



Park Woodlawn

area plan

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Charlotte-Mecklenburg
Planning
Department

Adopted by
Charlotte City Council
Date here

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Executive Summary

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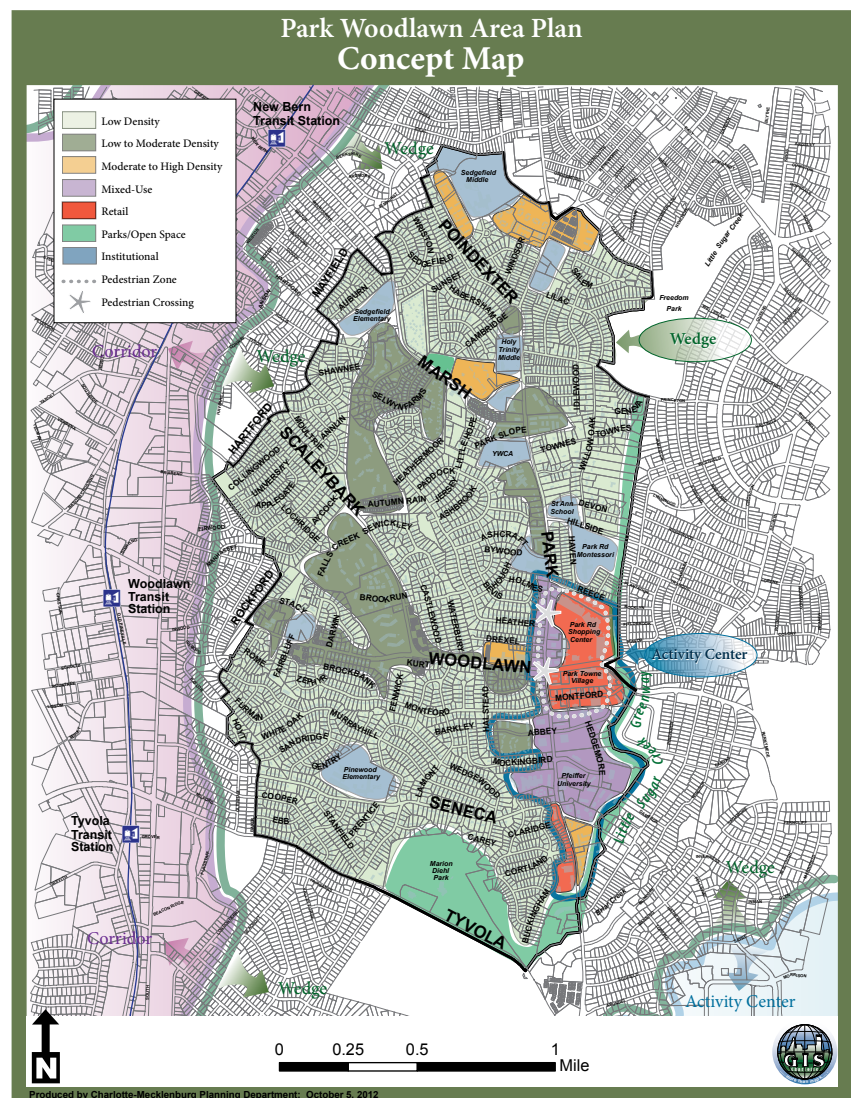


Executive Summary

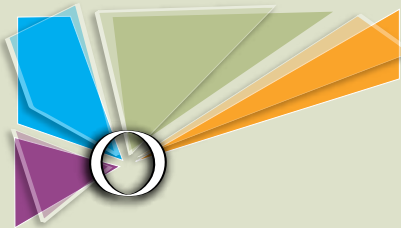
The *Park Woodlawn Area Plan* defines the vision and land use policies for the plan area and provides specific direction to guide future growth and development, consistent with the *Centers, Corridors and Wedges Growth Framework*. This Executive Summary provides the highlights of the plan document.

Plan Boundary

The plan area comprises approximately 2,000 acres, and is bounded by the Dilworth neighborhood on the north, Little Sugar Creek on the east, Tyvola Road on the south, and the South Corridor Transit Station Areas of New Bern, Scaleybark, Woodlawn and Tyvola stations on the west. The plan area encompasses the entire Park Road / Woodlawn Road Mixed Use Activity Center as defined by the *Centers, Corridors and Wedges Growth Framework*. The Activity Center includes the Park Road Shopping Center as well as Montford Drive and the office and retail developments along Park Road up to the intersection with Selwyn Avenue.



The remaining portion of the plan area lies within a Wedge, meaning that it is predominately residential with both single-family and multi-family developments as well as some commercial and institutional uses scattered throughout. The area is characterized by strong stable neighborhoods, such as Sedgefield Madison Park, Freedom Park, and Collingwood.



Vision Statement

This plan sets forth a vision for this area by moving forward with its unique focus on the following:

The Park Woodlawn area is recognized as one of Charlotte's most vibrant, attractive and diverse areas. The dynamic blend of mature neighborhoods led by engaged community leaders and residents; along with a thriving mixed-use activity center containing long standing local businesses; and active civic institutions help form part of this thriving community.

- **Neighborhoods** are the backbone to the stability of the area and will be essential in preserving the distinctive character of the community into the future. New development in and around the neighborhoods will take into account its impact on the quality of life of area residents by: protecting the tree canopy; providing a variety of enhanced and safe transportation connections that improve mobility; and consider community input during the development process.
- **Open Space** will be well integrated into the community through greenways, parks and plazas where appropriate as new developments take shape.
- **Transportation infrastructure** will be developed to connect neighborhoods with activity centers; and should safely accommodate pedestrians, bicyclists, transit users, and motorists.
- **Mixed-Use Activity Center** will be a thriving neighborhood serving mixed-use center, complimentary and enhancing to the surrounding neighborhoods; with a rich variety of retail, office, entertainment and multi-family residential uses; in a well designed and appropriately scaled development form that is conducive to a high quality of life for area residents and visitors.

Key Opportunities and Challenges

An understanding of existing conditions, including feedback received from the public during the planning process, reveals opportunities to build upon and challenges to address. Key opportunities include: **Neighborhoods, Local Business Core, Strategic Location, and Park, Recreation and Open Space.** Challenges include **Limited Locations for New Development, Limited east/west Connectivity, Connectivity and Walkability within Mixed Use Activity Center, and Traffic Congestion.**

Summary Of Goals And Policies

The *Park Woodlawn Area Plan* provides goals and policies to ensure that the future envisioned for this area is achieved.

Land Use Policies

GOAL:

Maintain and preserve the area's predominant neighborhood character, along with strengthening and enhancing the strong neighborhood business center. Future developments should be sensitive to the surrounding neighborhood. These developments should enhance the neighborhoods by building upon the area's assets and improving the pedestrian environment by providing more connections between the residential wedge areas and the mixed use activity center.

Key Policy Highlights

Activity Center

- Encourage a mix of land uses that incorporates good design elements in creating a neighborhood scale mixed use district.
- Support the continued evolution of the Montford

Drive area as it transforms into an emerging entertainment district.

- Encourage infill development where feasible, at an appropriate scale with the surrounding neighborhoods.

Wedge Area

- Support the strengthening of the single family neighborhoods in the area by limiting infill development to the established densities in the area, and maintaining the single family land uses throughout.
- Provide opportunities for limited amounts of moderate density development at key locations in the Wedge area, along major thoroughfares, and making sure new development is at a scale appropriate for the surrounding neighborhood.
- Provide pedestrian and bicycle connections on rights-of-ways that were not developed as streets.

Community Design Policies

GOAL:

Plan new developments that respect the scale, design, and proximity of adjacent buildings and land uses, especially when impacting single family residential. Good architecture and site design, as well as quality construction materials, should be used to ensure the long-term sustainability and economic vitality of all development regardless of the proposed use.

Key Policy Highlights

- Design infill residential buildings in harmony with the scale, size, massing and materials of the adjacent neighborhood.
- Minimize impacts to the tree canopy.
- Encourage architecture design that complements the scale, style, character, and materials of surrounding structures that contribute to the neighborhood character.
- Buildings immediately adjacent to single-family residential areas should step down to 40' or less along shared boundaries.
- Integrate functional open space into new development.

Park Woodlawn Area Plan Executive Summary

Charlotte-Mecklenburg
Planning Department

Transportation Policies

GOAL:

Support and develop a connected, affordable, balanced transportation system that encourages walking. It should safely and sensibly accommodate pedestrians, bicyclists, and transit users while allowing for neighborhood-friendly automobile speeds for motorists and business patrons traveling to and through the plan area.

Key Policy Highlights

- Enhance the intersection of Park Road and Woodlawn Road to make it safer for pedestrians and cyclists.
- Support the viability of Montford Drive as a destination Main Street, by working with area businesses in helping to create streetscape enhancements that help improve the pedestrian experience.
- Create multiple connections to the Little Sugar Creek Greenway in order to provide alternative access routes to area focal points.
- Implement design concepts for calming and enhancing Scaleybark Road.

Infrastructure and Public Facilities Policies

GOAL:

Maintain and enhance public facilities such as parks and greenways, and emphasize environmental and economic sustainability for new or updated public facilities and infrastructure.

Key Policy Highlights

- Enhance multimodal functionality of streets and provide direct access between the neighborhood and the mixed-use activity center via different modes of transportation (pedestrian, cyclists, and transit users).
- Enhance pedestrian way finding signs to link parks and greenways along major area destination.
- Seek public/private opportunities to create a public/civic open space in the Park Road Shopping Center, which is the heart of the community.

Natural Environment Policies

GOAL:

Preserve and protect the natural environment, parks, tree canopy, open space, and natural resources. Promote measures to minimize the adverse effects of new development on land, air, and water quality, and encourage the use of sustainable building practices. Increase the tree canopy in the plan area following the City's adopted *50/50 Tree Canopy* policy, especially in the mixed use activity center where the tree canopy is lowest.

Key Policy Highlights

- Encourage actions (measures) that will help ensure the long-term sustainability of the tree canopy.
- Establish tree canopy goals for the Park Woodlawn area, following the City's *50% Tree Canopy Goal in 2050*.
- Support mixed-use and compact development that preserve land, reduce vehicular trips and protect natural resources.
- Encourage new pedestrian and bicycle connections to reduce vehicle miles travelled.

Key Implementation Strategies

The Implementation Guide identifies strategies to implement the policies of the Concept Plan, outlining a number of projects for each of the plan components. However, it is not adopted by elected officials and is not a commitment for implementation projects. These strategies are both short term, such as improving pedestrian crossings on Park and Woodlawn Roads, and long term, such as improving the Montford Drive streetscape. Many will be implemented by the private sector over time as development occurs.

Key implementation items include:

- Using land use and design recommendations to guide and evaluate development proposals.
- Enhancing the Park Road and Woodlawn Road intersection which may include geometric design changes, pedestrian islands, directional curb ramps, and additional or longer turn lanes.
- Providing additional pedestrian crossings on Park Road and Woodlawn Road.
- Working with Montford Drive businesses in addressing the parking challenges in the area, and assisting if possible with some kind of shared parking agreement that can help alleviate the problem.
- Working to implement the City's *Tree Canopy Goal by 2050*, by working with local groups such as TreeCharlotte in helping to plant over 2,000 trees in the public rights-of- ways in the plan area, and seek opportunities on private properties where possible.



Park Towne Village and Park Road Shopping Center both have attractively designed public spaces that contribute to the overall pedestrian environment.

1 *Concept Plan*

1



1 Concept Plan

Plan Context

Purpose

This document establishes a vision for the plan area and provides policy direction to guide future growth and development consistent with the *Centers, Corridors and Wedges Growth Framework*. Once adopted this plan will:

- Update existing adopted plans for the area, including the 1992 *Park Road Corridor Plan*, 1993 *Central District Plan*, and 1991 *South District Plan*;
- Refine the boundaries for the portions of the Activity Center and Wedge included in the plan area;
- Address key land use, transportation, community design, and development issues identified through the planning process;
- Provide guidance for future land use and infrastructure decisions; and
- Function as the official streetscape plan.



Stable and highly desirable neighborhoods surround the Park Road/ Woodlawn Road Activity Center's service and shopping options. Proximity to Uptown Charlotte makes for quick access to Center City.

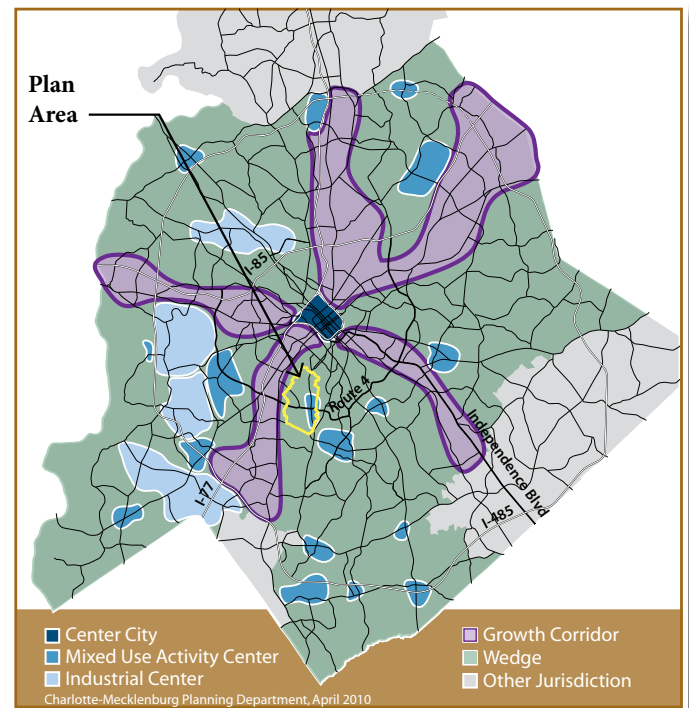
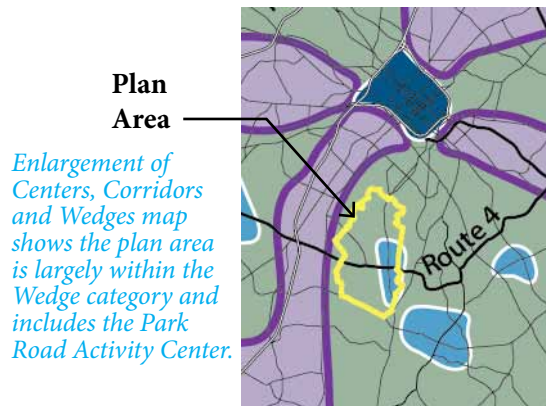
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The remaining portion of the plan area lies within a Wedge, meaning that it is predominately residential with both single-family and multi-family developments, with some commercial and institutional uses scattered throughout. The area is characterized by strong stable neighborhoods, such as Sedgfield Madison Park, Freedom Park, and Collingwood. See **Map 1**.



Organization of this Document

This document is organized into three parts: **Part I: Concept Plan**, which will become adopted City policy; **Part II: Implementation Guide**, which contains action steps identified to carry out plan policies; and **Part III: Appendix**, the existing conditions report and other data used to develop the plan.

Part 1: Concept Plan

- Purpose
- Vision Statement
- Goals
- Policies

■ *Concept Plan is adopted by City Council and will act as a policy guide for future decision making.*

Part 2: Implementation Guide

- Action Steps identified to Carry Out Plan Policies

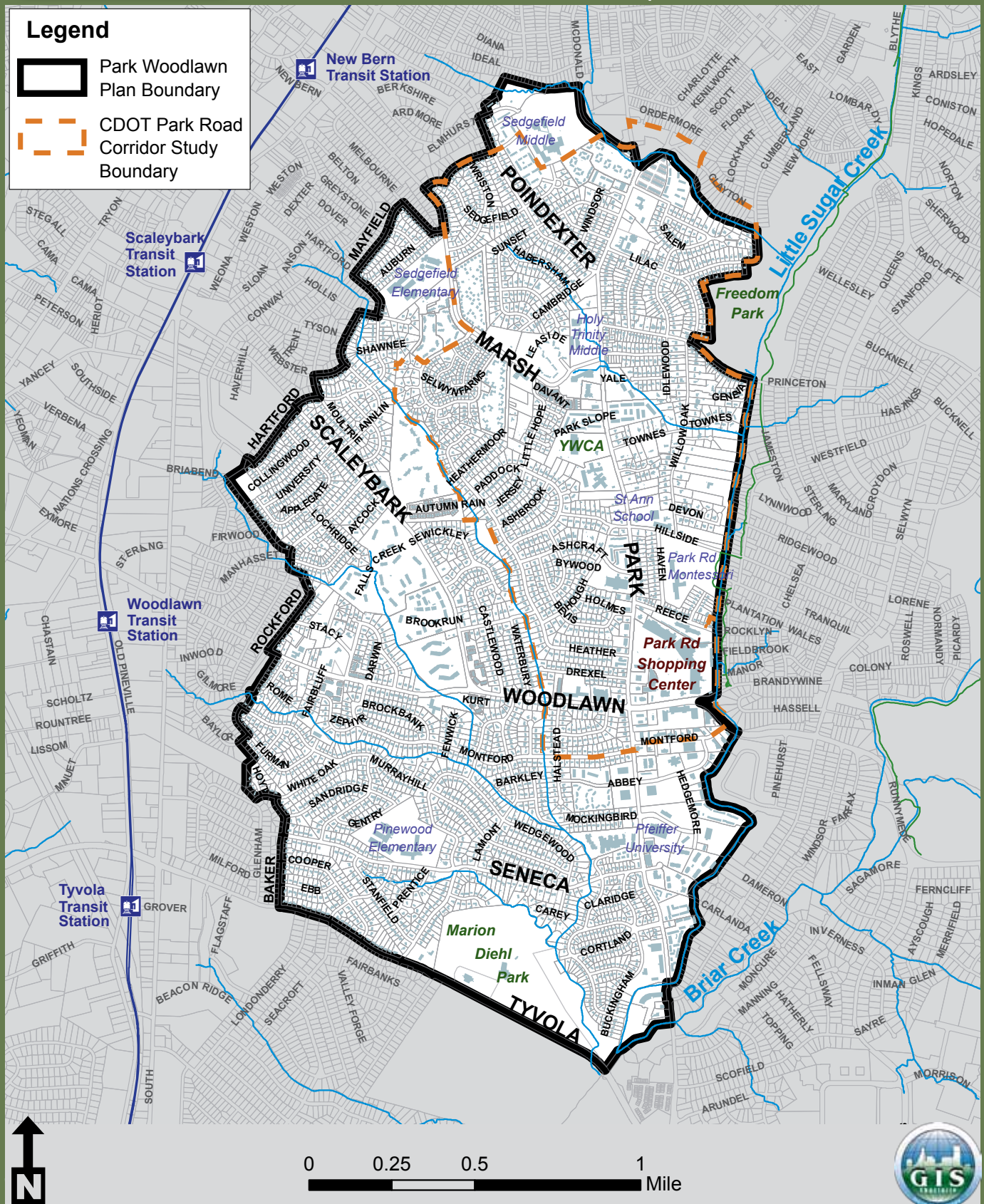
■ *Implementation Plan provides direction to staff in implementing plan policies*

Part 3: Appendix

- Existing Conditions Report
- Trends & Forecasts
- Other Supporting Information

■ *Appendix provides additional, supporting information*

Park Woodlawn Area Plan Area Plan Boundary

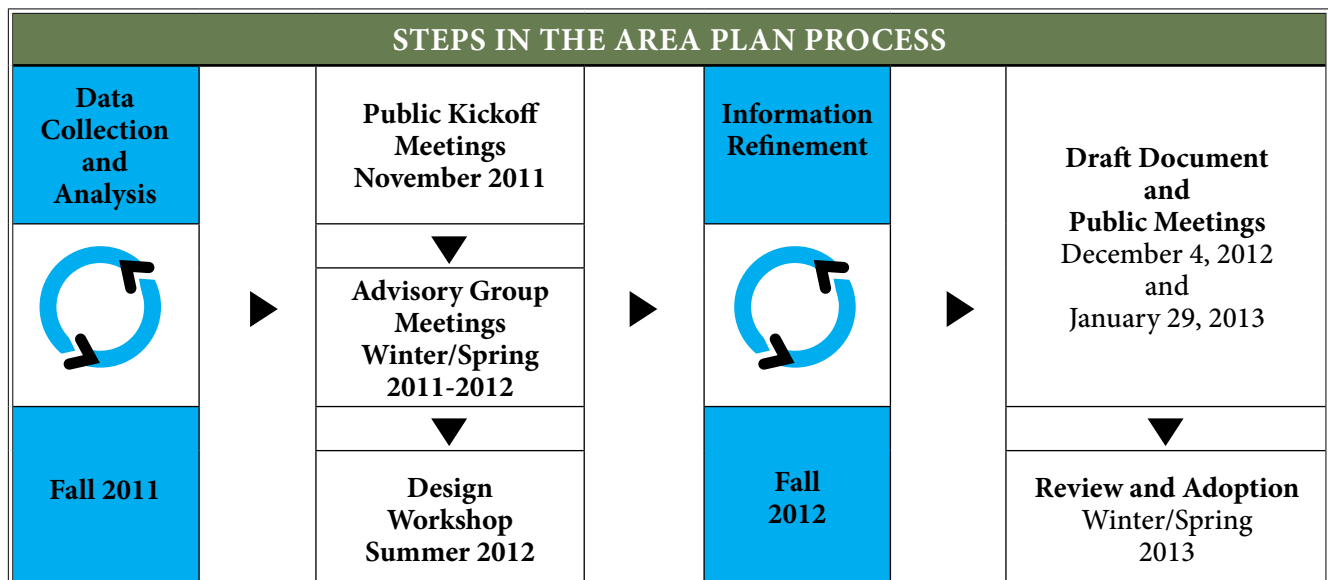


Produced by Charlotte Mecklenburg Planning Department: October 5, 2011

Planning Process

Development of a plan for the Park Woodlawn area was identified as a priority during the Plan Assessment process, due to recent development pressures in the area and the lack of a currently adopted Area Plan. The draft *Park Woodlawn Area Plan* was prepared over a year long process and included the following key public involvement opportunities:

- public kick off meeting held on November 17, 2011 with approximately 200 people in attendance;
 - 68-member Citizen Advisory Group met 7 times over the course of approximately one year to assist staff in developing plan policies;
 - final public meetings held on December 4, 2012 and January 29, 2013 with approximately xxx people in attendance.
- In addition to these public input opportunities, the Planning Committee of the Charlotte-Mecklenburg Planning Commission hears citizen comments, reviews the area plan and makes a recommendation to the Charlotte City Council. The City Council also hears citizen comments and makes the final decision concerning adoption of the plan.*



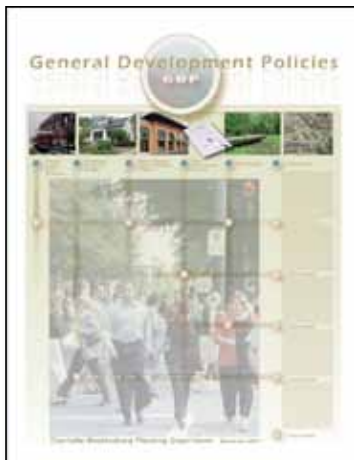
Policy Framework

The following key documents provided guidance and direction for this plan:

Centers, Corridors and Wedges Growth Framework establishes a vision for future growth and development. It does this by identifying three geographic types used to categorize land in Charlotte and its sphere of influence, and outlining the desired characteristics of each of these geographies:

- Activity Centers are generally appropriate for new growth, with the type and intensity of development corresponding to the Center type.
- Some parts of Growth Corridors are often priority locations for new growth, especially in identified Transit Station Areas.
- Wedges are envisioned to remain predominately low density residential with limited higher density housing and neighborhood serving commercial uses.

Much of Charlotte's future moderate to higher intensity development is expected to occur within the five Growth Corridors and in designated Activity Centers. This will help maximize existing infrastructure and services. This growth framework is important as the Park Woodlawn plan area encompasses a Mixed Use Activity Center and a Wedge. The framework provides direction for intensification and mixing of uses within the Mixed Use Activity Center while creating opportunities for lower intensity residential and supporting services in the Wedge area, consistent with plans for future infrastructure improvements.



General Development Policies are adopted policies on various topics relevant to development and redevelopment in Charlotte-Mecklenburg. In particular, they address residential location and design, retail-oriented mixed/multi-use centers and transit supportive development. Additionally, the *General Development Policies* provide guidance to minimize the negative environmental impacts of land development and to more closely

link land use and development decisions to the availability of infrastructure to support it. This area plan builds upon the General Development Policies' guidance in these areas: Residential Location and Design, Retail-Oriented Mixed-Multi-Use Centers, Natural Environment and Infrastructure. They also were used to improve the integration of transportation and land use by strategically locating land uses to increase opportunities for transportation choices.

Adopted Land Use Plans include six geographic districts which make up the City of Charlotte's planning jurisdiction: the Northeast, East, South, Southwest, Northwest and Central districts. Each of these areas have a district plan that addresses a wide range of physical development issues and provides parcel-specific land use recommendations for all properties within that district plan. This plan will update the *Central and South District Plans*.

2035 Long Range Transportation Plan defines the policies, programs and projects to be implemented over the next twenty years, providing transportation choices in Mecklenburg and western Union County.

Transportation Action Plan defines short and long-term policies for accommodating motor vehicles, transit riders, bicyclists and pedestrians together with an implementation "blueprint" for improvements. The Transportation Action Plan's comprehensive "toolbox" of transportation programs will help implement the recommendations made in this plan.

2030 Transit Corridor System Plan is a long-range multi-modal transportation plan that identifies five transportation corridors, a series of Center City improvements, and enhanced bus service and facility improvements throughout the region.

Urban Street Design Guidelines offer a comprehensive approach to designing new and modified streets within the City. They are also a key component of the Transportation Action Plan, and were used in developing this plan to assist with street classifications and cross-sections that will guide the design and redesign of streets. The *Urban Street Design Guidelines* also address integrating land use and transportation to create synergy between the streets and the land uses adjacent to them.

Greenway Master Plan Update identifies an expanded greenway network and greenway trail system throughout Mecklenburg County. As land is acquired and set aside over time, the greenway system should also improve water quality by reclaiming natural floodplains, protect wildlife habitats and open space, and provide recreational and educational opportunities for residents. The *Greenway Master Plan* provided guidance on the existing and proposed locations for park and recreational facilities such as the Little Sugar Creek Greenway, Marion Diehl Park, and Freedom Park.

Key Opportunities and Challenges

An understanding of existing conditions, including feedback received from the public during the planning process, reveals opportunities to build upon and challenges to address. Key opportunities and challenges, highlighted below, were discussed at the first public meeting, as well as during the Citizen Advisory Group meetings, formed the basis for the vision statement and ultimately, plan policies.

Opportunities:

Neighborhoods: The plan area consists of a number of strong stable neighborhoods such as Sedgfield, Madison Park and others which form the backbone of the area and make it an attractive and desirable place to live for long-time residents as well as newcomers.

Local Business Core: The long-standing and almost iconic Park Road Shopping Center, along with its diverse mix of local businesses, is a huge draw and a key element in defining the area's identity. Coupled with the growth of the restaurant and entertainment establishments on Montford Drive, the area has a unique and thriving neighborhood serving business core.

Strategic Location: The area's location as an in-town neighborhood makes it an attractive location for people to want to live close to employment centers such as Uptown, the hospital district and South Park as well as easy access to transit via either the LYNX Blue Line which is just to the west of the plan area, or bus service.

Park, Recreation and Open Space: The area includes or is adjacent to a number of park and recreational amenities such as the Little Sugar Creek Greenway, Freedom Park, and Marion Diehl Park.

Challenges:

Limited Locations for New Development: The plan area is completely built out for the most part, and has been since prior to 1970. This leaves limited opportunities for new development without assembling parcels and redeveloping underutilized properties in particular the shallow parcels fronting the west side of Park Road do not provide adequate opportunities for redevelopment; when infill development does happen the projects can appear to be encroaching upon the single-family residential neighborhoods behind them.

Limited East/West Connectivity: Woodlawn Road is the only true east/west access road traversing the area; this increases the impact of traffic for Woodlawn Road and also affects businesses in the mixed use activity center that depend on easy access from the surrounding neighborhoods.

Connectivity and Walkability within Mixed-Use Activity Center: The area surrounding the Park Road Shopping Center, and around the restaurant and entertainment area on Montford Drive have all the right elements in place for a great neighborhood business center, which it is for the most part. However, the area lacks the walkability and street grid pattern that would make it easier to connect from one place or another by vehicle or as a pedestrian. The connectivity for both vehicles and pedestrians in some instances is blocked or discouraged by long city blocks and lack of street connections, in order to accommodate for more single use private surface parking lots.

Traffic Congestion: Both Park Road and Woodlawn Road are major thoroughfares and move large amounts of traffic running through the plan area, which has an impact on properties fronting both roads.



Vision Statement



The Park Woodlawn area is recognized as one of Charlotte's most vibrant, attractive and diverse areas. The dynamic blend of mature neighborhoods led by engaged community leaders and residents; along with a thriving mixed-use activity center containing long standing local businesses; and active civic institutions help form part of this thriving community.

This plan sets forth a vision for this area by moving forward with its unique focus on the following:

- **Neighborhoods** are the backbone to the stability of the area and will be essential in preserving the distinctive character of the community into the future. New development in and around the neighborhoods will take into account its impact on the quality of life of area residents by: protecting the tree canopy; providing a variety of enhanced and safe transportation connections that improve mobility; and consider community input during the development process.
- **Open Space** will be well integrated into the community through greenways, parks and plazas where appropriate as new developments take shape.
- **Transportation infrastructure** will be developed to connect neighborhoods with activity centers; and should safely accommodate pedestrians, bicyclists, transit users, and motorists.
- **Mixed-Use Activity Center** will be a thriving neighborhood serving mixed-use center, complimentary and enhancing to the surrounding neighborhoods; with a rich variety of retail, office, entertainment and multi-family residential uses; in a well designed and appropriately scaled development form that is conducive to a high quality of life for area residents and visitors.

Goals

To achieve the future envisioned for the plan area, the following goals have been identified. The goals build upon adopted plans and policies.

■ LAND USE

Maintain and preserve the area's predominant neighborhood character, along with strengthening and enhancing the strong neighborhood business center. Future developments should be sensitive to the surrounding neighborhood. These developments should enhance the neighborhoods by building upon the area's assets and improving the pedestrian environment by providing more connections between the residential wedge areas and the mixed use activity center.

■ COMMUNITY DESIGN

Plan new developments that respect the scale, design, and proximity of adjacent buildings and land uses, especially when impacting single family residential. Good architecture and site design, as well as quality construction materials, should be used to ensure the long-term sustainability and economic vitality of all development regardless of the proposed use.

■ TRANSPORTATION

Support and develop a connected, affordable, balanced transportation system that encourages walking. It should safely and sensibly accommodate pedestrians, bicyclists, and transit users while allowing for neighborhood-friendly automobile speeds for motorists and business patrons traveling to and through the plan area.

■ INFRASTRUCTURE AND PUBLIC FACILITIES

Maintain and enhance public facilities such as parks and greenways, and emphasize environmental and economic sustainability for new or updated public facilities and infrastructure.

■ NATURAL ENVIRONMENT

Preserve and protect the natural environment, parks, tree canopy, open space, and natural resources. Promote measures to minimize the adverse effects of new development on land, air, and water quality, and encourage the use of sustainable building practices. Increase the tree canopy in the plan area following the City's adopted 50/50 Tree Canopy policy, especially in the mixed use activity center where the tree canopy is lowest.

View from Seneca Place area looking south. The section of Park Road which is within the Activity Center is shown surrounded by Wedge areas which contain established neighborhoods



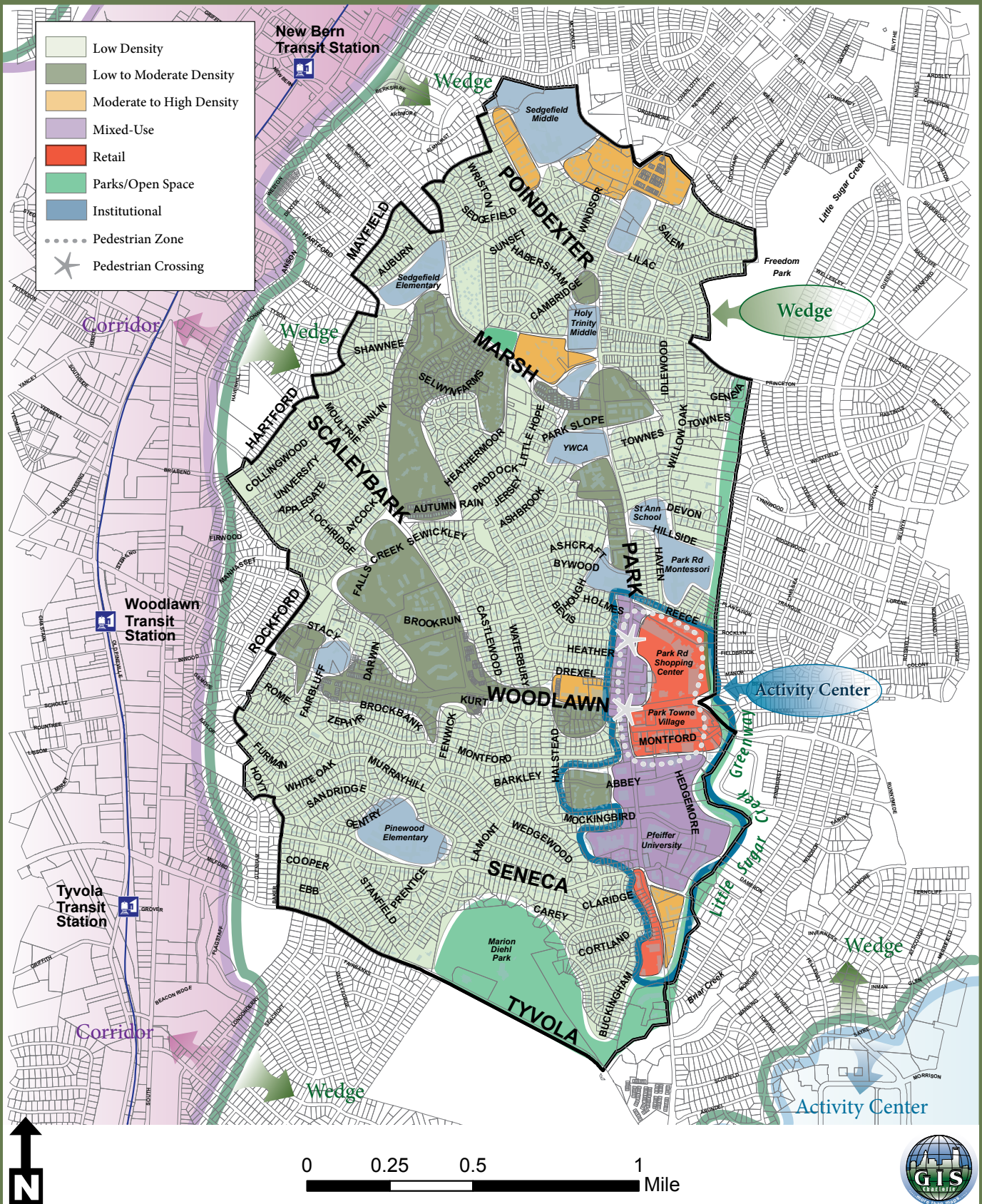
Overall Plan Concept

Taking the vision statement and goals into consideration, **Map 2** illustrates the recommended development pattern for the plan area. The basic premise is to guide growth and development in a way that protects the area's unique character, yet allows for appropriate infill development that compliments the existing built-up environment. The plan strategically places mixed use and moderate to high density residential development within the Mixed-Use Activity Center, and mainly low density and moderate density residential in the Wedge area. The plan concept also highlights pedestrian zones, and locations for improved pedestrian crossings within the Activity Center. The concept plan is further detailed in the following Land Use, Community Design, Transportation, Infrastructure & Public Facilities, and Natural Environment policies.



View headed south of Park Road Activity Center.

Park Woodlawn Area Plan Concept Map



Produced by Charlotte-Mecklenburg Planning Department: October 5, 2012

Plan Policies

Overview

The following sections set forth plan policies for land use, transportation, community design, public facilities, as well as the natural environment to realize the vision and goals for the plan area. Goals for each section are briefly summarized, followed by the recommended plan policies. Plan policies will guide future land use, zoning and other growth and development/redevelopment decisions.

Land Use

This section establishes future land use policies for the area and recommends appropriate land uses to realize the plan vision. In developing these policies, transportation and community design issues were also considered and incorporated as appropriate. However, separate sections for transportation and community design policies follow and build upon the policies discussed in the land use section.

GOAL

Maintain and preserve the area's predominant neighborhood character, along with strengthening and enhancing the strong neighborhood business center. Future developments should be sensitive to the surrounding neighborhood. These developments should enhance the neighborhoods by building upon the area's assets and improving the pedestrian environment by providing more connections between the residential wedge areas, and the mixed use activity center.

The land use policies are organized into two major geographic areas: Wedge and Activity Center. Each policy is numbered, and the specific location noted on **Map 3** or **Map 4**. The policies also are cross referenced by numbers in the Implementation Guide. The plan area incorporates Park Road / Woodlawn Road mixed-use activity center, with the majority of the area included in the wedge. Two areas of the Activity Center are recommended for reclassification as part of the Wedge due to their recommended characteristics. These are further discussed in policies L-11 and L-14.

1. ACTIVITY CENTER

Activity Centers are focal points of economic activity typically planned for concentrations of compact development. Many existing Activity Centers are appropriate locations for significant new growth along with enhancements to the supporting infrastructure. There are three types of Activity Centers: Center City, Mixed Use Centers, and Industrial Centers. The Park Woodlawn center is a Mixed Use Center and it is anticipated that the area will be developed far more intensely than it is currently. The projected new growth is because there are no more vacant parcels left within the activity center; any new development will need to redevelop existing

properties at higher intensities; and the surrounding demographics are favorable for more businesses wanting to locate there. However the land use, transportation and community design policies set forth in this document will continually emphasize that new development needs to be neighborhood focused; that is, buildings need to be sensitive to the surrounding single family neighborhoods in terms of height and design, and need to encourage pedestrian activity by providing inviting places for pedestrian such as public plazas, open spaces and active ground floor uses. See **Map 3: Recommended Future Land Use** for the Activity Center.

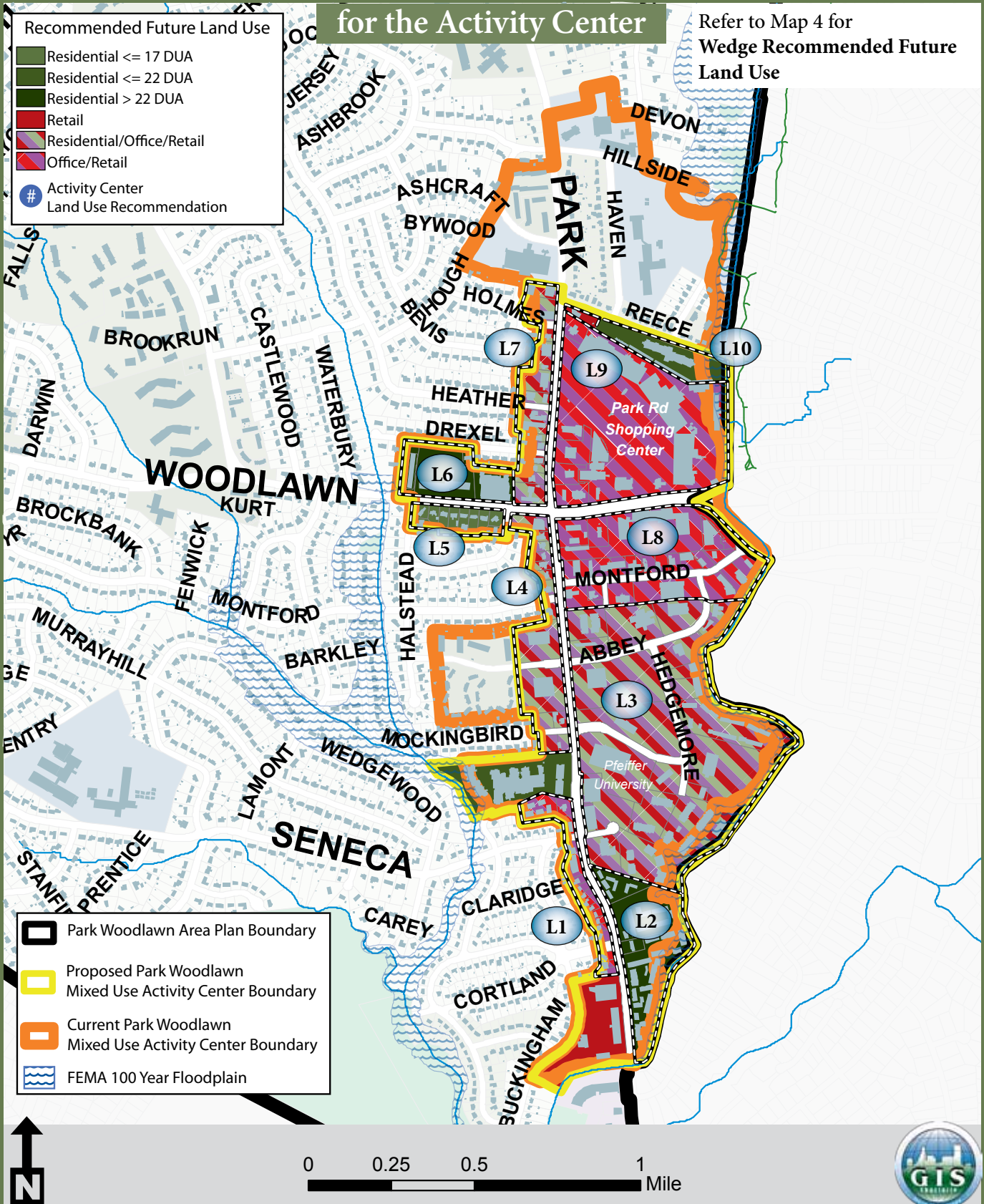
LAND USE POLICIES FOR THE ACTIVITY CENTER

L-1: Park Road, west side, from near Cortland Road to near Seneca Place: Support office and retail uses. The Existing office and retail properties located along the west side of Park Road just north of the Harris Teeter shopping center and Cortland Road are recommended for office and/or retail uses; however new developments should follow the design policies in the Community Design section. If all the parcels in the block are assembled, a multi-family development would be appropriate at a density not to exceed 12 dwelling units per acre (DUA). The proposed development would need to follow the design policies in the Community Design section, and be especially sensitive to the single family neighborhood behind them along Buckingham Drive; it should also be at a height not to exceed 40 feet. Assembling parcels from the wedge neighborhoods into a larger

Park Woodlawn Area Plan Recommended Future Land Use

for the Activity Center

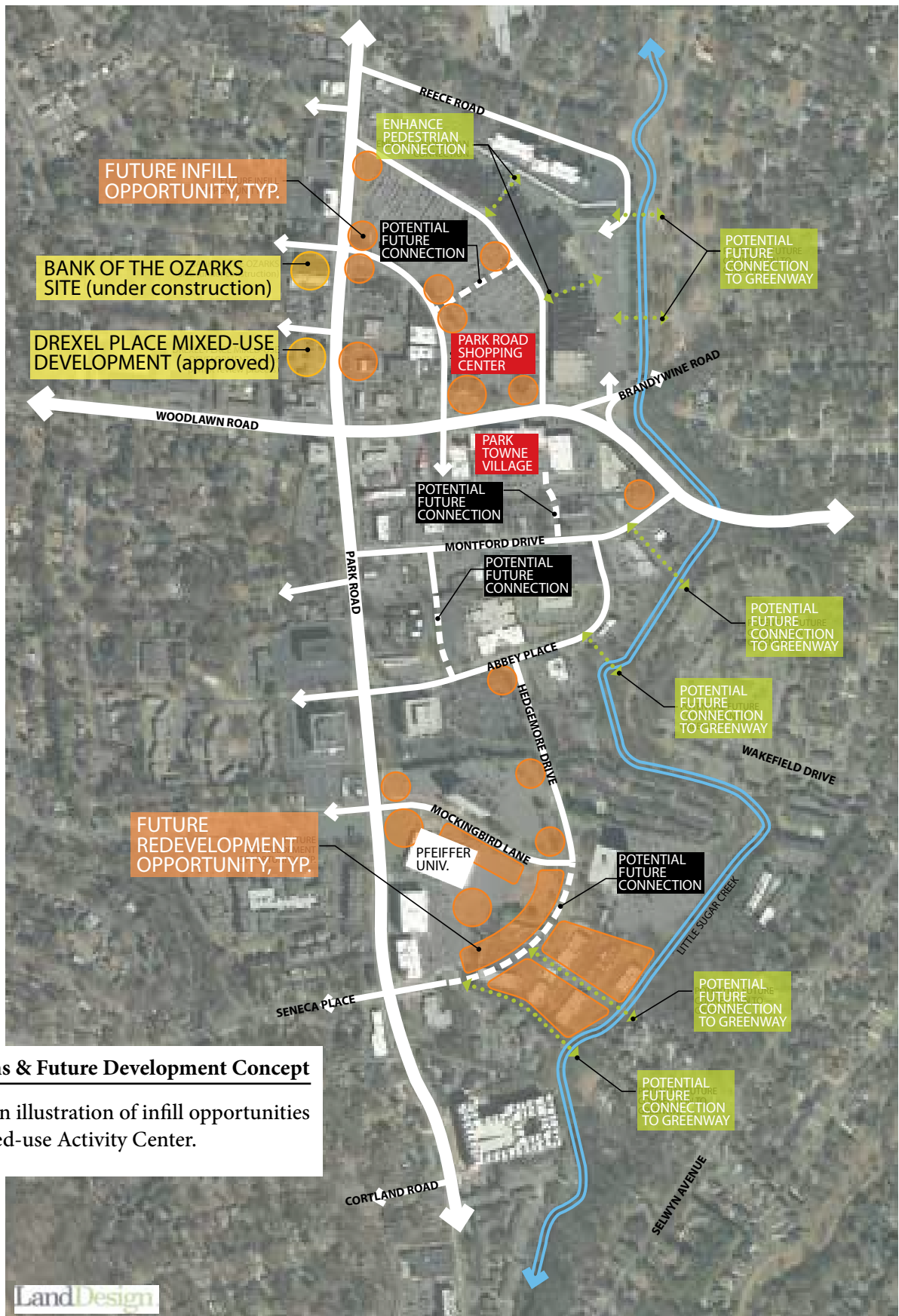
Refer to Map 4 for
Wedge Recommended Future
Land Use



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Park Woodlawn Area Plan Land Use

Charlotte-Mecklenburg
Planning Department



development, while not encouraged, would be reviewed on a case by case basis.

L-2: Park Road, east side, south of Seneca Place: Support the existing high density residential. Moderate to high density residential greater than 22 DUA is appropriate at this location under the design guidelines stipulated in the Community Design section of this document. One of the parcels in this area is already zoned for Multi-Family Residential up to 43 DUA, and one parcel is developed at that density.

L-3: Park Road, east side area from near Abbey Place to near Seneca Place: Support a mix of uses (Residential / Office and/or Retail). Mixed use (or mix of uses Residential / Office and/or Retail) land uses are appropriate for the parcels on the east side of Park Road from around Seneca Place to Abbey Place. Single use or multi-use is appropriate as long as the Community Design policies are adhered to. Future developments should reduce the amount of surface parking lots and help establish the area as part of the core of the mixed-use activity center. Emphasis should be given to creating walkable and pedestrian friendly developments with active ground floor uses, along the streets, creating a denser development pattern, however keeping it at an appropriate neighborhood scale. There is a 10-story building located in this area currently on Mockingbird Lane, new building heights would be limited to what is permitted in the zoning ordinance.

L-4: Park Road, west side from Woodlawn Road to Mockingbird Lane:

Support a mix of uses (Residential / Office and/or Retail). Mixed use or multi use is appropriate for the parcels on the west side of Park Road from Mockingbird Lane to Woodlawn Road; multi-family would also be appropriate with building heights limited to 40 feet, and specific detail should be given to design (guidelines stipulated in the Community Design section of this document should be adhered to). Assembling parcels from the wedge neighborhoods into a larger development, while not encouraged, would be reviewed on a case by case basis.

L-5: Parcels fronting Woodlawn Road on the south side, between Halstead and Drexmore:

Residential up to 17 DUA is appropriate for these parcels, however only if all the parcels are assembled together. Non-residential uses would not be appropriate here because the parcels are too shallow and would not provide enough of a buffer with the single family homes behind them. Assembling parcels from the wedge neighborhoods into a larger development, while not encouraged, would be reviewed on a case by case basis.

L-6: Parcels fronting Woodlawn Road on the north side from near Waterbury Drive to near Park Road (Woodlawn House):

Support moderate to high density residential. This property is already approved for residential density greater than 22 DUA. If the property were to be proposed for a different redevelopment this density would be appropriate if the development

is at a scale compatible with single family neighborhood behind it. A compatible scale would mean that the height of the building is not taller than 40 feet or four stories (whichever is greater), and the mass of the building is not overwhelming to the single family houses behind it, fronting Drexel Place.

L-7: Parcels fronting Park Road on the west side, from Woodlawn Road to near Holmes Drive.

Support a mix of uses (Residential / Office and/or Retail). Mixed use or multi use is appropriate for the parcels on the west side of Park Road from Woodlawn Road to Holmes Drive. Multi-family would also be appropriate however building heights should be limited to 40 feet, and the design guidelines stipulated in the Community Design section of this document should be adhered to, especially when immediately backing up to low density residential. Assembling parcels from the wedge neighborhoods into a larger development, while not encouraged, would be reviewed on a case by case basis.

L-8: Montford Drive from Park Road to Woodlawn Road.

Montford Drive has evolved into an entertainment district with its concentration of restaurants, bars and entertainment venues. However the area is experiencing some growing pains in terms of accommodating its new found success. Parking at times, especially at peak hours on weekend nights is becoming an issue, and improving the aesthetics of the street with wider sidewalks, pedestrian lighting and street trees would help. The future land use vision for Montford Drive and its surrounding

area is to continue with retail and office land uses, with an emphasis on improving the pedestrian experience by improving sidewalks, public spaces and adding street trees. New developments need to follow the community design guidelines for Non-Residential mixed use development. Moderate to high density residential and/or hotel development would be appropriate only with active non-residential ground floor uses that do not disrupt the pedestrian experience on Montford Drive.

L-9 Park Road Shopping Center. The Park Road Shopping Center has been a long standing iconic symbol for the area and the centerpiece of its identity. The continued use of the area for retail/office is supported by this plan. However, in recognizing the vastness of the shopping center parking lot, and the potential future demand for building on those parking lots, continued retail uses would be appropriate. In the event that residential development becomes a potential use, moderate to high density residential uses would be appropriate only if the ground floor contains retail and/or office uses.

L-10 Adjust Mixed-Use Activity Center Boundary. Adjust boundary of the Park Road Mixed-Use Activity Center at Reece Road and Park Road moving the Park Road Montessori School, St. Ann Catholic School and the residential areas surrounding them along Haven Drive and Arbor Lane into the Wedge. See **Map 3**. Also adjust the Mixed Use Activity Center boundary at Abbey Place just west of Park Road, bringing the multi-

family development Abbey Place Apartments into the Wedge. See **Map 3**.

2. WEDGE (Neighborhoods)

The Wedge areas are for the most part predominately low density mainly single family, with multi-family development at key locations. The Park Woodlawn Wedge area contains strong stable neighborhoods such as Sedgefield and Madison Park. The Wedge in the Park Woodlawn area is expected to continue to be mostly single family, along with strategically located multi-family developments. Because the availability of vacant land is scarce, new development is expected to be mainly multi-family or moderate density attached single-family or townhome developments on assembled parcels.

LAND USE POLICIES FOR THE WEDGE

L-11 Area behind Park Road Shopping Center along Reece Road and Arbor Lane. The residential area behind the Park Road Shopping Center includes existing moderate density multifamily development as well as low density single family development. It is appropriate for this development type to remain. In the event that the single family neighborhood along Arbor Lane adjacent to the Park Road Montessori School redevelops to a higher density, it would only be appropriate if all the parcels are assembled with no single family lot left isolated, and the maximum density not exceed 12 DUA.

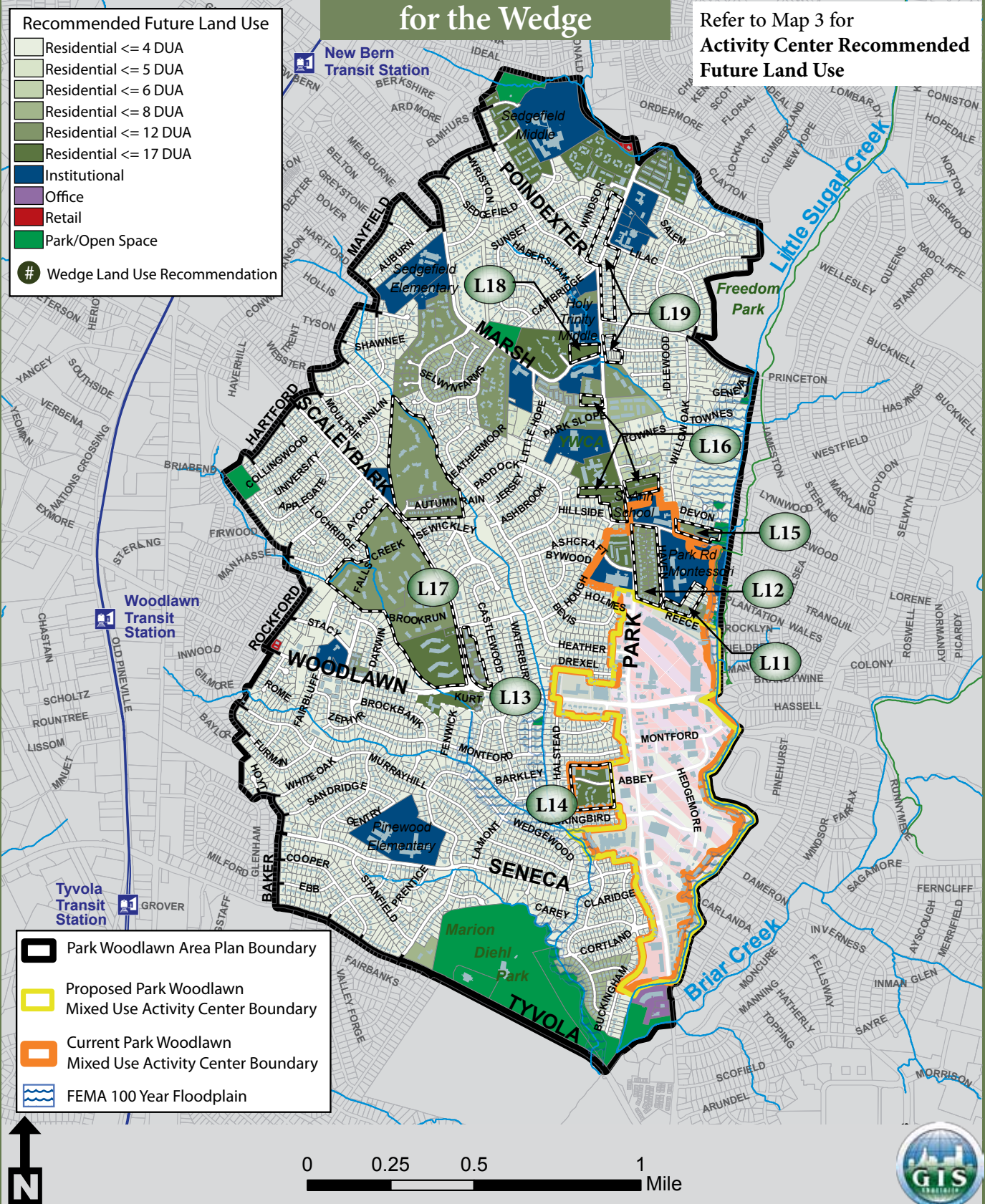
L-12 Park Road on the east side from Reece Road to Hillside Avenue. The area between Reece

Road and Hillside Avenue, and Park Road and Haven Drive contains mainly single family or duplex development and would be appropriate for a moderate density residential development up to 12 DUA; but only if all the parcels are assembled and no single family lot is left isolated. Any new development would need to follow the Community Design guidelines in this plan and ensure that the development is at a scale that is compatible with the school and remaining residential development. Parking should be either on the side or behind buildings, and height should be limited to 40 feet.

L-13 Parcels fronting Woodlawn Road from near Halstead Drive to near Rockford Court. The land use is recommended to remain low density residential up to 4 DUA. However, considering pressure for additional recent multi-family developments in the area, an increase in density up to 12 DUA may be appropriate, but only if parcels are assembled and no single family lot is left isolated. Under those circumstances the following would be applied: Development should be oriented to the public street with parking to the side or the rear of buildings. A townhome style of development is recommended with residential units having individual entrances onto the public street. Heights should be limited to 40 feet to maintain compatibility with the single family homes in the neighborhood. Setbacks should follow what is required in the zoning ordinance for multi-family zoning districts. Urban zoning districts such as Urban Residential and Mixed-Use Development are not appropriate

Park Woodlawn Area Plan Recommended Future Land Use for the Wedge

Refer to Map 3 for
Activity Center Recommended
Future Land Use



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for this portion of Woodlawn Road. All development should follow the recommendations of the Community Design section of this plan.

L-14 Multi-family development on Abbey Place just west of Park Road.

If these properties would redevelop, the current density could remain at 17 DUA, however special emphasis should be placed on ensuring an easy transition between the Mixed Use Activity Center and single family neighborhood behind them. The boundary between the Mixed Use Activity Center and Wedge is to be shifted as part of this plan, to include these parcels in the Wedge instead of the Activity Center. The change in CCW boundary is to ensure that any new proposed development be at a scale compatible with the single family neighborhood behind it.

L-15 Parcel on north side of Hillside Avenue between Willow Oaks Road and Little Sugar Creek.

The parcel along Hillside Avenue and Willow Oak Road is appropriate to be developed for residential uses up to 12 DUA, with strict community design guidelines adhered to, and being extremely sensitive to the single family development behind it on Devon Drive.

L-16 Multiple parcels along Park Road, see Map 4. The single family homes along Park Road are perfectly appropriate to remain in the future. The land use recommendation for this area is to remain predominately low density residential up to 4 DUA with scattered institutional uses, as well as scattered moderate density residential. Along both sides of Park Road just north of the intersection with Hillside Avenue and St. Ann

Catholic School are a number deep single family parcels that if assembled can be developed for moderate density residential up to 17 DUA. This would be appropriate only if all the parcels are assembled and the development's design follows closely the recommendations in the community design policy section of this document. Any new development along Park Road would need to be extremely sensitive in the scale and design of the project to the single family neighborhoods behind them. Setbacks should follow what is required in the zoning ordinance for multi-family zoning districts. Urban zoning districts such as Urban Residential and Mixed-Use Development are not appropriate for this portion of Park Road.

L-17 Scaleybark Road from Woodlawn Road to Hartford Avenue. Existing multi-family developments along Scaleybark Road were built mainly in the 1970's and 1980's. If there is any redevelopment proposed, densities should be maintained to their current levels. There are opportunities for more increases in densities at other locations at either end of Scaleybark Road, such as at the Transit Station Area at South Blvd. and at the Park Woodlawn Activity Center at Park Road and Woodlawn Road.

L-18 Charlotte Housing Authority Property on Marsh Road and Park Road is zoned R-43 MF and could be developed under those entitlements. However if the property is rezoned, a lower density would be more appropriate (less than 17 DUA). Considering the constraints on the site and the surrounding land uses of institutional, moderate density

residential (less than 17 DUA) and single family residential, a development at the current zoning would be overpowering on the site and out of scale with the surrounding neighborhood.

L-19 Single family parcels along Park Road. Maintain low density residential (≤ 4 DUA) in order to be consistent with current zoning..

Community Design

Community design policies help to ensure that new development complements the existing or desired character of the Park Woodlawn community. While design policies alone do not dictate land use or zoning, they can strengthen how various land uses fit together. Policies consider the built environment in conjunction with the natural environment, and also consider how people move through those spaces as pedestrians, bicyclists or automobile drivers.

GOAL

Plan new developments that respect the scale, design, and proximity of adjacent buildings and land uses, especially when impacting single family residential. Good architecture and site design, as well as quality construction materials, should be used to ensure the long-term sustainability and economic vitality of all development regardless of the proposed use.

RESIDENTIAL Design Policies ¹ D-1 Building Architecture and Site Design

Close attention to building and site design serves to enhance the community. Design policies do not result in conformity of design, but provide direction for both new and infill development.	Policy Number	Single Family Detached	Single Family Attached and Multi-Family
<i>Preserve historically or architecturally significant structures.</i>	1A	•	•
<i>Avoid blank walls along pedestrian circulation areas and public rights-of-way.</i>	1B	•	•
<i>Orient building towards internal street network (unless a thoroughfare) or common open space and provide pedestrian access to the street.</i>	1C	•	•
Transition the scale and height of development with respect to adjacent single family neighborhoods	1D	•	•
Multi-family development/redevelopment along Park Road should relate to the surrounding context and have pedestrian scale street presence. Building heights should be limited to 40 feet.	1E		•
Shallow depth lot redevelopment along Park Road should relate to the single family neighborhoods behind. Buildings should orient to Park Road with parking and circulation behind buildings.	1F	•	•
Encourage positive orientation of any proposed buildings toward existing or future greenway.	1G	•	•

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1D Transition the scale and height of development with respect to adjacent single family neighborhoods.



1F Shallow depth lot redevelopment along Park Road should relate to the single family neighborhoods behind. Buildings should orient to Park Road with parking and circulation behind buildings.

RESIDENTIAL Design Policies ¹ D-2 Natural Environment

These policies provide direction to accommodate growth and change while protecting the natural environment on which we depend, including the quality of our air, water and land.	Policy Number	Single Family Detached	Single Family Attached and Multi-Family
<i>Provide a meaningful amount of usable and accessible open space.</i>	2A	•	•
<i>Incorporate functional, unique, natural, and/or historical elements into the open space.</i>	2B	•	•
<i>Preserve steep slopes along streams or adjacent to significant natural landscape features.</i>	2C	•	•
<i>Preserve at least 10% of the site as "tree save area" consistent with residential tree ordinance.</i>	2D	•	•
Provide and encourage accessibility to Little Sugar Creek greenway and other natural amenities through better connected streets, sidewalks, and trails.	2E	•	•
Design open space to create a network of green spaces within new development and redevelopment.	2F	•	•
Use low maintenance native vegetation as much as possible.	2G	•	•
Preserve prominent stands of existing trees, where feasible.	2H	•	•
Design new development and redevelopment to minimize adverse impacts to the existing tree canopy.	2I	•	•
Encourage 'green' site design and building practices to reduce storm water runoff and minimize adverse environmental impacts.	2J	•	•

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2E *Provide and encourage accessibility to Little Sugar Creek greenway and other natural amenities through better connected streets, sidewalks, and trails.*



2F *Design open space to create a network of green spaces within new development and redevelopment.*

RESIDENTIAL Design Policies ¹ D-3 Pedestrian and Vehicular Network

Providing a safe, accessible and comfortable network of streets, sidewalks and pathways for all modes of transportation is an important step in establishing a livable community. The policies below provide guidance for achieving mobility and connectivity for pedestrians, bicyclists and automobile drivers. For additional policies concerning transportation, please see the Transportation section of the plan, as well as the <i>City of Charlotte Urban Street Design Guidelines</i> (2003).	Policy Number	Single Family Detached	Single Family Attached and Multi-Family
<i>Provide pedestrian amenities such as street furniture and pedestrian scale lighting.</i>	3A		•
<i>Provide bicycle parking in appropriate common areas (for example, playground, swimming pools.)</i>	3B	•	•
<i>Provide pedestrian/bicycle connections to nearby parks, greenways, bikeways and trails.</i>	3C	•	•
<i>Provide direct pedestrian and bicycle connections between all abutting or adjacent developments and transit stops.</i>	3D	•	•
<i>Design streets with pedestrian safety and comfort in mind.</i>	3E	•	•
<i>Design street system to calm traffic.</i>	3F	•	•
<i>Align collectors at thoroughfare intersections to promote safe crossings for pedestrians, cyclists and automobile drivers.</i>	3G	•	•
<i>Encourage on-street parking along public and private streets where appropriate to reduce the size of surface parking lots.</i>	3H		•
<i>Provide multiple vehicular entry points.</i>	3I	•	•
Maintain residential street typology and character of existing streets that function traditionally as neighborhood entrances, i.e. Cortland Road and Seneca Place.	3J	•	•

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3C *Provide pedestrian/bicycle connections to nearby parks, greenways, bikeways and trails. .*



3E *Design streets with pedestrian safety and comfort in mind.*



3J *Maintain residential street typology and character of existing streets that function traditionally, as neighborhood entrances, i.e. Cortland Road and Seneca Place.*

NON-RESIDENTIAL Design Policies ¹ D-4 Building Architecture and Site Design

Architecture and landscape design define streets and public spaces as areas of shared use. Streets lined by buildings and trees rather than vast expanses of parking lots provide a visually rich and physically safe environment.

	Policy Number	Retail-Oriented Mixed/Multi Use	Retail	Office	Mixed Use	Industrial
<i>Design buildings with transparent openings, ornamentation, and architectural character.</i>	4A	•	•	•	•	
<i>Establish entrances with pedestrian interest.</i>	4B	•	•	•	•	
<i>Orient buildings towards street and provide pedestrian access.</i>	4C	•	•	•	•	
<i>Arrange buildings in an orderly block pattern.</i>	4D	•	•	•	•	
<i>Discourage tearing down historic or architecturally significant structures.</i>	4E	•	•	•	•	•
<i>Design buildings with human scale and visual interest in mind.</i>	4F	•	•	•	•	
<i>Discourage further of drive-thru development.</i>	4G	•	•	•	•	
<i>Design for pedestrian safety.</i>	4H	•	•	•	•	•
<i>Locate dumpsters and service areas away from residential areas.</i>	4I	•	•	•	•	•
Shallow depth lot redevelopment along Park Road should relate to the single family neighborhoods behind. Buildings should orient to Park Road with parking and circulation behind buildings.	4J	•	•	•	•	
Transition the scale and height of development with respect to single family neighborhoods.	4K	•	•	•	•	
Encourage existing and future uses behind Park Road Shopping Center to address Little Sugar Creek Greenway frontage.	4L	•	•	•	•	•
Create a better sense of entry to Park Road Shopping Center by reinforcing drive intersections on Park Road and Woodlawn Road with architecture and urban open spaces.	4M	•	•	•	•	

Better define a 'center' to the Park Road Shopping Center and create a more comfortable urban environment with public open space and effective pedestrian linkages.

4N

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Define opportunities along Montford Drive for effective urban open spaces/courtyards that can be flexible in their use.

4O

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Better define gateways to Montford Drive at Park Road and Woodlawn Road.

4P

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4J *Shallow depth lot redevelopment along Park Road should relate to the single family neighborhoods behind. Buildings should orient to Park Road with parking and circulation behind buildings.*



4K *Transition the scale and height of development with respect to single family neighborhoods.*

Park Woodlawn Area Plan Community Design



NON-RESIDENTIAL Design Policies ¹ D-5 Pedestrian and Vehicular Network

Circulation in and through a site should be comfortable, safe, secure and efficient for pedestrians, bicyclists and automobile drivers.

	Policy Number	Retail-Oriented Mixed/Multi Use	Retail	Office	Mixed Use	Industrial
Create an interconnected sidewalk system.	5A	•	•	•	•	•
Design short blocks with an organized street pattern.	5B	•	•	•	•	•
Provide pedestrian/bicycle connections to nearby parks, greenways, bikeways and trails.	5C	•	•	•	•	•
Design streets with pedestrian safety and comfort in mind.	5D	•	•	•	•	•
Provide ample sidewalk width to accommodate pedestrian circulation, congregation, outdoor dining, and amenities such as street furnishings, trash receptacles, art work and trees.	5E	•	•	•	•	•
Establish clear "way-finding" signage for pedestrians and automobile drivers.	5F	•	•	•	•	•
Provide safe pedestrian circulation throughout the development, including through parking lots and decks.	5G	•	•	•	•	•
Provide safe and secure transit waiting facilities.	5H	•	•	•	•	•
Establish a central vehicular access into the site from the more auto-oriented street and provide secondary access options from the minor streets.	5I	•	•	•	•	•
Integrate landscaping with seating along facades when possible and, when practical, work to integrate the existing tree canopy into the site design.	5J	•	•	•	•	•
Encourage shared driveways and alleys within the development.	5K	•	•	•	•	•
Provide bicycle parking and storage areas.	5L	•	•	•	•	•
Design parking lots on a street/block pattern to minimize large surface lots.	5M	•	•	•	•	•
Minimize large surface parking lots with smaller pods and extensive landscaping.	5N	•	•	•	•	•
Encourage commercial or residential uses in parking decks fronting pedestrian circulation areas.	5O	•	•	•	•	•
Provide structured parking where feasible to conserve land and minimize surface parking.	5P	•	•	•	•	•
Reduce driveway cuts along Park Road and Woodlawn Road (Route 4), especially at the Park Road shopping center to increase ease of pedestrian movement.	5Q	•	•	•	•	•
Introduce pedestrian signals and/or pedestrian refuge islands along Park and Woodlawn Roads near the Park Road Shopping Center and Park Towne Village for better neighborhood and pedestrian connectivity.	5R	•	•	•	•	•
Improve Montford Drive streetscape experience by adding wider sidewalks with planting strips and defining onstreet parking (parallel, head-in angled, or reverse-in angled parking).	5S	•	•	•	•	•
Provide better interconnectivity, pedestrian and/or vehicular, between Park Towne Village and Montford Drive experience, as well as between Montford Drive and Abbey Place.	5T	•	•	•	•	•

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PARK TOWNE VILLAGE

PARK ROAD

SCULPTURAL GATEWAY

DRIVEWAY CONNECTION

SIDEWALK

EXISTING OUTDOOR PATIO

EXISTING PED. CONNECTION

BRAZILWELLS

NEW DRIVEWAY CONNECTION

NEW DRIVEWAY CONNECTION

MOOSEHEADS

DENTIST

MONTFORD DRIVE

DIAGONAL ON STREET PARKING

DRIVEWAY CONSOLIDATION

RE-STRIPED PARKING

PARK LANES

WELLS FARGO

ANGRY ALES

ANDREW BLAIR'S

SHARED PARKING

DRIVEWAY CONNECTION

NEW STREET EXTENSION

ABBOT PLACE

FUTURE TRAIL

LITTLE SUGAR CREEK GREENWAY

TACO BELL

GREENWAY CONNECTION

FUTURE REDEVELOPMENT

CLYDE HILL TOWN

LandDesign

25

NON-RESIDENTIAL Design Policies ¹ D-6 Freestanding Single Tenant Buildings

Freestanding single tenant buildings (out parcels) can be classified as either independent or interconnected. Independent buildings function separate of the surrounding complex while interconnected buildings share parking and site amenities with other buildings in the same complex whether independent or interconnected. The design of freestanding single tenant buildings should be sensitive to the surrounding neighborhood, both built and natural environments.	Policy Number	Retail-Oriented Mixed/Multi Use	Retail	Office	Mixed Use	Industrial
<i>Design buildings to relate to the scale, height and configuration of the center.</i>	6A	•	•	•	•	•
<i>If drive-thru(s) are included, they must not compromise pedestrian circulation.</i>	6B	•	•	•	•	•
<i>Design to facilitate walking to the free-standing building from other buildings within the center. Provide safe pedestrian pathways and crossings without creating conflicts with automobiles.</i>	6C	•	•	•	•	•
<i>Design the site so that dumpsters, service areas or auxiliary storage do not interfere with, and are not visible from the pedestrian circulation area and do not negatively impact surrounding residential areas.</i>	6D	•	•	•	•	•
<i>The site layout should be clustered in a village arrangement around shared amenities.</i>	6E	•	•	•	•	•
Design buildings to address the primary street with minimized setback wherever possible.	6F	•	•	•	•	•
Encourage 'green' site design and building practices, especially to reduce storm water runoff and provide other environmental benefits.	6G	•	•	•	•	•

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6C *Design to facilitate walking to the freestanding building from other buildings within the center. Provide safe pedestrian pathways and crossings without creating conflicts with automobiles.*



6F *Design buildings to address the primary street with minimized setback wherever possible.*

Park Woodlawn Area Plan Community Design



NON-RESIDENTIAL Design Policies ¹ D-7 Natural Environment

Site development should respect the natural environment.	Policy Number	Retail-Oriented Mixed/Multi Use	Retail	Office	Mixed Use	Industrial
<i>Preserve steep slopes along stream beds or adjacent to significant natural landscape features.</i>	7A	•	•	•	•	•
<i>Provide a meaningful amount of usable and accessible common open space and integrate the tree canopy into it.</i>	7B	•	•	•	•	•
<i>Consider using pervious pavement for overflow parking.</i>	7C	•	•	•	•	•
<i>Avoid piping creeks and minimize channelization. Use a bridge, rather than a culvert at existing creeks when possible.</i>	7D	•	•	•	•	•
<i>Retain existing landscaping when possible. Mass clearing is not preferable and existing tree canopy should be preserved where practical.</i>	7E	•	•	•	•	•
Provide and encourage accessibility to Little Sugar Creek greenway and other natural amenities through better connected streets, sidewalks, and trails.	7F	•	•	•	•	•
Design open space to create a network of green spaces within new development and redevelopment.	7G	•	•	•	•	•
Use low maintenance native vegetation as much as possible.	7H	•	•	•	•	•
Design new development and redevelopment to minimize adverse impacts to the existing tree canopy.	7I	•	•	•	•	•
Encourage 'green' site design and building practices to reduce storm water runoff and minimize adverse environmental impacts.	7J	•	•	•	•	•
Encourage the reduction of heat island effect of large unshaded commercial parking areas by introducing canopy trees and landscape areas.	7K	•	•	•	•	•

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7G *Design open space to create a network of green spaces within new development and redevelopment.*



7K *Encourage the reduction of heat island effect of large unshaded commercial parking areas by introducing canopy trees and landscape areas.*

Transportation

Streets connect people to places. As the public face of a neighborhood, streets can set the tone for the surrounding environment and create a sense of community. The Park Woodlawn Area is characterized by an Activity Center surrounded by established Neighborhoods. The area's streets should reflect and support a pedestrian-scaled environment that encourages walking while also supporting other modes of transportation.

GOAL

Support and develop a connected, affordable, balanced transportation system that encourages walking. It should safely and sensibly accommodate pedestrians, bicyclists, and transit users while allowing for neighborhood-friendly automobile speeds for motorists and business patrons traveling to and through the plan area.

The purpose of this section is to provide policy guidance that will both protect Neighborhoods and enhance the Activity Center. These recommendations include future City-sponsored capital improvements, as well as improvements facilitated by private investment during the land development process. The general location of these recommendations is shown for both the Activity Center and the Neighborhoods. The recommendations are also cross-referenced to item numbers in Volume 2: Implementation Guide section of this plan.

1. ACTIVITY CENTER

The Park Woodlawn Activity Center serves as the commercial nexus and community meeting place for multiple Charlotte neighborhoods. The Center is envisioned to retain this role, while continuing to evolve into a more walk-friendly place. Since Activity Centers are expected to serve as hubs of commercial, entertainment, and community activities, it is particularly important to establish very strong linkages to and within the Activity Center for pedestrians, bicyclists, transit users, and motorists. The following policies serve to strengthen the Activity Center. See **Map 5: Future Transportation Network** for the Activity Center, page 31.

TRANSPORTATION POLICIES FOR THE ACTIVITY CENTER

T-1 Create new local streets in the Activity Center. Additional connections will provide a more robust system of local streets, which offer alternatives to thoroughfares

for shorter trips. Smaller blocks also support denser development and pedestrian accessibility within an Activity Center. As redevelopment occurs, new local streets should connect:

- Woodlawn Road and Montford Drive;
- Montford Drive and Abbey Place;
- Woodlawn Road and Drexel Place;
- Seneca Place and Carlanda Circle; and
- Park Road and Buckingham Drive.

T-2 Support the viability of Montford Drive as a destination Main Street. Montford Drive has emerged as a destination for dining and entertainment. The street is envisioned to continue this role but faces some challenges in shared parking and pedestrian comfort. Streetscape enhancements, including additional on-street parking, are recommended to help retain and strengthen this business district.



Montford Drive at Park Road

T-3 Enhance the intersection of Park and Woodlawn Roads. This intersection serves as both a gateway to, and critical junction within, the Activity Center. The existing intersection is congested at peak periods for motorists and presents a challenging environment for pedestrians and cyclists. Additional turn lane capacity and multi-modal enhancements could improve the safety and operations of this key intersection for all users. Addressing congestion at this intersection will require consideration of operational, network-based or, potentially, turn lane capacity improvements. Any physical capacity improvements will require further analysis. Any physical changes should also increase the functionality of the intersection for pedestrians and bicyclists. Therefore, improvements may include a combination of geometric design changes, pedestrian islands, directional curb ramps, and additional or longer turn lanes.

T-4 Facilitate cross-access and parallel connections to Park Road. Short-distance trips originating near and along Park Road should have alternate routes other than this major thoroughfare. New development should incorporate multiple access points, secondary driveways located on side streets off Park Road, cross-access driveways, and parallel street connections.

T-5 Provide additional pedestrian crossings of thoroughfares. Create additional opportunities to cross Park and Woodlawn Roads within the Activity Center via mid-block or non-signalized crossings at or near the following locations:

- Park Road at Park Road Shopping Center Drive;
- Park Road at Montford Drive;
- Park Road at Mockingbird Lane;
- Park Road at Park-Selwyn Terrace;
- Woodlawn Road at Park Towne Village; and
- Woodlawn Road at significant redevelopment sites (usually in conjunction with a left turn lane).

T-6. Add new signalized intersections to enhance access and circulation. Provide additional access for all users by signaling the following intersections:

- Park Road and Reece Road, provided redevelopment along Reece Road and access to Park Road Shopping Center;
- Woodlawn Road and Montford Drive (include pedestrian refuge); and
- Park Road and Cortland Road or Park Road and Park-Selwyn Terrace, provided either location also connects Buckingham Drive and Park-Selwyn Terrace.

T-7 Create multiple connections to Little Sugar Creek Greenway.

The County's Greenway Master Plan will ultimately extend the Greenway south of its present terminus at Brandywine Road through the Activity Center. This route will provide grade-separated crossings of Brandywine Road and Woodlawn Road at the Creek. The following connections are recommended to provide more access points between key nodes in the Activity Center and the Greenway:

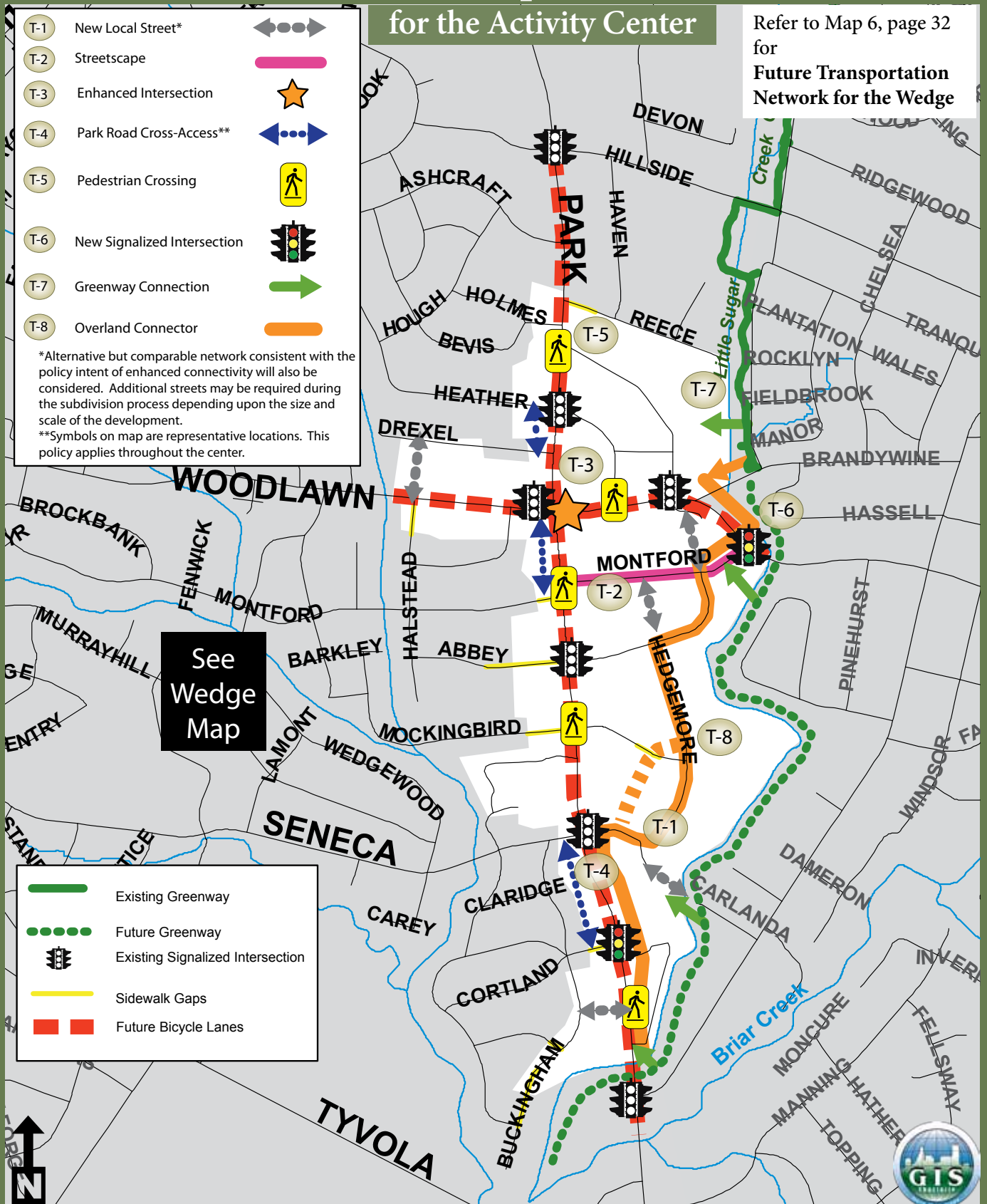
- Bike-Ped bridge to Park Road Shopping Center north of Brandywine Road;
- Bikeshare station at or near Park Road Shopping Center;
- Wider sidewalk along Brandywine Road and re-design of the island at Woodlawn Road;
- Bike-Ped bridge to Abbey Place near Montford Drive;
- Bike-Ped bridge or street connection at Carlanda Circle and Seneca Place extension; and
- Enhanced intersection of Park Road at Selwyn Avenue.



Proposed pedestrian and bike access from Little Sugar Creek Greenway will link to Activity Centers

Park Woodlawn Area Plan Future Transportation for the Activity Center

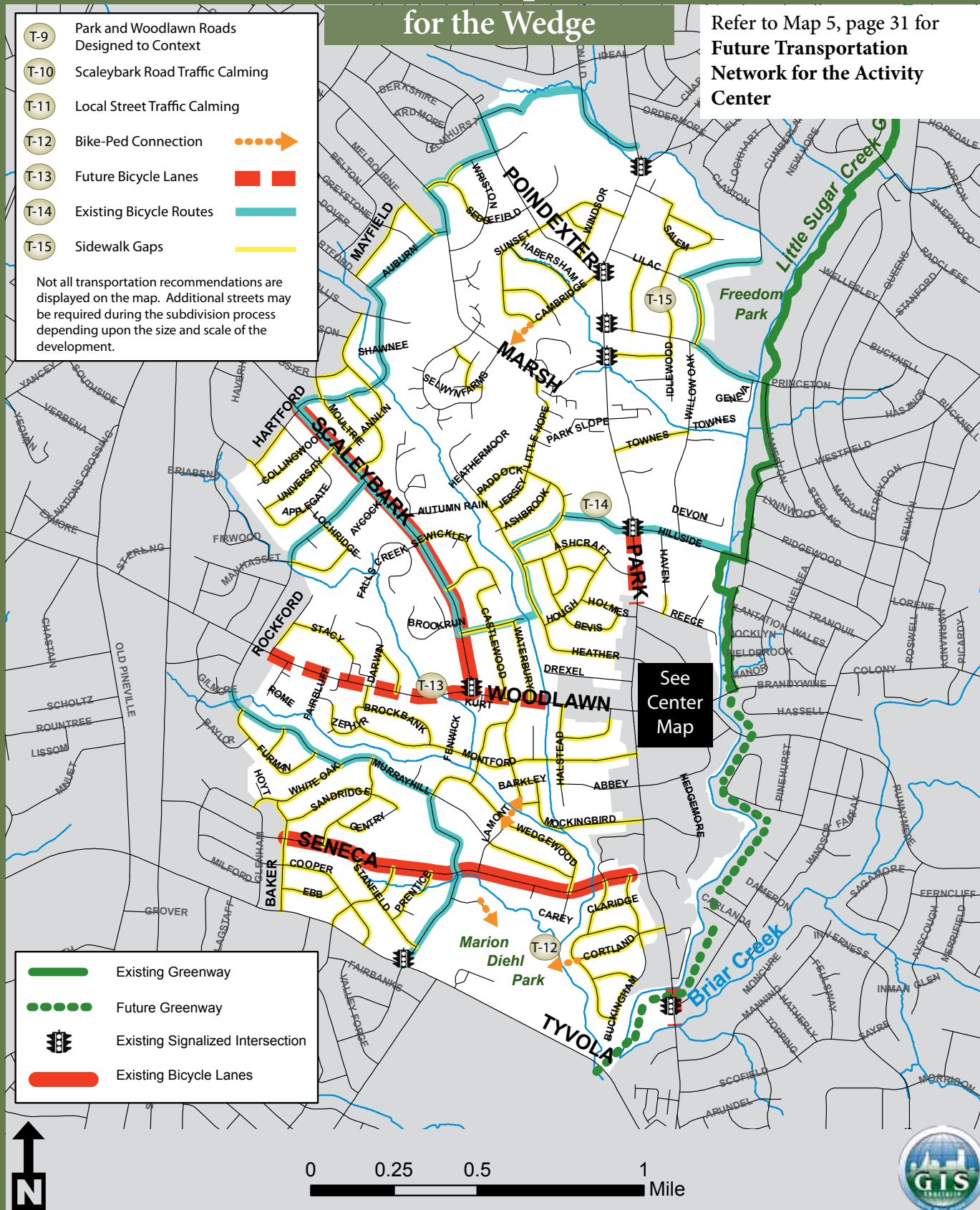
Refer to Map 6, page 32
for
Future Transportation
Network for the Wedge



Produced by Charlotte Mecklenburg Planning Department: October 2, 2012

Park Woodlawn Area Plan Future Transportation for the Wedge

Refer to Map 5, page 31 for
Future Transportation
Network for the Activity
Center



Produced by Charlotte Mecklenburg Planning Department: October 2, 2012

T-8 Provide an overland connector parallel to incomplete segments of the Little Sugar Creek Greenway.

The County's Master Plan will ultimately extend the Greenway south of its present terminus at Brandywine Road. However, this particular segment of planned Greenway along the edge of the Center may not be implemented until the distant future. In the interim, a combination of local streets and side paths could provide an overland connector parallel to the Creek within the Center. Between Brandywine Road and Montford Drive, this overland connector is envisioned as a side path along the northeast side of Woodlawn Road. From the new signal at Montford Drive and Woodlawn, the connector can follow local streets and development paths (i.e. Pfeiffer campus) within the Center (see map) to connect users over to

the signal at Seneca Place and Park Road. South of Seneca Place, a side path is envisioned on the east side of Park Road. At Selwyn Avenue, users of the connector would cross Park Road and connect with the planned segment of Greenway, for which the County has begun design.

2. WEDGE (Neighborhoods)

Streets within the Wedge neighborhoods should be comfortable to walk and support residents' expressed desire for better connections within the Neighborhoods and to the nearby Center. Thoroughfares will continue to balance the need for moving significant volumes of traffic, while equally addressing the overall pace of this traffic. Achieving balance between corridor mobility and localized livability is necessary to sustain the attractiveness of the area for living, working, and visiting,

as well as to provide safe and comfortable access for residents, workers, and visitors, no matter their travel mode of choice. See **Map 6: Future Transportation Network** for the Wedge, page 32.

TRANSPORTATION POLICIES FOR THE WEDGE

T-9 Improve the neighborhood context of Park and Woodlawn Roads. These thoroughfares will continue to carry significant traffic volumes, but it is critical that, moving forward, the pace of traffic on these streets support the area's livability. The development patterns along and the street design of these "Avenues" should always strive for a safer and more comfortable environment for residents, employees, and visitors, whether they are walking, bicycling,



Local street in wedge neighborhood

using transit, or driving. The following strategies are intended to promote appropriate traffic speeds, particularly when implemented together:

- a. Use signal spacing and timing to pace traffic at a level appropriate to the area;
- b. Consider adding signals at more intersections on Park and Woodlawn Roads throughout the area to allow easier access from side streets and affect the overall pace of traffic (e.g., Woodlawn at Murrayhill);
- c. Design intersections to encourage/support easier crossings by bicyclists and pedestrians;
- d. Provide streetscapes that will encourage and support more bicycle and pedestrian travel in the area (sidewalks, planting strips to buffer sidewalks from traffic, street trees, e.g.); and
- e. Provide more crossing opportunities (in addition to signalized intersections) to encourage and support bicycle and pedestrian travel in the area (mid-block and at non-signalized intersections).

T-10 Implement design concepts for calming and enhancing Scaleybark Road. The street was recently studied with extensive public input. Several design concepts emerged from the study, which could be implemented in phases. The various concepts would reduce speed and improve corridor aesthetics, safety and operations by:

- a. Reducing the posted speed limit, design speed, and operating speed to 30mph in order to increase safety and the perception of safety;
- b. Making all proposed changes to the street aesthetically pleasing;
- c. Making the street equally comfortable for pedestrians, cyclists, and motorists;
- d. Reducing storm water run-off; and
- e. Reducing speeding and aggressive driving behavior.

T-11 Calm traffic on Local Streets to Support Livable and Walkable Neighborhoods. Traffic calming for neighborhood streets is typically

provided through petition-based requests for calming on specific streets or blocks. However, other types of traffic calming tools can also be applied during other types of projects or through redevelopment, including:

- a. Re-aligning intersections to reduce turning radii (thereby slowing turning vehicles);
- b. Continuing to plant street trees as part of sidewalk projects (when planting strip widths are sufficient); and
- c. Providing curb extensions with dedicated on-street parking, where any new street connections might occur through development.

T-12 Create new bicycle-pedestrian connections. The existing bicycle-pedestrian connection between Belrose Lane and Heather Lane serves as a model for other potential connections. These connections will support accessibility for neighborhood residents by providing more travel choices. Additional locations to consider similar bicycle-pedestrian locations include:

- a. Cambridge Road to E. B. Moore Park;
- b. Lamont Drive to Longwood Drive;
- c. Keystone Court to Marion Diehl Park; and
- d. Valley Stream Road to Marion Diehl Park.

T-13 Extend bicycle lanes along Woodlawn Road. A past City capital project resulted in bicycle lanes along Woodlawn Road west of Murrayhill Road. Long-term, a new curb line will be required of



Create new bicycle-pedestrian connections.

development along Woodlawn Road, especially when moving the curb for other needs, such as left turn lanes. If not needing to move the curb at the development, a wider planting strip with trees offset from the curb and closer to the sidewalk should be provided, in order to preserve the space for future bicycle lanes. Short-term, CDOT may pursue interim strategies, such as wider outside lanes, share-the-road pavement markings and/or signage along the street.

T-14 Provide alternative bicycle routes to Park Road. New bicycle lanes are largely impractical along much of Park Road in the Wedge, given the physical constraint of established curb lines. Area bicycle strategies should include signing parallel routes as alternatives to Park Road.

T-15 Eliminate gaps in the sidewalk system. Some neighborhood streets do not have sidewalks because

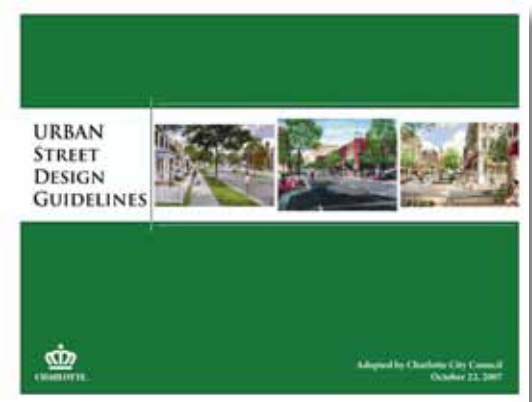
they were built prior to standards requiring them. The City's Sidewalk Program could help to slowly address some of these gaps, by prioritizing and implementing continuous and direct routes that link residential areas to parks, schools and shopping. If implemented via the Sidewalk Program, identified sidewalks on Map 6 would require residential support through a petition-based process prior to implementation. Other City programs may assist with sidewalk construction as well.

Street Cross-Sections

The streetscape cross-sections and development standards on the following pages will help shape the character of the future street network. Future cross-sections have been determined for plan area streets except for streets where few changes to existing conditions are anticipated, primarily in residential neighborhoods. These cross-sections have been developed in accordance with the *Urban Street Design Guidelines (USDG)*, adopted by City Council in 2007. Map 7, Streetscape Cross-Sections, indicates where each of the cross-section types are located, referenced by number.

The streetscape cross-sections specifically define the character and width of the area behind the curbs, including accommodations for sidewalks, landscaping, and pedestrian amenities as well as building setback guidelines. The cross-sections also illustrate the future character of the street inside the curbs, visualizing the conceptual location and number of travel lanes, bicycle lanes, transit, and provisions for on-street parking.

The dimensions on the streetscape cross-section diagrams reflect typical street sections at mid-block locations. They will be used by the Charlotte Department of Transportation (CDOT), in consultation with the Charlotte-Mecklenburg Planning Department, to determine the location of the ultimate curb lines. If variations from the typical street sections are needed to address physical conditions, enhance operations, or better meet the intent of this Area Plan, such variations may be identified during future design and analysis phases.



Urban Street Design Guidelines also address integrating land use and transportation to create synergy between the streets and land uses adjacent to them

When this plan is adopted, the streetscape standards specified herein will become the official “Streetscape Plan” for the plan area, as referenced in the Charlotte *Zoning Ordinance*. As such, future development zoned MUDD, NS, UR, TOD, TS, PED, UMUD, or other urban zoning districts that may be established, must be designed in accordance with these standards. Future development not zoned to one of these urban districts is not required, but may be requested, to construct the new street cross-section elements along with any other applicable regulations. See **Map 7**.

Street Types

Recommended street types correspond to the street types identified in the USDG

AVENUES

Avenues can serve a diverse set of functions in a wide variety of land use contexts. They are the most common non-local street type in the plan area, as well as in Charlotte. Avenues provide access from neighborhoods to commercial areas, between areas of the city, and in some cases, through neighborhoods. Avenues are designed to provide a balance of service for all users, but with special emphasis on pedestrians and localized transit services. Avenues may also have options for on-street parking and dedicated bicycle lanes. Avenues in the plan area have differing recommended cross-sections according to location. These include:

- A1-Scaleybark Road
- A2-Park Road (*Kenilworth Avenue to Hillside Avenue*)
- A3-Park Road (*Hillside Avenue to Seneca Place*)
- A4-Park Road (*Seneca Place to Briar Creek*)
- A4-Woodlawn Road (*Rockford Court to Brandywine Road*)
- A5-Woodlawn Road (*Brandywine Road to Montford Drive*)
- A6-Tyvola Road (*Baker Drive to Farmbrook Drive*)

BOULEVARDS

Boulevards are intended to move large numbers of vehicles from one part of the city to another, and to other lower level streets in the network. Thus, the modal priority shifts toward motor vehicles, while still accommodating pedestrians and cyclists as safely and comfortably as possible. One segment of Tyvola Road is classified as a Boulevard:

B1-Tyvola Road (*Farmbrook Drive to Little Sugar Creek*)

Main Streets are destination locations

MAIN STREETS

that provide access to and function as centers of civic, social and commercial activity. Main Streets are designed to be pedestrian-oriented to complement the development next to the street. Although the streets also serve transit, bicyclists, and automobiles, Main Streets are designed to provide the highest level of comfort, access, and security to pedestrians. A key segment of Montford Drive is recognized as a Main Street:

M1- Montford Drive (*Park Road to Abbey Place*)

LOCAL STREETS

Local Streets provide access to residential, commercial, or mixed use districts. The majority of Charlotte’s streets are classified as local streets and are typically built as development occurs. Local Streets within the Wedge neighborhoods are likely to remain unchanged and therefore a specific cross-section is not provided for them. Cross-sections are provided for three conditions in the plan area:

S1-Overland Connector

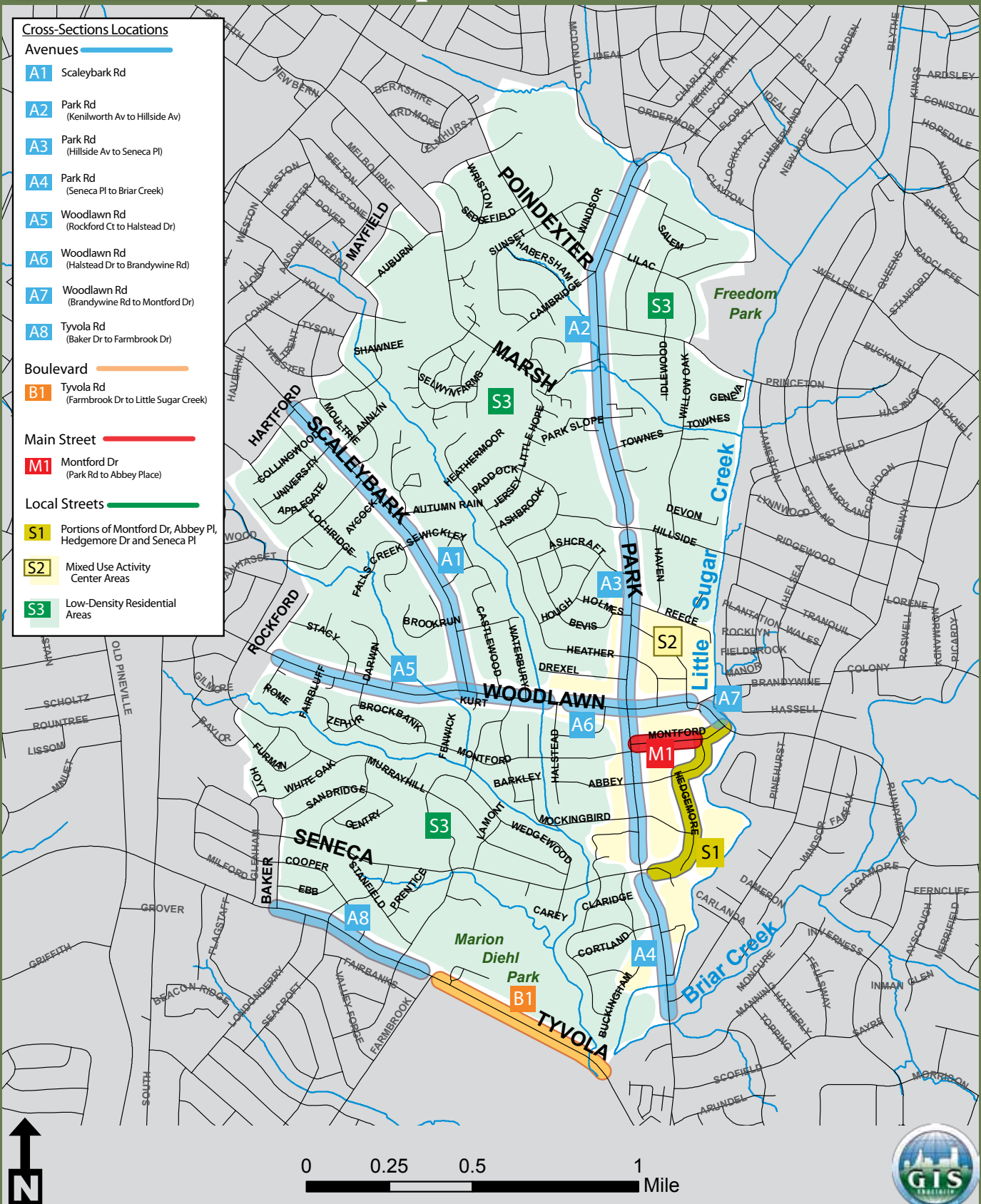
(*portions of Montford Drive, Abbey Place, Hedgemore Drive, Seneca Place*)

S2-Other Activity Center Local Streets

S3-Wedge Neighborhood Local Streets

Park Woodlawn Area Plan

Streetscape Cross-Sections



Produced by Charlotte Mecklenburg Planning Department: October 2, 2012

Street Cross-Sections

Scaleybark Road

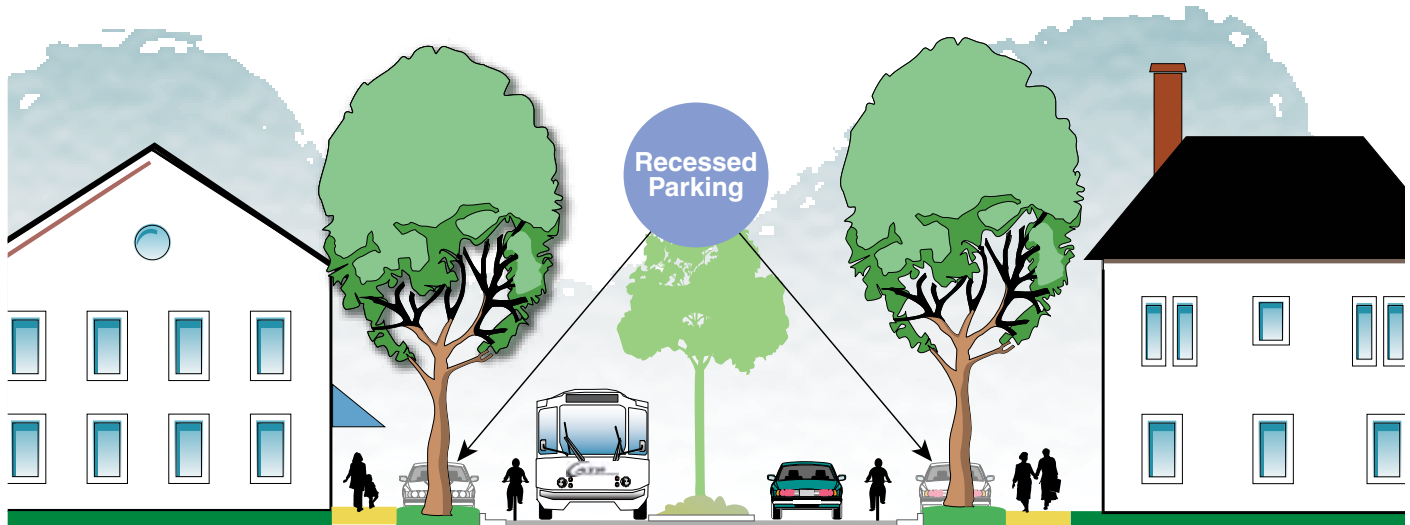


Scaleybark Road traverses Wedge Neighborhoods as a minor arterial linking Woodlawn Road and the South Transit Corridor. The street already functions as a complete street with two travel lanes and bicycle lanes. However, there are still some existing deficiencies, in terms of on-street parking, sidewalk locations, pedestrian crossings, and traffic calming.

The Charlotte Department of Transportation (CDOT) recently completed a traffic calming study for Scaleybark Road between Conway Avenue and East Woodlawn Road. The purpose of the study was to develop a traffic calming plan for the corridor, which would discourage speeding and improve the corridor for residents, pedestrians, and cyclists. The resulting plan was primarily based on feedback received from area residents and other corridor stakeholders. The following cross-sections (**A1 options**) reflect the recommendations of that study, including for some locations options for on-street parking with curb extensions or center turn lanes with median islands.

Scaleybark Road

Refer to **Map 7** for location of
street cross-sections



drawing not to scale

	Walk	Plntg / Prkg	Bike	Travel Lane	Median Zone	Travel Lane	Bike	Plntg / Prkg	Walk
A1 — Scaleybark Road (<i>Option 1, CDOT Traffic Calming Study</i>)									
	6'	8'	6'	10'	n/a	10'	6'	8'	6'
	60' Right-of-Way								
(<i>Option 2, CDOT Traffic Calming Study</i>) *									
	6'	8'	5'	10'	10'	10'	5'	8'	6'
	60' Right-of-Way								
Notes: <ul style="list-style-type: none"> • Additional setback footage may be required for utilities, etc. • Setbacks set by zoning * Recessed parking may alternate with curb extensions in some locations									
Source: Charlotte Department of Transportation									

Park Road



Park Road is one of the few major arterials that connect multiple neighborhoods, shopping centers, employment centers, and civic destinations south of Uptown Charlotte. Recent changes in growth and development along this corridor prompted the City and neighborhoods between Kenilworth Avenue and Montford Drive to assess this corridor from a multimodal transportation perspective. The results of that prior corridor study, as well as input from the Area Plan process, informed the three following cross-sections recommended for Park Road.

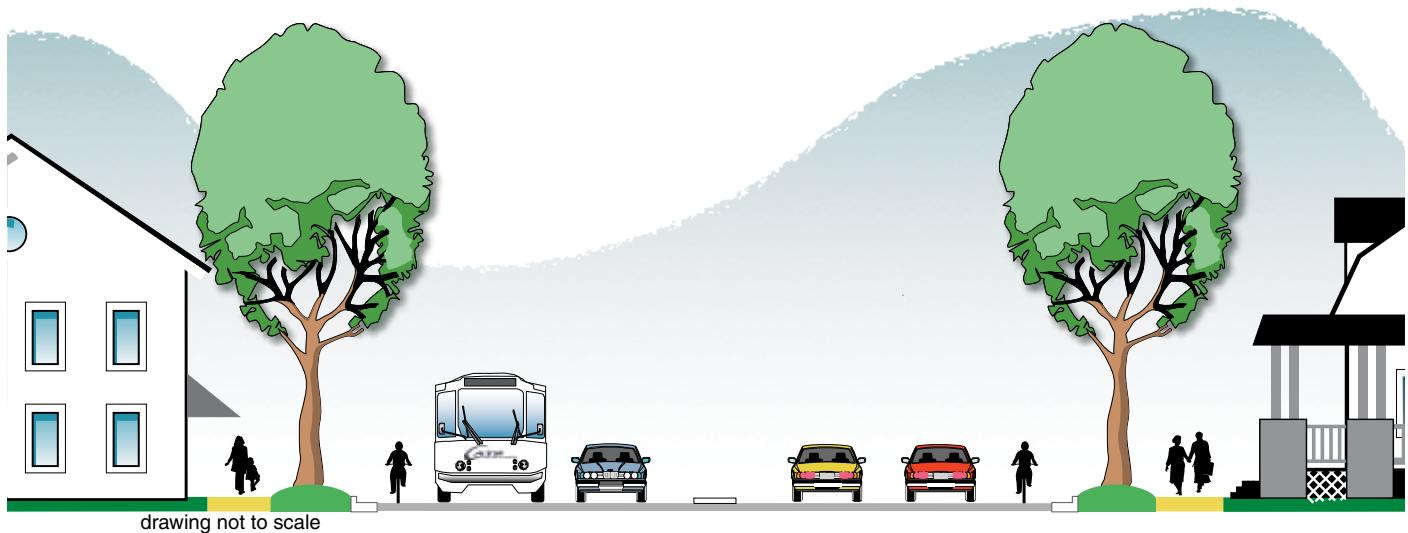
North of Hillside Avenue (A2), Park Road may see little change, except left turn lanes and enhanced pedestrian crossings at signalized intersections. This segment corresponds to the Wedge Neighborhood area, where minimal change is also expected in the area's land use character. Since physically constrained to widen for bicycle lanes, the bicycle strategy relies upon alternate, parallel routes within the adjoining neighborhoods.

South of Hillside Avenue (A3), Park Road becomes a more important route for bicycle network and a street largely within the Activity Center. Reflecting this character change, the cross-section for Park Road between Hillside Avenue and Seneca Place includes more consistent turn lanes and the reservation of space for future bicycle lanes.

South of Seneca Place (A4), Park Road becomes part of the overland route for the Little Sugar Creek Greenway. Long-term, the County's Greenway Master Plan calls for completion of the Greenway between Brandywine Road and Briar Creek along Little Sugar Creek. However, this segment of Greenway remains unfunded for the foreseen future. Short-term, a network of local streets within the Activity Center can create an alternative "overland connector" of side paths and other treatments. South of Seneca Place, this short-term strategy includes a side path along Park Road.

Park Road

Refer to **Map 7** for location of street
cross-sections



	Walk	Plntg Strip	Bike	Travel Lane	Travel Lane	Median Zone	Travel Lane	Travel Lane	Bike	Plntg Strip	Walk
A2 — Park Road (Kenilworth Avenue to Hillside Avenue)											
	6'	8'	n/a	11'	11'	10'	11'	11'	n/a	8'	6'
Maintain existing curb except turn lanes at signalized intersections											
A3 — Park Road (Hillside Avenue to Seneca Place)											
	8'	8'	5'	11'	11'	Traffic Island/ 10' Turn Lane	11'	11'	5'	8'	8'
13' planting strip should be used to preserve future curb. Specific setback requirements apply											
A4 — Park Road (Seneca Place to Briar Creek) 10' wide path on east side linking Greenway with Park/Seneca and Park/Selwyn intersections											
	8'	8'	5'	11'	11'	Traffic Island/ 10' Turn Lane	11'	11'	5'	8'	8'
13' planting strip should be used to preserve future curb. Specific setback requirements apply											
Notes:											
<ul style="list-style-type: none">• Additional setback footage may be required for utilities, etc.• Setbacks set by zoning except for road cross-sections A3 and A4, where the setback for urban zoning districts will be 16' for mixed-use and non-residential land uses; and 24' for residential land uses											
Source: Charlotte Department of Transportation											

Woodlawn Road



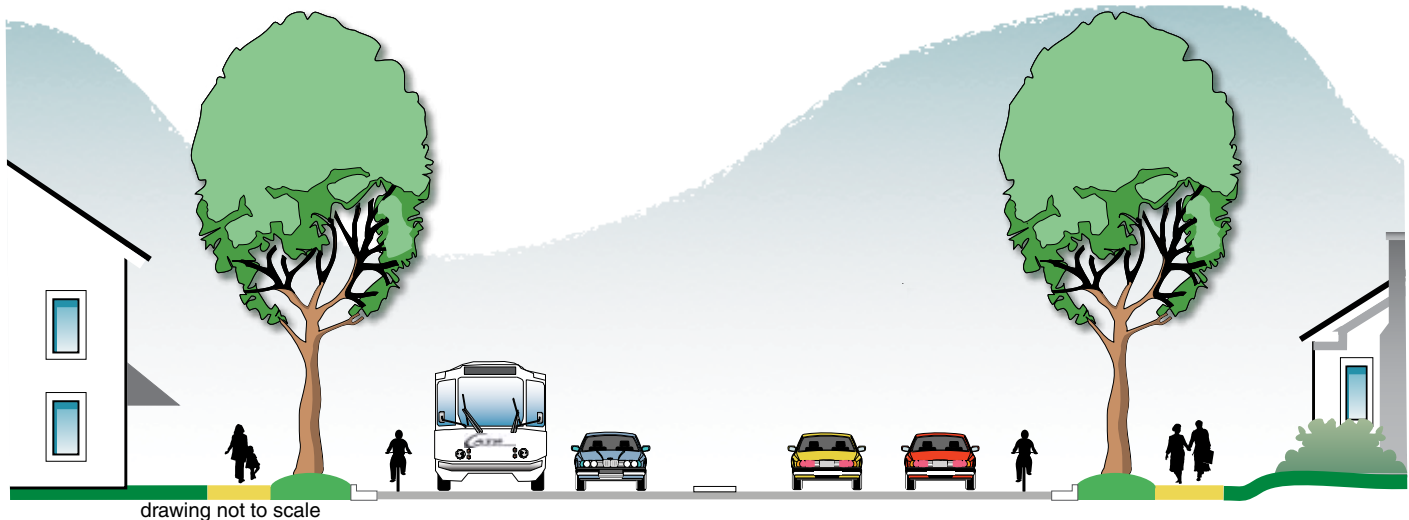
Woodlawn Road is a major arterial that connects Park Road and Interstate 77, as well as the heart of the Park Woodlawn Mixed-Use Activity Center. Combined with Billy Graham Parkway, Runnymede Lane, Sharon Road, Wendover Road, Eastway Drive, and Sugar Creek Road, the streets form Charlotte's Route 4. The circumferential route is roughly four miles from the city's center, serving crosstown travel.

The prior District Plan actually recommended a future widening to six lanes. This new area plan instead recommends a cross-section (A5 — A7) that retains the number of travel lanes to the existing four. However, new left turn lanes will be important at major redevelopment sites. These turn lanes also provide an opportunity to create pedestrian crossing islands. Bicycle lanes, or the preservation of space to someday add them, are also recommended along Woodlawn Road, since the street connects the Activity Center with the South Transit Corridor, where bicycle lanes have already been added to Woodlawn Road.

East of Brandywine Road to Montford Drive, a separate cross-section (A7) is recommended to accommodate a side path for the Little Sugar Creek Greenway. This side path serves as a short-term implementation strategy for extending the Greenway beyond its current terminus at Brandywine Road via Woodlawn Road to a new signal and pedestrian crossing island at Montford Drive.

Woodlawn Road

Refer to **Map 7** for location of
street cross-sections



	Walk	Plntg Strip	Bike	Travel Lane	Travel Lane	Median Zone	Travel Lane	Travel Lane	Bike	Plntg Strip	Walk
A5 — Woodlawn Road (Rockford Court to Halstead Drive)											
	6'	8'	5'	11'	11'	10'	11'	11'	5'	8'	8'
	13' planting strip should be used to preserve future curb.										
A6 — Woodlawn Road (Halstead Drive to Brandywine Road)											
	8'	8'	5'	11'	11'	10'*	11'	11'	5'	8'	8'
	13' planting strip should be used to preserve future curb. Specific setback requirements apply										
A7 — Woodlawn Road (Brandywine Road to Montford Drive) 10' wide path on northeast side linking Greenway with Woodlawn/Montford intersection											
	8'	8'	5'	11'	11'	Island/ refuge/ 10' Turn Lane	11'	11'	5'	8'	10'
	13' planting strip should be used to preserve future curb. Specific setback requirements apply										
Notes:											
<ul style="list-style-type: none">• Additional setback footage may be required for utilities, etc.• Setbacks set by zoning except for road cross-sections A6 and A7, where the setback will be 16' for mixed-use and non-residential land uses; and 24' for residential land uses											
* 10' center left-turn lane with opposing pedestrian crossing island at major driveways/intersections											
Source: Charlotte Department of Transportation											

Tyvola Road



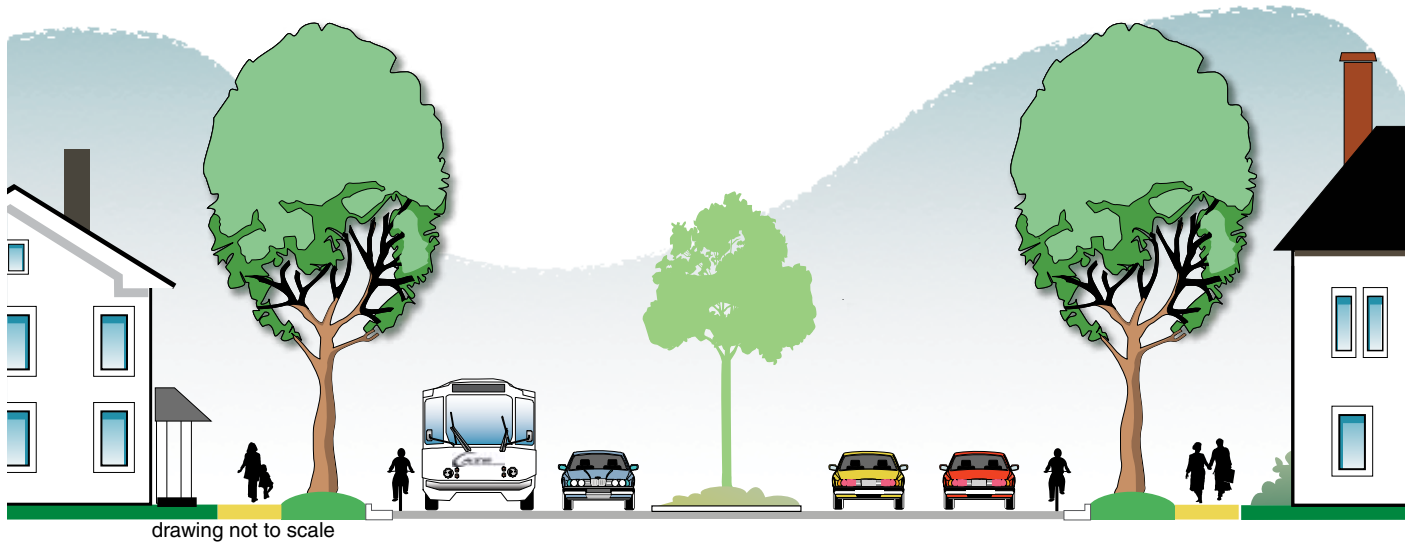
Similar to Woodlawn Road, Tyvola Road is a crosstown, circumferential arterial connecting Park Road and Interstate 77. Adding to its network importance, Tyvola Road also connects the South Park Activity Center.

West of Farmbrook Drive (A7), Tyvola Road has established residential edges with no median. For this segment of Tyvola Road, the Plan recommends an “Avenue” cross-section. The future cross-section preserves space for bicycle lanes in a wider planting strip, but otherwise, largely retains the existing cross-section without any median.

East of Farmbrook Drive (B1), Tyvola Road has a landscaped median, more non-residential land uses, and fewer driveways. For this segment, the Plan recommends a “Boulevard” cross-section. The future cross-section largely reflects the existing conditions, except preserving added space for bicycle lanes in a wider planting strip, if development retains the existing curb.

Tyvola Road

Refer to **Map 7** for location of
street cross-sections



	Walk	Plntg Strip	Bike	Travel Lane	Travel Lane	Median Zone	Travel Lane	Travel Lane	Bike	Plntg Strip	Walk
A8— Tyvola Road (Baker Drive to Farmbrook Drive)											
	6'	8'	5'	11'	11'	n/a	11'	11'	5'	8'	6'
	13' planting strip should be used to preserve future curb										
B1 — Tyvola Road (Farmbrook Drive to Little Sugar Creek)											
	6'	8'	5'	11'	11'	14'	11'	11'	5'	8'	6'
	13' planting strip should be used to preserve future curb										
Notes: <ul style="list-style-type: none">• Additional setback footage may be required for utilities, etc.• Setbacks set by zoning											
Source: Charlotte Department of Transportation											

Montford Drive

Montford Drive is located at the heart of the Park Woodlawn Activity Center. The street has become a major destination for dining and nightlife. The Plan vision is to retain area businesses and support their expansion by addressing some of the pedestrian and parking deficiencies of the street. By creating more on-street parking and enhancing the pedestrian realm, this Main Street can continue to function as a “park once” district, where patrons walk comfortably from shared parking to multiple locations during their visit. The plan recommends a “Main Street” cross-section for Montford Drive as shown on the following page.

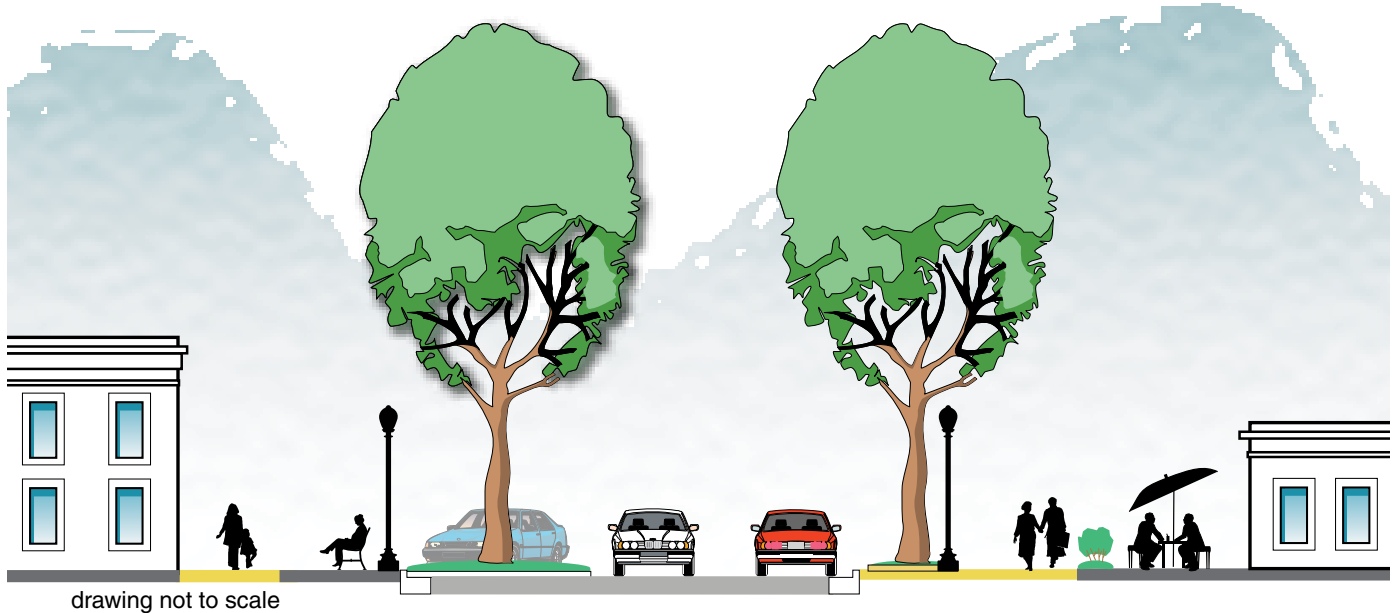
In an effort to reinforce the current “park once” environment for businesses and visitors, it is also possible that CDOT can work with businesses to enhance the existing street's functionality. During the area plan development process, LandDesign led a design workshop that looked at streetscape concepts for Montford Drive. The schematic below illustrates the workshop's recommended design scenario for the street. Staying largely within existing right-of-way and curb line, the cross-section elements include wider sidewalks, plantings, and angled parking on alternating sides of the street.

Montford Drive as it exists today does not hint at the vibrancy and destination spot that this street has become. A proposed streetscape design option shown below has improvements that include plantings and angled parking, making this a safer and more pleasant environment for all modes of transportation.



Montford Drive

Refer to **Map 7** for location of
street cross-sections



	Walk	Amenity/ Zone	Planter/ Parking	Travel Lane	Travel Lane	Amenity/ Planter Zone	Walk
M1 — Montford Drive (Park Road to Abbey Place)							
	8'	10'	16'	13'	11'	10'	8'
	20' setback* ←←←		Angled Parking one side - <i>Angled parking may alternate sides if travel lanes shift mid-block</i>			20' setback* →→→	
Notes: * 2' additional setback footage required for utilities, etc.							
Source: Charlotte Department of Transportation							

Local Streets



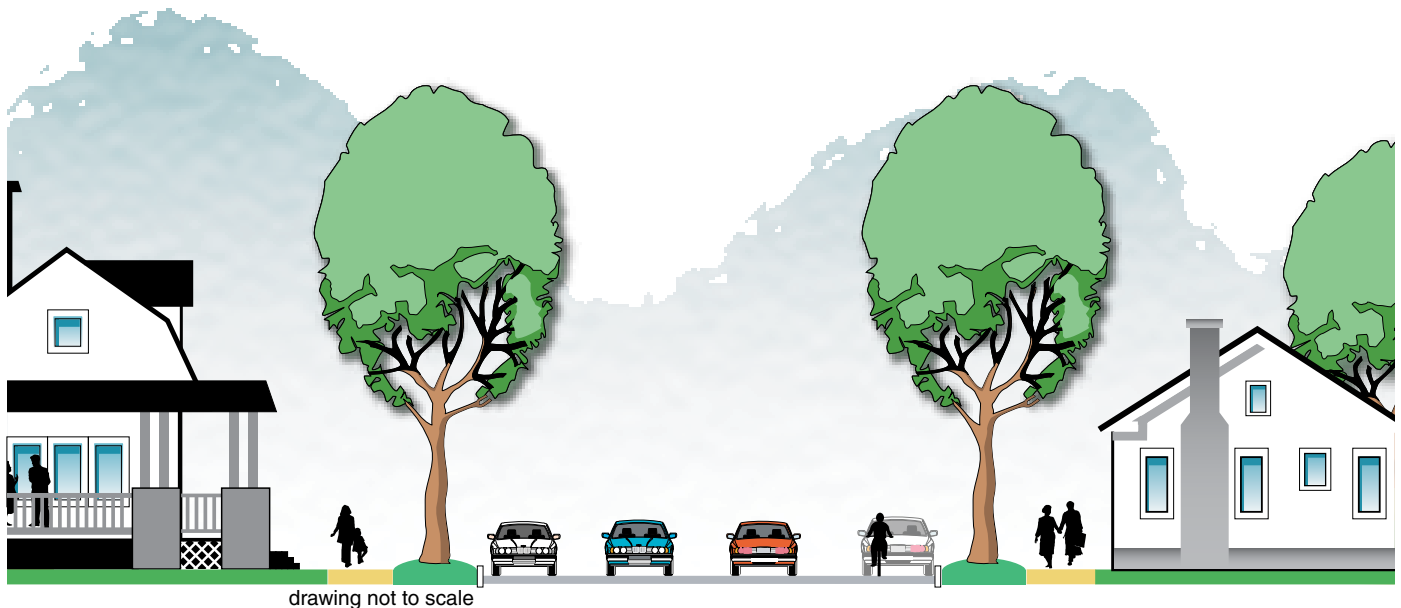
Local streets reflect the most direct relationship to land uses. Unlike thoroughfares that must balance mobility pressures, the focus on local streets is accessibility. The local streets within the Area Plan fall within three types-- a unique street (S1) for the Greenway overland connector, other local streets (S2) within the Activity Center, and local streets (S3) within the Wedge Neighborhoods.

Similar to key segments (A4 and A7) of Park and Woodlawn Roads, there are key segments of local streets within the Activity Center that comprise the Overland Connector concept (Policy T-8) for the Little Sugar Creek Greenway. Unlike the Avenues, which include side paths, the cross-section (S1) for these local streets may use a buffered or protected “cycle track” within the street. Many of the streets identified for the Overland Connector (Policy T-8 and S1) have fairly wide existing pavement. Hence, the loss of parking on one side provides an opportunity to create a “striped trail” within the existing curb lines of these low volume, low speed streets.

Elsewhere within the Activity Center, local streets follow the prescriptive cross-section (S2) of an Office/Commercial Street – Wide from the City’s *Urban Street Design Guidelines* (USDG). Finally, the bulk of the plan area’s local streets are located within the Wedge Neighborhoods. These local streets follow the prescriptive cross-section (S3) of a Residential Street – Medium from the City’s USDG.

Local Streets

Refer to **Map 7** for location of
street cross-sections



	Walk	Plntg Strip	Pkg	Mixed Travel Zone	Pkg	Plntg Strip	Walk
S1 — Overland Connector within Activity Center (Hedgemore Drive)							
	8'	8'	7'	26'	7'	8'	10'
	16' setback		Overland Connector *				22' setback →→→
S2 — Other Local Streets within Activity Center							
	6'	8'	7'	26'	7'	8'	6'
	16' setback		Office/Commercial - Wide				22' setback →→→
S3 — Local Streets within Wedge Neighborhoods							
	5'	8'	7'	12' Yield Flow	7'	8'	5'
	setback set by zoning		Residential - Medium				setback set by zoning
Notes: <ul style="list-style-type: none">• Additional setback footage may be required for utilities, etc.• Parking zone may include curb extensions * 10' Multi-use path on northeast side for Greenway Overland Connector							
Source: Charlotte Department of Transportation							

Infrastructure & Public Facilities

The Infrastructure and Public Facilities services are delivered by several departments in the City of Charlotte and Mecklenburg County. This section addresses public facilities, storm water services, water, and sewer. As Charlotte-Mecklenburg continues to grow, proper planning and coordination are necessary to maintain a high quality of life for the residents of the community.

GOAL:

Maintain and enhance public facilities such as parks and greenways, and emphasize environmental and economic sustainability for new or updated public facilities and infrastructure.

The following policies are proposed to enhance and preserve the community and to encourage

infrastructure that serve the needs of all residents and visitors.

INFRASTRUCTURE AND PUBLIC FACILITIES POLICIES

P-1 Create a more pedestrian friendly environment by redeveloping sidewalks, crosswalks, planting strips, and bicycle lanes to promote a multimodal community. Shops, restaurants and other amenities are main reasons why residents value the Park Woodlawn area. The center area has a unique identity in scale, character, and services. The policy goal is to create a more pedestrian friendly environment.

Provide a pedestrian bridge connection from the Little Sugar Creek Greenway between Rockland Place and Fieldbrook Place to the rear of the Park Road Shopping Center. Identify and enhance scenic views of the Little Sugar Creek greenway in the rear of the Park Road Shopping Center.

P-2 Parks, greenways, and open spaces are valuable assets to a community and should be leveraged to achieve the overall land use vision. Create a multimodal community that is interconnected to all major focal points in the area, especially connecting the wedge neighborhoods to the mixed-use activity center. The following actions are intended to support this policy:

Enhance pedestrian way-finding signs to link parks, greenways and major area destinations with transit users.

Meet the resident's strong desire to enhance the tree canopy by creating more public open spaces and recreational opportunities for area residents.

Create a pedestrian/cyclist network throughout the plan area using existing streets and existing rights-of-way not built as streets.

Complete the connection of the Little Sugar Creek Greenway, or as an interim an overland connector path that will act as an important corridor in the Carolina Thread Trail.

Little Sugar Creek Greenway is on the eastern boundary of the plan area, and meanders from Princeton Avenue to Brandywine Road. This 1-mile stretch of greenway that runs along the plan area is a major amenity for area residents. Once finished, it will feature 19 miles of trails, habitats, and flowing streams from Cordelia Park just north of Uptown, through the plan area and south to South Carolina.



Park Road Shopping center is seen in the background from the Little Sugar Creek Greenway. A pedestrian and bike connection will be a great asset

P-3 Seek public/private opportunities to create civic plaza in the parking lot of the Park Road Shopping Center.

The Park Road Shopping Center continues to draw shoppers from the community and beyond, and has evolved as the de facto focal point of the area. However, the vast parking lot even at peak Christmas shopping times is not completely used, and has the potential of transforming into a key meeting and gathering place for the community. A reworking of the parking lot, in partnership with the shopping center owners can create a truly unique gathering place for civic and community events, much like a town center or village square.



The iconic Park Road Shopping Center has been a gathering place for over 50 years. Maintaining adequate parking for shopping is important, but there is an opportunity to renew a portion of the existing parking lot into a unique and inviting village square

Water and Sewer

P-4 Maintain a level of service equal to or superior to the capacity and condition of current service to support future development. Water and sewer systems should be repaired, updated, or replaced as they become outmoded or outdated. As redevelopment occurs, a periodic review of water and sewer service may be needed to meet new service demands. The review also includes water pressure for fire protection.

Stormwater

P-5 Upgrade storm water management facilities as needed when improvements are made to the street network. Upgrading storm water management facilities at the same time as road improvements will minimize unnecessary disruption of traffic and be more cost effective.

Natural Environment

GOAL:

Preserve and protect the natural environment, parks, tree canopy, open space, and natural resources. Promote measures to minimize the adverse effects of new development on land, air, and water quality, and encourage the use of sustainable building practices. Increase the tree canopy in the plan area following the City's adopted 50/50 Tree Canopy policy, especially in the mixed use activity center where the tree canopy is lowest.



Protection of the existing mature trees and increasing the tree canopy is a high priority in the Activity Center as redevelopment occurs.

The Park Woodlawn study area is full of thriving neighborhoods; flourishing, well attended educational, religious, and civic institutions; and highly successful retail and business destinations. While these activities are essential to the vitality of the area and to Charlotte as a whole; air, land and water resources must also be protected in order to promote a healthy balance and co-existence of the urban and natural environments. Improved access to the Little Sugar Creek greenway and neighborhood parks (Marion Diehl Community Park and Park Road Community Park); protection of the tree canopy; and impacts of redevelopment activities on impervious surface and storm water (including possible flooding of Little Sugar Creek) were some of the concerns expressed by area residents and property owners during the planning process. The Area Plan seeks to recognize the natural environment as an essential part of the community's character, as well as to promote policies that serve to protect and

enhance these resources. Creative, quality and responsible developments that address the concerns of area residents, and are sensitive to the natural resources in the area are key to creating successful sustainable communities. The tools to implement these goals are found in these policies, intended to minimize the impacts of land development and redevelopment activities on the natural environment, while at the same time allowing for continued economic growth.

Land Quality

Little Sugar Creek generally serves as the eastern boundary for the study area. Little Hope Creek and other streams are also located within the plan area (parks and greenways are addressed in the Infrastructure and public facilities section of this plan). The Park Woodlawn area enjoys a mature tree canopy especially in the wedge neighborhoods (48.1%

canopy coverage). However in the mixed-use activity center it is a different story with a scarce tree canopy (13.0% canopy coverage). This is partly due to the fact that the majority of development in the area occurred prior to 1970- before tree save measures were put in place. The mixed-use activity center is impacted by development, redevelopment, and a general lack of green space and street trees. The environmental benefits provided by trees include improved water quality, a reduction in ground level ozone and other forms of air pollution, and carbon capture and sequestration. Carbon capture and sequestration is a means of mitigating the contribution of fossil fuel emissions to global warming, based on capturing carbon dioxide from large point sources.

NATURAL ENVIRONMENT POLICIES

E-1 Encourage actions (measures) that will help ensure the long-term sustainability of the tree canopy.

As redevelopment continues to increase in the area, impacts to tree coverage should be monitored and measures supported to help mitigate loss of canopy, and to increase it especially in the mixed-use activity center. One strategy to help reverse the loss of tree canopy is to plant additional trees in parks, public and private open spaces, and in planting strips along public streets where practical. This strategy should also address replacement of trees that are lost or damaged through disease, development, or other causes.

E-2 Minimize impacts to existing tree canopy when developing, maintaining, or constructing

streets, sidewalks, pedestrian/ bicycle paths, public facilities, and utilities. A strategy is needed to prevent practices that damage or destroy mature trees. Such a strategy should be developed in consultation with utility companies and the construction industry to increase its acceptance and enhance compliance efforts.

E-3 Establish tree canopy goals for the Park Woodlawn area, following the City's 50% Tree Canopy Goal in 2050. City Council adopted a 50% Tree Canopy Goal by 2050 in 2011, and set different goals based on the *Centers, Corridors and Wedges Growth Framework*. The adopted policy has a low goal of 55% in Wedges and 25% in Centers. The objective of the *Park Woodlawn Area Plan* is to set tree canopy goals for this area, for the Wedge at 50% and for the Mixed-Use Activity Center at

20%. The tree canopy goals can be attained by increasing the amount of street trees and tree plantings in parking lots, and providing open space in new developments.

E-4 Support mixed use and compact development that preserves land, reduces vehicular trips and protects natural resources. Mixed use and compact development reduces trip lengths and supports alternative travel options. This type of development encourages walking and biking as well as protects air quality, water quality, and other natural resources. The land use and design sections provide details about the appropriate location for mixed use development and provide guidance for appropriate design. The land use and design policies help insure that impacts of growth are mitigated.



This mixed use development located within the greater Charlotte area shows the combination of residences and shops into one building. Attracting desirable retail tenants can greatly reduce required auto trips.

Air Quality

The Park Woodlawn area draws visitors from the entire region due to its location within Charlotte's urban core, and its variety of institutional and retail/entertainment facilities. Visitors rely mainly on the automobile and travel via the major thoroughfares of Park Road and Woodlawn Road. This combination has a negative impact on air quality from vehicle emissions. During the planning process, property owners and residents expressed support for improved accessibility to a wide mix of uses in the area to help reduce the number of vehicle trips. However, some concerns were also expressed about traffic conditions, safety, and lack of pedestrian connectivity. Some of these issues identified are as follows:

Vehicular emissions compromise air quality. A combination of solutions can be helpful, but full endorsement of changing commuting habits will be necessary to solve the problem

- Congestion at the intersection of Park Road and Woodlawn Avenue;
- Speeds along Park Road and Woodlawn Avenue;
- Possible increase in congestion resulting from potential site redevelopment;
- Gaps in pedestrian connections between neighborhoods, and between neighborhoods and retail areas.
- Need for CATS service improvements along Park Road such as additional waiting pads/shelters and potential mini transit center location with shuttle services and linking improved access to Park Road Shopping Center (including providing internal circulation).

A way to help mitigate air pollution due to vehicular emission, while also reducing speeding and closing gaps in connectivity, is to improve bicycling and walking conditions. Enhanced connectivity to key destinations via completion of sidewalk systems and adding bicycle lanes can improve convenience and the likelihood of use of alternative modes of transportation such as walking or bicycling.

E-5 Encourage new pedestrian and bicycle connections to reduce vehicle miles travelled. Encourage pedestrian and bike connections especially between the mixed-use activity center and the residential wedges by creating a neighborhood focused ped/bike network. The network can be built on existing local streets as well as on existing rights-of-way that have not been built as streets (paper streets). Also encourage more ped/bike bridges over creeks and streams in the area.

E-6 Implement the land use, community design, and transportation policies to reduce vehicle emissions. Future development should strive to reduce vehicle trips, improve connectivity and provide travel choices/route options. The reduction of vehicles miles traveled (VMT) and the implementation of policies recommended throughout the plan will help improve air quality.

Specific policies promoting improved sidewalks, pedestrian and bicycle paths, the preservation of existing walking connections, and enhancement of greenways are in the Infrastructure and Public Facilities section.



Water Quality

The Little Sugar Creek and associated floodplain serve as the Park/Woodlawn area plan's eastern boundary. There are a number of streams and creeks in the plan area protected by Surface Water Improvement and Management (SWIM) and Post-Construction Controls Ordinance (PCCO) buffers. To protect the area's water resources, effective water quality measures should be implemented as development or redevelopment occurs.

E-7 Encourage the use of low impact design to facilitate better water quality protection. Low impact design measures such as minimizing the clearing of natural areas and working with the existing

natural topography to minimize grading can improve the storm water quality and reduce the amount of storm water run-off.

E-8 Utilize innovative best management practices (BMP) for development and redevelopment projects. The use of innovative and eco-friendly storm water management practices should be integrated into the design of new developments to the greatest extent practicable. This may include rain gardens, green roofs, rain barrels, dedicated use of gray water, porous concrete parking areas, and other approved measures that may come into accepted use. The testing of proprietary storm water management measures for possible approval should be considered as well.

E-9 Support environmental education as a means of reducing the amount of pollutants entering area streams and creeks. Encourage local community involvement in efforts to educate area residents about topics such as the proper use of lawn fertilizers and pesticides, disposal of leaves, grass clippings, and yard debris, storm drain marking, and Adopt-A-Stream programs. Vigorous enforcement of existing water quality regulations by the appropriate local and State agencies is also vitally important.



Water fowl, such as the Mallard duck, are inhabiting the Little Sugar Creek. Protection of the creek's water quality from a variety of pollutants is critical to the creek's health



Conclusion

The Park Woodlawn area is known for its stable neighborhoods, a successful activity center, and religious, educational, and civic institutions. Its relative proximity to Uptown and Southpark, along with direct access to two major thoroughfares, contribute to its continuation as a popular destination.

As the City of Charlotte continues to grow and attract new residents and businesses, development pressures will likely intensify. The ability to successfully accommodate new growth while maintaining a

high quality of life is not likely to occur without appropriate planning policies. The *Park Woodlawn Area Plan* sets the stage for managing future growth and development consistent with the *Centers, Corridors, and Wedges Growth Framework* and the community's vision of its future. The *Park Woodlawn Area Plan* provides opportunities for acknowledging and protecting the community's natural resources by encouraging good development and redevelopment that effectively link land use, transportation and community design. Adoption and implementation of effective

sustainable development policies are a critical component in creating vibrant and livable communities.

The plan policies set forth in this document provide guidance for future developments that are consistent with the character and vision for the area; with improved pedestrian and bicycle connections, preservation of strong and healthy mix of uses that enhance the community, along with provisions for public facilities to serve residents and development practices that have minimal impact on the environment.

2 Implementation Guide

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2 Implementation Guide

Introduction

Achieving the vision articulated in this plan will occur incrementally over time through the combined efforts of local/state government, private property owners, residents, developers and many others. This Implementation Guide is primarily a staff document that outlines specific steps that can be taken by various public and private bodies so that the future envisioned in this plan may be realized. These strategies are listed in the table that follows. The lead responsible agency and tentative time frame is also included. The strategies are numbered sequentially and correspond to the policies discussed in the Concept Plan. This allows staff to track progress of the plan's policies over time.

Some strategies are physical such as improving the streetscape along Montford Drive, and improving the Park Road and Woodlawn Road intersection so it's safer for pedestrians. Other strategies involve continued enforcement of adopted City policies and regulations such as the Tree Ordinance that deals with protecting the City's tree canopy; still others have to do with implementing recently adopted City Policy such as the City's *50% Tree Canopy by 2050*, and detailing how that policy goal will be implemented through this area plan.

These implementation strategies do not imply a specific public or private sector commitment. The Charlotte City Council will not be asked to adopt this implementation Guide. However, many of the strategies in this section will require future action by elected officials and will be presented to them for approval as needed on a case-by-case basis.

Finally, since conditions change over time, staff will periodically update this Implementation Guide to reflect changes and progress.



Recommended Implementation Strategies

The number of each action corresponds to the policies beginning on page 12 of the Concept Plan.

Recommended Implementation Strategies					
No.	Policy No.	Action Item	Project Type	Lead Agency	Time Frame
Land Use & Community Design					
1	L-1— L-19	Use land use policies to guide and evaluate development proposals.	Land Use	Planning	Ongoing
2	L-11	Work with Planning GIS staff to modify the Centers, Corridors and Wedges boundary layer reflecting the changes adopted for the Park Road Mixed-Use Activity Center, as indicated on Map 3 .	Land Use	Planning	Short (0-5 yrs)
3	L-8	Work with Economic Development Division of N&BS to address the parking demand issue in and around Montford Drive, helping to address short and long term solutions.	Economic Development	Planning / N&BS	Long (>10 yrs)
4	L-18	Work with the Charlotte Housing Authority (CHA) when they begin preliminary design options for the property on Marsh Road and Park Road.	Zoning	Planning	As development occurs
5	D-1— D-7	Use Community Design policies to guide and evaluate development proposals.	Community Design	Planning	As development occurs
6	D-4L— D-4N	Work with Park Road Shopping Center owner as they implement their improvements to the center.	Community Design	Planning	As development occurs
7	D-4O— D-4P	Work with Montford Drive property owners and business community to improve the public spaces, streetscape and parking issues.	Land Use / Community Design	Planning	Ongoing

No.	Policy No.	Action Item	Project Type	Lead Agency	Time Frame
Transportation and Streetscape					
8	T-1	Create new local streets as redevelopment occurs, offering alternatives to the major thoroughfares in the area Park Road, and Woodlawn Road.	Transp.	CDOT / Planning	As development occurs
9	T-2	Upgrade Montford Drive streetscape as shown on the Montford Drive cross-section, which would include: <ul style="list-style-type: none"> • Angled Parking (alternating sides) • Planting Strip • Wider Sidewalks 	Transp.	CDOT / E&PM	As funding becomes available and/or as development occurs
10	T-6	Enhance the Park Road and Woodlawn Road intersection to improve traffic flow, area accessibility and safety; and further analyze their feasibility through the capital needs assessment process.	Transp.	CDOT / E&PM / Planning	As funding becomes available and/or as development occurs
11	T-4	Work with petitioner in the rezoning process to request that parallel connections to Park Road as shown on Map 5, are provided as development occurs.	Transp.	CDOT / Planning	As development occurs
12	T-5	Provide additional pedestrian crossings of thoroughfares at the following locations: <ul style="list-style-type: none"> • Park Rd. at Park Rd. Shopping Center Drive • Park Rd. at Montford Drive • Park Rd. at Mockingbird Lane • Park Rd. at Park-Selwyn Terrace • Woodlawn Rd. at significant redevelopment sites (usually in conjunction with a left turn lane). 	Transp. / Zoning	CDOT / Planning	As development occurs
13	T-6	Add new signalized intersections to enhance access and circulation at the following intersections: <ul style="list-style-type: none"> • Park Rd. and Reece Rd. • Woodlawn Rd. and Montford Dr. (including pedestrian refuge) • Park Rd. and Cortland Rd. or Park Rd. and Park-Selwyn Tr. 	Transp.	CDOT	Short (0-5 yr)

No.	Policy No.	Action Item	Project Type	Lead Agency	Time Frame
14	T-7	Create multiple connection to Little Sugar Creek Greenway	Transp. / Parks	CDOT / Park & Rec / Planning	As development occurs
15	T-9	<p>Improve the neighborhood context of Park and Woodlawn Roads, using the following strategies:</p> <ul style="list-style-type: none"> • Use signal spacing and timing to pace traffic at a level appropriate to the area. • Consider adding signals at more intersections throughout the area. • Design intersections to encourage / support easier crossing by cyclist and pedestrians. • Provide streetscapes that will encourage and support more bicycle and pedestrian travel in the area. • Provide more crossing opportunities in addition to signalized intersections. 	Transp.	CDOT	Short (0-5 yr) and ongoing
16	T-10	Implement design concepts for calming and enhancing Scaleybark Road.	Transp.	CDOT	Medium (5-10 yr)
17	T-11	Calm traffic on local streets to support livable and walkable neighborhoods.	Transp.	CDOT	Ongoing
18	T-12— T-14	Create new bicycle-pedestrian connections.	Transp.	CDOT / Park & Rec / Planning	As development occurs
19	T-15	Eliminate gaps in sidewalk system, but would have to be done through citizen initiated petition process.	Transp.	CDOT / E&PM / Planning	As development occurs

No.	Policy No.	Action Item	Project Type	Lead Agency	Time Frame
Infrastructure and Public Facilities					
20	P-1	Help coordinate efforts between various property owners in the Activity Center and Park and Recreation Dept. to enhance ped/bike bridge connections to Little Sugar Creek Greenway.	Transp. / Planning	CDOT / Planning / P&R	Ongoing
21	P-2	Establish ped/bike network throughout the plan area, connecting the neighborhoods to the activity center.	Utilities	CMU	Ongoing
22	P-3	Work with property owners in coordinating private / public partnerships as they become available to improve open space in the area, especially in the activity center.	Planning / Park	Planning / P&R	Ongoing
23	P-4— P-5	Maintain and upgrade as needed water and sewer as well as stormwater infrastructure in the area.	Utilities	CMU / E&PM	Ongoing
Natural Environment					
24	E-1	Use the rezoning and subdivision process to implement land use and environmental policies.	Zoning	Planning	As development occurs
25	E-2— E-3	Work to implement the City's Tree Canopy Goal by 2050, by working with local groups such as Tree Charlotte in planting over 2,000 trees in the public rights of ways and seek opportunities on private properties when possible.	Land Development / Tree Ordinance	E&PM / Planning	As development occurs
25	E-3— E-9	Continue to apply the City's existing environmental ordinances such as the tree Ordinance (and subsequent text amendments) as well as Post Construction Controls Ordinance in preserving the city's tree canopy, and improving storm water runoff.	Land Development	E&PM / Planning	As development occurs

3 *Appendix*

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3 Appendix

The information contained in this Appendix was gathered throughout the planning process and was used to produce the *Park Woodlawn Area Plan*.

Existing Conditions, Trends, and Forecasts



A diverse population makes up this vibrant and desirable community.

A thorough review and understanding of existing conditions – physical, economic, and social – was needed to identify the opportunities and constraints facing the plan area, and to develop the plan vision, goals, and policies. The following discussion highlights the existing conditions, trends, and forecasts most significant to the plan area.

Demographics

The 2010 U.S. Census indicated that 12,164 people reside in the Park Woodlawn plan area; of those, the majority (78%) are Caucasian, with about 11% being African-American. Hispanic residents comprise 11% of the area's population, similar to the 13% citywide. As of the 2010 Census, there are more females (52%) than males. Seventeen percent of the population is under age 18, less than the 28% citywide. Senior citizens (65+) comprise 12% of the total, more than the 8% citywide.

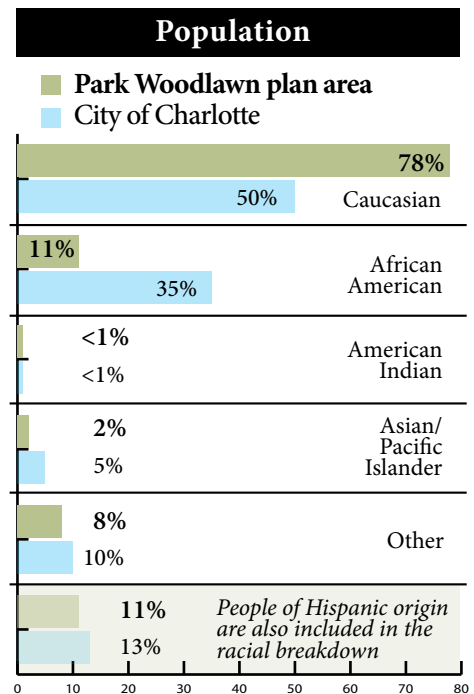
The Park Woodlawn plan area has seen a slight decrease in population since the 2000 Census. The 2010 population for the area is 12,164, which is an average annual decrease of less than one quarter of one percent.

The 2010 Census also provides the following information:

A majority (58%) of the households in the area were non-family households, with single-person households accounting for 44% of all households. The average household size in the plan area was 1.95 persons.

Population Trends			
Geographic Area	1990 Census data	2000 Census data	2010 Census data
Park Woodlawn Plan Area	11,533	12,467	12,164
		8.1%	-2.4%
City of Charlotte	395,934	540,828	731,424
		36.6%	35.2%
Mecklenburg County	511,433	695,454	919,628
		36.0%	32.2%

Source: U.S. Census Data (Block Level)



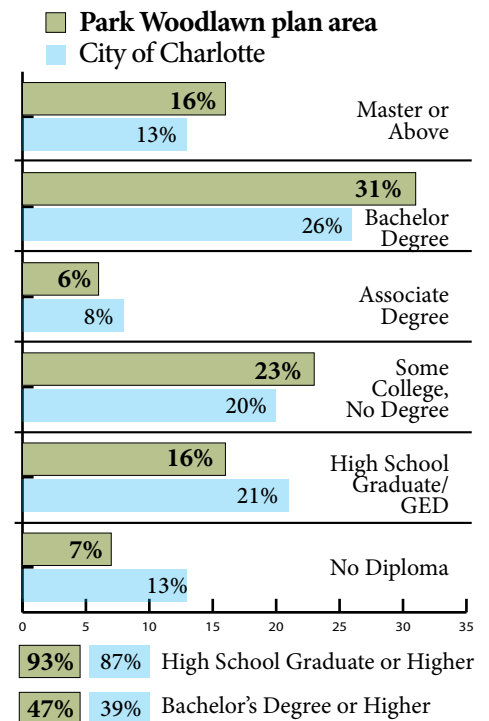
Source: 2010 Census (Block Level)



The 2006-2010 American Community Survey 5-Year Estimate provides the following information:

- The median weighted household income of the plan areas was \$51,703 (2010 Inflation-Adjusted Dollars). This was slightly less than the median household income for the City of Charlotte as a whole, \$52,446.
- About 58% of area homes were valued at more than \$200,000 dollars. The median weighted home value was \$232,024; this was significantly greater than the median home value for the City which stood at \$173,300.
- Residents of the plan area were generally more well-educated than residents city-wide, as illustrated in the following table.
- More area residents worked in the management/professional category (48%) than in any other. This was followed by residents employed in sales/office work (23%) and the service sector (20%).

Highest Attained Education Level for Population 25 and over



Source: 2006 to 2010 American Community Survey (ACS) 5-Year Estimates (Block Group)

Occupation for Employed Population 16 and over

Occupational Group	Park Woodlawn Plan Area		City of Charlotte
	Pop.	%	%
Management/ Professional	3,272	48%	39%
Service	1,394	20%	15%
Sales/ Office	1,565	23%	27%
Construction/ Maintenance	271	4%	8%
Production/ Transportation	378	5%	11%
TOTAL	6,880	100%	100%

Source: 2006 to 2010 American Community Survey (ACS) 5-Year Estimate (Block Group)

Land Development and Community Design

The Park Woodlawn plan area encompasses a wide variety of housing, commercial, and institutional development styles, much of which developed in the late 1950's. See **Map 8**. The plan area is heavily developed, including a strong commercial core surrounded by single-family neighborhoods and scattered multi-family residential. Much of the Park Woodlawn area is auto-oriented, but it is transitioning to a more pedestrian oriented mixed-use environment in the commercial center. The single-family residences in the area were developed largely in the 1950's and 1960's with similar design features and materials. The area was also once a dairy farm and the boyhood home of world renowned evangelist Billy Graham, whose home stood on what is today an office building on Park Road and Abbey Place.



Billy Graham monument on the site where his boyhood home stood.

Residential

With its proximity to Uptown Charlotte, the Park Woodlawn area remains strong and stable with a number of well-established single family neighborhoods and several newer multi-family developments. The area is going through a renewal with many longtime homeowners looking to downsize and young professionals and families looking to be close to Uptown moving in.



Madison Park remains a beautiful neighborhood today – 60 years after its development.



Sedgefield's convenient location to Uptown combined with many different housing options and amenities make this a popular neighborhood.

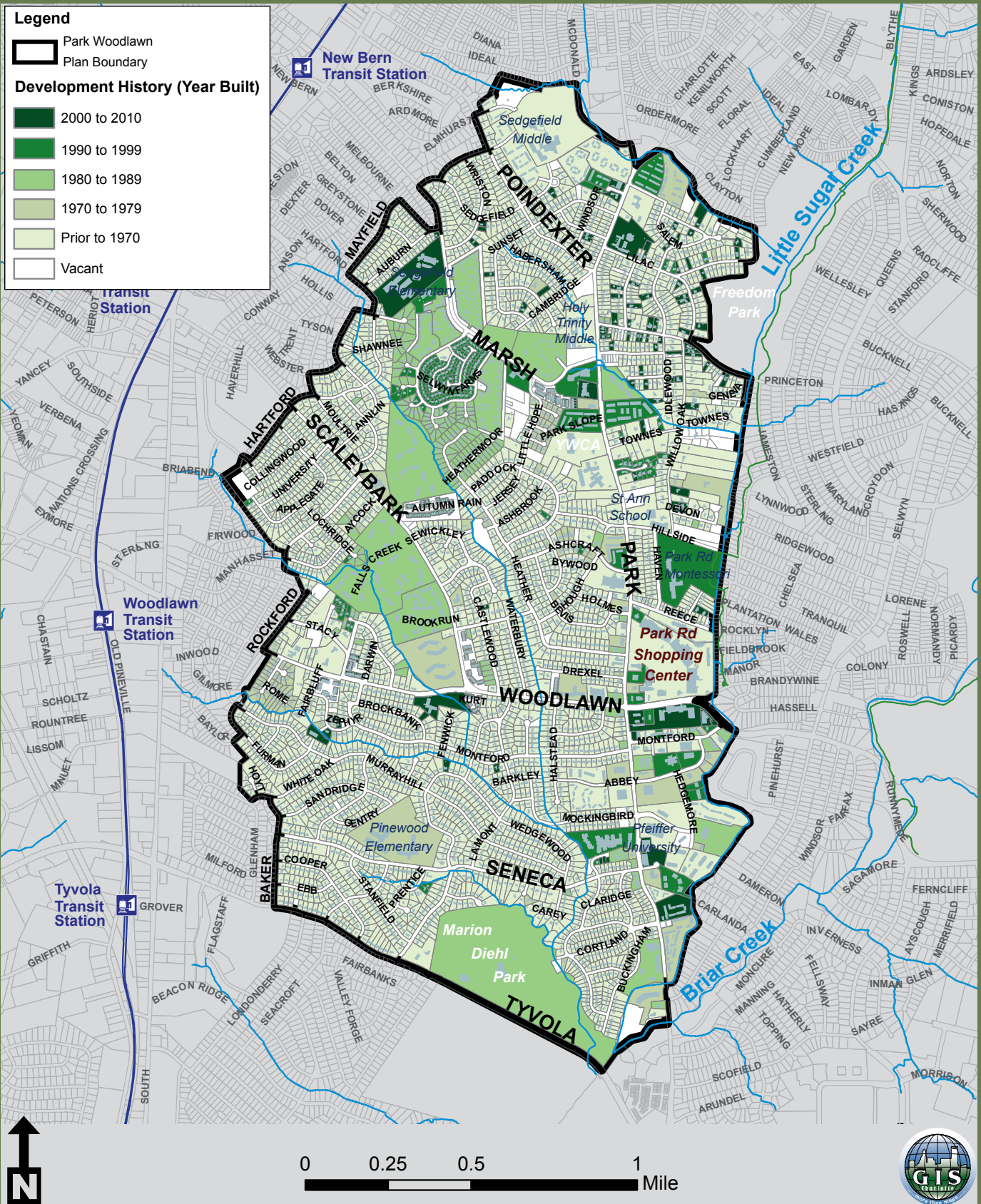
Sedgefield

Located just south of Uptown Charlotte and nestled between Park Road and South Boulevard is Sedgefield, a World War II neighborhood consisting of a combination of cottages, brick ranches and colonials. It was the family farm of Lex Marsh. Sedgefield's wagon roads were converted to paved streets and Mr. Marsh turned farmland into a planned community with 70 homes and 60 duplexes. By the 1960's, Sedgefield grew into a vast community with numerous apartment complexes, homes and duplexes. The sought after neighborhood boasts a five acre park with basketball courts, tennis courts, a playground, and numerous walking trails. The Sedgefield neighborhood is being revitalized and restored, and is becoming a popular area due to its location, affordable homes, and great schools.

Madison Park

Started in the late 1950's, Madison Park was one of the first modern post World War II neighborhoods in Charlotte. Designed to cater to middle class families, yet incorporating the latest home features of the day, this neighborhood has shown great staying power even into the 21st Century with its all brick ranch style homes. Madison Park is situated within walking distance to Park Road Shopping Center and nearby South Park Mall, Freedom Park and Montford Drive.

Park Woodlawn Area Plan Development History



Produced by Charlotte-Mecklenburg Planning Department: October 5, 2011

Office and Commercial

Much of the office space in the area is located along Park Road in 1960's and 70's office buildings of 4-8 stories. There is a scattering of newer office complexes. Many older homes located on Park Road have also been converted to office or commercial uses. Commercial centers in the plan area vary from older, strip type shopping and converted homes along Park Road, to newer mixed use centers concentrated around the Park Road and Woodlawn Road intersection.

Park Road Shopping Center

One of the highlights of the Park Woodlawn area is Park Road Shopping Center, a commercial center that boasts a history unlike any shopping center in Charlotte. Charlotte shopping was concentrated in the downtown area, but as neighborhoods began to sprawl further southeast from the center city, the idea of a neighborhood shopping center began in the mind of developer A.V. Blankenship. Mr. Blankenship purchased the land on the corner of Woodlawn and Park Roads in what was then between two growing neighborhoods (Myers Park and Sedgefield), building the shopping center on its 40 acres. It opened on November 15, 1956, with 32 new stores and was considered a bold move by business leaders at

the time. It was the first open air shopping center in Charlotte, and the largest between Washington D.C. and Atlanta.

The opening of the center was a notable event in the neighborhood and spectators like J.C. Penney made the trip to Charlotte to welcome shoppers to his first store in the city. The center was a first not only for Charlotte, but for the entire Southeast. The crescent-shaped shopping center sported a brand new Colonial grocery store, and shoppers that first day received a complimentary five pound bag of sugar. A drawing was held for visitors to win one of three brand new 1957 Fords. The 2,400 parking spaces allowed shoppers plenty of

space to park close to the shops where seemingly endless rows of merchandise were waiting. Park Road Shopping Center unveiled its trademark - the large, red sign that could be seen for blocks. The sign stands today as a testament to the center's enduring longevity.

Through its 48 years, Park Road Shopping Center has remained a strong and vibrant commercial core in the neighborhood. With its mix of stores and services, combined with low turnover rates, the center has maintained a sense of community and continues to serve the purpose for which it was first created – to be a user friendly neighborhood shopping center.

Three brand new 1957 Fords were given away at the opening



Upper level today



Opening in 1956, Park Road Shopping center was the largest shopping center of any kind between Washington, D.C. and Atlanta

Thirty two new stores opened in 1956



Lower level today

Park Towne Village & Montford Drive

Park Towne Village, located at the corner of Woodlawn and Park across from Park Road Shopping Center, was developed in 2003 to complement the historic center. Another notable commercial concentration occurs along the recently popular Montford Drive. This small side street, tucked between large office complexes and Woodlawn Drive, has become home to a large grouping of over 14 bars and restaurants. Though originally developed in the late 1950's and 60's with a number of commercial businesses, Montford has transitioned into a lively nightlife scene.



Park Towne Village on the corner of Woodlawn and Park Roads offers multi-levels of retail, restaurant and office space.



Montford Drive becomes a vibrant area in the evening hours with patrons visiting bars, restaurants, and the bowling alley.

Civic

Due to the abundance of residential development in the Park Woodlawn area, a number of civic uses also exist throughout the plan area. There are several parks and recreational opportunities within the plan area, including Marion Diehl Park, E.B. Moore Neighborhood Park and the Little Sugar Creek Greenway. Schools include both public and private institutions like Park Road Montessori, St. Ann Catholic School, Holy Trinity Middle, and Pinewood Elementary. The YWCA located along Park Road was constructed in 1965 with housing, exercise, and meeting areas and the corporate offices. A gymnasium was added (1977) and the facilities were remodeled to accommodate the changing needs for the organization.

The area also includes higher education facilities with Pfeiffer University located on Park Road and Mockingbird Lane, as well as the Queens University Sports Complex at Marion Diehl Park on Tyvola Road.



YWCA on Park Road incorporates housing, exercise space, and meeting areas.



Marion Diehl Park contains a picnic shelter, walking trail and playground within its 59 acres.

Building Permits

Residential

Residential building permit activity for the plan area from 2001 to October 2011 is shown in the tables below and on **Map 9**. Single family residential permits peaked in 2007 and 2008 with 14 issued each year. Permitting significantly decreased from 2009 onward, with three issued in 2009, four issued in 2010, and six issued through October 2011. Building permits issued for multi-family residential units experienced sharp increases in 2004 and between 2006 and 2008, before sharply decreasing from 2009 onward.



Cielo Apartments located on Park Road just north of Selwyn Avenue contains both flats and lofts. It advertises itself as "the green way to live in Southpark".

Residential Building Permits			
Year	Single Family Detached	Multi-Family*	Total
2001	3	0	3
2002	14	17	31
2003	2	3	5
2004	8	42	50
2005	8	11	19
2006	13	101	114
2007	14	57	71
2008	14	206	220
2009	3	0	3
2010	4	4	8
2011	6	0	6
TOTAL	89	441	530
* includes Townhomes Source: LUESA Building Permit Database			

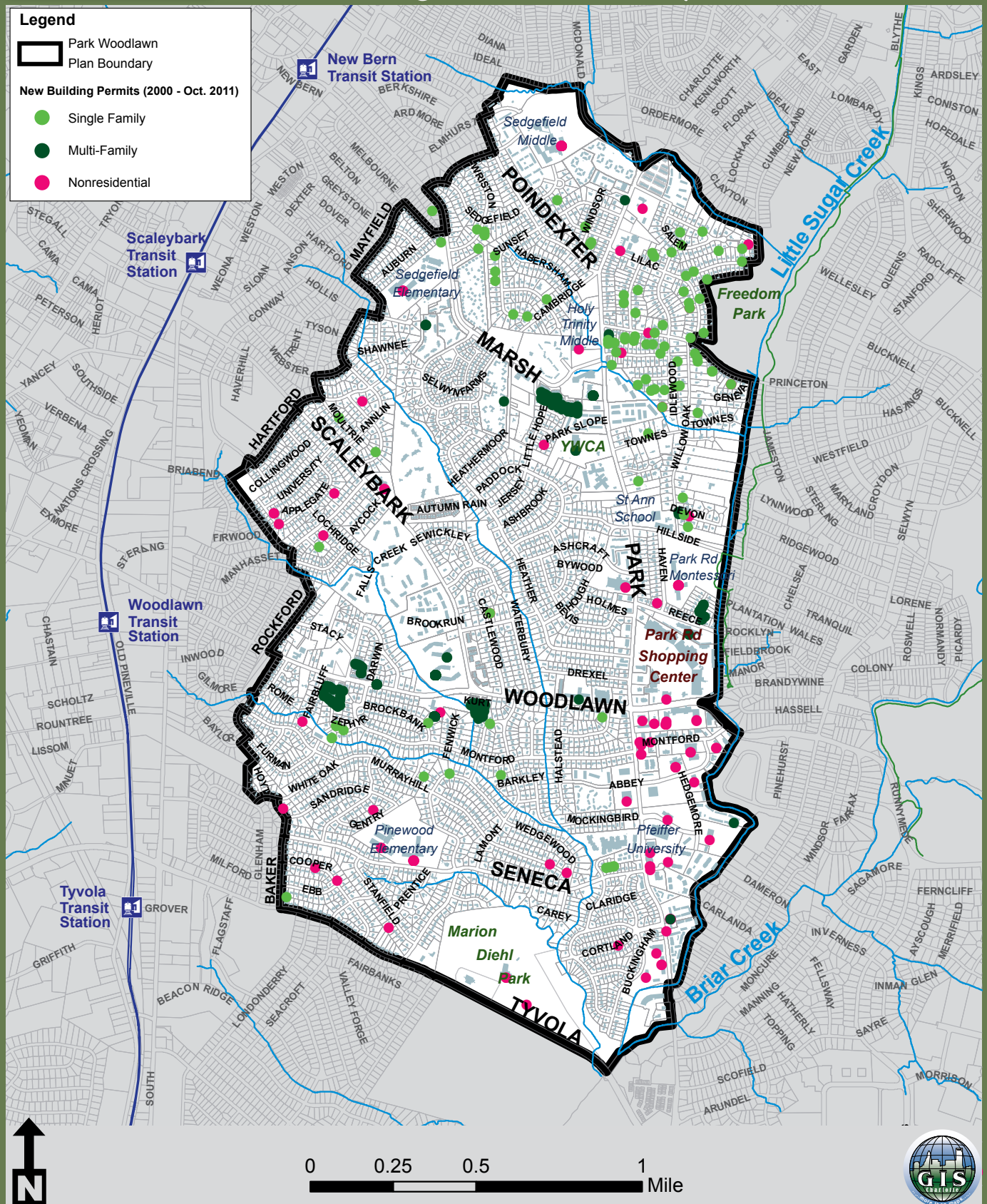
Non-Residential Building Permit Summary (Heated Square Feet)

Year of Issue Date	Office	Retail	Institutional	Other
2001	—	22,000	88,500	—
2002	—	3,500	—	—
2003	8,784	—	—	—
2004	8,864	2,369	113,617	—
2005	49,414	89,011	5,520	—
2006	16,896	—	66,379	768
2007	8,205	2,947	130,630	693
2008	5,337	7,751	37,235	30,750
2009	517	—	18,864	—
2010	—	4,223	—	—
2011	—	—	—	—
TOTAL	98,017	131,801	460,745	32,211
Source: LUESA Building Permit Database				

Nonresidential

Between 2001 and 2011, there were 111 building permits issued for nonresidential activity in the *Park Woodlawn Area Plan* plan area, totaling 722,774 square feet. Nonresidential activity may be divided into three categories: office (offices, banks, medical offices), civic/institutional (schools, libraries, universities, museums), and retail. During this time period, permit activity approvals allowed for 408,534 square feet of institutional uses; 131,801 square feet of retail; and, 98,017 square feet of office. The greatest number of building permits (74) were issued between 2005 and 2008, with almost one-half represented by civic/institutional activity.

Park Woodlawn Area Plan Building Permit Activity



Existing Zoning

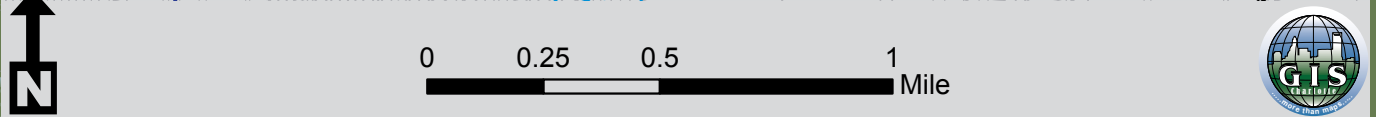
As shown in the table below and in **Map 10**, more than half of the plan area is zoned single family residential. The single family residential neighborhoods generally dominate the study area.

The second largest zoning category is multi-family. Large pockets of multi-family development are generally located off Scaleybark and Marsh Roads. Pockets of multi-family development are also located along and just off Park Road.

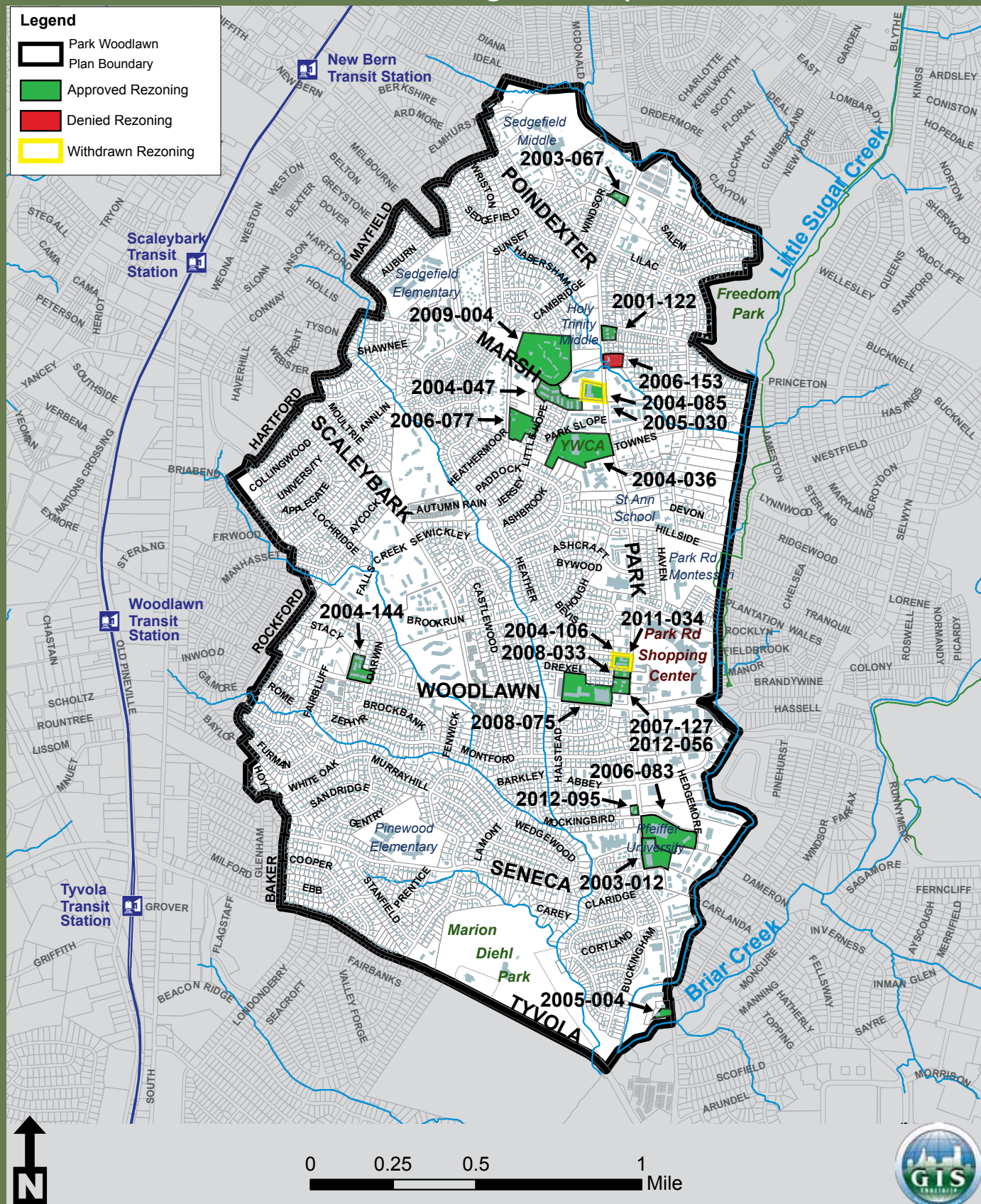
The third largest zoning category is business. The business zoning districts (B1 and B2), along with office, are generally located along Park Road north and south of Woodlawn Road. This area, generally located in the southeast portion of the study area, is considered an activity center. As is often the case with many activity centers, the Park Road activity center generally consists of a mix of commercial, retail, office, personal service and multi-family residential uses. Other zoning districts represented in this activity center include UR-2, MUDD, B-D, and NS. The wide array of zoning districts reflects the different types of activities and uses represented in the activity center.

EXISTING ZONING for the Plan Area		
Generalized Zoning Category	Acres	% of Total
Single Family	1458.4	70.6%
Multi-Family	419.6	20.3%
Urban Residential	13.3	0.6%
Business (Retail)	76.6	3.7%
Office	74.2	3.6%
Mixed Use	11.3	0.5%
Institutional	9.9	0.5%
Industrial	2.7	0.1%
TOTAL	2066.0	100.0%
<i>Source: Charlotte-Mecklenburg Planning Department, Oct 2011</i>		

Park Woodlawn Area Plan Existing Zoning Designations



Park Woodlawn Area Plan Rezoning History



Produced by Charlotte-Mecklenburg Planning Department: October 5, 2011

Park Woodlawn Area Plan Rezoning

Rezoning 2001-2012

Between 2001 and 2012, City Council heard and made decisions on a total of 17 rezoning petitions located in the plan area boundary. See **Map 11**. The majority of these rezoning requests involved rezoning property from single family to multi-family and UR districts to accommodate apartments, townhomes, and condominiums. A few requests involved the rezoning office, single family and multi-family zoned properties to MUDD in order to accommodate a mix of uses. These sites are generally located in the activity center, along Park Road. Another MUDD request involved rezoning property from O-1 to accommodate the addition a 60,000 square foot office building, and 75 condominium residential units in association with Pfeiffer University.

Rezoning					
Approved (2001-2012)					
Petition Number/ Name	Location	Acres	Former Zoning	Requested Zoning	Action
2001-122 Tuscan Development II, LLC	Southeast corner of Park Road and Princeton Avenue	0.88	R-8(CD)	R-8(CD) SPA	Approved 11/19/2001
	DEVELOPMENT PROPOSED Four single family units, one duplex unit (six residential units on five lots with an overall density of 6.8 du/ac)				
2003-012 Brigham Enterprises, LLC	East side of Park Road south of Mockingbird Lane	3.63	O-2	B-D	Approved 9/15/2003
	DEVELOPMENT PROPOSED 73,000 square feet of climate controlled self-storage, up to 9,000 square feet of office space, up to 48,000 square feet of uses permitted in NS district. Administrative approvals to: modify square footage to 76,114 square feet of self-storage, 7,200 square feet of office and 34,000 square feet of office and retail uses permitted in the NS; relocate dumpsters and clarify the screening of rooftop mechanical equipment; allow minor changes in elevations.				
2003-067 Anderson & Beverly Pearson	Southwest corner of Park Road and Park West Drive	0.61	R-4	R-17MF(CD)	Approved 9/15/2003
	DEVELOPMENT PROPOSED Allow a maximum of seven for sale residential condominiums at a density of 11.48 du/ac. Provide an 11.25' class "C" buffer along the common property line with Devonshire Place.				
2004-036 YWCA of Charlotte	West side of Park Road north of Hillside Avenue	9.65	R-4(SUP)	INST(CD) & termination of SUP	Approved 4/19/2004
	DEVELOPMENT PROPOSED Allow 11 dwelling units, one of which is for staff and meeting space, to bolster their Families in Transition program.				
2004-047 St Lukes Village II, LLC	South side of Marsh Road, east of Little Hope Road	3.79	R-17MF(CD)	R-17MF(CD) SPA	Approved 5/17/2004
	DEVELOPMENT PROPOSED Eliminate the age restriction for the for-sale townhome project approved in 1999; allow a maximum of 29 residential units at an overall density of 7.65 du/ac.				
2004-144 Bridge Partners	North side of East Woodlawn Road between Stacy Boulevard and Darwin Circle	2.03	R-4	UR-2(CD)	Approved 2/21/2005
	DEVELOPMENT PROPOSED Allow 23 townhomes on two acres with an overall density of 11.5 du/ac.				

Park Woodlawn Area Plan Rezoning

Rezoning					
Approved (2001-2012)					
Petition Number/ Name	Location	Acres	Former Zoning	Requested Zoning	Action
2005-004 Dr. Wayne C. Mercer	West side of Park Road, south of Selwyn Avenue	0.88	R-22MF	O-2	Approved 2/21/2005
	DEVELOPMENT PROPOSED Conventional petition to allow the expansion of the existing animal hospital.				
2005-030 3234 Park Rd, LLC	West side of Park Road south of Marsh Road	2.02	R-4	R-12MF(CD)	Approved 4/18/2005
	DEVELOPMENT PROPOSED Allow 24 condominiums in one building.				
2006-077 Modern Dwellings, LLC	West side of Little Hope Road south of Marsh Road	3.6	R-4	UR-1(CD)	Approved 7/17/2006
	DEVELOPMENT PROPOSED Allow 13 single family lots, two lots for duplexes and a public and private road system.				
2006-083 Pfeiffer University	East side of Park Road south of Mockingbird Lane	6.2	O-1	MUDD(CD)	Approved 7/17/2006
	DEVELOPMENT PROPOSED Allow a 60,000 square foot classroom/administration building for Pfeiffer College, a 60,000 square foot office building, and 75 condominium residential units.				
2007-127 Charlex Development Corp., LLC	Southwest corner of Park Road and Drexel Place	1.2	R-4, R-43MF and O-2	MUDD(CD)	Approved 1/22/2008
	DEVELOPMENT PROPOSED Allow 9,900 square feet of office/commercial uses and up to 25 for sale residential condominiums (20.8 du/ac) in a vertically integrated mixed-use project (restaurants not allowed) , with building limited to four stories in height.				
2008-033 Pacific Southern Development, LLC	West side of Euclid Avenue between Heather Lane and Drexel Place	2.08	R-4 and O-2	MUDD(CD)	Approved 3/17/2008
	DEVELOPMENT PROPOSED Allow 38,000 square feet of retail, general and medical offices, and restaurant. Dedication of additional right-of-way along Heather Lane to provide 55 feet of right-of-way, potentially permitting a future left turn lane. No restaurant permitted to operate after 11:00 pm and alcohol sales not to exceed 35% of gross receipts.				
2008-075 Fairfield Development LP	North side of East Woodlawn Road between Park Road and Halstead Drive	6.4	R-4, R-43MF and R-6MFH(CD)	UR-3(CD) and R-4	Approved 11/17/2008
	DEVELOPMENT PROPOSED Approve redevelopment of two existing multi-family projects for 302 apartment units on 6.4 acres (overall density of 47.2 du/ac).				
2009-004 City of Charlotte Housing Authority	North side of Marsh Road between Park Road and Selwyn Farms Lane.	12.48	R-15MF(CD)	R-17MF(CD)	Approved 3/19/2009
	DEVELOPMENT PROPOSED Approve redevelopment of an existing 49-unit apartment complex with 181 multi-family apartments, with a resulting density of 14.5 units per acre.				

Park Woodlawn Area Plan Rezoning

Rezoning					
Approved (2001-2012)					
Petition Number/ Name	Location	Acres	Former Zoning	Requested Zoning	Action
2011-034 Bank of the Ozarks	Southwest corner of Park Road and Heather Lane	1.25	MUDD(CD)	MUDD-O	Approved 7/18/2011
	■ DEVELOPMENT PROPOSED ■ Allow a maximum 12,000 square-foot two-story branch bank building or other office uses permitted in the MUDD zoning district. Limit building height to two stories and up to 60 feet. Optional provisions include: drive-through service lanes and windows permitted only as an accessory use to a bank or financial institution; parking and maneuvering between the building and Heather Lane; and ground mounted detached sign to have a maximum sign face area of 50 square feet.				
2012-056 Selwyn Property Group Investments, LLC	Southwest corner at the intersection of Park Road and Drexel Place	1.13	MUDD(CD)	MUDD-O	Approved 07/16/12
	■ DEVELOPMENT PROPOSED ■ Site plan amendment (amending petition 2007-127) allowing a reduction in nonresidential square footage, an increase in number of residential units, and a reduction in parking spaces. Amendment allows for maximum of 4,200 sq ft (decrease from 9,900 sq ft) of nonresidential uses including retail, general and medical office uses, and business services; nonresidential uses limited to ground floor; maximum building square footage of 48,000 sq ft; up 42 residential dwelling units (increase from 25 residential units) that may be for sale or for rent; prohibition of restaurants; permit neighborhood food and beverage service use; maximum building height of four stories and 55 feet.				
2012-095 Michael Brawley	Northwest corner at the intersection of Mockingbird Land and Park Road	0.35	O-2	MUDD-O	Approved 12/17/12
	■ DEVELOPMENT PROPOSED ■ The petition proposed a 9,052 square foot development with uses allowed in the MUDD zoning district. The requested optional provisions pertain to signage, parking and maneuvering between the building and the street, and screening requirements.				
Denied or Withdrawn (2001-2012)					
2004-085 Renaissance Development Group, LLC	West of Park Road and south of Marsh Road	2.23	R-4	UR-2(CD)	Withdrawn
	■ DEVELOPMENT PROPOSED ■ Request to construct three-story condominium building with underground parking with up to 26 units (average 1700 sq ft/unit).				
2004-106 The Reservoir Co.	Southwest corner of Park Road and Heather Lane	1.19	O-2	MUDD(CD)	Withdrawn
	■ DEVELOPMENT PROPOSED ■ Up to 19,295 square feet of retail, restaurant, office and residential uses. 12,360 square feet of existing office uses on site. Existing buildings to be retained and two additions constructed.				
2006-153 New South Properties of the Carolinas / Damon Mitchell	Southeast corner of Park Road and Yale Place	1.3	R-4	UR-1(CD)	Denied 1/22/2007
	■ DEVELOPMENT PROPOSED ■ Allow the construction of seven single family detached homes on 1.26 acres at a density of 5.5 du/ac.				
Source: Charlotte-Mecklenburg Planning Department					

Existing Land Use

The predominant land use is residential with single family comprising the largest category at 58.8% of total acres or 1,050 acres. Multi-family use is the second largest existing land use category with 16.7% or 297 acres. Civic and institutional uses such as schools and churches, is the third largest category with 9.2% or 163 acres. See Map 12.



Single Family



Multi-Family



Retail



Office



Civic/Institutional



Parking



Vertical Mixed Use



Horizontal Mixed Use
Non-Residential



Open space/Recreation



Vacant

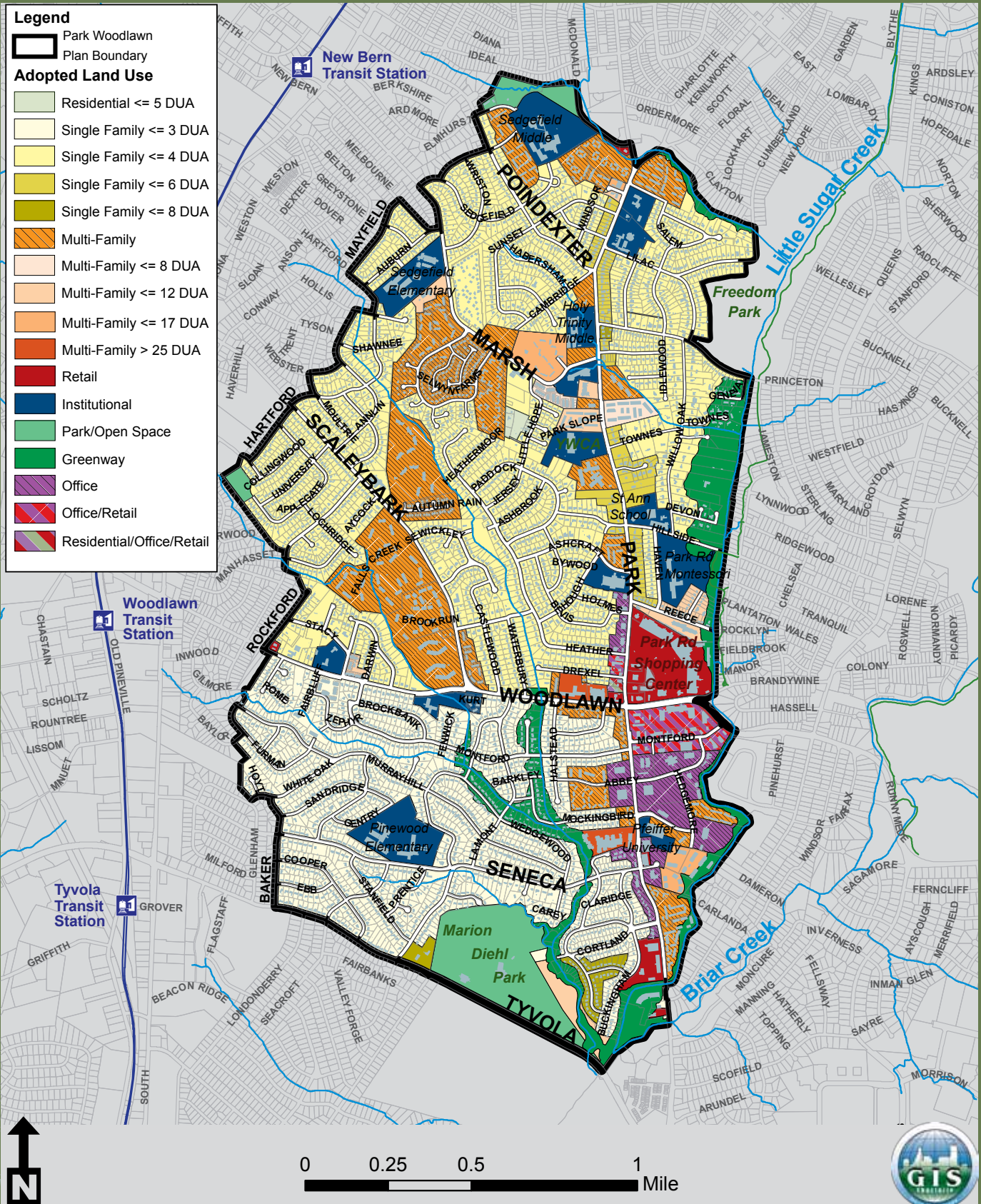
Existing Land Use for the Plan Area

Land Use	Acres	% of Total
Single Family	1,050.1	58.8%
Multi-Family	297.4	16.7%
Civic/ Institutional	163.5	9.2%
Office	50.1	2.8%
Open Space/ Recreation	107.0	6.0%
Retail	32.7	1.8%
Mixed Use Vertical Horizontal	43.2	2.4%
Vacant	40.5	2.3%
Parking	1.0	0.1%
TOTAL	1,785.5	100.0%
<i>Source: Charlotte-Mecklenburg Planning Department, Oct 2011</i>		

Park Woodlawn Area Plan Existing Land Use



Park Woodlawn Area Plan Adopted Future Land Use

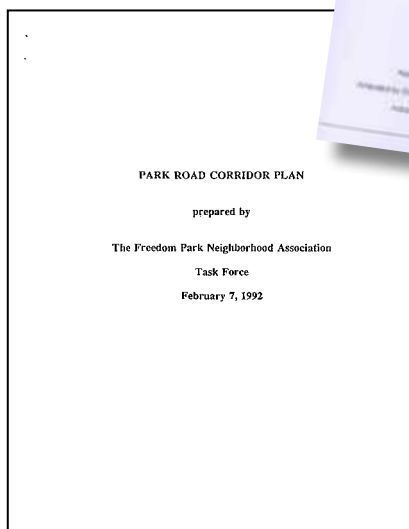
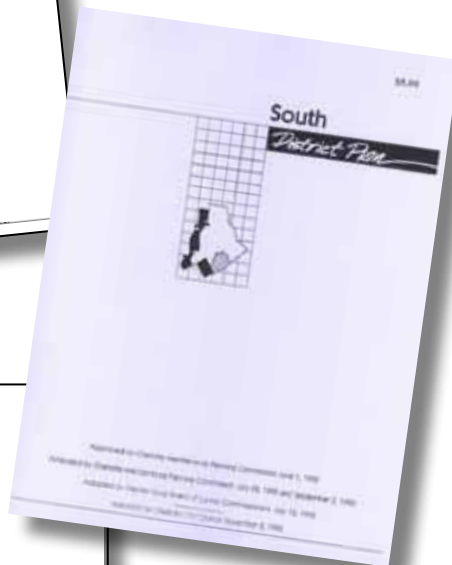
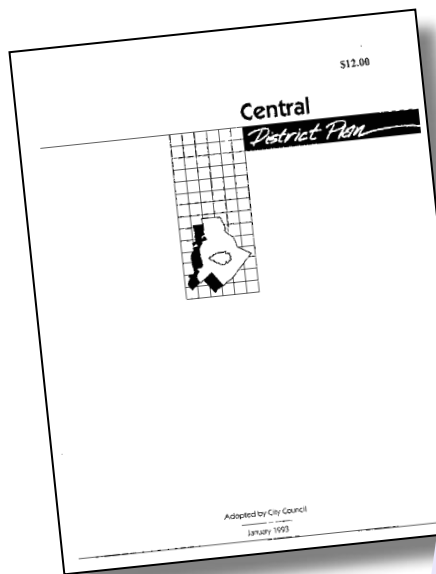


Produced by Charlotte-Mecklenburg Planning Department: October 5, 2011

Park Woodlawn Area Plan Adopted Future Land Use

Adopted Future Land Use

The *South District Plan* (1992), *Central District Plan* (1993) and *Park Road Corridor Plan* (1992) provide land use guidance for this portion of Charlotte. These plans are updated by subsequent approved rezonings and area plans. As depicted on **Map 13**, the future land use recommendations generally follow the pattern of existing land use.



Adopted Land Use for the Plan Area

Land Use	Acres	% of Total
Single Family	1,030.7	57.7%
Multi-Family	320.9	18.0%
Retail	43.4	2.4%
Office	48.7	2.7%
Institutional	130.8	7.3%
Park/Open Space/ Greenway	187.1	10.5%
Mixed Non-Residential	22.9	1.3%
Mixed Residential and Non-Residential	1.2	0.1%
TOTAL	1,785.7	100.0%

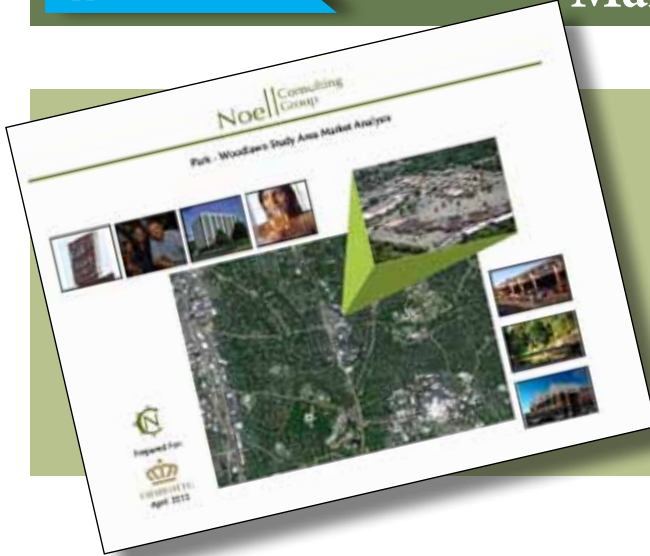
Source: Charlotte-Mecklenburg Planning Department, Oct 2011

Non-Residential Square Footage in the Plan Area Through 2010

Non-Residential Category	Heated Sq Ft	% of Total
Institutional	1,097,385	36.9%
Office	1,027,711	34.6%
Retail	682,299	23.0%
Parking Garages/ Decks	66,496	2.2%
Warehouse	96,810	3.3%
TOTAL	2,970,701	100.0%

Source: Mecklenburg County Tax Parcel Data, 2010

Park Woodlawn Area Plan Market Study



Noell Consulting Group out of Atlanta, GA was contracted to conduct a market study for the study area in order to examine what the projected long term market demands are for the area.

The study found that some of the STRENGTHS for the area are:

- Area is surrounded by strong and stable neighborhoods with abundant charm.
- Easily accessible to three major employment cores – Uptown, Hospital District, and Southpark.
- Little Sugar Creek Greenway and access to Freedom Park and Marion Diehl Park, are examples of two community amenities found in the area.
- Strong neighborhood-serving retail environment with neighborhood feel
 - Park Road Shopping Center a local institution and performing well.
 - Montford Drive and “organic” neighborhood dining/bar district.
- Quality infill residential development along Park Road, including for-sale and rental projects.
- Proximity to high incomes creates improved opportunities for attached residential, retail and office uses.
- High traffic counts create retail opportunities around Park / Woodlawn.

The study also found that some of the CHALLENGES for the area are:

- Office market stable, although dynamics are relatively weak.
- Proximity to Southpark tempers retail potential and keeps the focus on neighborhood serving retail uses.
- Small properties around Park/Woodlawn intersection very difficult to assemble/redevelop also run into sensitivities with adjacency to neighborhoods.

The study found that some OPPORTUNITIES in the area are:

- Maintain and enhance core as neighborhood center.
- Continue infilling residential, including in core of Park/Woodlawn area.
- Reinforce neighborhoods via connections to greenways, parks, and retail core.
- Create a greater, more walkable lifestyle in the area through stronger horizontal and vertical integration of low-rise uses.

The market study focused individual attention to different land use categories (retail, office, residential - for sale and rental) separately and offered its interpretation of what the opportunities and issues are for each category.



RETAIL – The study area contains strong and stable neighborhoods supporting local neighborhood serving retail establishments along with an authentic retail/entertainment core. Opportunities include Infill development in Park Road Shopping Center parking lot to create more of a Main Street look and feel. Investigate potential to add residential above shops to further create a sense of place and destination. Support and strengthen Montford Drive’s restaurant core with additional retail and residential uses. Because of the lack of major intersections with major east-west roads (Woodlawn and Park Rd. is the only one), results much of the area being a weak retail location for national chains needing more regional access.

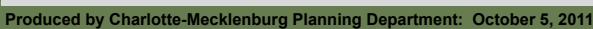
OFFICE – The future focus for the office market should be on smaller, local-serving office users, those largely under 5,000 SF. The Market Study highlighted the challenges facing office developments in the study area and its limits in regards to regional access and a dated product (the majority of office buildings in the area were built prior to 1970), including lack of walkability and amenities in the immediate area that are typical of class A office buildings.

RENTAL RESIDENTIAL – Locally like much of the country the rental residential market is performing better than most of the other real estate sectors, mainly as a result of the foreclosure crisis which has forced a lot of people who had been homeowners before into the rental market. The benefit of the Park Woodlawn area is that it represents a more affordable alternative to South End and Southpark, yet provides similar in-town convenience as well access to a small neighborhood retail core. There has only been one new modern apartment community constructed recently (Cielo) in the study area, and there is opportunity for more similar projects to be developed, the only question is one of feasibility and assemblage of parcels.

FOR SALE RESIDENTIAL – The for-sale residential market in the study area experienced a drop in prices similar (but not to the same extent) as in Mecklenburg County and the rest of the Country experienced during the past several years. The suburbs in most metros, including Charlotte, have largely been hit harder than in-town neighborhoods such as Park Woodlawn, which have solid fundamentals, such as a limited land supply which tempers infill opportunities, and very good access to neighborhood-serving retail, community and regional serving retail, as well as job centers. With its stable neighborhoods, convenient access to employment and shopping, and its local vibe and neighborhood core, the Park Woodlawn area has been, and will continue to be a strong location for infilling new for-sale residential housing. Given the limited supply of land in the area, the overwhelming majority of demand potential in the area will be on properties already existing as residential uses, with a greater share of demand being for townhouses and condos (roughly 500 units in the next 20 years) rather than infill single-family houses (141 net new homes). The higher density development in the area would in turn then be used to bolster more retail and entertainment development, especially along Montford Drive and in the Park Road Shopping Center.

Park Woodlawn Area Plan

Average Daily Traffic Counts



Transportation

Street Network

The Park Woodlawn Area benefits from strong land use accessibility and a moderate degree of connectivity. Area residents benefit from being in close proximity to a mixture of land uses that enable them to live, work, play, socialize, and shop within a short distance of home. In addition, the connected street network helps shorten travel distances for all transportation users and improves overall transportation system capacity by providing area residents and visitors with more route choices.

Key measures for the Park Woodlawn area include:

Connectivity Index:	1.25
Intersection Density (3.2 sq.mi.):	90
Signalized Intersections Density:	4

The connectivity index quantifies how well a street network connects places. The index is calculated by dividing the number of roadway links by the number of roadway nodes. A connectivity index of 1.45 or more is desirable for in-town neighborhoods, such as Park Woodlawn. The current score of 1.25 suggests a need for some added connectivity. This is especially important for blocks closest to shopping, schools, parks and local transit routes.

Intersection density measures the number of intersections per square mile. The higher the density is, the greater the travel options there are. An intersection density of 200 or more is desirable for in-

town neighborhoods. Signalized intersections are especially important for pedestrians as safer places to cross major thoroughfares.

Due to the established “in-town” character of the Area, residents already benefit from having a connected street network and a balanced mixture of land uses, both of which support a more urban lifestyle. As shown in the table below, the Area currently receives high marks relating to land use accessibility relative to the city average, with the major exception being shopping. This exception is surprising, given the proximity of the Park Woodlawn Mixed-Use Activity Center. However, many of the blocks within Plan Area boundaries are located well over a quarter mile from this Center.

Land Use Accessibility			
	PlanArea		City
	Pop	%	%
Within 1/4 mile of:			
Shopping	3,884	32%	67%
Schools	5,958	49%	26%
Parks	5,686	47%	28%
Local transit route	8,884	73%	64%
Total Population	12,164	100%	100%
Source: Charlotte Department of Transportation			

Thoroughfare Plan

The *Mecklenburg-Union Thoroughfare Plan* is the adopted major roadway plan for Mecklenburg and Union counties and is used to assure that the most appropriate street system is developed to meet existing and future travel needs. Streets are classified and designed according to their intended functions so that land use and traffic

conflicts are minimized. The street classifications applicable to the Park Woodlawn Area are as follows:

THOROUGHFARES: Park Road, Tyvola Road, and Woodlawn Road are major thoroughfares serving the area. As major thoroughfares, these streets are designed to accommodate large volumes of traffic at moderate speeds and provide access to major commercial, employment and residential land uses. Scaleybark Road is a minor thoroughfare that collects traffic from the local collector streets and carries it to the major thoroughfares. Minor thoroughfares are designed to carry moderate traffic volumes at moderate speeds and provide access to residential, commercial and institutional land uses.

COLLECTORS & LOCALS:

Abbey Place, Hillside Avenue, Marsh Road, Murrayhill Road, Poindexter Drive, and Seneca Place are major collectors. Auburn Avenue, Hartford Avenue, Little Hope Road, Montford Drive, Princeton Avenue, Selwyn Farms Lane, Sunset Drive, and Wedgewood Drive are minor collectors. Collectors are designed to carry traffic between

the thoroughfares and local streets at moderate volumes yet lower speeds, while providing access to adjacent land uses. The remaining roadways are local streets that carry low traffic volumes, have slow operating speeds and provide access to individual properties. See **Map 14.**

Level of Service

Level Of Service (LOS) measures the quality of service provided by a transportation facility to its users. The City of Charlotte evaluates LOS for signalized intersections and uses the results to help balance competing user needs when planning and designing streets. LOS ranges from A through F (A being best), with desirable levels of service based on the street typologies of the Urban Street Design Guidelines.

Pedestrian and bicycle LOS is rated according to the level of comfort and safety offered by the design features at signalized intersections. Motorist LOS is measured by the Volume to Capacity ratio (V/C), which describes an intersection's ability to process traffic within peak time periods. Values greater than .95 suggest an intersection is near its capacity during this time period and that motorists may experience substantial congestion. Levels of service ratings are shown on **Map 15**.



Pedestrian System:

Of the 49 miles of streets within the Area, 40% have sidewalks on one or both sides of the street, while 60% have no sidewalks at all. The existing pedestrian system is shown on **Map X: Existing Pedestrian & Bicycle Facilities**. Of the 13 signalized intersections in the Area, 8 have a LOS C or higher for pedestrians.



Bicycle System:

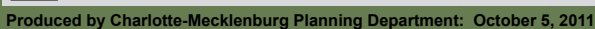
Both Scaleybark Road and Seneca Place have bicycle lanes. A recent NCDOT resurfacing of Woodlawn Road adjusted lane striping to create wide outside travel lanes for mixed travel by bicyclists with motorists. Signed bicycle route #2 follows multiple local streets within area neighborhoods, including Dorchester Place, Auburn Avenue, Hartford Avenue, Murrayhill Road, and Wedgewood Drive. Of the 13 signalized intersections within the Area, none have an LOS C or higher for cyclists.



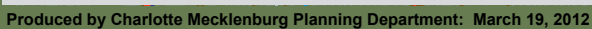
Motor Vehicle Network:

The Area is currently experiencing moderate levels of congestion as indicated by the number of signalized intersections in the plan area with a congested V/C ratio (.95, as shown on **Map 15**). Congestion levels are expected to remain the same or slightly increase as the area and its greater surroundings grow in travel demand.

Park Woodlawn Area Plan Transportation Levels of Service



Park Woodlawn Area Plan Existing Pedestrian & Bicycle Facilities



Benefits of Sidewalks

From the *Federal Highway Administration's Designing Sidewalks and Trails for Access*

- Pedestrians have an easier time pushing shopping carts, luggage and other wheeled devices
- All pedestrians are able to choose trail experiences that best suit their needs, skill level and desired experience
- Parents are able to more easily push children in strollers on sidewalks and trails with accessible surfaces
- Young children can use curb ramps when riding their bicycles
- Children, parents and school systems benefit from sidewalk networks with visible pedestrian crossings and other safety features
- Children learn independence by having a safer place to travel
- A good system of sidewalks may allow older pedestrians who no longer drive to walk to services and socialize in their community, while offering a continued independent lifestyle
- Businesses can load and unload goods more easily
- Wide sidewalks can be gathering places in neighborhoods and business districts, and offer space for family and friends to walk and socialize together
- Wide sidewalks in business communities offer an opportunity for trees, landscaping and other amenities that create an inviting place for customers to shop and do business
- Well-maintained sidewalks and trails encourage exercise and provide the benefits of a healthy lifestyle

Benefits of Bicycling

Bicycling provides an alternative to congested streets, a cleaner environment and contributes to improved physical health and mental well being. See **Map 16**. As in other cities, more people in Charlotte are discovering these and many other benefits of bicycling. In recognition of its efforts to improve its bicycle environment, the City of Charlotte was recognized by the League of American Bicyclists as a 2008-2012 Bicycle Friendly Community (BFC) at the bronze level.

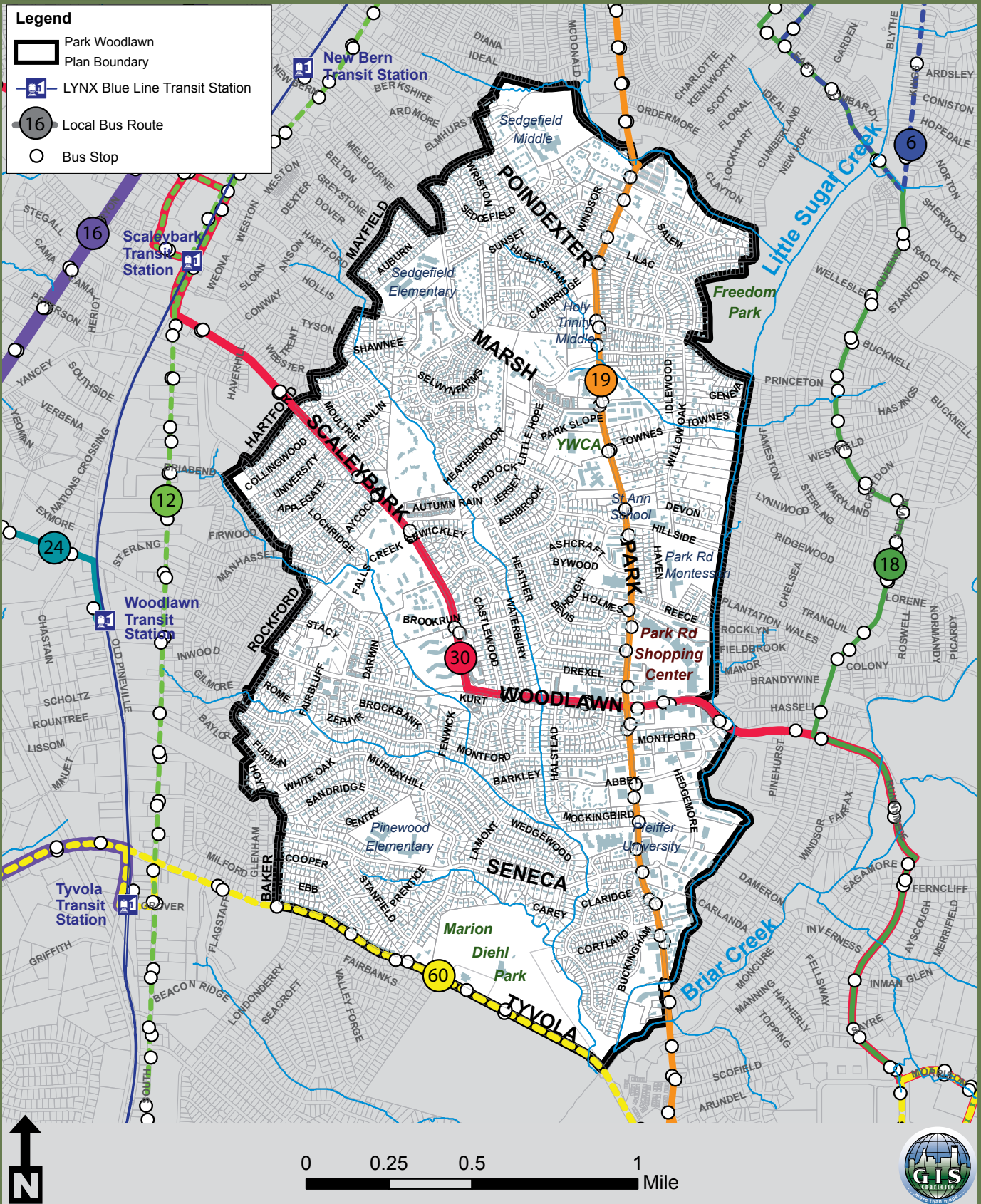
Helpful information on cycling can be found on the City of Charlotte Transportation website at the link below:

<http://charmeck.org/city/charlotte/Transportation/PedBike/Pages/Home.aspx>



Park Woodlawn Area Plan Transit Service

Map 17



Produced by Charlotte-Mecklenburg Planning Department: October 5, 2011

Transit Services



Six transit shelters are located within the plan area boundary.

Fixed-Route Bus Services

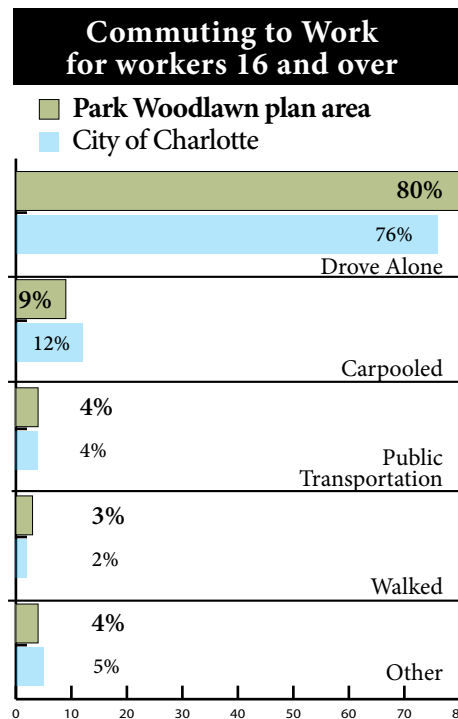
The Charlotte Area Transit System (CATS) currently provides the following fixed-route bus service in the Park Woodlawn area. All routes have a significant number of bus stops within the plan area. See Map 17.

Fixed Route Ridership

In Fiscal Year 2011 the routes serving the Park Woodlawn Area carried an average of 5,091 passengers per month.

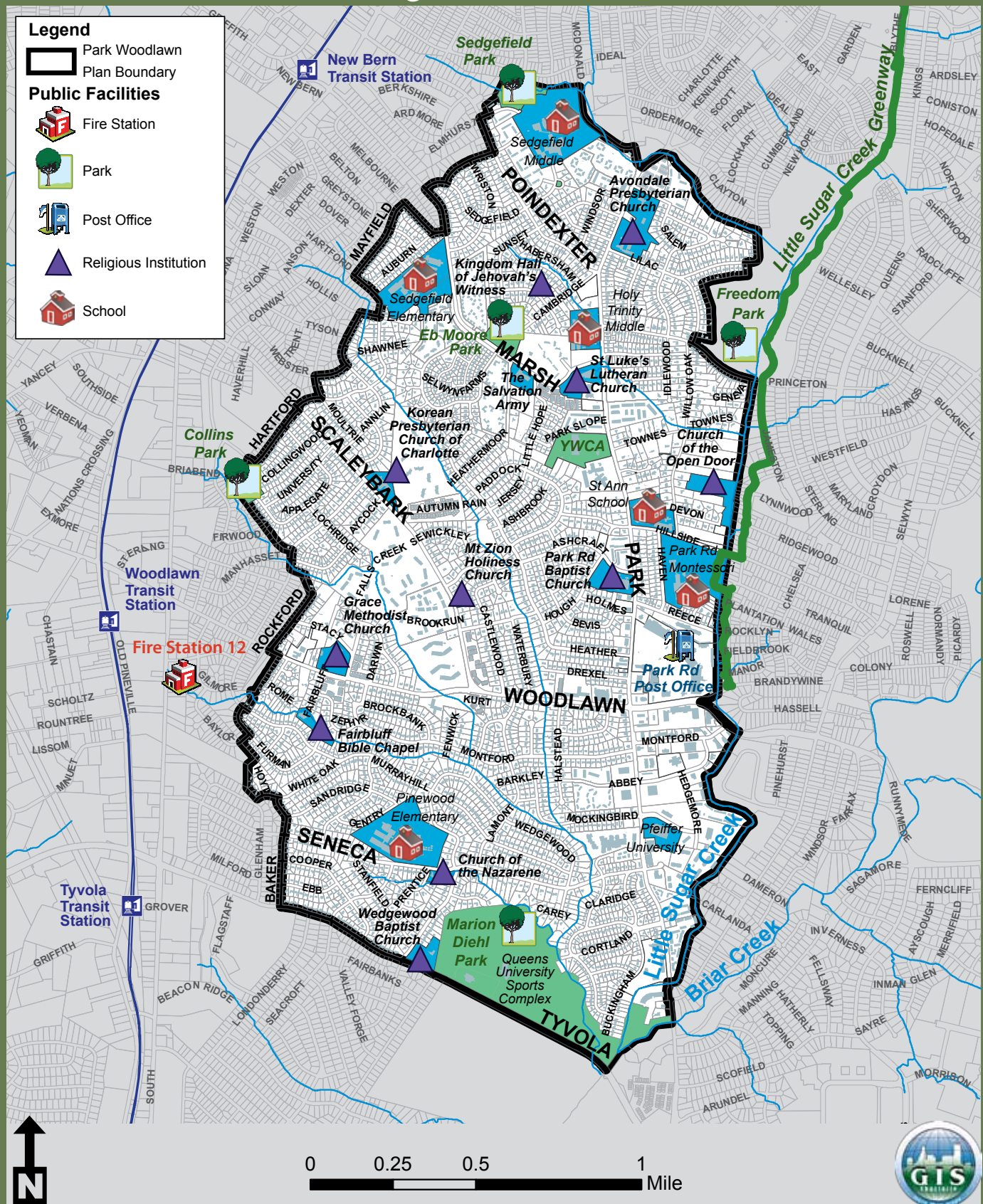
Fixed-Route Bus Service				
Route #	Route Extent	Days of Service	Monthly Ridership in FY2010	Monthly Ridership in FY2011
Route 19	Park Road operates between Uptown and the Sharon Road West LYNX station, with service to South Park	Weekdays/ Sat/ Sun	32,137	33,956
Route 30	Woodlawn/Crosstown operates between South Park and the Scaleybark Road LYNX station	Weekdays/ Sat	15,199	18,011
Route 60	Tyvola/Southpark operates between West Charlotte and South Park	Weekdays/ Sat	7,826	9,126

Source: Charlotte Area Transit System (CATS)



Source: 2006 to 2010 American Community Survey (ACS) 5-Year Estimates (Block Group)

Park Woodlawn Area Plan Existing Public Facilities



Produced by Charlotte Mecklenburg Planning Department: March 19, 2012

Infrastructure and Public Facilities

Public Services

The Park Woodlawn area is a stable and thriving community. The needs of the residents in the community are being met by existing public services such as police, fire, water, sewer, and post offices. See **Map X: Existing Public Facilities**. Currently, there are no fire stations, libraries, and police stations, in the plan area. However, The *Park Woodlawn Area Plan* is serviced by the following public services:

Charlotte Fire Departments

Fire Station 12, located on Inwood Drive, opened in 1961 and houses engine Company 12.

Fire Station 16, located on Park South Drive, opened in 1966 and houses engine Company 16, and Ladder 16.

Charlotte-Mecklenburg Police Department

The plan area is protected by Westover Division located at the intersection of West Boulevard and Remount Road in the Westover Shopping Center.

Libraries

The plan area is in proximity of two Charlotte-Mecklenburg Libraries, the Meyers Park branch is located at the intersection of Queens and Providence Road and the Scalybark branch is located on the corner South Boulevard and Scalybark Road.

Post Offices

Park Road Shopping Center is home to a United States Post Office. The post office located on 4117 Park Road is in the plan area.



Fire Station 16



US Post Office at Park Road Shopping Center

Water and Sewer

Charlotte-Mecklenburg Utilities (CMU) provides for public water distribution and sanitary sewer systems. The goal is to continue to provide water and sewer service in new ways by upgrading infrastructure for the residents of the community. There is a CMU Waste Water Treatment Plant just south of the plan area. Currently there are no updates to the water distribution and sewer systems.

Schools

The area is served by a number of public and private schools as well as higher education institutions.

- **Holy Trinity Catholic Middle School**
is part of the Catholic Schools of the Roman Catholic Diocese of Charlotte, located at 3100 Park Road, and operates grades 6th through 8th.
- **Park Road Montessori School**
is part of the Charlotte-Mecklenburg School system located on Haven Drive and has grades from kindergarten through 6th grade. The original school building was built in 1948 and sits on a rolling landscape of 26 acres.
- **Pinewood Elementary School**
is part of the Charlotte-Mecklenburg School system located on 805 Seneca Place and has grades from kindergarten through 5th grade.
- **Saint Ann's Catholic School**
is part of the Catholic Schools of the Roman Catholic Diocese of Charlotte, located at 600 Hillside Avenue, and operates PK, K through 5th grade.
- **Sedgefield Elementary School**
is part of the Charlotte-Mecklenburg School system located on 715 Hartford Avenue and has grades from kindergarten through 5th grade.
- **Sedgefield Middle School**
is part of the Charlotte-Mecklenburg School system located on 2700 Dorchester Place and has grades from 6th through 8th.
- **Pfeiffer University**
is located on 4701 Park Road in the plan area. The Charlotte campus opened in 1997 and is one of three campuses. Current enrollment for all three campuses is 2020 students. The Charlotte campus offers undergraduate, graduate and on-line courses for students.



Park Road Montessori School



Saint Ann's Catholic School

Parks & Greenways

Parks, greenways, open spaces, and recreational facilities are important amenities to the *Park Woodlawn Area Plan* residents. The goal of the area plan is to have greenways, parks and open spaces well integrating into the community, as new development takes place. Currently there are 4 parks and the Little Sugar Creek Greenway in the plan area.

Collins Park

is 4 acres located on 4500 Applegate Road and features multi-purpose field, tennis courts, playgrounds and other natural amenities.

E.B. Moore Park

is 4 acres located on 901 Marsh Road and features a multi-purpose field, tennis courts and playgrounds.

Marion Diehl Park

is located on Tyvola Road between Park Road and Wedgewood Drive. The 65 acre site features a sports complex that was built as a collaborative agreement between

Mecklenburg County and Queens University. The multi-purpose facility includes sports fields, track & field facilities, conference center, pool, walking trails, and other natural amenities that can be accessed by both students of the university and Mecklenburg County residents.

Sedgefield Park

is 5 acres located on 621 Elmhurst Road and features basketball courts, tennis courts, playground and other natural amenities.

Little Sugar Creek Greenway

is on the eastern boundary of the plan area. The greenway meanders from Princeton Avenue to Brandywine Road. The 1-mile stretch of greenway that runs in the plan area is a major amenity for area residents. The greenway replaces a number of residential properties that had been built in the floodplain. These homes were built prior to flood plain ordinances. The Westfield project in the plan area was designed to increase open space, and wetlands to improve storm water runoff and filter contaminants. When finished, the Little Sugar Creek Greenway will feature 19 miles of trails, habitats, and flowing streams from Cordelia Park just north of uptown, through the plan area and on to the South Carolina line.

Institutional

YWCA of the Central Carolinas

was established in Charlotte, North Carolina in 1902. The current location at 3420 Park Road opened in 1965. This facility offers transitional housing for women and families. They also provide services such as workshops, meeting areas, and a computer resource center. It also has full exercise programs such as a gymnasium, indoor pool, and exercise facility.

Salvation Army of Greater Charlotte

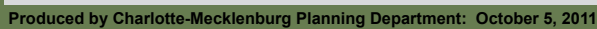
is located on 940 Marsh Road in the plan area. The charity provides services ranging from emergency assistance, shelter, clubs, adult rehabilitation, and disaster relief.

Religious institutions

Currently there are at least 12 religious institutions in the plan area. The following religious institutions are currently located in the plan area: Avondale Presbyterian Church, Church of the Nazarene, Church of the Open Door, Fairbluff Bible Chapel, Grace Methodist Church, Holy Comforter Episcopal Church, Korean Presbyterian Church, Kingdom Hall of Jehovah's Witnesses, Mt Zion Holiness Church, Park Road Baptist Church, Saint Luke's Church, and Wedgewood Baptist Church.

Park Woodlawn Area Plan

Environmental Conditions



Natural Environment

Natural Features

The plan area is bordered on the east by the Little Sugar Creek, and contains a number of its smaller tributaries flowing from the northwest to the southeast throughout the area. The Little Sugar Creek and Briar Creek converge around the southern boundary of the plan area, just north of the Tyvola Road and Park Road intersection. The area along Little Sugar Creek is also a designated FEMA Floodplain. The entire plan area is located within the Upper Little Sugar Watershed Basin, and within the Central Catawba River Basin. See **Map X: Environmental Conditions**.





One of the smaller tributaries in the area is Little Hope Creek. This tributary meanders through single family residential areas west of Park Road.

Park Woodlawn Area Plan

Land Cover

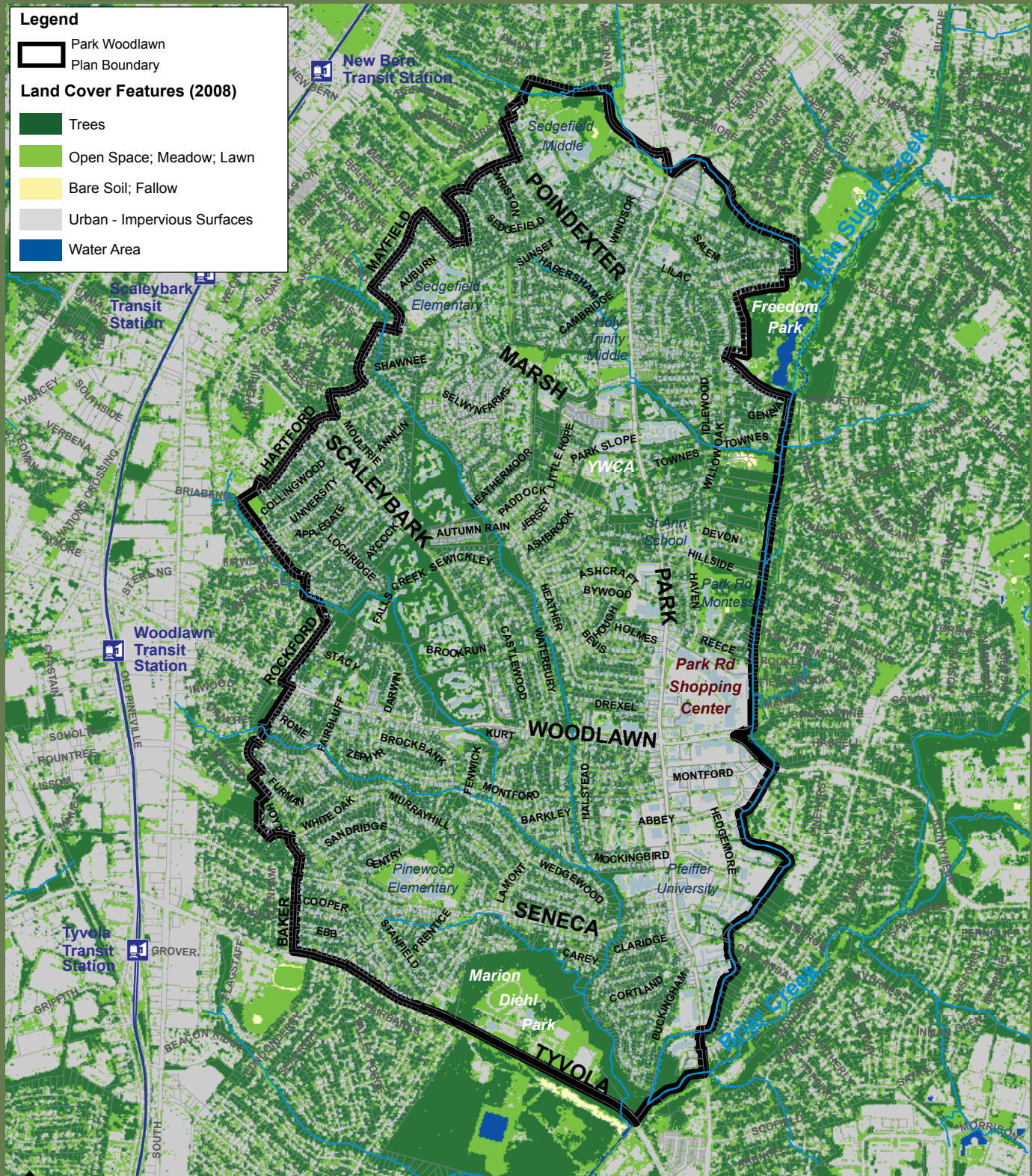
Map 20

Legend

-  Park Woodlawn
-  Plan Boundary

Land Cover Features (2008)

-  Trees
-  Open Space; Meadow; Lawn
-  Bare Soil; Fallow
-  Urban - Impervious Surfaces
-  Water Area



0 0.25 0.5 1 Mile



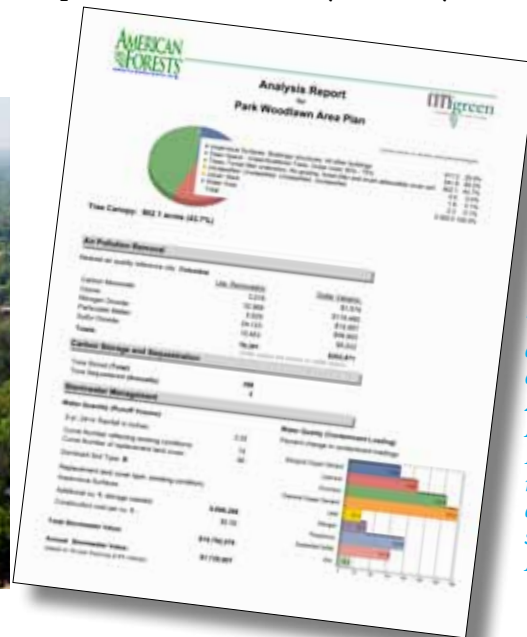
Produced by Charlotte-Mecklenburg Planning Department: October 5, 2011

Tree Cover

About 902 acres or 43.7% of the plan area is covered with trees, out of the total 2,065 total acres of the plan area. See **Map 20**. In the plan area there are an additional 541 acres or 26% considered open space, which are parcels that have grass cover or scattered trees on 50% -75% of the total parcel area. Around 30% of the plan area is considered impervious surface; that is buildings, structures, and any other hard surface that prevents the soil from absorbing water, which includes streets and parking lots. Part of the reason why the plan area has a relatively high percentage of land covered by trees and/or open space is because 87.5% of the area is within a Wedge as defined by Charlotte's Centers, Corridors and Wedges Growth Framework. The Wedges are the areas where the majority of development is low density residential, mainly through single-family developments which normally include yards with trees and grass cover.



Freedom Park in foreground with Park Woodlawn mixed use activity center in the background.



The analysis was derived using land cover classification of 2008 NAIP (National Agriculture Imagery Program) high-resolution imagery along with CityGreen software created by American Forests.



Park Towne Village on the corner of Park and Woodlawn Roads has, in addition to surface parking, a large patio of impervious material.

The Mixed-Use Activity Center on the other hand is where you will have more intense commercial development, and thus more impervious surface. In the Park Woodlawn Mixed-Use Activity Center there are a total of 259.7 acres, and 33.7 acres or 13% are covered with trees, with an additional 53.2 acres or 20.5% as open space. The majority of the Activity Center, 172.6 acres or 66.5% is considered impervious surface.



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