



East Boulevard Pedscape Plan



Charlotte-Mecklenburg Planning Commission
Adopted by City Council June 2002



Tree-lined ♦ Pedestrian-oriented ♦ Sidewalk activity ♦ Historic fabric ♦ Transportation choices

Table of Contents

Executive Summary i

Part I: Concept Plan

Introduction/Background 2
Context for Plan and Future Development 4
Map 1: Context Map 3
Map 2: Current Zoning 5
Map 3: Current Land Use 6
Vision for Corridor 12
Recommended Land Uses 14
Map 4: Proposed Land Use 15
Recommended Cross-Sections and Streetscape Development Standards 16
Figure 1: Cleveland Ave to Dilworth Road West 18
Figure 2: Dilworth Rd. West to Charlotte Drive 20
Figure 3a: Charlotte Drive to Kenilworth Avenue 22
Figure 3b: Kenilworth Avenue to Scott Avenue 24
Figure 3c: Scott Avenue to Cumberland/Garden Terrace 27
Figure 4: Garden Terrace to Little Sugar Creek bridge 28
Map 5: Maximum Building Heights 30
Recommended Streetscape and Pedestrian Improvements 32

Part II: Implementing the Vision 41

Appendix

A. East Boulevard Pedscape Plan Process Major Actions 45
B. Average Daily Traffic Volumes for East Boulevard (1994-2001) 46
C. Features of Future East Blvd. Streetscape Mentioned at 1st Public Meeting 47
D. Official Street Plan (concept only) 48

Executive Summary

Purpose of Plan:

The Pedscape Plan defines the vision and the land use policy for the East Boulevard corridor and shows how this vision will be achieved: first, by **providing standards for private sector investment** in new development; and second, by making **recommendations for public sector improvements** designed to make the area more pedestrian friendly. This plan also **supercedes the governing Central District Plan as the land use policy plan for the area** within the East Boulevard PED boundaries.

How will the Pedscape Plan be used?

- As **the governing land use policy document** for the area;
- To **identify the building setback** for PED zoned properties;
- As **guidance for streetscape improvements** required by MUDD, NS, and PED zoning districts; and,
- As **guidance for possible public investments** to enhance the pedestrian environment.

The Plan includes the recommendations in this document as well as a companion *Official Street Plan* (see Appendix D), which depicts the official future curb lines and setback lines for the East Boulevard PED area.

Plan Development & Process

The Pedscape Plan is the product of more than one year of work, beginning in the fall of 2000, by residents, property

owners and business owners, developers, the Dilworth Community Development Association (DCDA), the Historic District Commission, elected officials, and an interdepartmental team of city staff.

Description of the Corridor

Plan Boundaries: (*Map 2, page 6*)

The PED boundary was drawn to include all of the parcels fronting on East Boulevard from Cleveland Avenue to Freedom Park, and any parcels contiguous to those fronting on East Boulevard that are *currently* zoned for business, office or mixed-use. **More than half of the East Boulevard PED area falls into the Dilworth Historic District.**

Existing Zoning: (*Map 2, page 6*)

The majority of properties within the East Boulevard PED boundary are zoned for office uses (0-2 zoning comprises 54% of area) and “Neighborhood Business” (B-1 zoning; 32% of area).

Current Land Use/Community Design (*Map 3, page 7*)

The Pedscape Plan divides the East Boulevard into four sections which represent the distinct development and roadway characters of the corridor. Between South Boulevard at one end and Freedom Park at the other, the character of the street changes from a wide, tree-lined boulevard with grand houses (mostly converted to offices) and churches (Sections 1 and 2); to a neighborhood commercial center around Kenilworth and Scott Avenues, where the street is narrower and the buildings are closer to the roadway (Section 3); and then to a curving section where canopy trees reappear and handsome houses

(again, mostly used as offices) with generous setbacks lead into Freedom Park and the leafy, winding streets of the Myers Park neighborhood (Section 4).

Land Use Plan (Central District Plan)

The *Central District Plan* (1993) is the governing land use policy document for the area that includes East Boulevard. The *East Boulevard Pedscape Plan* recommends some changes to the *Central District Plan* proposed land uses. Once adopted, **the Pedscape Plan will become the governing land use policy document for the East Boulevard PED area.**

Historic Areas & Structures

The portion of the East Boulevard PED area from Cleveland Avenue to approximately Charlotte Drive (see *Map 3*, page 7) falls within the Dilworth Historic District. The Historic District also functions as a local overlay zoning district. In this district, the requirements of the Charlotte Historic District Commission and the current edition of the Commission's *Policy and Design Guidelines for Historic District Property Owners, Businesses & Residents* will apply. **Any new development within the portion of the PED area that is also in the historic district must meet the minimum PED and Pedscape Plan standards.** The Historic District Commission may increase these requirements beyond the minimum PED and Pedscape standards.

Transportation System

This plan intends to re-establish a balance of transportation options on the corridor by providing accommodations for transit, pedestrians, cyclists, *and* private motor vehicles.

Thoroughfare Status

East Boulevard is classified as a **major thoroughfare** on the Mecklenburg-Union Metropolitan Planning Organization's official *Thoroughfare Plan*. Current development standards for major thoroughfares inside of Charlotte Route 4 include a minimum 80-foot right-of-way and two vehicle travel lanes in each direction.

Vision for Corridor

The overall vision for the East Boulevard corridor is a **tree-lined** avenue, with **pedestrians** on sidewalks filled with attractive amenities such as sidewalk **cafés** in the commercial section and **public art** at appropriate locations. People would feel comfortable moving along the corridor on foot, by **bicycle**, on **transit**, or by private automobile. Well-designed, **pedestrian-oriented new development** mixed with the existing **historic fabric** will define the character of the street.

Recommended Land Uses (*Map 4*, page 16)

The land use vision for the East Boulevard PED corridor is generally consistent with what is proposed in the *Central District Plan*. Office uses are the primary recommended land use in between Cleveland and Dilworth Road East and between Lombardy Circle and Freedom Park, with commercial uses, mixed-use development, and multifamily development recommended between Dilworth Road East and Lombardy Circle. These **recommended land uses support the community vision of a mixture of land uses, which will provide pedestrian accessibility to services, workplaces, and residential choices within the neighborhood.**

For the East Boulevard PED corridor, the *Pedscape Plan* recommends several changes to the land uses proposed in the *Central District Plan*, which are shown in *Table 1* and *Map 4*, pages 15 and 16.

Recommended Cross-Sections and Streetscape Development Standards

The standards in this section supplement the requirements of the PED zoning. The Streetscape Development Standards are requirements for development that will primarily be the **responsibility of private developers or private land owners** to implement as property on the corridor is redeveloped or improved.

This section recommends future cross-sections for East Boulevard and identifies building setbacks and streetscape standards based on the ultimate curbline location. **The cross-sections are *not* plans for immediate road improvements, but are recommended long-term changes to be considered for implementation once a major portion of the necessary right-of-way is available.**

Building Setbacks and Future Street Cross-sections

The PED text states: **“The minimum setback will be measured from the back of all *existing* or *future* curbs, whichever is greater.”** (*City of Charlotte Zoning Ordinance*, Section 10.803.3) **Standards for setbacks, sidewalks, and**

planting strips will be met by developers who undertake new development or major renovations in the PED area. The required setbacks are defined in the cross-sections that follow for the different portions of East Boulevard.

Tree planting requirements in this section supercede the “Perimeter Planting Requirements” in Section 21-13(C) of the *Charlotte Tree Ordinance*.

The cross-sections in this document provide a *conceptual* view of the streetscape for each section, but the **Official Street Plan should be referred to** in order to determine the exact location of the future curb lines and setback lines for each block of East Boulevard. A summary of the recommended cross-sections and required setbacks is shown below and in *Table ex1* on the following page.

Cross-section Requirements for Sidestreets:

Section or Street	Setback	Planting Strip	Sidewalk
Sections 1, 2, Charlotte Drive Lombardy Circle Ashville Road	18'-0"	8'-0"	5'-0"
Kenilworth Avenue Scott Avenue	16'-0"	None required (trees in raised planters or tree pits required)	8'-0" minimum
Fountain View Floral Avenue Garden Terrace Cumberland Avenue	16'-0"	8'-0"	6'-0"

East Boulevard Pedscape Plan: Summary of Proposed Cross-Section Features (Table ex 1)

	Section 1 Cleveland Ave to Dilworth Rd. West	Section 2 Dilworth Rd. West to Charlotte Drive	Section 3a Charlotte Drive to Kenilworth	Section 3b Kenilworth Ave. to Scott Ave.	Section 3c Scott Ave. to Garden Terrace/Cumberland Ave	Section 4 Garden Terrace to Little Sugar Creek
Existing right-of-way	100 feet	70 feet – 66 feet	66 feet	61 feet	61 feet	60 – 61 feet
Future right-of-way	100 feet	90 feet	94 feet	78 feet	85 ft (option I) 101 ft (option II)	73 feet
Existing curbline (back-of-curb to back-of-curb)	71 feet	47 feet – 40 feet	40 feet	40 feet	39 – 40 feet	39 – 40 feet
Future curbline	71 feet	62 feet	62 feet	54 feet	53 ft (option I) 69 ft (option II)	45 feet
Minimum Setback (from back of curb)	34 feet (front porch) 44 feet (building face)	<i>In Historic District:</i> same as Section 1 <i>Outside Historic District:</i> 34 feet	16 feet	12 feet	24 ft (option I) 16 ft (option II)	34 feet
Sidewalk width	6 feet	6 feet	16 feet	12 feet	8 ft (option 1) 16 ft (option II)	6 feet
Planting strip width	8 feet	8 feet	none; Trees in raised planters	none; Trees in grates	8 ft (option I) raised planter (option II)	8 feet
Recommended cross-section features	<i>To be determined through public input process</i>	median/left-turn vehicle lanes (4) bike lanes	median vehicle lanes (4) bike lanes	vehicle lanes (4) left-turn lane	vehicle lanes (4) bike lanes on-street parking (option II only)	vehicle lanes (3) bike lanes

Recommended Streetscape and Pedestrian Improvements for Corridor

The most significant changes to the East Boulevard corridor will happen incrementally as new development occurs under the PED standards. There are minor improvements to the existing public right-of-way, however, that can be undertaken by the City in the near term to make East Boulevard more pedestrian friendly.

The following are general recommendations for improvements to the public right-of-way that will need to be the subject of detailed engineering plans once funding is secured for their implementation.

A. Near Term Recommendations (3-5 years)

Recommended Changes to Existing Street

Roadway and Pedestrian Improvements in Section 1:

These could include pedestrian refuge islands, bulb outs, improvements to signalized crossings, the construction of a planted median, and accommodations for bicycles. These options will be explored through a public input process and a detailed engineering study when funding is available for such improvements.

Conversion of Right-Turn Channel at Dilworth Road West:

This plan recommends that the **right turn-lane be abandoned for automobile use** and converted to an area for public and pedestrian use only and as a possible location for public art.

Bulb-Outs:

This plan recommends bulb-outs at intersections in the PED area where on-street parking exists and where there is adequate roadway width to provide this extension of the curb area.

Recommended bulb-out locations are shown on the Official Street Plan (see Appendix D).

Pedestrian Crossings: crosswalks, pedestrian signals, refuge islands

Pedestrian Refuge Islands locations:

- Between Euclid and Dilworth Rd West
- Near Floral Ave and Fountain View
- Near Garden Terrace and Cumberland Ave.
- Near entrance to Freedom Park:

Marked Cross-Walk Improvements

Cross-walks at signalized intersections (Euclid Avenue, Dilworth Road West, Dilworth Road East, Scott Avenue, Kenilworth Avenue) need to be enhanced to make them more apparent to pedestrians and drivers. Improvements could include stamped concrete or pavers with contrasting colors.

Other near-term recommendations for improving the pedestrian environment would include:

- “pedestrian zone” signage in Section 3
- countdown pedestrian lights at Scott/Kenilworth
- no-right turn on red regulations at Scott/Kenilworth

Pedestrian-Scale Lighting

Pedestrian-scale, decorative lighting should be installed in the public right-of-way, with **priority given to areas of highest pedestrian activity including Section 3, the commercial area around Cleveland and Euclid, and near intersections and pedestrian crossings.**

Bicycle Parking:

This plan recommends that as part of near-term streetscape improvements, the City of Charlotte install **racks for short-term bicycle parking in the public right-of-way in Section 3** of East Boulevard (between Charlotte Drive and Cumberland Avenue/Garden Terrace).

Street Trees

Street trees should be planted in the public right-of-way where there are opportunities for new trees or replacement trees. The Division of Landscape Management has identified existing trees that need to be replaced and the locations where new trees can be planted.

Public Art:

The City of Charlotte will work with the neighborhood organizations, property owners, local residents, business owners, and area arts organizations to determine appropriate locations and designs for public art installations for the corridor.

ADA/Curb Ramps:

The City of Charlotte should conduct an inventory of existing ADA curb ramp locations and alignments to assess needs for new construction or realignment of these ramps.

Benches, Trash Cans:

The City should also install public benches and trash cans in appropriate high-pedestrian traffic locations, especially in Section 3 of the corridor.

B. Long-term Recommendations (5 years +)

Utility Line Consolidation/Relocation

This plan recommends that **utilities be relocated, to alleys where possible or out of the East Boulevard right-of-way, and/or buried** along the entire East Boulevard corridor.

Improved Connectivity

This Plan recommends that a **future connection for pedestrians and cyclists, at least, and preferably for automobiles as well, be provided between the two disconnected portions of Worthington Avenue** between Floral and Charlotte Drive (see above map at right).

Part I: Concept Plan

1. Introduction/Background

The East Boulevard corridor developed in the pre-automobile era, when the primary modes of transportation were walking, the trolley, bicycling, and horse drawn carriage. Over the last 75 years or more, automobiles have become the predominant transportation mode and development along East Boulevard has changed to accommodate the car. However, Charlotte’s citizens are placing new emphasis on providing a wider range of transportation choices and development forms for the livability and sustainability of our neighborhoods. **The pedestrian scale is a basic standard for development of livable places.**



Trolley in Dilworth in the 1930’s (source: C-MHLC)

With the objective of creating and protecting liveable, walkable neighborhoods, Charlotte City Council adopted a new zoning category called the **Pedestrian Overlay District** (referred to as “*PED*”) in March, 2000. The *City of Charlotte Zoning Ordinance* states that:

“The purpose of the Pedestrian Overlay District (PED) is to reestablish an urban fabric by promoting

a **mixture of uses** in a **pedestrian-oriented setting** of **moderate intensity**. The district encourages the **reuse of existing buildings** that contribute to the unique character or history of the area. The standards also encourage **high quality design**, **mixed use development**, the use of **public transit**, and development which **complements adjacent neighborhoods**” (Section 10.801).



Auto-oriented development on East Boulevard Today

Pedestrian Overlay District (PED) zoning:

- **Overlay on existing zoning**; that is, it superimposes **new development and urban design standards**, which replace the development standards of the underlying zoning district.
- **Allows most current uses** that are currently allowed in the underlying zoning district.
- **Requires and refers to individual streetscape plans** (referred to here as “*Pedscape Plan*”) for each corridor, which define building setback, sidewalk requirements, and street tree planting.

Purpose of Plan:

The *Pedscape Plan* defines the vision and land use policy for East Boulevard and shows how this vision will be achieved: first, by providing **standards for private sector investment** in new development; and second, by making **recommendations for public sector improvements** designed to make the area more pedestrian friendly. This plan also supercedes the governing *Central District Plan* as the land use policy plan for the East Boulevard PED corridor.

The *East Boulevard Pedscape Plan* will be used:

- As **the governing land use policy document** for the area;
- To **identify the building setback** for PED zoned properties;
- As **guidance for streetscape improvements** required by MUDD, NS, and PED zoning districts; and,
- As **guidance for possible public investments** to enhance the pedestrian environment.

The Plan includes the recommendations in this document as well as a companion *Official Street Plan* (see Appendix D), which depicts the official future curb lines and setback lines for the East Boulevard PED area. The Charlotte-Mecklenburg Planning Commission (CMPC) and the Charlotte Department of Transportation (CDOT) will maintain the Official Street Plan for reference.

Plan Development & Process

The Pedscape Plan is the product of more than one year of work, beginning in the fall of 2000, by residents, property owners and business owners, developers, the Dilworth

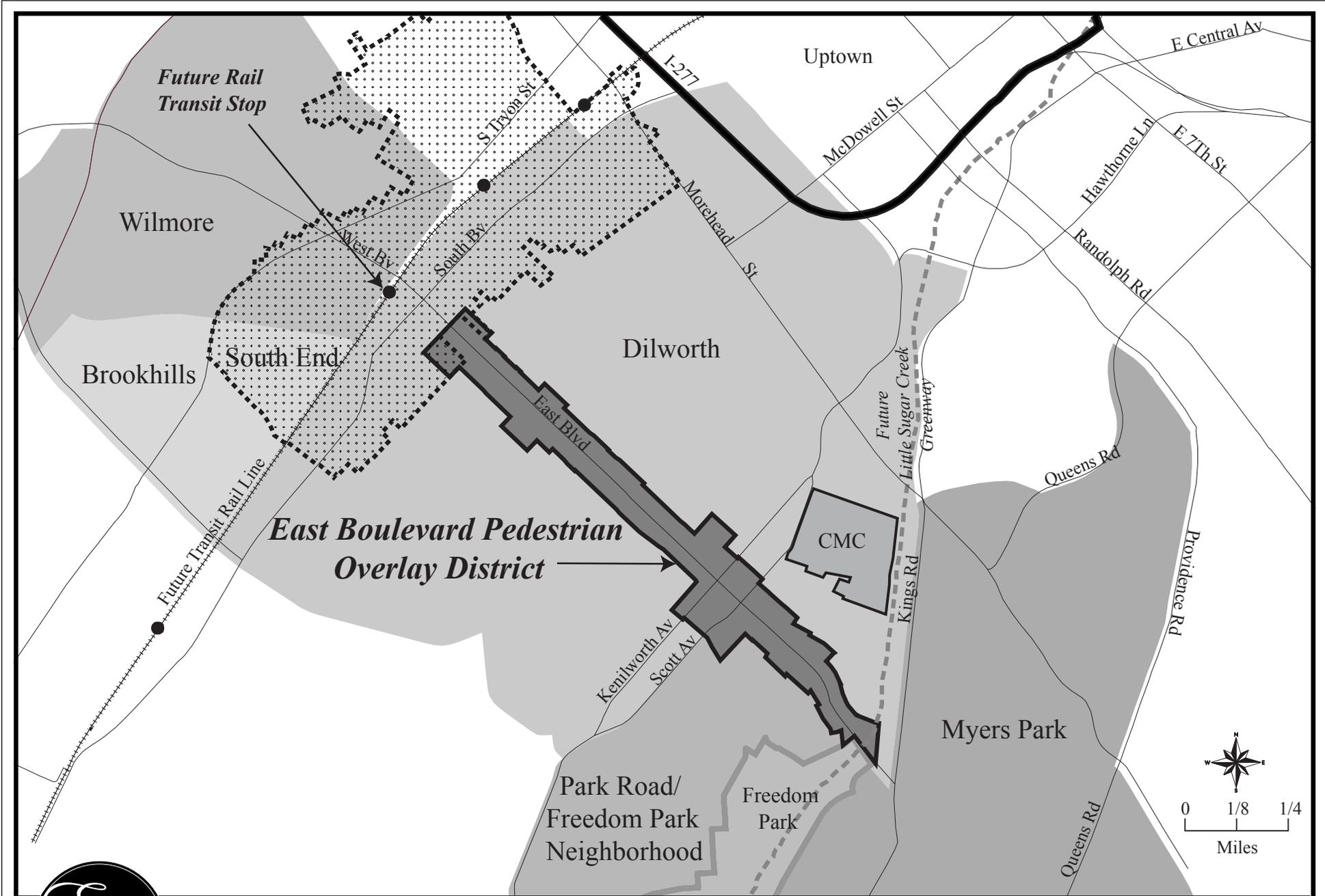
Community Development Association (DCDA), the Historic District Commission, elected officials, and an interdepartmental team of city staff. This effort included five public meetings, including three in the spring of 2001 (one of which was a hands-on design workshop hosted by DCDA); a public meeting in August 2001 to present initial recommendations; and a final public meeting in November, 2001 to present final recommendations.



Citizens at the DCDA design workshop, March 2001

In February 2002, the Charlotte-Mecklenburg Planning Commission approved the plan. **The plan was adopted by City Council on June 10, 2002.**

A list of the major actions in the Pedscape Plan process is listed in Appendix A.



East Boulevard Pedscape Plan

Produced by the Charlotte-Mecklenburg Planning Commission, Nov. 2001



Context Map

Map 1

2. Context for Plan and Future Development

A. Description of the Corridor

East Boulevard is the commercial spine for Dilworth, the neighborhood that was founded in the 1890's as Charlotte's first streetcar suburb. In its early incarnation, a trolley ran down the center of East Boulevard. The portion of the Dilworth neighborhood east of Dilworth Road was designed in the 1910s by the renowned Olmstead Brothers firm. Because Dilworth was developed in the pre-automobile era, it has **fundamental elements that contribute to pedestrian scale and orientation**, including an extensive network of streets; grand trees and comfortable sidewalks; short blocks; and a mix of housing types, services, schools, churches, business and open space in a compact setting that allows for easy pedestrian access.



"The Boulevard" in Dilworth, 1907 (source: C-MHLC)

The East Boulevard corridor is approximately **two miles long**. South Boulevard and the South End area bound the corridor at its western edge. There are plans for a future South Transit Corridor light rail transit/trolley stop at East Boulevard, west of the South Boulevard intersection. At the other end of East

Boulevard is the Myers Park neighborhood, the Little Sugar Creek Greenway, Freedom Park and the Carolinas Medical Center Hospital complex. (see Map 1)

Plan Boundaries:

Map 2 shows the blocks and parcels that will be subject to the PED zoning standards and the Pedscape Plan recommendations. Boundaries for the East Boulevard Pedestrian Overlay District are based on existing zoning district boundaries in the corridor. The PED boundary was drawn to include all of the parcels fronting on East Boulevard from Cleveland Avenue to Freedom Park, and any parcels contiguous to those fronting on East Boulevard that are *currently* zoned for business, office or mixed-use. The boundary ends one block before South Boulevard because the blocks between Cleveland and South will be covered under a separate pedscape plan for South End. **More than half of the East Boulevard PED area falls into the Dilworth Historic District.**

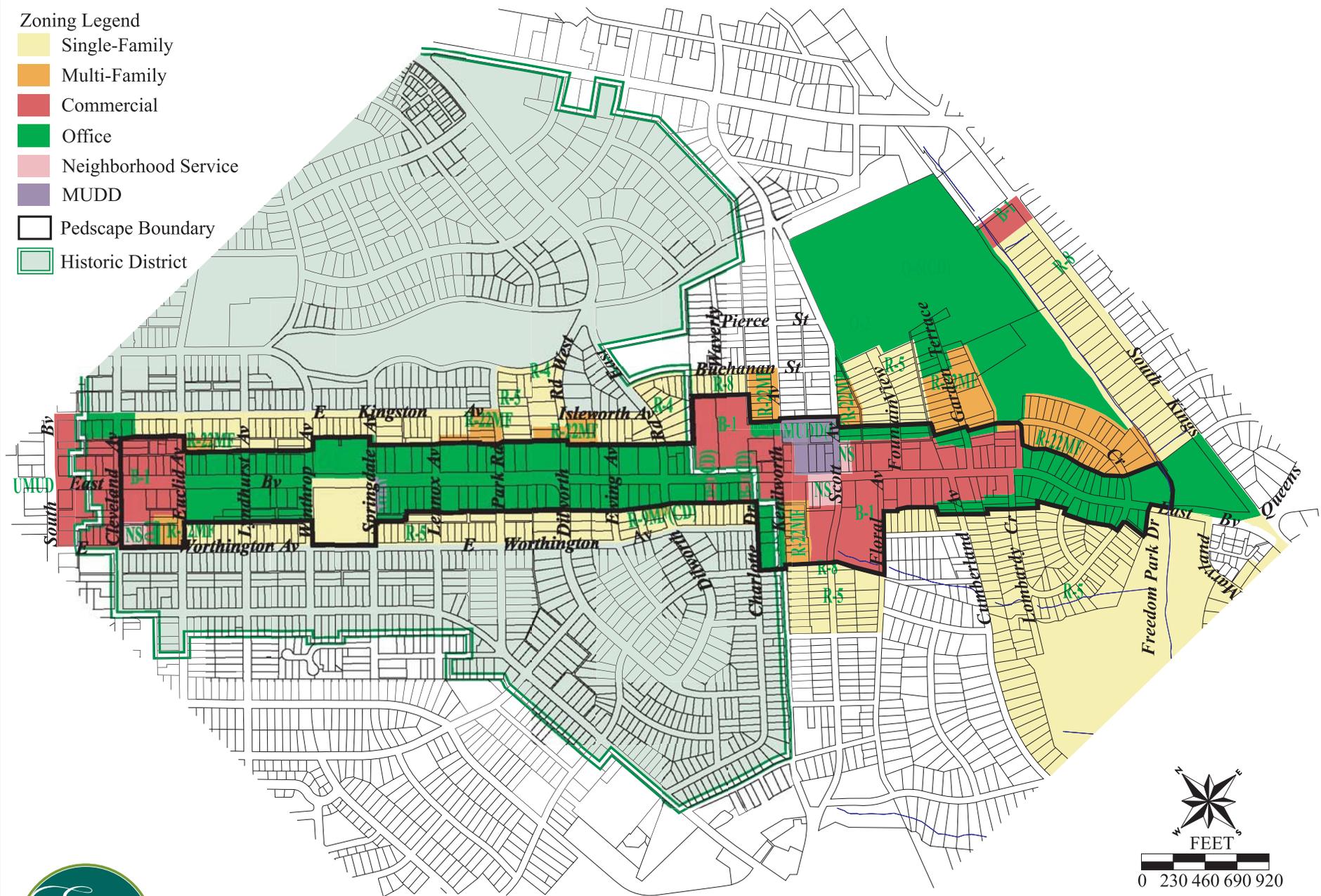
Existing Zoning

Map 2 shows that the majority of properties within the East Boulevard PED boundary are zoned for office uses (0-2 zoning comprises 54% of area) and "Neighborhood Business" (B-1 zoning; 32% of area).

The **B-1 (Neighborhood Business)** zoning district is intended to "create and protect business centers for the retailing of merchandise. . . and the provision of professional services for the convenience of nearby residential areas." Office and residential uses are allowed in this district. Residential densities up to 22 units per acre are allowed.

Zoning Legend

- Single-Family
- Multi-Family
- Commercial
- Office
- Neighborhood Service
- MUDD
- Pedscape Boundary
- Historic District



East Boulevard Pedscape Plan
 Produced by the Charlotte-Mecklenburg Planning Commission, Nov. 2001

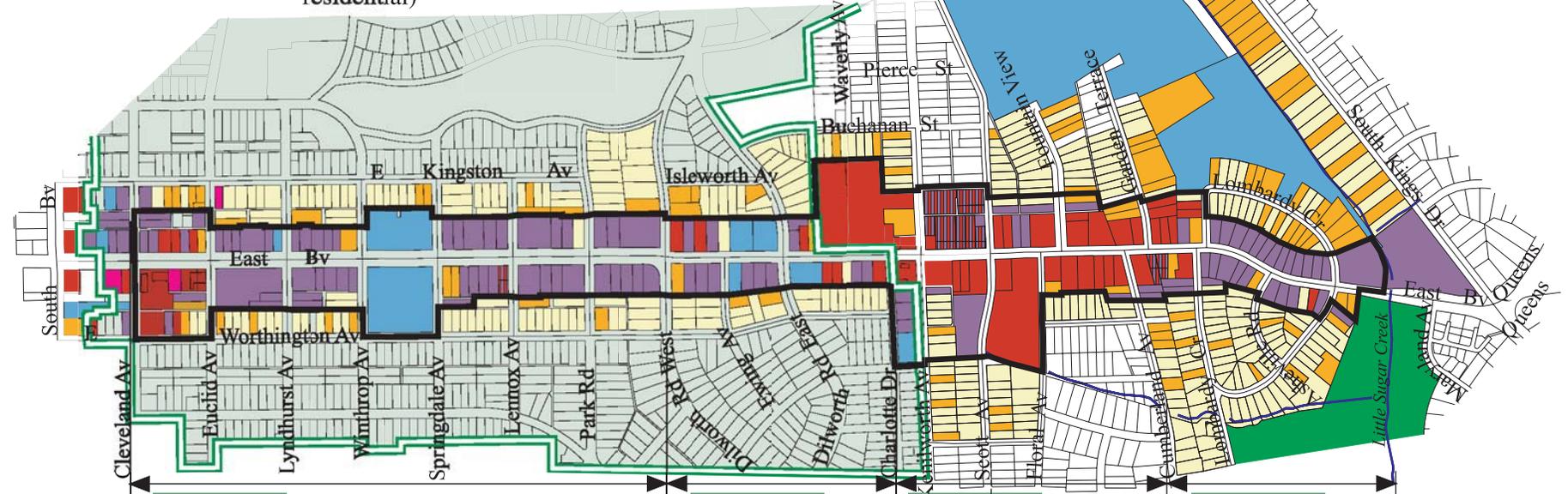


Current Zoning

Map
2

Legend

- Single-Family
- Multi-Family
- Mid high rise Multi-Family
- Mixed Residential
- Commercial
- Mixed Use (retail, office, residential)
- Office
- Institutional
- Open Space
- Historic District Boundary
- Pedscape Boundary
- Future Connection



- Section 1** entirely within Historic District
Cleveland Av to Dilworth Rd West
- Section 2** within Historic District
Dilworth Rd West to Charlotte Dr
- Section 3**
Charlotte Dr to Garden Terrace
3a 3b 3c (Options 1 & 2)
- Section 4**
Garden Terrace to Little Sugar Creek



...historic... "old-Charlotte" ...
...green, large trees...median
...mostly office, institutional,
some retail



...historic... "old-Charlotte" ...
...green, large trees...median
...office, some retail



...urban, mixed use (retail/office/residential) ...
...neighborhood center...



...office, some residential....
...green route from neighborhood center to Freedom Park

O-2 (Office) zoning is intended “to provide areas [for] offices, institutions, and commercial activities not involving the sale of merchandise.” Residential development is allowed in this district, at a density of no more than 22 units per acre.

The minimum setback allowed under the B-1 and O-2 zoning districts is 20 feet from the back of the public right-of-way. (For most of the East Boulevard PED corridor, the public right-of-way includes the sidewalk and the area between the sidewalk and the street.) For properties within the PED boundary, the **current setback standards will be replaced by the new standards in this plan.**

The remainder of the area is zoned for mixed-use (NS or MUDD; approximately 6%) and residential (8%). The uses allowed in the current zoning districts will not change after PED is applied (with certain limited exceptions such as drive-thru windows for retail). However, the development standards – including setbacks -- for the districts *will* change under the PED zoning.



Existing pedestrian-oriented development on East Blvd.

Current Land Use/Community Design

Map 3 shows the existing land uses of properties in the East Boulevard PED corridor. Current land uses and their location help determine the character of the corridor and influence the type of land uses that may be appropriate in the PED area in the future. Approximately **42% of the area** in the PED corridor is currently **devoted to office uses; 33% is used for commercial purposes; 11% is occupied by institutional uses; 9% is used for multifamily buildings; and just less than 5% is used for single-family residences.**

The *Pedscape Plan* divides East Boulevard into four sections, which represent the distinct development and roadway characters of the corridor. Between South Boulevard at one end and Freedom Park at the other, the character of the street changes from a wide, tree-lined boulevard with grand houses (mostly converted to offices) and churches (Sections 1 and 2); to a neighborhood commercial center around Kenilworth and Scott Avenues where the street is narrower and the buildings are closer to the roadway (Section 3); and then to a curving section where canopy trees reappear and handsome houses (again, mostly used as offices) with generous setbacks lead into Freedom Park and the leafy, winding streets of the Myers Park neighborhood (Section 4).

The majority of the existing buildings in the corridor are built with a pedestrian-oriented scale and design, but **over the last few decades a number of auto-oriented developments have been built on the Boulevard**, especially in the commercial section.

Land Use Plan (Central District Plan)

The *Central District Plan* (1993) is the governing land use policy document for the area that includes East Boulevard. Between Cleveland and Dilworth Road East the *Central District Plan* recommends mostly office and some institutional; between Dilworth Road East and Lombardy Circle: primarily commercial uses interspersed with office, residential and mixed-use development; and between Lombardy and the Little Sugar Creek: primarily office uses and some single-family residential (see *Map 4*, page 15). The East Boulevard Pedscape Plan recommends some changes to the *Central District Plan* proposed land uses. These changes are discussed in Section 4 of this document. Once adopted, **the Pedscape Plan will become the governing land use policy document for the East Boulevard PED area.**

There are two specific land use policy recommendations for East Boulevard in the *Central District Plan* that coincide with the *Pedscape Plan* vision for the East Boulevard corridor and the purpose of PED:

1. **“Promote the development of mid-high rise development** in the blocks between Kenilworth and Scott, north and south of East Boulevard.”
2. **“Encourage infill commercial development** on property currently zoned for non-residential uses [in the Kenilworth/Scott Avenue/East Boulevard area]. **New development should be designed at a pedestrian scale.**”

Historic Areas & Structures

The portion of the East Boulevard PED area from Cleveland Avenue to approximately Charlotte Drive (see *Map 3*) falls

within the Dilworth Historic District. The Historic District also functions as a local overlay zoning district. In this district, the requirements of the Charlotte Historic District Commission and the current edition of the Commission’s *Policy and Design Guidelines for Historic District Property Owners, Businesses & Residents* will apply. These guidelines do not deal with issues of land use, but matters of appropriate architectural design for overall preservation of the historic character of the Dilworth Local Historic District. **Any new development within the portion of the PED area that is also in the historic district must meet the minimum PED and Pedscape Plan standards.** The Historic District Commission may increase these requirements beyond the minimum PED and Pedscape standards.

The Historic District Commission’s Guidelines cover the following issues in the Pedestrian Overlay District regulations (*City of Charlotte Zoning Ordinance*: Section 10.801 – 10.812; *Historic District Ordinance*: sections 10.201-217):

- Setback
- Minimum Side Yards
- Maximum Heights
- Parking Standards
- Outdoor Lighting
- Street Walls
- Structured Parking Facilities
- Canopies
- Building Entrances
- Signs, Banners, Flags, and Pennants



Historic Rogers House built in 1902 (source: C-MHLC)

There are three historic landmark buildings, designated by the Charlotte-Mecklenburg Historic Landmarks Commission, within the East Boulevard PED boundary:

1. The [Crutchfield-Bomar-Brem House](#) at 307 E. Boulevard, which is a Queen Anne style house built in 1903.
2. The [Mayer House](#) at 311 E. Blvd., a Victorian style house built in 1907.
3. The [Rogers House](#) at 524 East Blvd., a Dutch Colonial house built in 1902.

B. Transportation System

The defining element of East Boulevard is, of course, its function as a transportation corridor. Originally designed as a grand avenue that comfortably accommodated a trolley car and

other modes of transportation in turn of the century Charlotte (horses and carriages, bicycles, pedestrians, and early motor cars), East Boulevard has become a thoroughfare that primarily serves automobiles. **This plan intends to re-establish a balance of transportation options on the corridor** by providing accommodations for transit, pedestrians, cyclists, and private motor vehicles.

Network of streets

The portion of Dilworth west of Kenilworth Avenue has a very complete network of streets, which allows for multiple means of access for pedestrians, cyclists, and automobiles. The grid network also disperses traffic because of the various routes drivers can take, which results in decreased automobile congestion on the thoroughfares.

East of Kenilworth, the network of neighborhood streets along East Boulevard is much less well connected because of a partial-grid street pattern and street rights-of-way that have been abandoned because of new development or never opened. In this area, due to the lack of connectivity, almost all trips beyond the immediate neighborhood must use East Boulevard, which results in more traffic congestion on the thoroughfare.

Thoroughfare Status

East Boulevard is classified as a **major thoroughfare** on the Mecklenburg-Union Metropolitan Planning Organization's official *Thoroughfare Plan*. Major thoroughfares are the primary traffic arteries of the area. Their main function is to accommodate large volumes of traffic at moderate speeds and provide access to major commercial, residential and institutional land uses. South Boulevard, Kenilworth and Scott Avenues, Kings Drive, and Queens Road West are the other

thoroughfares in the immediate area. Current development standards for major thoroughfares inside of Charlotte Route 4 include a minimum 80-foot right-of-way and two vehicle travel lanes in each direction.

Traffic Volumes

Automobile traffic volumes provide a very basic picture of the current demand for road capacity. Traffic volume trends on East Boulevard over the past seven years (see Appendix B for complete list of traffic counts volumes since 1994) show that the volumes tend to be heaviest in the central section of the corridor, east of Kenilworth Avenue, and lighter at both ends of the Boulevard. **Automobile traffic volumes have tended to grow at about 1% per year in this part of Charlotte.**

Traffic Count Location along East Blvd.	Avg. Daily Traffic Volume/year
East of South Blvd.	21,400 (2000)
East of Kenilworth	30,800 (1998)
West of Kings Dr.	21,500 (1999)
Scott (north of East Blvd.)	11,700 (1999)
Scott (south of East Blvd.)	15,500 (1999)
Kenilworth (north of East Blvd.)	11,400 (1999)
Kenilworth (south of East Blvd.)	12,600 (1999)

The table above shows the latest traffic volume counts for various sections of East Boulevard. These volumes indicate that the existing four lanes on East Boulevard can manage the current and projected automobile traffic demand. Traffic volumes on East Boulevard are not spread out evenly through the day, but are concentrated in the morning and evening commute hours and the lunch hour.

Existing Pedestrian Facilities and Activity

Observation and input from area residents and business owners indicate that the section between Dilworth Road East and Cumberland Avenue is the area of greatest pedestrian activity because of its concentration of stores and restaurants. These businesses are accessed on foot by residents of the surrounding neighborhood and by employees from area business, including Carolinas Medical Center.



Pedestrians and sidewalk café in Section 3

Because of the relative density of businesses in this section of the corridor, customers who arrive by car can park in one location and walk to other nearby shops or offices. Freedom Park at one end and the South End area at the other are also major pedestrian destinations for the neighborhoods around East Boulevard.

Some of the major existing features of the pedestrian transportation network on the corridor include:

- *Sidewalks:* Sidewalks exist throughout the corridor and on the side streets; almost all of the sidewalks in the corridor currently are 5 to 6 feet wide.

- *Planting Strips:* The landscape areas between the sidewalk and the curb that provide areas for tree planting and give pedestrians extra distance from vehicle travel lanes exist uniformly for the majority of the corridor, but are very narrow or non-existent in the commercial section of East Boulevard.
- *Cross walks/Pedestrian Signals:* These exist at intersections with traffic signals, which include Euclid Avenue, Dilworth Road West, Dilworth Road East, Kenilworth Avenue, and Scott Avenue.
- *ADA/Curb Ramps:* Ramps exist at most intersections, but there are some corners that lack adequate curb ramps.

Citizens who participated in the Pedscape Plan process indicated that the **difficulty in crossing East Boulevard in the commercial section** is one of the major problems with the pedestrian environment in the corridor. In this portion of East Boulevard, crosswalks are limited, automobile traffic is heavy, turning movements into driveways are frequent, and some intersections are offset (such as Floral and Fountain View). One resident who lives on a side street off of East Boulevard related his family's experience of not patronizing stores on the opposite side of the street because it was easier to walk *along* East Boulevard than it was to *cross* it, especially with children.

Pedestrian and Bicycle Safety

Charlotte Department of Transportation crash statistics reveal that the commercial section of East Boulevard is an area of concern for the pedestrian and bicycle safety. A total of **14 pedestrian or bicycle crashes** involving motor vehicles have been *reported* on the corridor for the 2 ½ years between 1998 and mid-2001. **Nine of these (4 pedestrian, 7 bicycle) occurred between Kenilworth Avenue and Garden Terrace.** This is the section of the corridor with the most pedestrian

activity, narrowest roadway width, heaviest motor vehicle traffic; and where there are no alternate routes for automobiles, cyclists or pedestrians. The intersection of Scott Avenue and East Boulevard is also one of Charlotte's "high accident locations" due to the number of automobile crashes at this location.

Transit Service:

Two bus routes serve the East Boulevard corridor. Route 19 (Southpark to Uptown) travels along East between Euclid and Park Avenues. This route averages 35,000 riders per month (the 15th highest ridership of 28 local bus routes). Route 18 (Selwyn Avenue) travels along East between Queens Road West and Scott/Kenilworth. This route averages 19,000 riders per month (25th of 28 routes in ridership). No changes to these routes are planned at this time.

There is no current transit service on East Boulevard between South Boulevard and Euclid and between Park Avenue and Kenilworth Avenue. A light rail transit stop is planned near the intersection of East Boulevard and Camden Road, less than ¼ mile from the East Boulevard PED corridor. Light rail transit service is scheduled to begin operation on the South Boulevard corridor by the end of 2005, and **bus feeder service to the light-rail station will be provided along the length of East Boulevard.**

Bicycle Facilities

There are no specific bicycle facilities on the roads in the East Boulevard corridor, although the very wide outside lanes (20 feet) and limited on-street parking on the section of East between Cleveland and Dilworth Road West provide comfortable width for cyclists. In addition, the grid network of

The following elements, based on public input, further articulate the vision for the corridor:

Trees and pedestrian activity will define the character of the corridor. The corridor will be tree-lined throughout, with fewer cars and more shoppers, walkers, and families on the street.

Major aesthetic improvements and new amenities will enhance the pedestrian environment. The vision for these improvements includes: a planted median; public benches; pedestrian-scale lighting; more trash cans and less litter; limited signage; no overhead wires; public art; wide, safe sidewalks; enhanced cross walks; and wide, comfortable areas for pedestrians at street corners.

In addition to providing a safe environment for pedestrians, the corridor will accommodate a range of transportation choices, from slow-moving cars, with on-street parking spaces provided for them, to lanes for cyclists, to public transit with gracious, covered bus stops.

Outdoor commercial activity such as sidewalk cafes and sidewalk vendors will be a defining element of the retail section of the corridor.

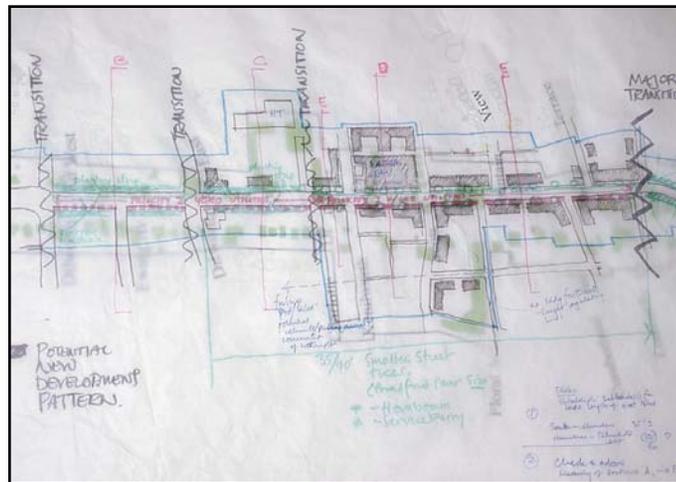
Development on East Boulevard will include a variety of well-designed, pedestrian-oriented building types with a mixtures of styles and colors, that will honor the existing historic fabric. In the central section, between Charlotte Drive and Garden Terrace, new development will include mid-rise buildings, higher-density housing, street-level stores and new mixed-used buildings. Existing historic buildings will be preserved throughout the corridor. Front doors will be

accessible and visible from the street, and in the central section, buildings will be built to the sidewalk edge.

DCDA Community Design Workshop

The vision for the central portion of East Boulevard was further refined and articulated at a charrette (hands-on design workshop) sponsored by the Dilworth Community Development Association. Participants sketched potential road cross-sections and suggested setbacks. They tested their recommendations against existing parcels to determine development potential.

Charrette participants recommended that the central portion of East Boulevard be the priority area for moving or **burying underground utilities**; and that **a new connection (pedestrian/bicycle and/or motor vehicular) be reserved between the two sections of Worthington Avenue.**



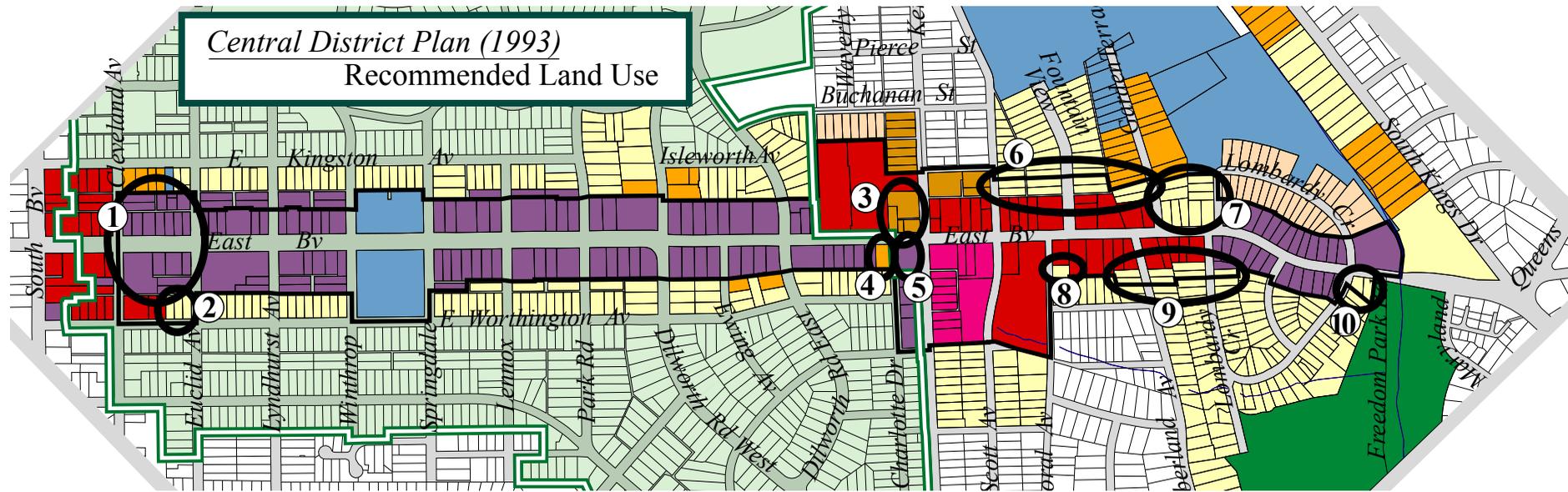
Sketch of Sections 2 & 3 done at DCDA Design Charrette

4. Recommended Land Uses

The land use vision for the East Boulevard PED corridor is generally consistent with what is proposed in the *Central District Plan*. Office use is the primary recommended land use between Cleveland and Dilworth Road East and between Lombardy Circle and Freedom Park, with commercial use, mixed-use development, and multifamily development recommended between Dilworth Road East and Lombardy Circle. **These recommended land uses support the community vision of a mixture of land uses, which provide pedestrian accessibility to services, workplaces, and residential choices within the neighborhood.**

The *Pedscape Plan* recommends several changes to land uses proposed in the *Central District Plan*. These changes are consistent with existing land uses, current zoning, and the intent of the Pedestrian Overlay District. The recommended changes are shown in the table below and correspond to circled parcels in the PED boundary that are noted on *Map 4*:

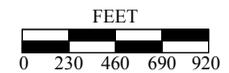
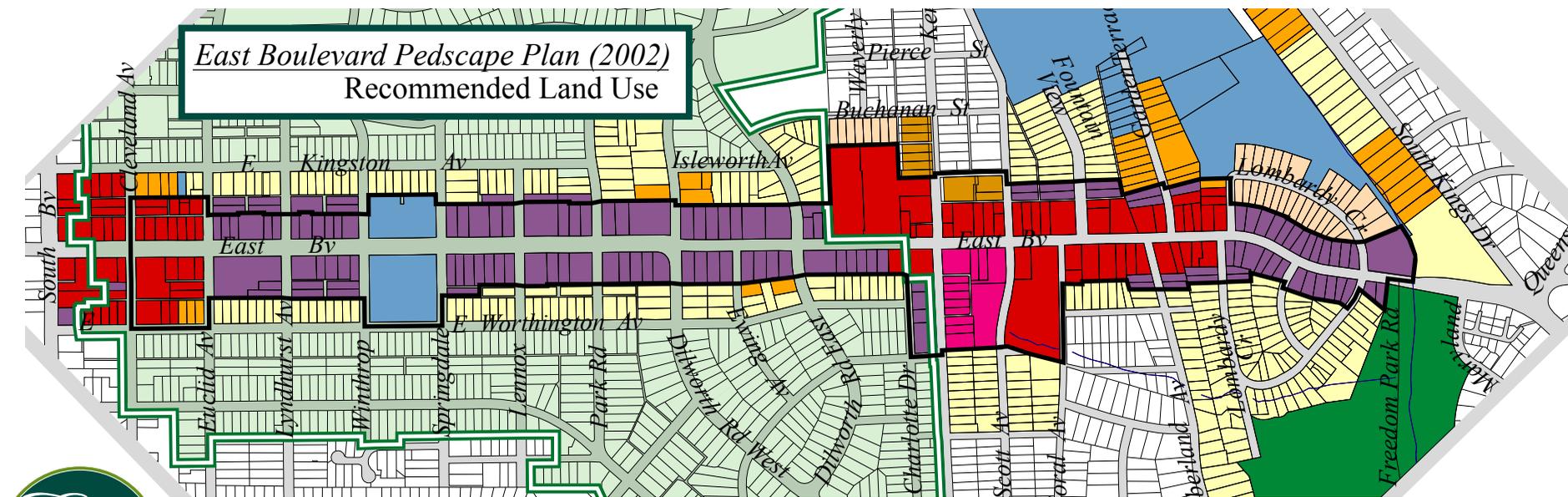
RECOMMENDED LAND USES				
<i>Table 1</i>				
<i>(Numbers correspond with circled parcels within PED boundary on Map 4)</i>				
#	Pedscape Plan Recommended Land Use	Central District Plan (1993) Land Use Recommendation	Existing Zoning (Map 2)	Existing Land Use (Map 3)
1.	Commercial	Office	B-1	Office, commercial, mixed-use, residential
2.	Multi-family	Single family	R-22MF	multifamily, single family
3.	Commercial	Multifamily (mid-highrise)	B-1	Commercial, multi-family
4.	Commercial	Multi-family	B-1 (CD)	Office
5.	Commercial	Office	B-1	Commercial
6.	Office	Single family	O-2	Office, single-family, multi-family
7.	Commercial, Office, Multifamily	Single family	B-1, O-2, R-22MF	Commercial, office, Residential
8.	Commercial	Single family	B-1	Multi-family
9.	Office	Single family	O-2	Office, residential
10.	Office	Single family	O-2	Multi-family



○ Proposed land use changes to the Central District Plan (1993) recommended by East Boulevard Pedscap Plan (2002)

Proposed Land Use

- Single-Family
- Multi-Family
- Mid High Rise Multi-Family
- Mixed Residential
- Commercial
- Office
- Institutional
- Open Space
- Mixed Use
- Historic District
- Pedscap Boundary



East Boulevard Pedscap Plan

Produced by the Charlotte-Mecklenburg Planning Commission, Feb. 2002



Proposed Land Use

Map 4

5. Recommended Cross-Sections and Streetscape Development Standards

The standards in this section supplement the requirements of the PED zoning. PED zoning refers to the streetscape standards identified here for new development and changes to existing development. The Streetscape Development Standards are requirements for development that will be primarily the **responsibility of private developers or private land owners** to implement as property on the corridor is redeveloped or improved. In combination with the development requirements in the PED text, the standards below will provide the primary mechanism for shaping the streetscape into the kind of pedestrian-oriented space that the community envisions for the corridor.

This section recommends future cross-sections for East Boulevard and identifies building setbacks and streetscape standards based on the ultimate curblines location. **The cross-sections are not plans for immediate road improvements, but are recommended long-term changes to be considered for implementation once a major portion of the necessary right-of-way is available.** In the meantime, City staff will request funding for minor improvements to the public right-of-way to make it more pedestrian friendly.

Building Setbacks and Future Street Cross-sections

The building setback is one of the most important features in defining the character of a streetscape. Different land uses and different street characters require different setbacks. The building setback is defined in the PED standards as the distance from the back of the roadway curb to the front of a building. Where buildings are set back far from the street, there is a sense of openness, privacy, and in some cases grandeur. Shallower

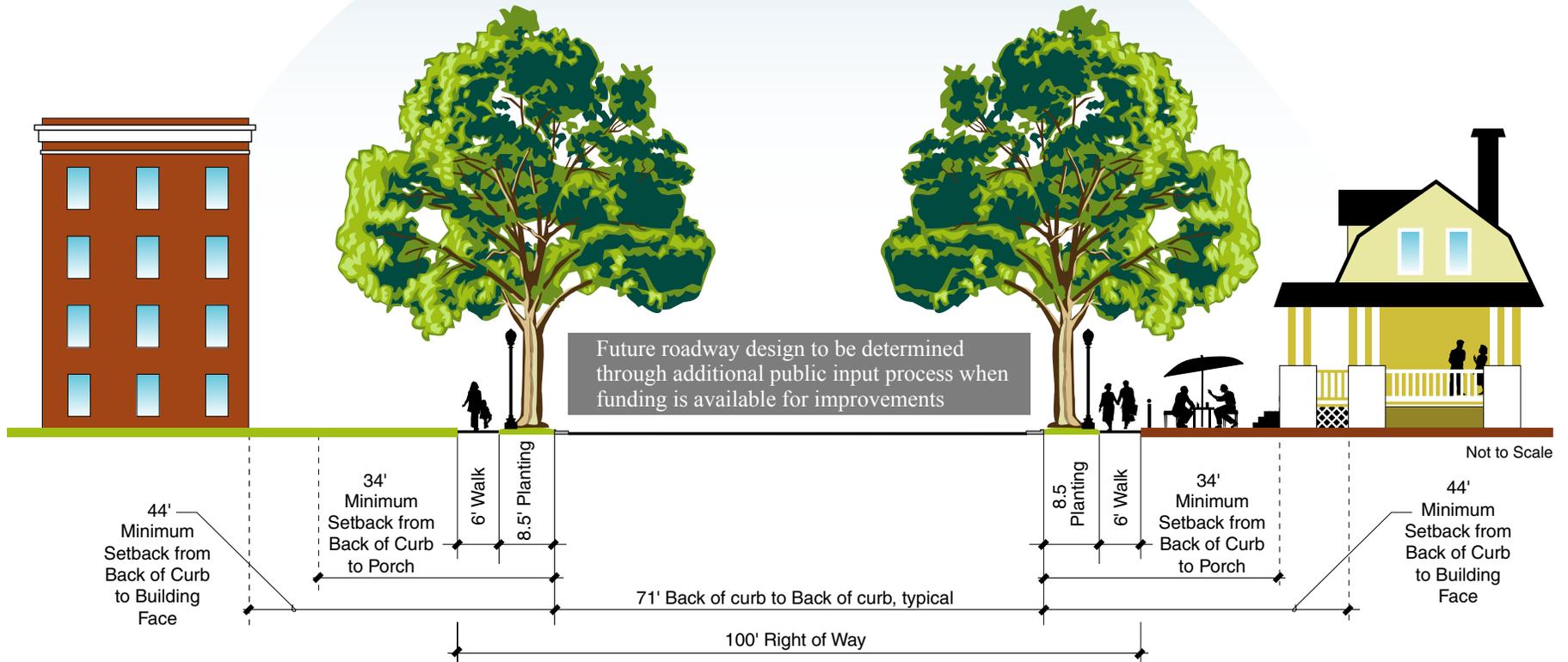
building setbacks, where buildings are closer to the sidewalk, can provide enclosure, intimacy, and opportunities for more interaction between the activity in the building and the people on the street.

The PED text states: **“The minimum setback will be measured from the back of all existing or future curbs, whichever is greater.”** (Section 10.803.3) The setback area (located between the back of curb and the setback line) will include, at a minimum, the width needed for sidewalks and planting strips and any additional width that is deemed appropriate for the character of the streetscape. **Standards for setbacks, sidewalks, and planting strips will be met by developers who undertake new development or major renovations in the PED area.** The required setbacks are defined in the cross-sections that follow for the different portions of East Boulevard.

Tree planting requirements in this section supercede the “Perimeter Planting Requirements” in Section 21-13(C) of the Charlotte Tree Ordinance. However, all other requirements of the ordinance will apply to new development in the East Boulevard Pedestrian Overlay District.

The cross-sections in this document provide a *conceptual* view of the streetscape for each section, but the **Official Street Plan** (see Appendix D) should be referred to in order to determine the location of future curblines and setback lines for each block of East Boulevard. The Official Street Plan will be maintained by the Charlotte-Mecklenburg Planning Commission and the Charlotte Department of Transportation.

Proposed Section for
Cleveland Avenue to Dilworth Road West
Section 1



Section 1
 Existing Right of Way = 100'
 Existing Back of Curb to Back of Curb = 71'



Section 1: Cleveland Avenue to Dilworth Road West
[Figure 1]

Section 1 the widest section of East Boulevard, defined by large trees in generous planting strips, and many grand, historic buildings with deep setbacks. Offices, some commercial and residential uses, and two large churches are the primary land uses in this section. This section of East Boulevard is entirely within the Dilworth Historic District. This plan maintains the existing curb location, which is currently 71 feet from back-of-curb to back-of-curb. **Two different setback lines are established for this section – one for porches at 34 feet and the other for building face at 44 feet** – in order to maintain and preserve the general setback established by the historic buildings in this portion of the corridor. The building at 300 East Boulevard (pictured below) provides an example of a building with these setbacks. This building was identified in the Pedscape public meetings as an appropriate setback model for Section 1.



View of Setback at 300 East Blvd. from front and side

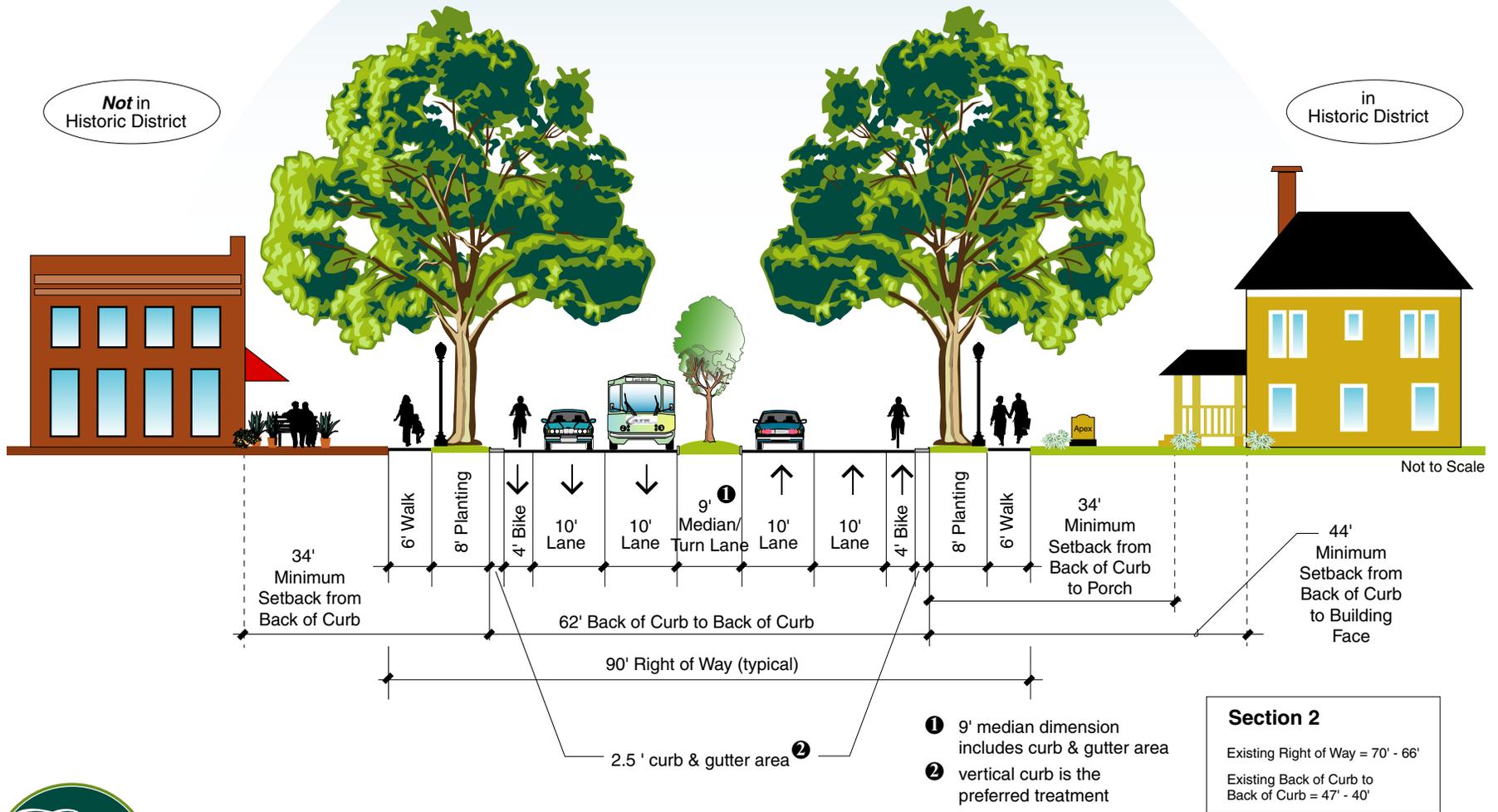
The minimum sidewalk and planting strip requirements match what exists currently in this section: **6-foot wide sidewalks and 8.5-foot planting strips.**



Existing road cross-section in Section 1 (70 feet wide)

The **recommended future road cross-section** for Section 1 will be determined through an additional public input process when funding is available for improvements to the roadway. Changes to the road design for Section 1 and their possible implementation will be the subject of a detailed engineering study, which will include public meetings with area residents and property owners. Such a study would include recommendations for: 1) the appropriate locations for left turns onto side streets and left-turn lanes; 2) the provision and location of on-street parking and bicycle accommodations; 3) the location of transit stops; and 4) the possibility of additional improvements including, but not limited to: a planted median, pedestrian refuge islands, and bulb outs.

Proposed Section for
Dilworth Road West to Charlotte Drive
Section 2



Section 2: Dilworth Road West to Charlotte Drive
[Figure 2]

Section 2 is also within the Dilworth Historic District. Large oaks and historic buildings with professional offices characterize this section as well, but a transition to more commercial uses begins at the Harris Teeter shopping center on the north side of the street. The existing curb line narrows to approximately 41 feet from back-of-curb to back-of-curb in Section 2.

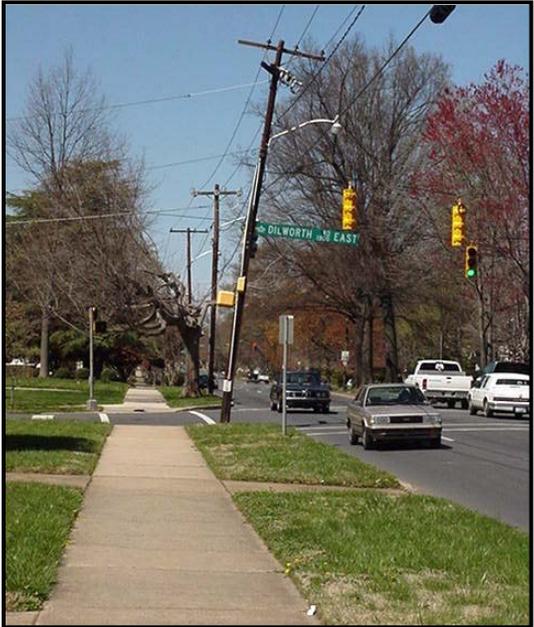


Existing streetscape in Section 2

The Pedscape Plan curb line dimension for this section is 62 feet from back-of-curb to back-of-curb. **The minimum setbacks** for this section continue those required in Section 1, *for the area within the Historic District only*: **34 feet from back-of-curb for the porch and 44 feet from back-of-curb for the building face.** In the portion of Section 2 that is **not in the Historic District**, the **building setback is 34 feet.** These setbacks allow for the

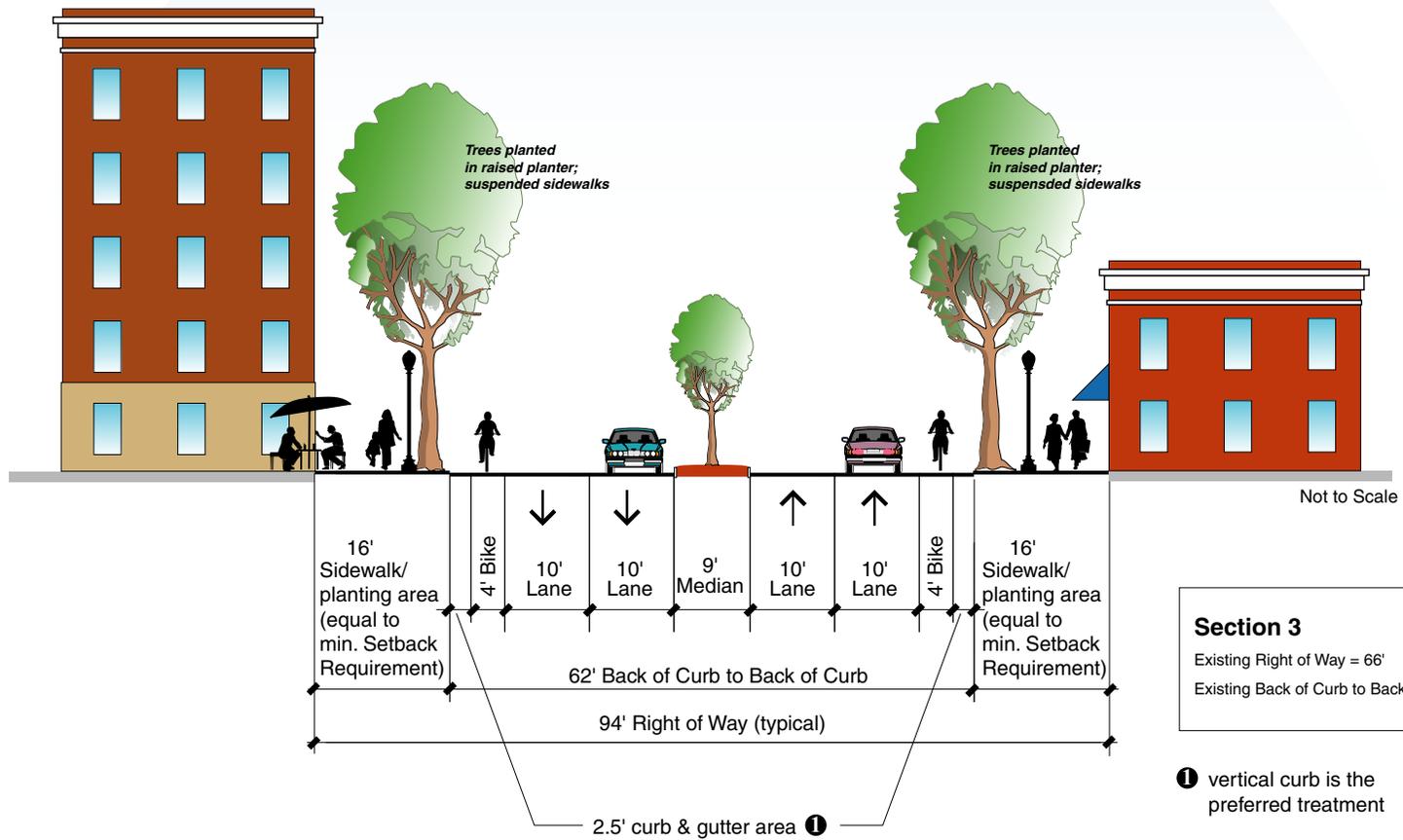
preservation of the setback lines established by the historic buildings in this section. **8-foot planting strips and 6-foot sidewalks** are the minimum requirements for this section.

The **recommended future roadway cross-section** for this portion of East Boulevard would include two motor vehicle lanes in each direction, a bicycle lane in each direction and a 9-foot area in the center for a planted median and/or left turn lanes. Left turn lanes are proposed at Dilworth Road West and in the entrance area to the Harris Teeter shopping center.



Existing sidewalk and planting strip in Section 2

Proposed Section for
Charlotte Drive to Kenilworth Avenue
Section 3a



Section 3: Charlotte Drive to Garden Terrace

This section is the commercial center of the East Boulevard corridor. It is the most densely developed section and the most urban in character. There are existing buildings and new buildings under construction or planned that are models for the type of pedestrian-oriented development that this plan seeks to foster, with building frontages at the sidewalk edge and parking behind or under the buildings. This section is already the area of most pedestrian activity on the corridor due to the commercial land uses, and this activity should only increase as more pedestrian-oriented development occurs and pedestrian improvements outlined in this plan are put in place. **The land use future of this section is envisioned to be a mixture of residential and commercial uses.**



Existing pedestrian-oriented building in Section 3

This section is divided into **three sub-sections**, since there are two major developments underway on the block between Scott and Kenilworth that were planned before the Pedscape planning process began, and which have already established

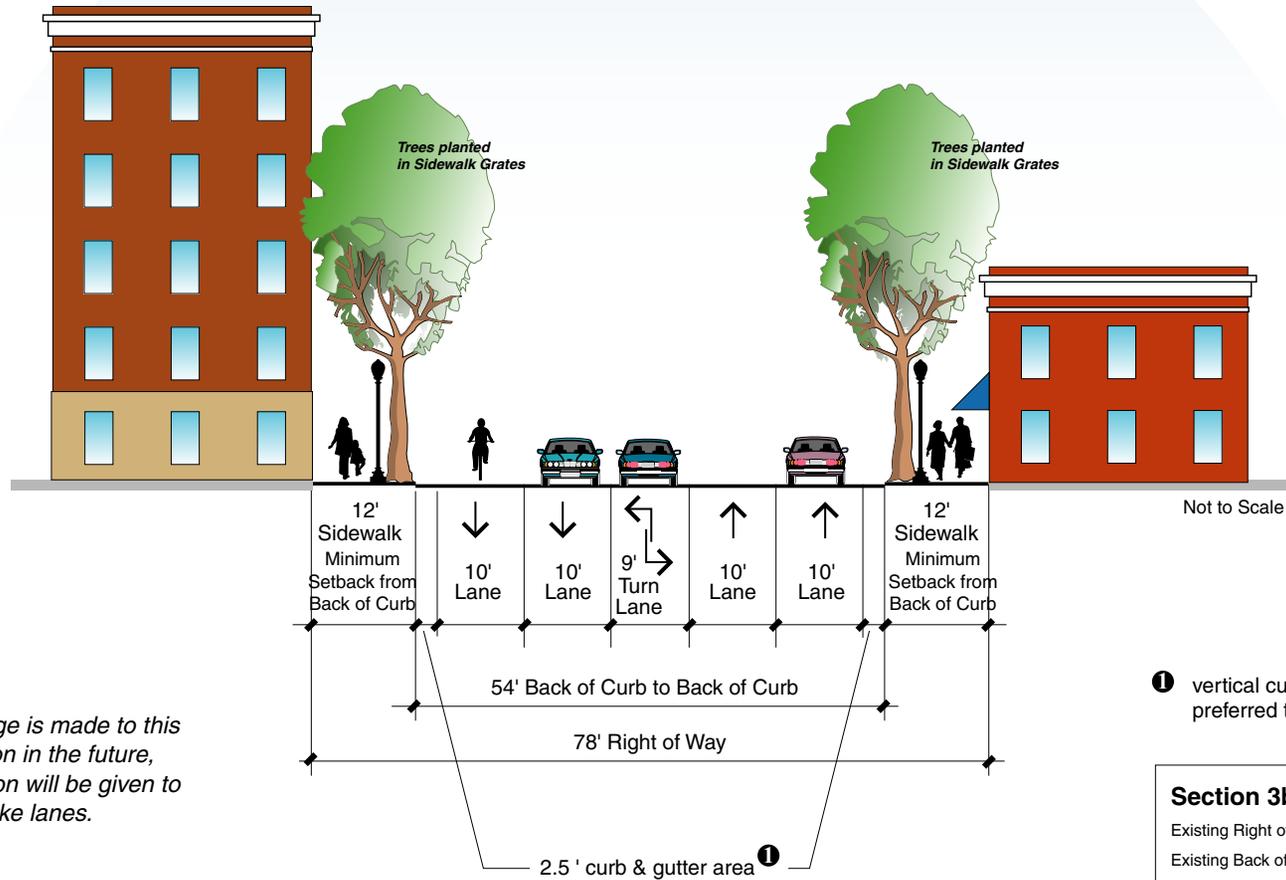
the future roadway cross-section and setback lines for that block.

Section 3a: Charlotte Drive to Kenilworth Avenue
[Figure 3a]

A more urban style of development will begin at this section, with buildings closer to the street and wider sidewalks to accommodate pedestrians and uses such as outdoor dining. The **future curb line** will continue the **62-foot** dimension (back-of-curb to back-of-curb) from Section 2. The **setback** in this portion of East Boulevard will drop to **16 feet from back-of-curb**. This dimension will allow for a wide sidewalk that will include a planting area for trees. **No planting strip is proposed** in this section, but **trees planted in raised planters** are required (see picture on page 28). The **16-foot sidewalk area** will provide room for some on-street retail activity such as sidewalk cafes and space for 3-4 pedestrians to walk abreast. Commercial uses on the sidewalk such as outdoor dining will need Charlotte Department of Transportation approval if they are on public property.

The **recommended future roadway cross-section** continues the cross-section of Section 2, with room to accommodate two motor vehicle lanes in each direction, a bicycle lane in each direction, and a 9-foot median. The median is envisioned to be urban in character, with a surface such as brick rather than grass or mulch. This treatment will reflect the character of this portion of the corridor and also provide a more appropriate surface for pedestrian refuge since there are likely to be many people crossing the street near the intersection with Kenilworth.

Proposed Section for
Kenilworth Avenue to Scott Avenue
Section 3b



NOTE:
 If any change is made to this cross section in the future, consideration will be given to providing bike lanes.

Section 3b
 Existing Right of Way = 61'
 Existing Back of Curb to Back of Curb = 40'

Section 3b: Kenilworth Avenue to Scott Avenue [Figure 3b]

The dimensions of this section were established during the approval process for the Latta Pavillion and Epicurean development projects, which occupy the major portion of each side of the block. These mixed-use developments will have many of the pedestrian oriented development features that are outlined in the PED zoning requirements such as retail frontage on the sidewalk, front doors that address the street, and parking behind or under the buildings. Under the requirements of this plan, redevelopment of the two remaining parcels fronting East Boulevard in this section would have to conform to the setbacks defined by these projects. The established **back-of-curb to back-of-curb dimension is 54 feet**. The established **minimum setback is 12 feet**, which will be devoted to sidewalk, including space for **trees in raised planters**.



Rendering of Latta Pavillion at Scott Avenue & East Blvd.

The **future recommended roadway cross-section** has been planned (due to two previously approved development plans in this block) to accommodate two vehicle lanes in each direction and future left-turn lanes at Kenilworth and Scott Avenues. There is not enough room in this roadway width to provide bicycle lanes, so when this cross-section is built, cyclists would either share the vehicle lanes or take their bicycles onto the sidewalk in this section. ***If there is ever any change to this cross section, consideration will be given to providing room for bike lanes to match the lanes proposed for the other sections of East Boulevard.***



Existing streetscape and cross-section

Section 3c: Scott Avenue to Garden Terrace/Cumberland Avenue [Figure 3c]

The future cross-section for this portion of East Boulevard will continue the urban pattern established in Section 3a. There are two possible setbacks in this section:

Option I is a 24-foot minimum setback from back-of-curb. Under this option, a minimum **8-foot tree planting area** (for either planting strip or a raised planter) and a minimum **8-foot sidewalk** is required. The remaining setback distance may be used for outdoor seating or other outdoor commercial activity.

Option II allows for 8-foot wide recessed on-street parking, which results in a minimum **16-foot sidewalk**, including tree planting area for raised planters. Developers may choose to build either option. If Option II is chosen, the developer must provide a minimum of 50 feet of on-street parking space (room for two cars). The distance of buildings from the roadway travel lanes is the same for both options. **The back-of-curb to back-of-curb dimension is 53 feet for Option I and 69 feet for Option II.**



Outdoor seating and existing streetscape Section 3c

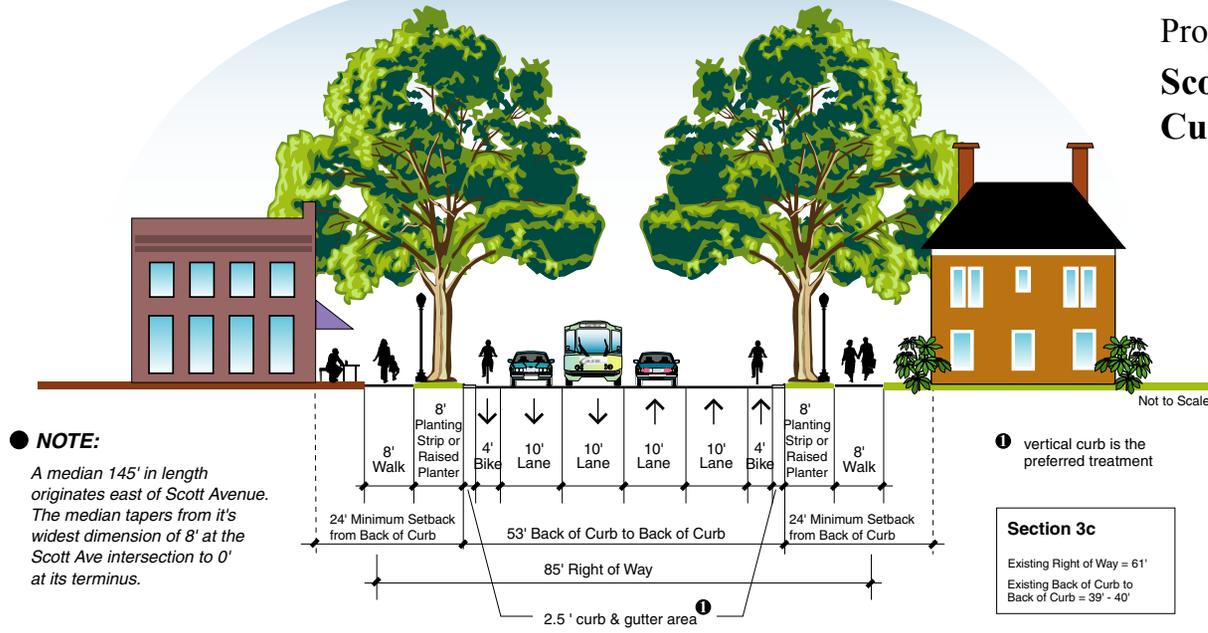
The **recommended future roadway cross-section** for Section 3c is two vehicle lanes in each direction and a bicycle lane in each direction, and the possibility of on-street parking in Option II



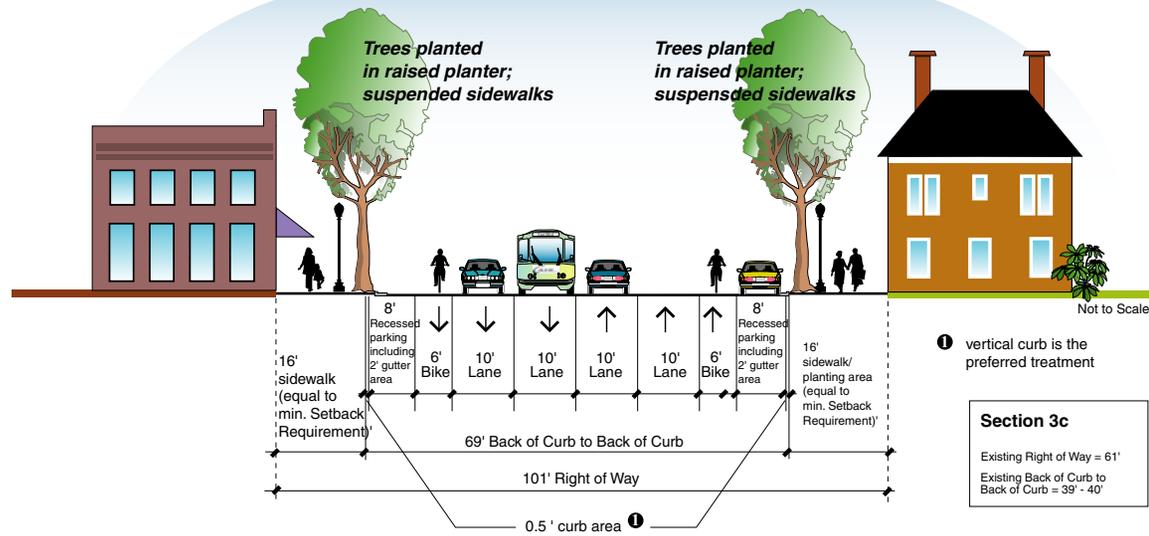
Existing streetscape in Section 3 c

Proposed Sections for Scott Avenue to Cumberland/Garden Terrace

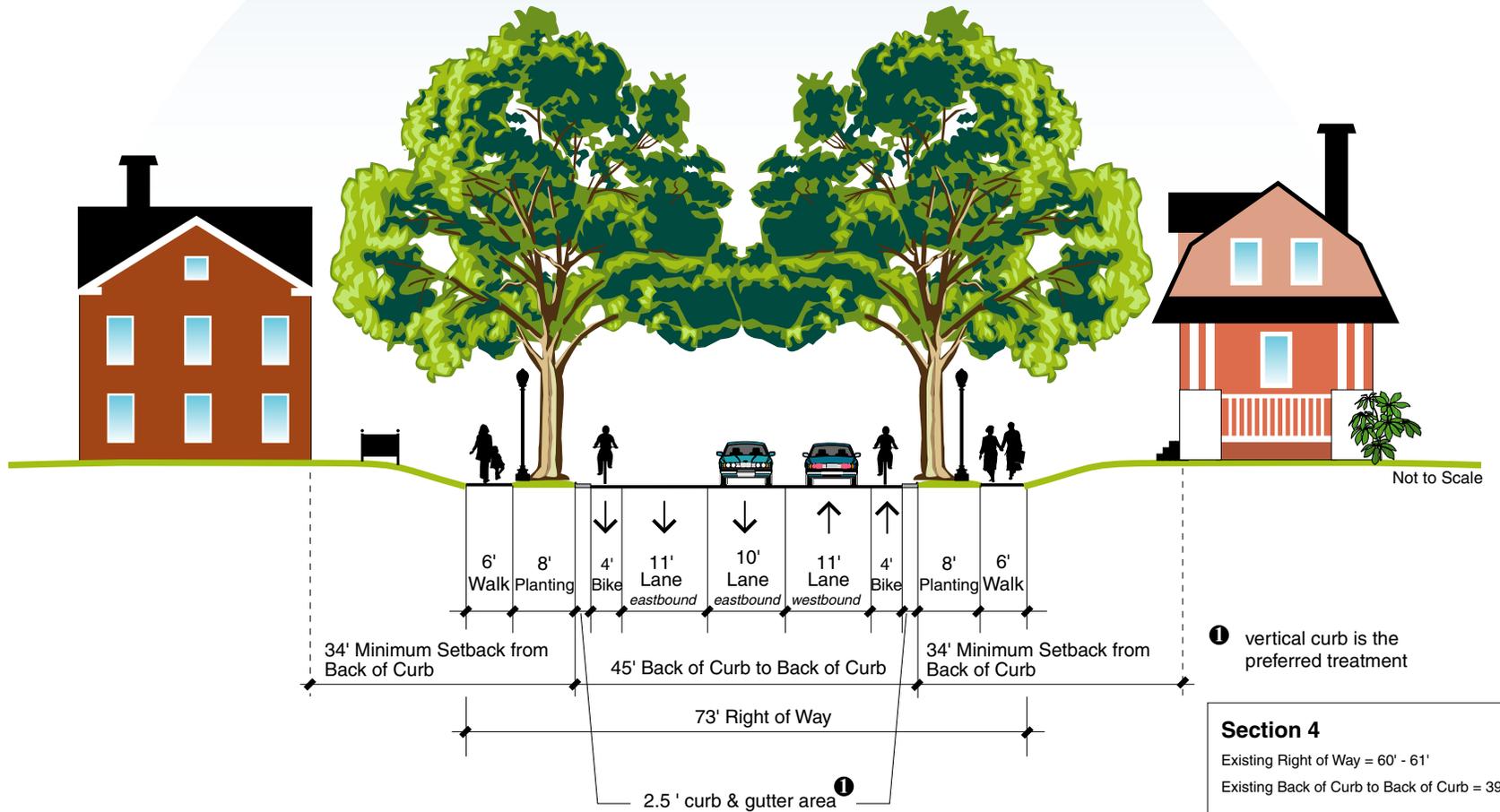
Section 3 Option I



Option II



Proposed Section for
Garden Terrace to *Little Sugar Creek Bridge*
Section 4



Section 4: Garden Terrace to Little Sugar Creek
[Figure 4]

In this section, land uses transition from commercial and mixed-use to office uses. The character of the street becomes more residential in nature, with large houses (most used as offices) on generous setbacks and large oaks lining the street down to Freedom Park. The traffic volumes in this section decrease considerably as one moves toward Kings Drive. The **future back-of-curb to back-of-curb dimension for this section is 45 feet**, five feet wider than the existing dimension. The **minimum setback for this section is 34 feet** from back-of-curb, which will generally maintain the setback established by the existing buildings in this section. The **minimum sidewalk width for this section is 6 feet**, with an **8-foot planting strip for large, maturing trees**.



Existing Streetscape in Section 4

The **recommended future roadway cross-section** for this portion of East Boulevard accommodates 3 motor vehicle lanes – one west-bound and two east-bound – and a bicycle lane in each direction. The proposed 3-lane cross-section will provide adequate capacity for the automobile traffic volumes on this section of East Boulevard.

Streetscape Requirements for Side Streets:

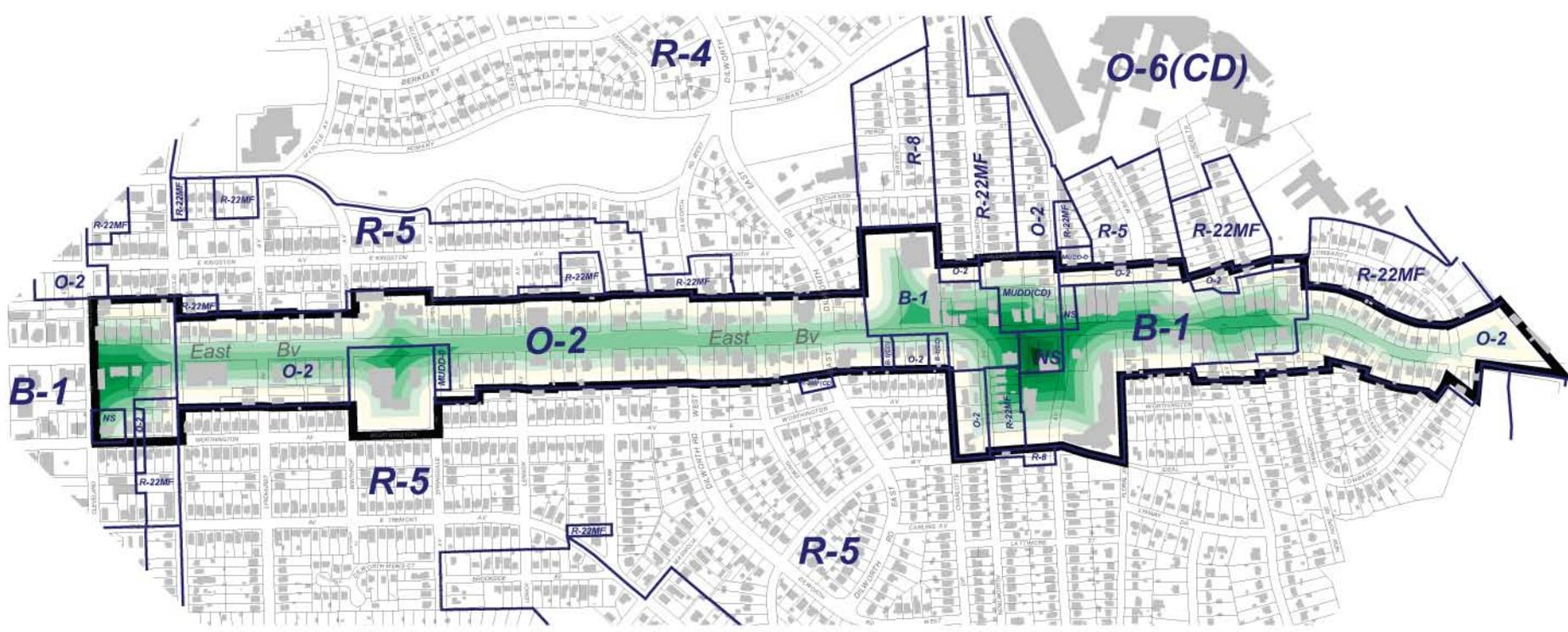
Tree planting and sidewalks improvements will also be required for side streets within the in the East Boulevard PED corridor. The following chart shows the development requirements for side streets. **Where no change is proposed, the existing conditions for setback, planting strip and sidewalk are the standard for future development.**



Example of raised planter

Section or Street	Setback	Planting Strip	Sidewalk
Sections 1, 2, Charlotte Drive Lombardy Circle Ashville Road	18'-0"	8'-0"	5'-0"
Kenilworth Avenue Scott Avenue	16'-0"	None required (trees in raised planters or tree pits required)	8'-0" minimum
Fountain View Floral Avenue Garden Terrace Cumberland Avenue	16'-0"	8"-0"	6'-0"

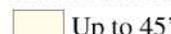
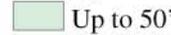
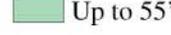
 Zoning Boundary
 Pedscape Boundary



*This map is for illustrative purposes only. The actual maximum building heights will be based on the zoning ordinance.

NOTE: _____
 The base allowable height for the PED district is 40 feet. "The building height may increase one foot in height, over 40 feet, for every 10 feet in distance from the property line of the nearest site used and/or zoned for residential purposes, except for property zoned *PED*, *MUDD*, or *UMUD*."

Maximum Building Height

 Up to 45'	 Up to 60'	 Up to 75'
 Up to 50'	 Up to 65'	 Up to 80'
 Up to 55'	 Up to 70'	 Up to 85'



Tree planting – type and location

Trees are one of the most important features of the existing East Boulevard streetscape and one that the vision for the corridor hopes to enhance. Street trees provide many benefits, including helping to define the public space of the street, providing shade and buffering for pedestrians, and even slowing automobile speeds by narrowing the appearance of the roadway.



Existing street trees in Section 1

Street trees will be required for all new development in the PED area. **Large maturing trees such as red maples** are required for locations along East Boulevard where there are no overhead utilities. **Small maturing trees such as Crape Myrtles are required for areas where there are utilities overhead or in planted median areas.** Where it is not practical to plant in the ultimate planting strip location, small maturing trees should be planted in an interim planting strip. Tree planting plans for new development must conform to the *Charlotte Tree Ordinance* requirements except where superseded by cross-section standards above.

The following tree species are recommended by the City Arborist **for the side streets** within the PED area:

Side Street	Large Maturing	Small Maturing
Cleveland	Willow Oak	Crape Myrtle
Euclid	Overcup Oak	Crape Myrtle
Lyndhurst	Ginkgo	Crape Myrtle
Winthrop	Red Maple	Crape Myrtle
Springdale north of East Blvd	White Oak	n/a
Springdale south of East Blvd	Red Maple	n/a
Lennox	Shumard Oak	Dogwood
Park Road	Sugar Maple	Serviceberry
Dilworth Rd West	Willow Oak	Crape Myrtle
Dilworth Rd East	Willow Oak	Crape Myrtle
Charlotte	Willow Oak	n/a
Kenilworth	n/a	Dogwood
Scott	n/a	Crabapple
Floral	n/a	Crape Myrtle
Fountain View	Sugar Maple	Persian Ironwood
Cumberland	n/a	Kousa Dogwood
Garden Terrace	n/a	Serviceberry
Lombardy Circle – south	Allee Elm	n/a
Lombardy Circle – north	n/a	Hornbeam
Ashville	Willow Oak	n/a

Height limitations

Map 5 shows maximum allowable building heights for the area within the East Boulevard PED boundaries based on the height regulations for the overlay district (PED: Section 10.803(5)). The tallest portions of buildings allowed in the East Boulevard PED corridor will be 75 feet. For most of the parcels in the corridor, however, building heights of no more than 60 to 65 feet will be allowed. The maximum height will be allowed closest to the East Boulevard right-of-way, and will step down as it moves away from the street and closer to residential areas.

6. Recommended Streetscape and Pedestrian Improvements for Corridor

The most significant changes to the East Boulevard corridor will happen incrementally as new development occurs under the PED standards. New development will reserve right-of-way for the new wider cross-sections to be constructed, but road widening will not occur for many years. There are minor improvements to the existing public right-of-way, however, that can be undertaken by the City in the near term to make East Boulevard more pedestrian friendly. These improvements could be implemented as early as the next two to five years, **subject to the availability of public funding.**

The following are general recommendations for improvements to the public right-of-way that will **be the subject of detailed engineering plans and further public input once funding is secured** for their implementation.

A. Near Term Recommendations (3-5 years)

Recommended Changes to Existing Street

1. Roadway and Pedestrian Improvements in Section 1:

These could include pedestrian refuge islands, bulb outs, improvements to signalized crossings, the construction of a planted median and accommodations for bicyclists. **These options will be explored through an engineering study and a public input process when funding is available for such improvements.**

A painted median already exists in this section of East Boulevard (see picture on page 13). A pedestrian refuge islands or a planted

median would provide a safer crossing for pedestrians; would have potential traffic calming effects; would ease traffic flow; and would aesthetically enhance this grand section of East Boulevard.

Changes to the road design for Section 1 and the possible implementation of any improvements to the roadway would be the subject of a detailed engineering study. Such a study would include recommendations for: 1) the appropriate locations for left turns onto side streets and left-turn lanes; 2) the provision and location of on-street parking and bicycle accommodations; 3) the location of transit stops; and 4) the possibility of additional improvements such as: a planted median, pedestrian refuge islands, and bulb outs.



Right-turn channel at Dilworth Road West

2. Conversion of Right-Turn Channel at Dilworth Road West:

A right-turn channel lane (also called a “slip lane”) exists at Dilworth Road West and East Boulevard. This lane was originally the turning location for the Dilworth trolley line. Since this lane is not merited for automobile traffic purposes, and since the slip-lane provides a hazard to pedestrians (right turning vehicles do not need to slow down as much), this plan

recommends that the **slip-lane be abandoned for automobile use** and converted to an area for public and pedestrian use only. Once abandoned to automobile traffic, the space could be a **future location for installation of public art and/or a place for a historic marker of the former trolley route.**

3. *Bulb-Outs:*

Bulb-outs, also known as curb extensions or neck-downs, are an effective way to reduce the crossing distance and increase the sidewalk area for pedestrians at intersections. They also demarcate parking areas, narrow the roadway pavement width and increase the landscape area at intersections, which provides some measure of traffic calming for automobiles.



A corner bulb-out around on-street parking

This plan recommends bulb-outs at intersections in the PED area where on-street parking exists and where there is adequate roadway width to provide this extension of the curb area. The bulb-outs recommended for Section 1 could be installed as soon as

public funds are available and appropriate engineering design studies are complete, independent of proposed future cross-section widenings. Those bulb-outs in the sections where the cross-section is to be widened can be constructed at the time when a new roadway section is put in place.



Example of a pedestrian refuge median

4. *Pedestrian Crossings: crosswalks, pedestrian signals, refuge islands*

Marked pedestrian crossings in the East Boulevard PED corridor currently only exist at intersections with traffic signals – Euclid Avenue, Dilworth Road West, Dilworth Road East, Kenilworth Avenue, and Scott Avenue.

Pedestrian refuge medians supplement crossings at signalized intersections and provide one of the safest methods to cross the street. The pedestrian only has to look for gaps in traffic in one direction at a time.

This plan recommends the following additional pedestrian crossing locations and treatments:

- Between Euclid and Dilworth Rd West: A true median or pedestrian refuge islands should be explored in this section. If constructed, they should be designed with pedestrian crossing cut outs. Depending on the number of pedestrians crossing the street, pedestrian signs may be also be appropriate.
- Between Scott and Garden Terrace (near Fountain View): A pedestrian refuge median is recommended in the commercial section of the corridor, near Fountain View and Floral Avenue.
- Between Garden Terrace and Freedom Park: Another pedestrian refuge median is recommended in the vicinity of Cumberland Avenue and Garden Terrace, which is also in the commercial section of East Boulevard.
- Near entrance to Freedom Park: This plan recommends a pedestrian refuge median and possibly a marked crosswalk at this location. Another possibility would be a pedestrian underpass at Little Sugar Creek Bridge using the existing greenway path along the creek.

5. Marked Cross-Walks and Other Pedestrian Improvements

In addition to the crossing treatments recommended above, the existing cross-walks at signalized intersections (Euclid Avenue, Dilworth Road West, Dilworth Road East, Scott Avenue, Kenilworth Avenue) need to be enhanced to make them more apparent to pedestrians and drivers. Improvements could include stamped concrete or pavers with contrasting colors.

Other near-term recommendations for improving the pedestrian environment include:

- “pedestrian zone” signage in Section 3
- countdown pedestrian lights at Scott/Kenilworth
- no-right turn on red regulations at Scott/Kenilworth



Pedestrian-scale lighting on East Blvd.

Pedestrian-Scale Lighting

Typical streetlights illuminate the roadway, but do not provide lighting for the pedestrian area of a street. Pedestrian-scale lighting is shorter in height than typical streetlights, and is designed to provide lighting for the pedestrian area as well as for the vehicle lanes. Pedestrian-scale, decorative lighting should be installed in the public right-of-way, with **priority given to areas of highest pedestrian activity including Section 3, the commercial area around Cleveland and Euclid, and near intersections and pedestrian crossings.**

Bicycle Parking:

Because of its convenience to nearby neighborhoods, many residents can travel to the stores, coffee shops, and restaurants

in the commercial section of East Boulevard by bicycle. There is currently no bicycle parking along East Boulevard provided for these patrons. The City of Charlotte is undertaking a revision to the Zoning Ordinance that would require bicycle parking in new developments, just as automobile parking is required. However, there are many existing businesses that will not redevelop in the near future and will not be required to retrofit bicycle parking on their property, even if a new bicycle parking standards are added to the ordinance.



Bicycle parking (on a sidewalk bulb-out with street trees)

This plan recommends that, as part of near-term streetscape improvements, the City of Charlotte install **racks for short-term bicycle parking in the public right-of-way in Section 3** of East Boulevard (between Charlotte Drive and Cumberland Avenue/Garden Terrace). Bicycle parking in the public right-of-way will be especially useful near those businesses whose doors front on the sidewalk, since cyclists are most likely to park where their bicycles can be seen from their intended destination. For current buildings in this section whose entrances are set back from the right-of-way, this plan recommends that the City of Charlotte

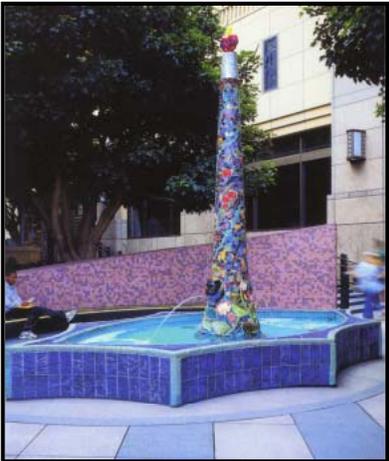
work with business owners to share the cost of installing bicycle racks in an appropriate location on their private property, outside of the public right-of-way.

Street Trees

Street trees should be planted in the public right-of-way where there are opportunities for new or replacement trees. The Division of Landscape Management has identified existing trees that need to be replaced and the locations where new trees can be planted.

Public Art:

The City of Charlotte will work with the neighborhood organizations, property owners, local residents, business owners, and area arts organizations to determine appropriate locations and designs for public art installations for the corridor. Potential locations might include the traffic island at Dilworth Road West and the future medians.



Example of Public Art

ADA/Curb Ramps:

The City of Charlotte should conduct an inventory of existing ADA curb ramp locations and alignments to assess needs for new construction or realignment of these ramps.

Benches, Trash Cans:

The City also should install public benches and trash cans in appropriate high-pedestrian traffic locations, especially in Section 3 of the corridor. City policy is to install trash cans at bus stop locations.

B. Long-term Recommendations (5 years +)

Utility Line Consolidation/Relocation

Overhead utility lines are one of the most prominent and unsightly features of the East Boulevard streetscape, especially in the commercial section between Dilworth Road West and Garden Terrace. Citizens, staff and developers have voiced strong interest in having the utility lines buried and/or moved out of the East Boulevard right-of-way.

This plan recommends that **utilities be relocated, to alleys where possible, or out of the East Boulevard right-of-way, and/or buried** along the entire East Boulevard corridor.

1st Priority section: Section 3, the approximately ¼ mile section between Charlotte Drive and Cumberland Drive/Garden Terrace. This section is the commercial center of the corridor and the location of most pedestrian activity.

2nd Priority section: The remainder of the corridor.

In the near term, utility lines in Section 1 could be moved to alley rights-of-way. Power lines for streetlights in Section 1 could be buried.



“pole-scape” in Section 3 of East Boulevard

The main obstacle to removing utility lines, however, is cost, which is estimated by Duke Power to be around \$3-4 million per mile along a major thoroughfare. Utility companies may be willing to move overhead utility lines, but will not assume the cost themselves. The Charlotte-Mecklenburg Planning Commission has convened a group to look at the costs and develop a policy for relocating or burying utilities, but at this time there are no budgeted public funds for such efforts.

Improved Connectivity

This Plan recommends that a **future connection for pedestrians and cyclists at least, and preferably for automobiles as well, be provided between the two disconnected portions of Worthington Avenue** between Floral and Charlotte Drive (see map below).

One of the challenges for the East Boulevard corridor is that east of Charlotte Drive, there are no parallel routes for automobiles, pedestrians or cyclists. Thus, east of Charlotte Drive all traffic is funneled onto East Boulevard, which adds to the traffic volume in this section (traffic volumes on the corridor are 50% higher in this area). This condition also reduces means of access to properties in this area, and forces pedestrians and cyclists that are seeking a quiet route *through* the neighborhood rather than to destinations on the thoroughfare, to travel on East Boulevard.



Possible future connection at Worthington Street

Such a connection will not be possible until redevelopment occurs on properties between the existing Worthington links, and would need to be the subject of an engineering and property ownership study. This recommendation simply provides a policy **basis for protecting a future right-of-way** in this location when redevelopment does occur.

Part II: Implementing the Vision

The work of transforming the streetscape of East Boulevard will occur over many years and will require the efforts of local government, private property owners, residents, and developers. The following is an outline of the responsibilities of the public and private sectors for implementing the Pedscape Plan.

Public Sector Responsibilities

The public sector will provide the policy basis, the oversight and some infrastructure improvements for the implementation of the Plan. However, the major changes to the corridor will become evident only as private investment begins to fill in the framework that the community and the City of Charlotte have laid out in this Plan. **Local government will provide some investment in streetscape amenities that can be accommodated within the existing right-of-way.** Major public investment in the form of constructing new roadway cross-sections will not occur until significant redevelopment has occurred in the sections of the corridor that are recommended for widening.

1. Transportation Network and Streetscape Infrastructure Improvements

Near term (2-5 years): The following streetscape improvements can be implemented by the City of Charlotte pending commitment of capital improvement funding. The Charlotte-Mecklenburg Planning Commission and/or the Charlotte Department of Transportation will be responsible for submitting requests for funding of these projects and initiating engineering plans for these recommended improvements:

- Pedestrian and roadway improvements in Section 1
- Conversion of right-turn channel at Dilworth Road West
- Construction of recommended bulb-outs, especially in Section 1 and on side streets
- Installation of pedestrian refuge islands at recommended locations
- Installation of bicycle parking racks in public right-of-way
- Construction of ADA/curb ramps
- Installation of public art
- Installation of benches and trash cans

Long term (5-20 years or more): Longer term improvements include the construction of the future roadway cross-sections for East Boulevard. These improvements are dependent upon the acquisition of necessary right-of-way, which will be reserved by developers as new development and redevelopment occurs in accordance with the Plan standards.

2. Land Use and Community Design

Monitoring/reviewing new development plans (on-going): CMPC and other City departments will be responsible for the on-going plan review for new developments within the PED area, as prescribed in the PED zoning standards. This responsibility will include ensuring that the standards of PED and this plan are met by developers.

Private Sector Responsibilities

The private sector will be responsible for pedestrian-friendly development that is consistent with PED zoning requirements and the **development standards** of this plan. As soon as the PED rezoning has taken effect, development under the PED standards will be required for all new development and major renovation on the corridor.

Appendix

Appendix A: Pedscape Plan Process Major Actions

Fall 2000:

- Determine PED boundaries
- Engineering survey of PED area complete
- Interdepartmental staff team determines road cross-section options

Spring 2001:

- Staff meets with Dilworth Community Development Association (DCDA) for input
- Field work to document existing conditions
- Held **two public workshops** – including walking audit of corridor
- Charette (community design workshop) for corridor hosted by DCDA
- Interdepartmental staff team prepares initial recommendations
- Staff develops maps, cross-section drawings, and composite future roadway and setback plan
- Staff meets with Historic District Commission to review recommendations
- Tree survey completed by City’s Division of Landscape Management

Summer 2001:

- Staff meets with DCDA to present initial recommendations

- **Third public meeting** to present initial recommendations
- Interdepartmental staff team revises recommendations, maps, and plan drawings

Fall 2001:

- Additional research, information gathering, field checking
- Planning staff prepares recommendations for streetscape standards for side streets
- Staff prepares Pedscape Plan document, revises drawings, maps
- Meeting with DCDA
- **Final public meeting** to present draft Plan

Winter 2001/2002:

- Public comment and plan recommendation by Planning Commission, Historic District Commission, and Tree Commission

Spring/Summer 2002:

- Planning Commission approves plan February 2002
- City Council’s Economic Development & Planning Committee recommends the plan
- City Council adopts plan June 10, 2002
- PED rezoning for East Boulevard corridor July 2002

Appendix B: Average Daily Traffic Volumes for Sections of East Boulevard (1994-2001)

The Charlotte Department of Transportation (CDOT) measures traffic volumes on roads throughout the city on a rotating basis. Traffic volumes are measured for a 48 hour time period and averaged to give an indication of the number of vehicles that travel on a section of road during a given day.

Count location/ year	1994	1995	1996	1997	1998	1999	2000	2001
East of South Blvd.	-	17,400	21,900	-	-	19,500	21,400 20,100	-
East of Kenilworth	32,100 (11/28/94) 21,600 (04/04/94)	21,600 (8/28/95)	26,200 (12/2/96)	-	30,800 (11/18/98)	-	-	23,700 (2/14/01)
West of Kings Dr.	21,700	20,300 21,500 19,100	18,700	-	21,500	-	-	19,700
Scott (north of East Blvd.)	15,500	10,900	-	11,800	-	11,700	-	11,300
Scott (south of East Blvd.)	13,300	9,300	-	15,300	-	15,500	-	11,300
Kenilworth (north of East Blvd.)	10,700	11,100	-	-	-	11,400	-	11,300
Kenilworth (south of East Blvd.)	12,900	12,100	13,800	-	11,000	12,600	-	11,300

Appendix C: Desired Features of Future East Boulevard Streetscape

(Responses are transcribed verbatim from public input at first public meeting, but are grouped by similar intent. Numbers in parentheses indicate the number of times a given feature was mentioned.)

<p>Sky</p> <p>Trees (6)</p> <p>Landscaping</p> <p>Large trees</p> <p>Tree-lined streets (2)</p> <p>Small trees</p> <p>More vegetation, trees</p> <p>Tree-lined</p> <p>Trees, plants</p> <hr/> <p>Sidewalk cafés (4)</p> <p>Outdoor dining</p> <p>Sidewalk café/restaurant</p> <p>Cafés w/ outdoor seating</p> <p>Coffee/tea shops</p> <p>Sidewalk restaurants & retail</p> <p>Patio dining</p> <p>Awnings/patios</p> <p>Sidewalk vendors</p> <hr/> <p>3,4,5 story buildings</p> <p>mid-rise buildings (2)</p> <p>high-density housing</p> <p>mixture of uses</p> <p>homes over shops</p> <p>offices over shops</p> <p>front doors(2)</p> <p>stores</p>	<p>more street-level shops</p> <p>well designed building facades</p> <p>mix of architectural styles & colors</p> <p>beautiful architecture</p> <p>architecture</p> <p>buildings at sidewalk edge</p> <p>buildings close to street</p> <p>historic fabric</p> <p>some existing buildings remaining</p> <p>art</p> <p>outdoor art (sculpture)</p> <p>public art</p> <hr/> <p>Activity</p> <p>More peds</p> <p>Pedestrian</p> <p>Less cars, more walkers</p> <p>People (3)</p> <p>Pedestrians</p> <p>People walking</p> <p>Families</p> <p>Walkers, shoppers</p>	<p>More pedestrians (foot traffic)</p> <p>Police on foot</p> <p>Wide, safe sidewalks</p> <p>Wide sidewalks</p> <p>Wider sidewalks</p> <p>Crosswalks to get across</p> <p>Crosswalk at Freedom Park</p> <p>Crosswalks w/ brick pavers</p> <p>Pedestrians able to cross East Blvd.</p> <p>[traffic] light at Fountainview</p> <p>Wide, safe street corner</p> <p>(Scott & East)</p> <p>Safe ped environment</p> <p>Slow-moving cars</p> <p>Safe walking</p> <hr/> <p>Cyclists</p> <p>Bike Lanes</p> <p>A bicycle lane</p> <p>Bike lanes</p> <hr/> <p>On-street parking</p> <p>Angled street parking</p>	<p>Parking on street (2)</p> <hr/> <p>Median</p> <p>Landscaped median</p> <p>Median from South to Dilworth Rd.</p> <p>Grass median</p> <p>Median with landscape w/ trees</p> <hr/> <p>Benches (4)</p> <p>More trash cans; less litter</p> <p>Street lamps</p> <p>Lighting</p> <p>Lights</p> <p>Pedestrian-scale lighting</p> <hr/> <p>Trolley (2)</p> <p>Trolley in median</p> <p>Gracious covered bus stops</p> <hr/> <p>Limited signage</p> <p>No-overhead wires</p> <p>Nothing from Carolina’s Medical</p>
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Appendix D: Official Street Plan

The following maps show the location of required setbacks and future curblines for the *East Boulevard Pedscape Plan* area. Where no change to the curblines is shown, the existing curblines will remain.

These maps are provided for concept only. The official versions of these maps will be held by the Charlotte-Mecklenburg Planning Commission (CMPC) and Charlotte Department of Transportation (CDOT). These maps are subject to change.



SOUTH BOULEVARD
R/W VARIES

EAST BOULEVARD
R/W VARIES

CLEVELAND AVENUE
60' R/W

MATCH LINE SHEET 2 OF 14

123-074-01
Hazel Dr CD
Deed: 04706-160
201 East Bv
Zoning: B1

123-074-02
State Street LLC
Deed: 09923-700
211 East Bv
Zoning: B1

123-074-03
Tucker Curry Partnership LLC
Deed: 07865-087
219 East Bv
Zoning: B1

123-074-04
Mr & Mrs Eric McComb
Deed: 07886-989
223 East Bv
Zoning: B1

ONE STORY BRICK HOUSE
123-074-05
Mr & Mrs Paul Lovitt
Deed: 05965-845
227 East Bv
Zoning: B1
"BUSINESS"

ONE STORY BRICK HOUSE
123-075-01
Key LLC
Deed: 10616-732
301 East Bv
Zoning: B1
"BUSINESS"

121-054-15
Honey Properties, Inc.
Deed: 01185-490
1801 South Blvd
Zoning: B1

121-054-14
TMC-NC LLC
Deed: 09833-299
208 East Bv
Zoning: B1

121-054-12
Towers Fund Fund LTD
Deed: 06427-523
224 East Bv
Zoning: B1

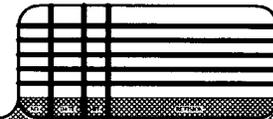
TWO STORY FRAME HOUSE
121-054-11
C&S Properties
Deed: 06491-445
228 East Bv
Zoning: B1
"BUSINESS"

"RESTAURANT"

LEGEND

- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

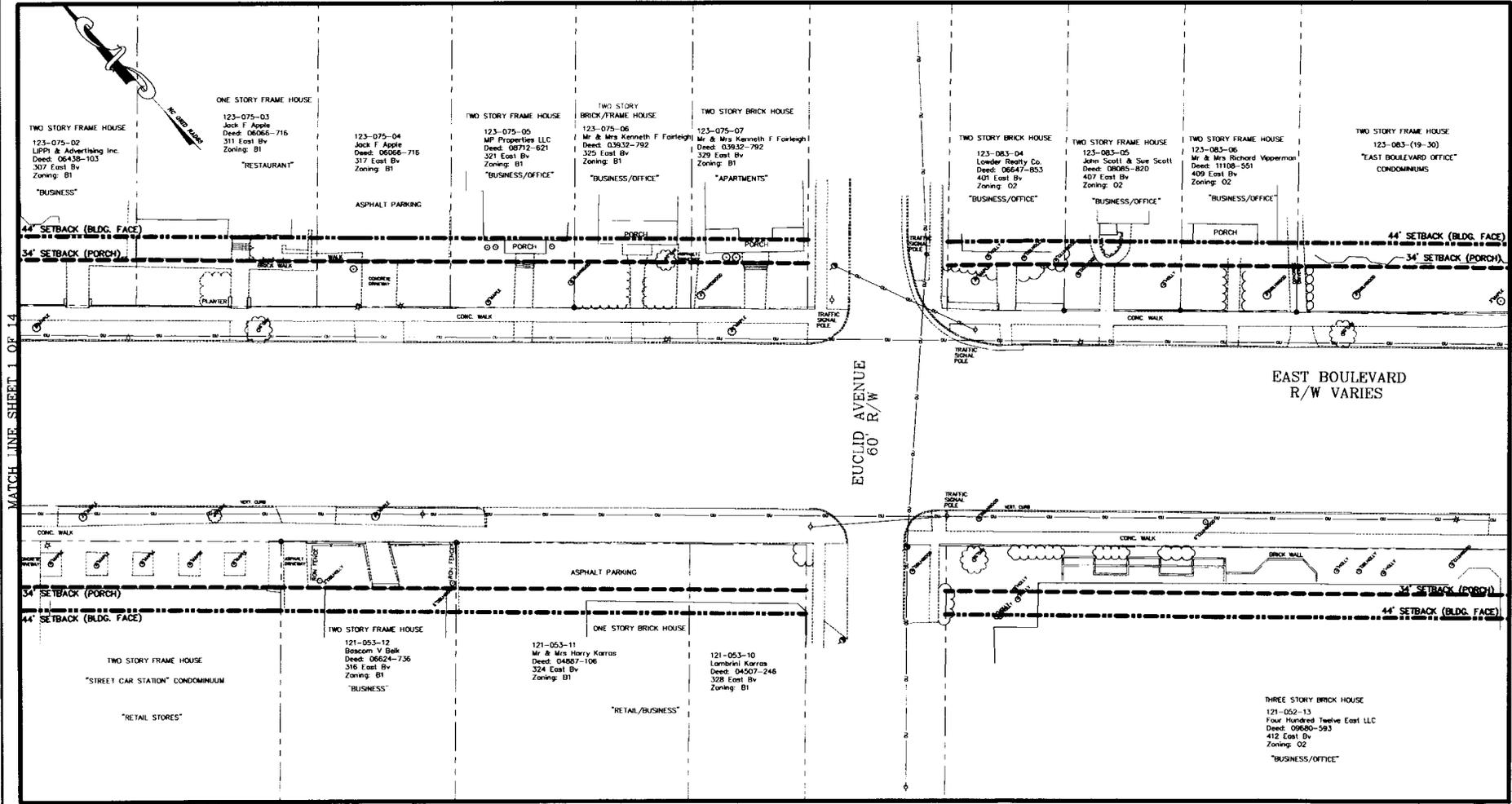
**THIS MAP PROVIDED FOR CONCEPT ONLY.
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MECKLENBURG PLANNING COMMISSION OR
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DEVELOPMENT PURPOSES.**



OFFICIAL STREET PLAN

**EAST BOULEVARD
PEDSCAPE PLAN
APPENDIX D**

DESIGNED BY	1	DATE
APPROVED BY	1	DATE
SCALE	1" = 10'-0"	SHEET NO. 13



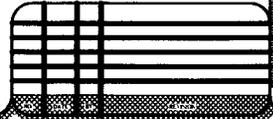
MATCH LINE SHEET 1 OF 14

MATCH LINE SHEET 3 OF 14

LEGEND

- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

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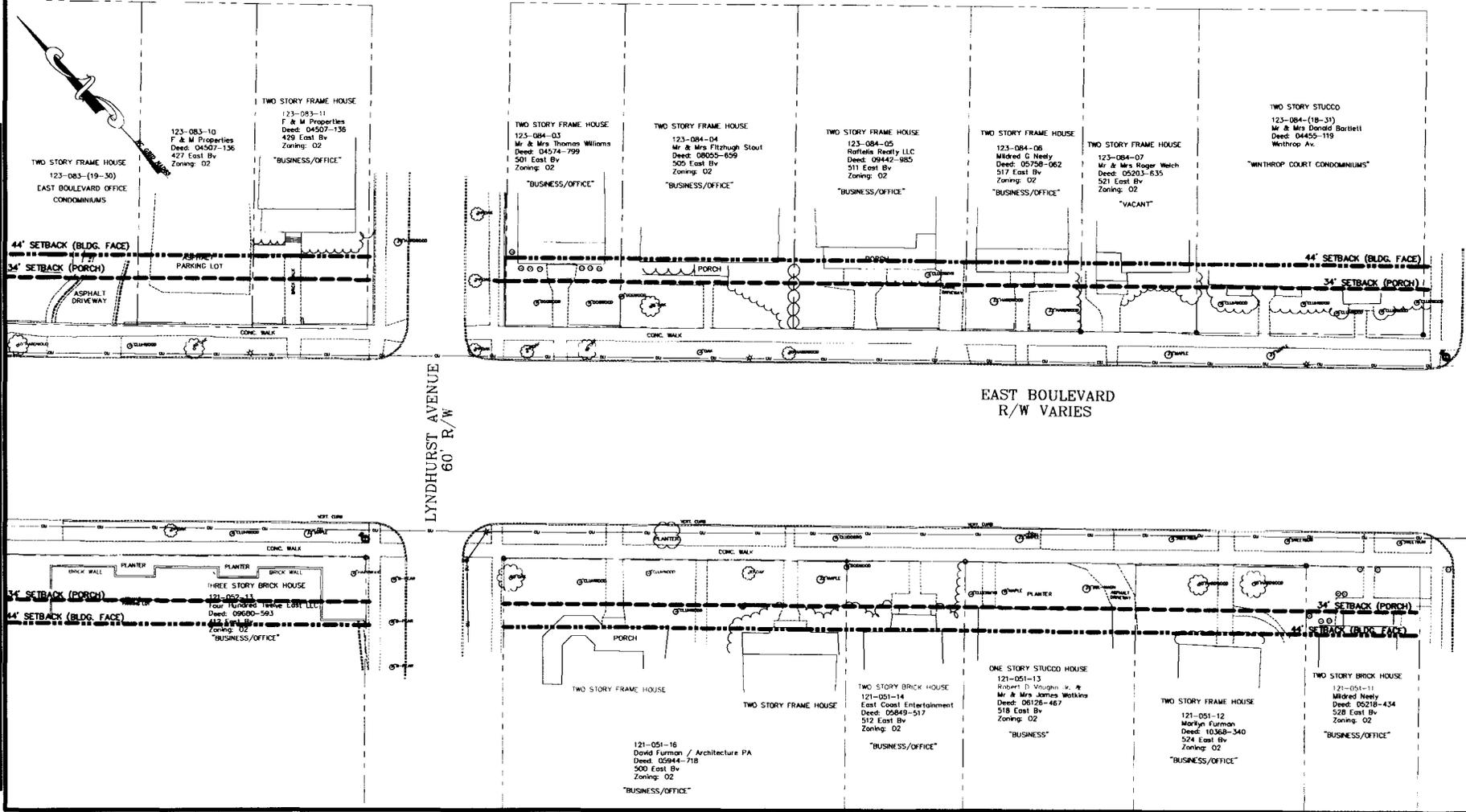
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**EAST BOULEVARD
PEDSCAPE PLAN
APPENDIX D**

PREPARED BY	APPROVED BY	DATE
2	2	13

MATCH LINE SHEET 2 OF 14

MATCH LINE SHEET 4 OF 14

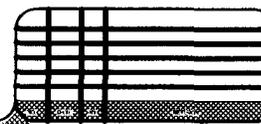


LEGEND

- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

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GRAPHIC SCALE



OFFICIAL STREET PLAN

EAST BOULEVARD PEDSCAPE PLAN APPENDIX D

DESIGNED BY	DRAWN BY	CHECKED BY
DATE	DATE	DATE
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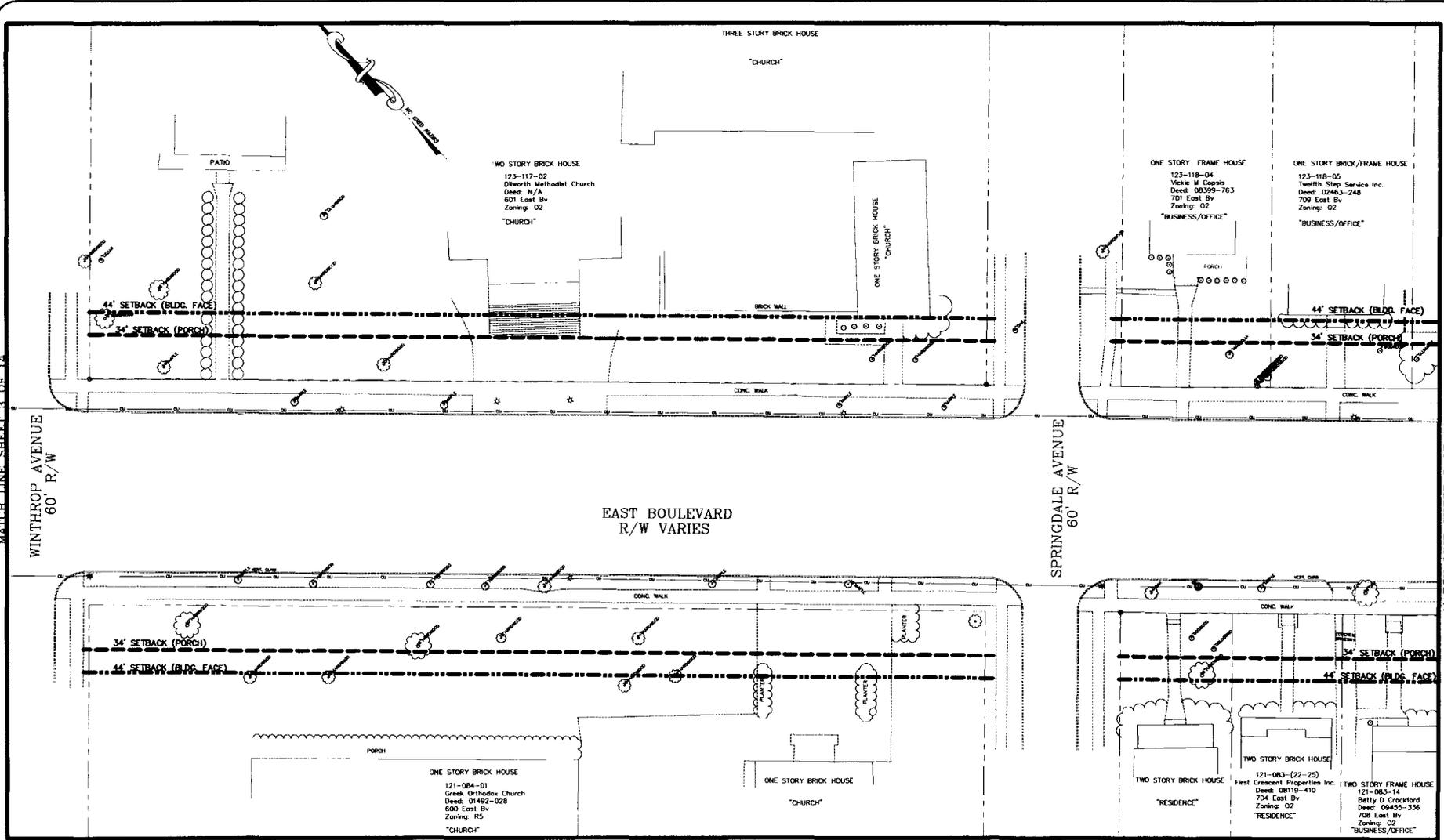
MATCH LINE SHEET 3 OF 14

WINTHROP AVENUE
60' R/W

SPRINGDALE AVENUE
60' R/W

MATCH LINE SHEET 5 OF 14

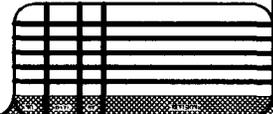
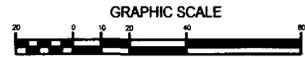
EAST BOULEVARD
R/W VARIES



LEGEND

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- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

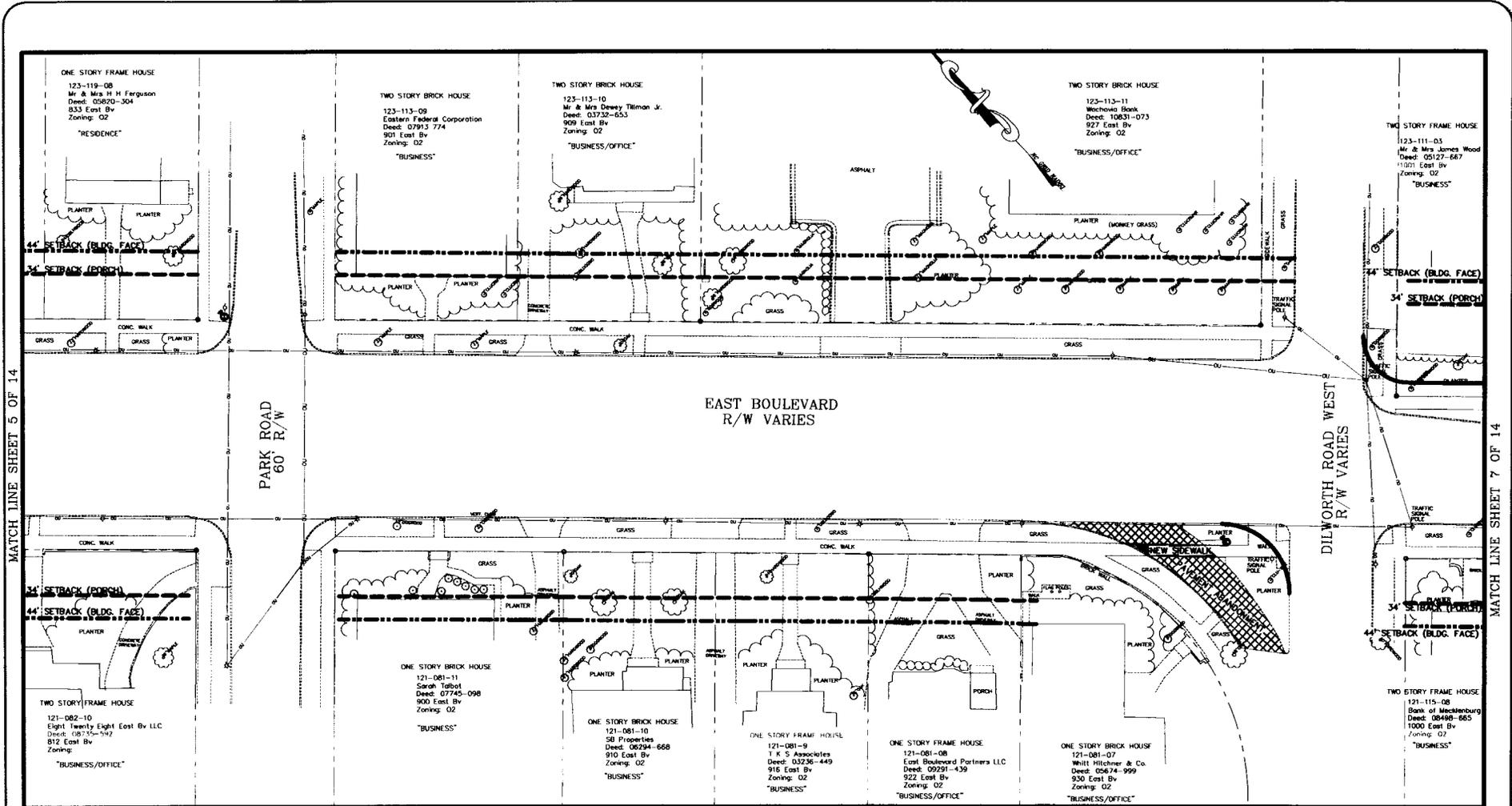
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**EAST BOULEVARD
PEDSCAPE PLAN
APPENDIX D**

DATE	BY	REVISED	BY
12/13	4	1/14	13



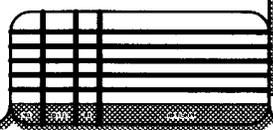
MATCH LINE SHEET 5 OF 14

MATCH LINE SHEET 7 OF 14

LEGEND

- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

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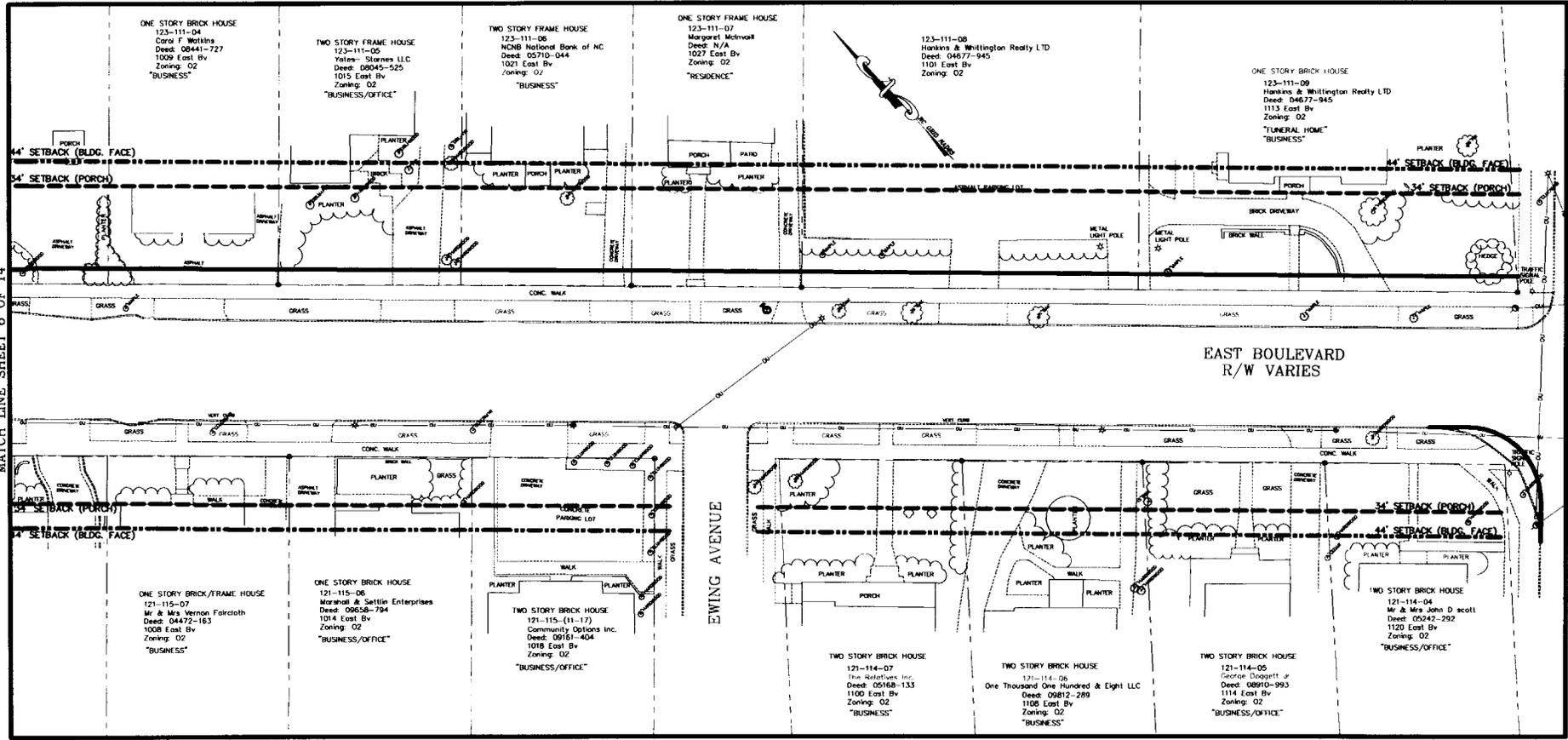
OFFICIAL STREET PLAN

EAST BOULEVARD PEDAPOSE PLAN APPENDIX D

PREPARED BY	DATE	SHEET NO.	TOTAL SHEETS
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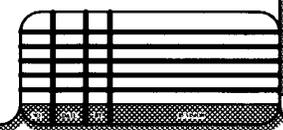
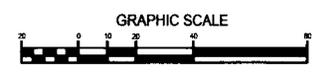
MATCH LINE SHEET 8 OF 14



LEGEND

- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

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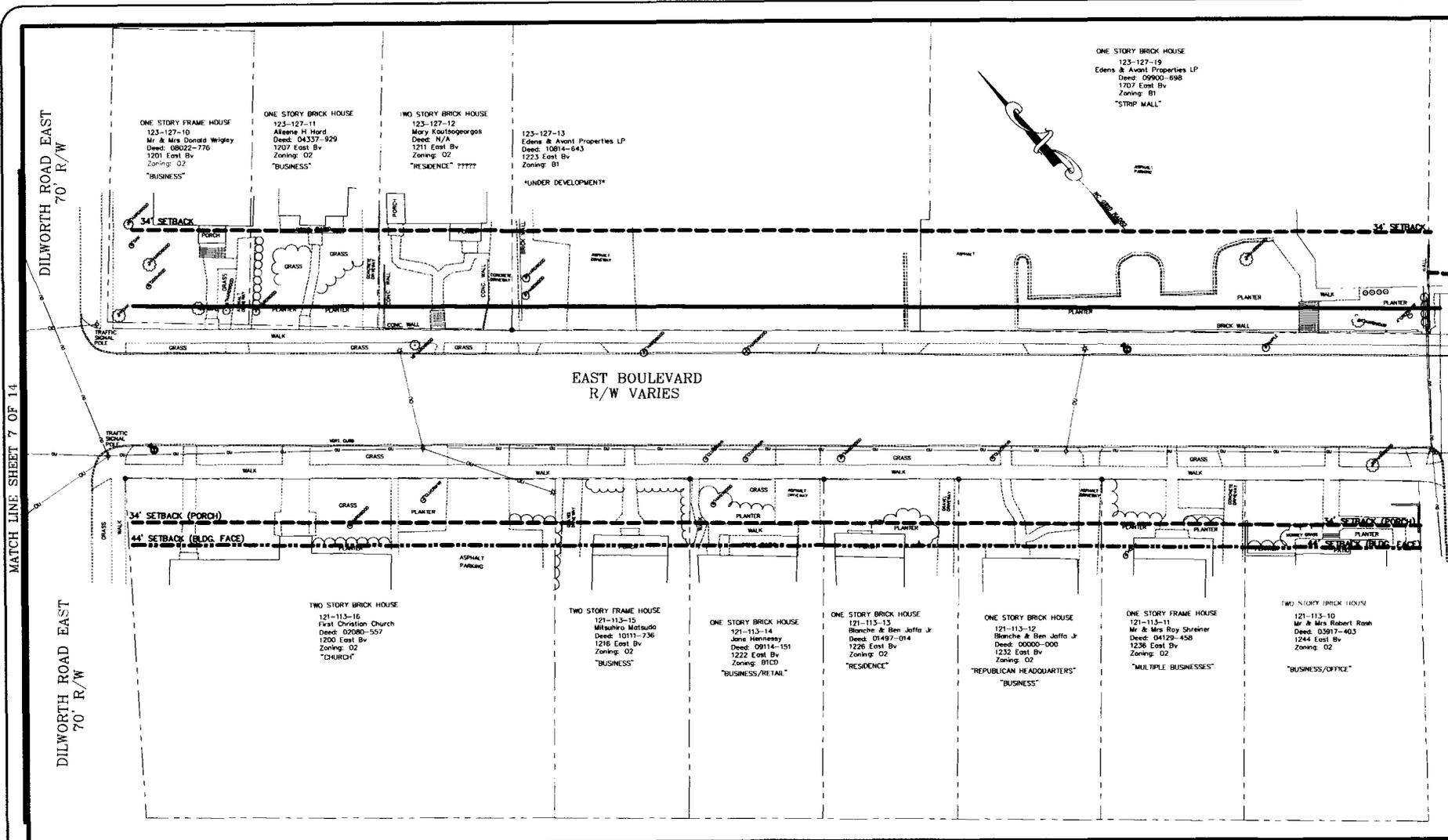


OFFICIAL STREET PLAN

EAST BOULEVARD PEDSCAPE PLAN APPENDIX D

PREPARED BY	APPROVED BY	DATE
7/15/15		7/15

7/15



ONE STORY FRAME HOUSE
123-127-10
Mr. & Mrs. Donald Whigley
Deed: 08022-776
1201 East Bv
Zoning: O2
"BUSINESS"

ONE STORY BRICK HOUSE
123-127-11
Allene H. Nord
Deed: 04337-929
1207 East Bv
Zoning: O2
"BUSINESS"

TWO STORY BRICK HOUSE
123-127-12
Mary Koutsogoras
Deed: N/A
1211 East Bv
Zoning: O2
"RESIDENCE" ?????

123-127-13
Edens & Avanti Properties LP
Deed: 10016-043
1223 East Bv
Zoning: B1
"UNDER DEVELOPMENT"

ONE STORY BRICK HOUSE
123-127-15
Edens & Avanti Properties LP
Deed: 09900-698
1207 East Bv
Zoning: B1
"STRIP MALL"

TWO STORY BRICK HOUSE
121-113-16
First Christian Church
Deed: 02080-557
1200 East Bv
Zoning: O2
"CHURCH"

TWO STORY FRAME HOUSE
121-113-15
Mitsuhro Matsuda
Deed: 10111-736
1216 East Bv
Zoning: O2
"BUSINESS"

ONE STORY BRICK HOUSE
121-113-14
Jane Hennessy
Deed: 09114-151
1222 East Bv
Zoning: B1CD
"BUSINESS/RETAIL"

ONE STORY BRICK HOUSE
121-113-13
Blanche & Ben Joffa Jr
Deed: D1497-014
1226 East Bv
Zoning: O2
"RESIDENCE"

ONE STORY BRICK HOUSE
121-113-12
Blanche & Ben Joffa Jr
Deed: 00000-000
1232 East Bv
Zoning: O2
"REPUBLICAN HEADQUARTERS"
"BUSINESS"

ONE STORY FRAME HOUSE
121-113-11
Mr. & Mrs. Roy Shreiner
Deed: 04129-458
1236 East Bv
Zoning: O2
"MULTIPLE BUSINESSES"

TWO STORY BRICK HOUSE
121-113-10
Mr. & Mrs. Robert Roth
Deed: 03017-403
1244 East Bv
Zoning: O2
"BUSINESS/OFFICE"

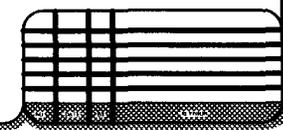
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MATCH LINE SHEET 9 OF 14

LEGEND

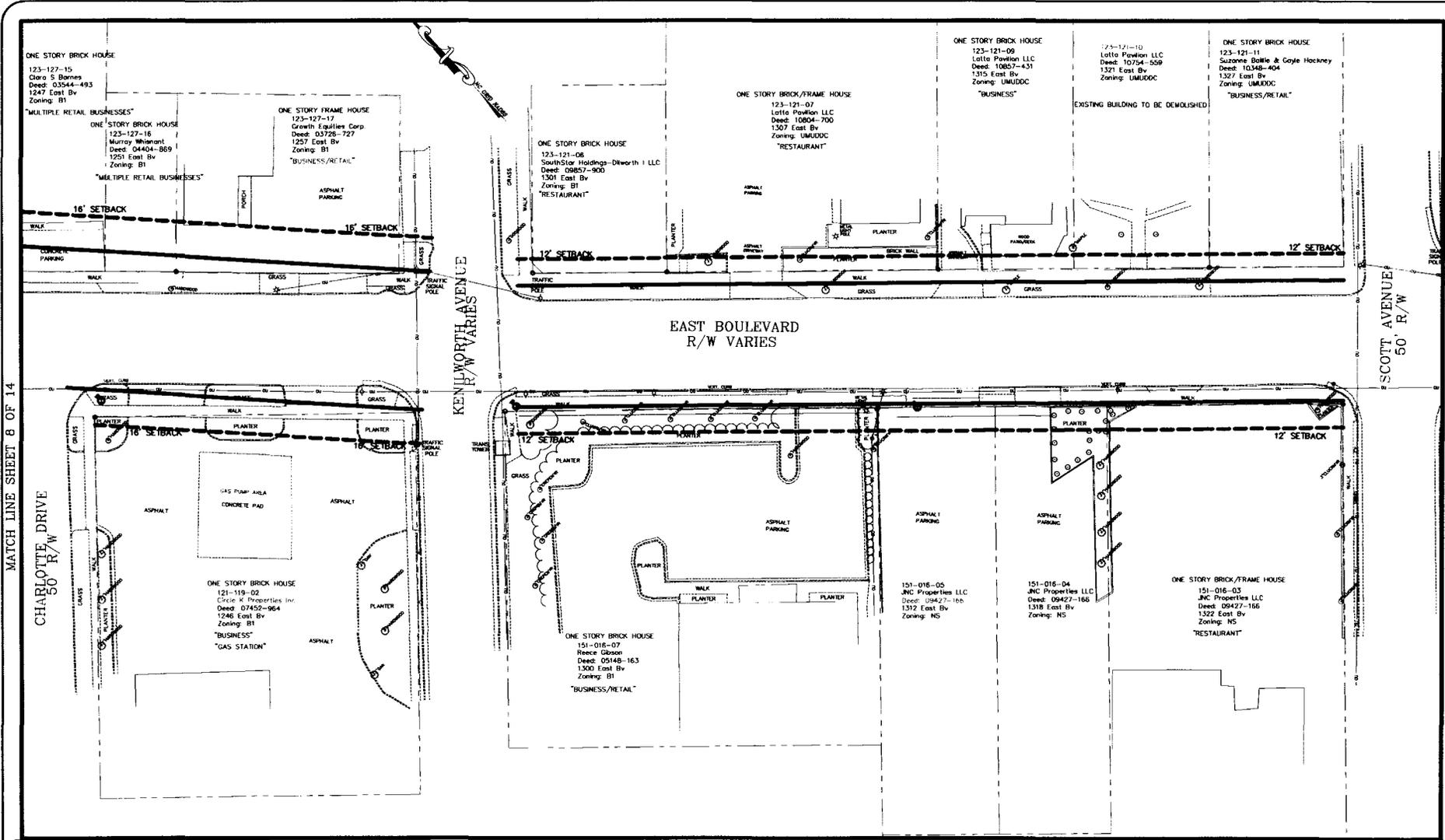
- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

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OFFICIAL STREET PLAN
**EAST BOULEVARD
PEDSCAPE PLAN
APPENDIX D**

PREPARED BY	1	DATE
APPROVED BY	1	DATE
CAD FILE PATH	8	13



MATCH LINE SHEET 8 OF 14

CHARLOTTE DRIVE
50' R/W

KENWORTH AVENUE
R/W VARIES

EAST BOULEVARD
R/W VARIES

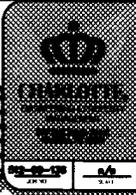
SCOTT AVENUE
50' R/W

MATCH LINE SHEET 10 OF 14

LEGEND

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- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

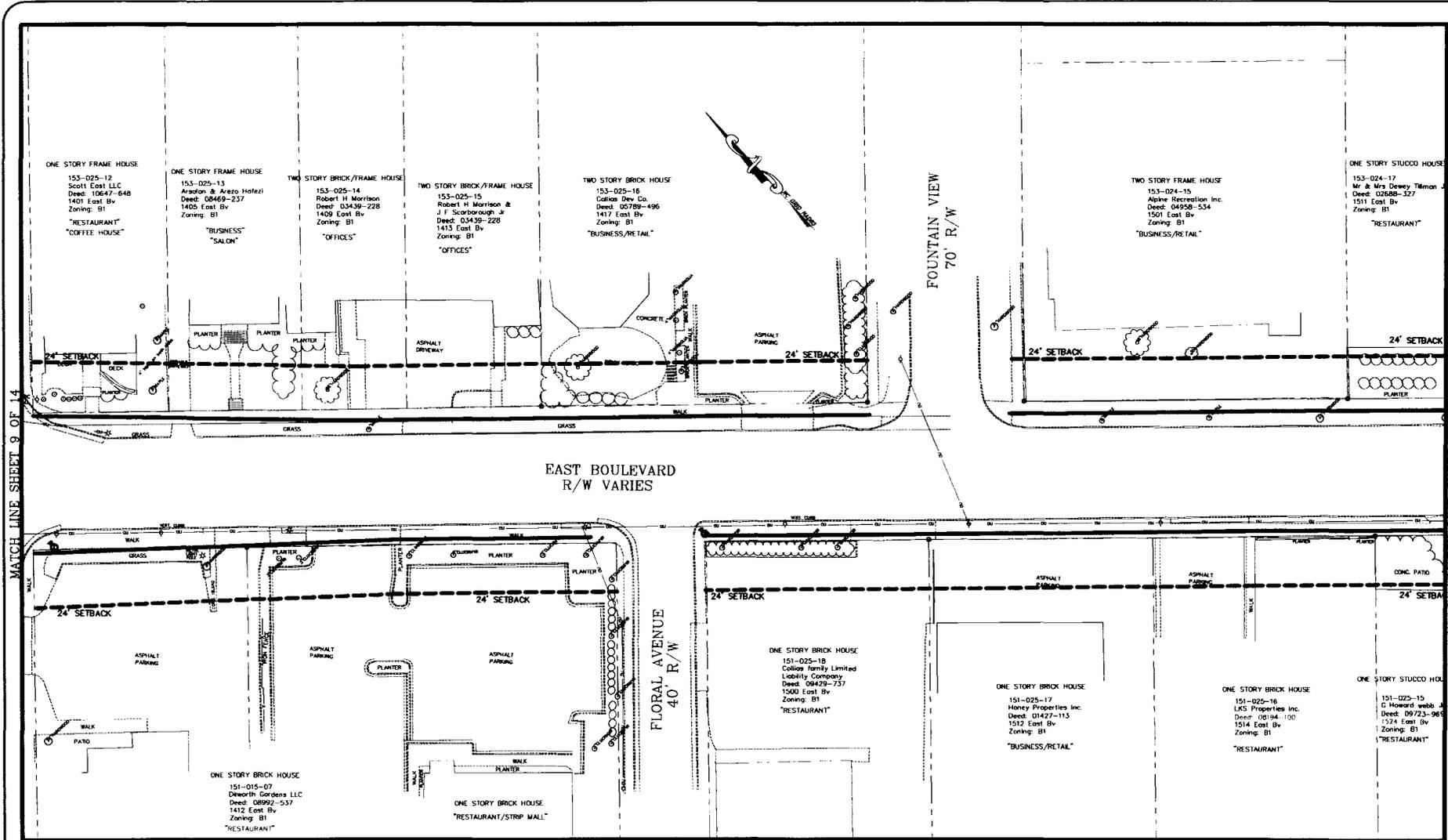
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OFFICIAL STREET PLAN

**EAST BOULEVARD
PEDSCAPE PLAN
APPENDIX D**

DATE PREPARED	1	DATE
DATE	9	13



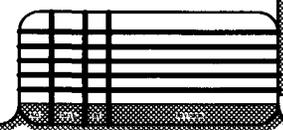
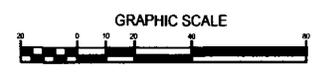
MATCH LINE SHEET 9 OF 14

MATCH LINE SHEET 11 OF 14

LEGEND

- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

THIS MAP PROVIDED FOR CONCEPT ONLY. PLEASE SEE OFFICIAL MAP AT CHARLOTTE MECKLENBURG PLANNING COMMISSION OR DEPARTMENT OF TRANSPORTATION FOR DEVELOPMENT PURPOSES.



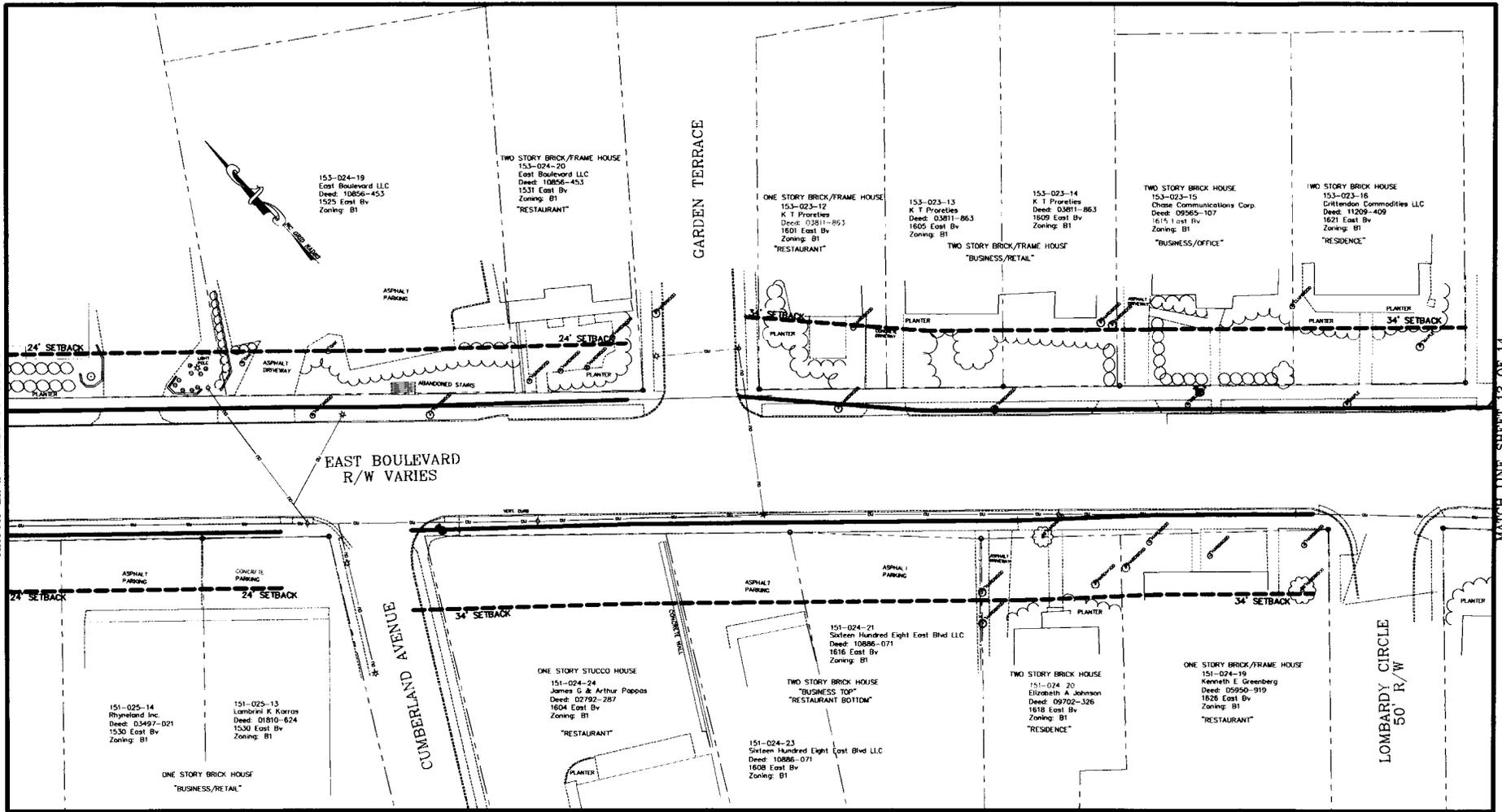
OFFICIAL STREET PLAN

EAST BOULEVARD PEDSCAPE PLAN APPENDIX D

PREPARED BY	DATE
OPERATED BY	DATE
PROJECT NO.	SHEET OF
10	15

MATCH LINE SHEET 10 OF 14

MATCH LINE SHEET 12 OF 14

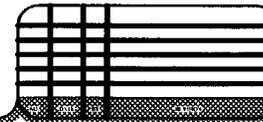


LEGEND

- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

THIS MAP PROVIDED FOR CONCEPT ONLY. PLEASE SEE OFFICIAL MAP AT CHARLOTTE MECKLENBURG PLANNING COMMISSION OR DEPARTMENT OF TRANSPORTATION FOR DEVELOPMENT PURPOSES.

GRAPHIC SCALE



OFFICIAL STREET PLAN
EAST BOULEVARD
PEDSCAPE PLAN
APPENDIX D

REV	DATE	BY	CHKD
1	11/11/11	AS	AS
2	11/11/11	AS	AS

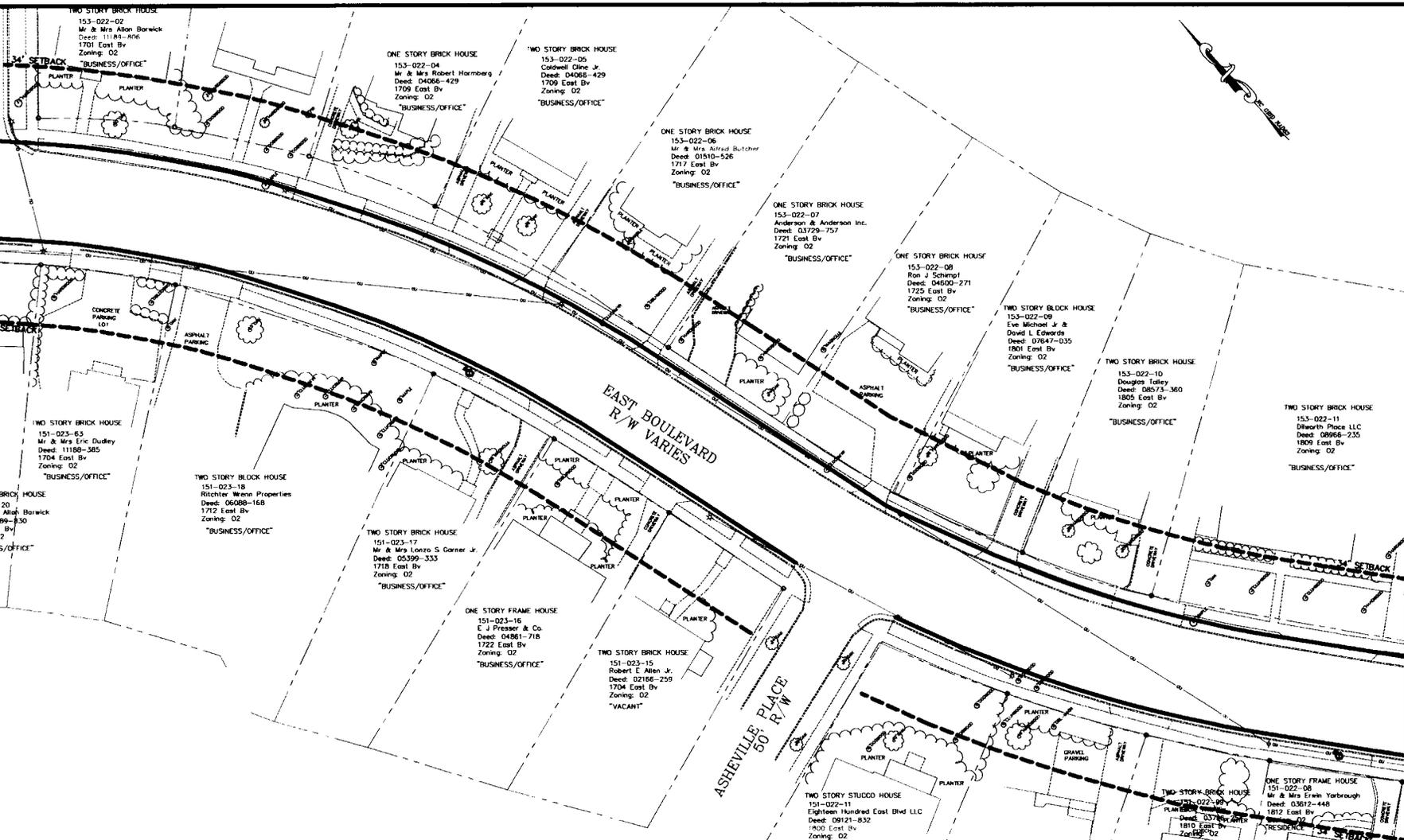
MATCH LINE SHEET 11 OF 14

LOMBARDY CR.
50' R/W

EAST BOULEVARD
R/W VARIES

ASHEVILLE PLACE
50' R/W

MATCH LINE SHEET 10 OF 14



LEGEND

- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

**THIS MAP PROVIDED FOR CONCEPT ONLY.
PLEASE SEE OFFICIAL MAP AT CHARLOTTE
MECKLENBURG PLANNING COMMISSION OR
DEPARTMENT OF TRANSPORTATION FOR
DEVELOPMENT PURPOSES.**

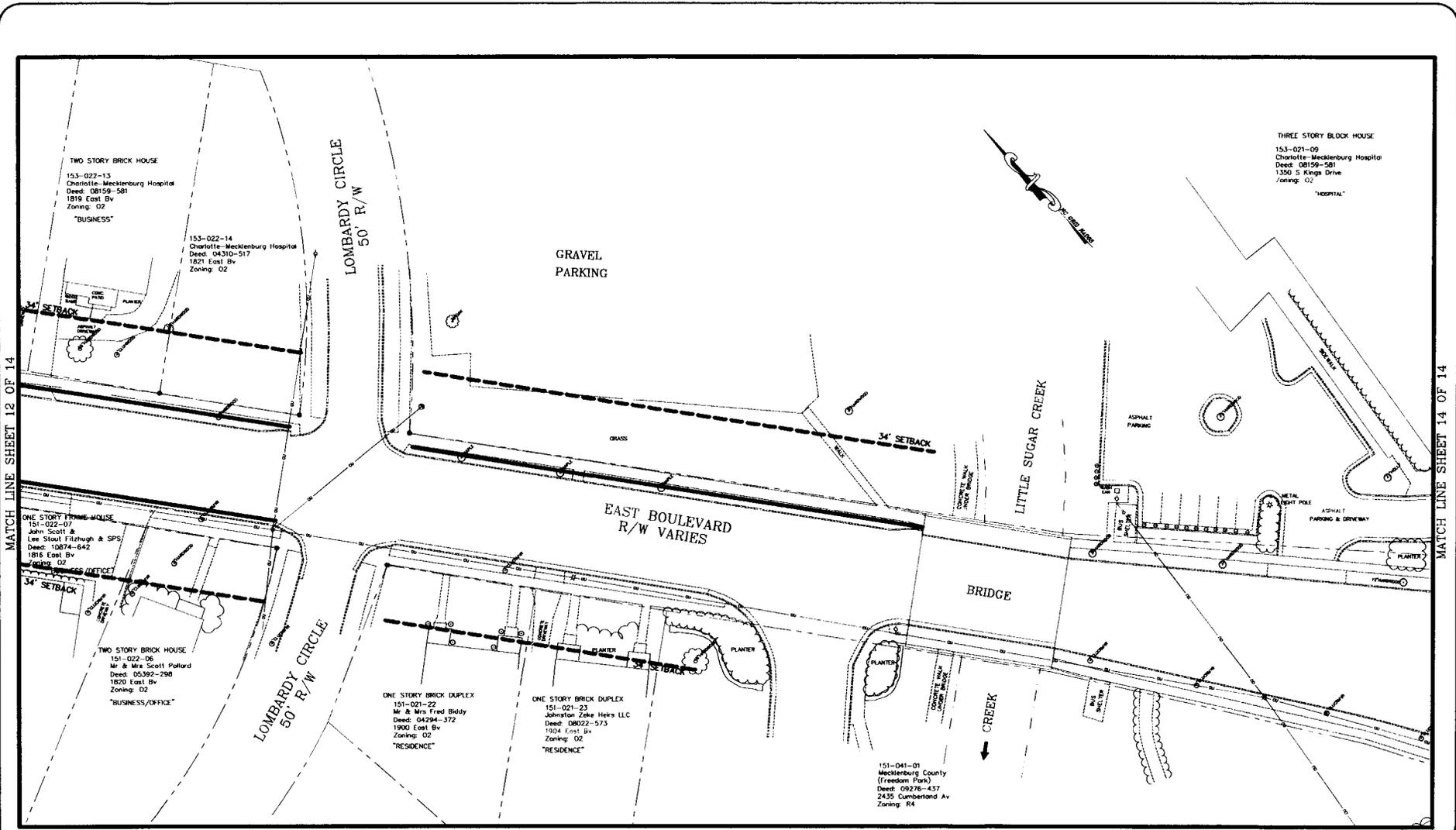


OFFICIAL STREET PLAN

EAST BOULEVARD PEDSCAPE PLAN APPENDIX D

CHARLOTTE
MECKLENBURG COUNTY

DATE	1	1	1	1
BY				
SCALE	1" = 10'			
SHEET	12	13		



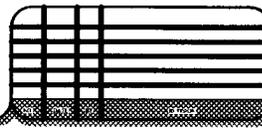
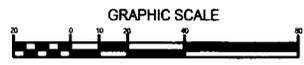
MATCH LINE SHEET 12 OF 14

MATCH LINE SHEET 14 OF 14

LEGEND

- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

THIS MAP PROVIDED FOR CONCEPT ONLY. PLEASE SEE OFFICIAL MAP AT CHARLOTTE MECKLENBURG PLANNING COMMISSION OR DEPARTMENT OF TRANSPORTATION FOR DEVELOPMENT PURPOSES.



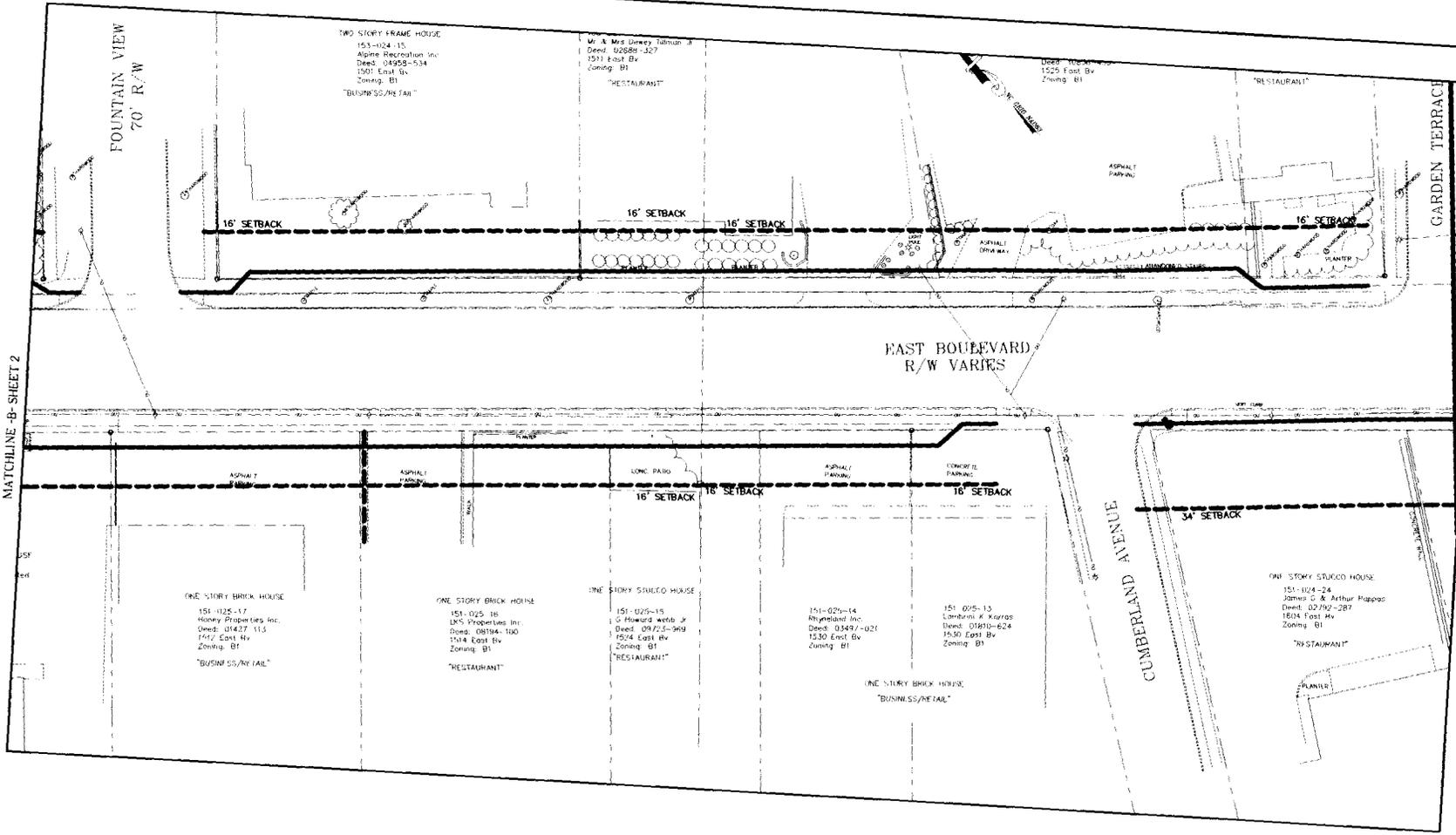
CHARLOTTE-MECKLENBURG GOVERNMENT

PLANNING & TRANSPORTATION

OFFICIAL STREET PLAN

EAST BOULEVARD PEDSCAPE PLAN APPENDIX D

DATE 10/13/11	1 APPROVED BY	DATE 10/13/11
13	13	13



LEGEND

- FUTURE CURB LINE
- PORCH SETBACK LINE
- BUILDING FACE SETBACK LINE

THIS MAP PROVIDED FOR CONCEPT ONLY. PLEASE SEE OFFICIAL MAP AT CHARLOTTE MECKLENBURG PLANNING COMMISSION OR DEPARTMENT OF TRANSPORTATION FOR DEVELOPMENT PURPOSES.



**SECTION 3C
OPTION II**



OFFICIAL STREET PLAN

**EAST BOULEVARD
PEDSCAPE PLAN
APPENDIX D**

DATE	1	DATE
PREPARED BY	UNIVERSITY	DATE
DATE	2	DATE
DATE	2	DATE