and lighting are all critical elements of the streetscape and should be selected based on the appropriate context. When on-street parking is provided and commercial/high density residential uses are adjacent, trees in tree pits with grates are typically more appropriate than planting strips or curbed planters, as they provide an amenity area for outdoor dining and streetscape furniture. Tree wells are appropriate for residential only frontages that are moderate densities – including townhouses and small scale apartments. A planting strip is typically appropriate only when there is no adjacent on-street parking and the uses are lower density residential.



Above Street tree in tree grate and well, Tempe, AZ

Design Review Required for Important Frontages

Consider additional design review to supplement administrative review for buildings located on Main Streets and Signature Streets.